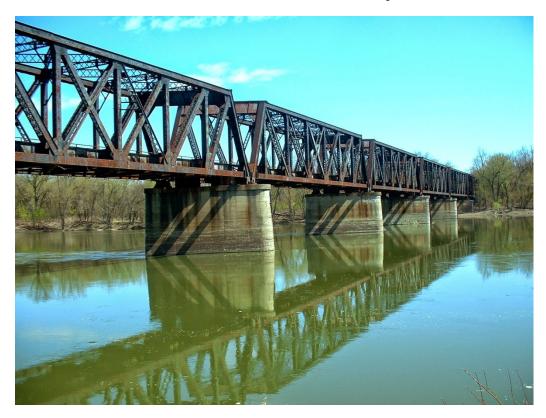
Delaware and Hudson Canal Company Reaching Out: D&H Steam Lines beyond the Lackawanna Valley



Black Diamond Bridge* (Fish's Island Bridge) on the former Wilkes-Barre Connecting Railroad, photograph by Frank Dutton, April 29, 2015. This bridge, now part of Norfolk Southern's Sunbury Line (at milepost 690) crosses the Susquehanna River, from Kirby Park, Kingston (on the right) to South Wilkes-Barre, PA. *See Black Diamond Notes on pp. 625-631.

By

S. Robert Powell, Ph.D.

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A History of the

Delaware and Hudson Canal Company

in 24 Volumes

S. Robert Powell, Ph.D., 1974 Indiana University, Bloomington, IN

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II	Gravity Railroad: 1845 Configuration
III	Gravity Railroad: 1859 Configuration
IV	Gravity Railroad: 1868 Configuration
V	Gravity Railroad: 1899 Configuration
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The Birth and First Maturity of Industrial America

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Acknowledgements

Margaret Mead, the renowned American cultural anthropologist (December 16, 1901—November 15, 1978), joined the American Museum of Natural History, New York City, as assistant curator in 1926. She worked at the American Museum of Natural History for 52 years, and served as curator of the museum's Department of Anthropology. In 1976, in commemoration of her 75th birthday, the museum decided to pay tribute to her work with a film festival of the top ethnographic and other documentary films. The Margaret Mead Film Festival has been held annually since then.

On display in the museum in 1976 was an educational exhibition about Margaret Mead and her work. One of the panels in that display featured Margaret Mead's work on Raratonga, the largest of the fifteen Cook Islands in the central South Pacific. On that panel was the following statement by Margaret Mead: "The last man on Raratonga who knows anything about the past may die tomorrow. We must hurry."

The impact of that statement on the present author, who was present at the first Margaret Mead Film Festival in 1976, was immediate and profound, and from that time forward he has devoted virtually all of his intellectual and physical energy to recording data about the world around himself in those spheres which to him were/are regarded as historically important.

That enterprise on the part of the present author, not surprisingly, is very much in accord with the intellectual perspective of the world of Margaret Mead who, in speaking of how she was raised, once said: "I was brought up to believe that the only thing worth doing was to add to the sum of accurate information in the world."

That very same belief/intellectual perspective of the world was present in the intellectual environment in which the present author was raised and resulted in historical documents/books by family authors, including Edna Pearl Loomis, Eleanor Pritchard Jones, and Alice Voyle Rashleigh.* That very same intellectual perspective of the world was held by George H. Burgess, William J. Coughtry, and Henry Opdyke, who were the driving forces behind the writing of *A Century of Progress History of The Delaware and Hudson Company*, 1823-1923.

Fifteen years ago, we began our research on the Delaware and Hudson Canal Company and, through the volumes in this series on the D&H now completed, and through those that will be completed during the next two years, our objective has been/will continue to be "to add to the sum of accurate information in the world" about the Delaware and Hudson Canal Company. To Margaret Mead, therefore, we here offer, posthumously, our thanks for having been the primary stimulus behind the present series.

Just as Margaret Mead recognized that her research on Raratonga could not be postponed, so, too, we recognize that our research on the Delaware and Hudson Canal Company, which is founded on first-person accounts from former D&H employees and on historical documents and records, must proceed at "full steam ahead", knowing as we do that (1) the ranks of those persons with first-person knowledge about the D&H are thinning with each passing year, and (2) that historical documents and records have a way of disappearing as the years pass.

As we continue "to add to the sum of accurate information in the world" (to use the words of Margaret Mead) about the Delaware and Hudson Canal Company, we do so, not for ourselves but for those who will come after us. Most significantly, Leonor F. Loree, the president of the D&H at the time when *A Century of Progress History of The Delaware and Hudson Company,* 1823-1923 was written, began the Foreword to that monumental work, dated August 24, 1925, with the celebrated and profound advice of John Ruskin:

"When we build let us think that we build forever. Let it not be for the present delight nor for present use alone. Let it be such work as our descendants will thank us for. [. . .]"

S. Robert Powell June 12, 2016

^{*}Alice Voyle Rashleigh: Celebrated and legendary historian of Carbondale in the twentieth century, whose assistance in the writing of *A Century of Progress History of The Delaware and Hudson Company*, 1823-1923 is acknowledged by Leonor F. Loree in the *Foreword* of that work.

Overview

The industrial revolution in America was born on October 9, 1829, in Carbondale, PA, when the first cut of Delaware & Hudson Gravity Railroad coal cars, loaded with mass produced anthracite coal, headed up Plane No. 1 out of Carbondale for Honesdale and to market in New York City.

Those cars, filled with anthracite coal from mines in Carbondale, traveled over 16 miles of railroad tracks, made up of eight inclined planes and three levels, to Honesdale, where the coal was transferred into canal boats and hauled 108 miles, through the D&H Canal, to the Hudson River.

Most of the coal that was sent through the D&H system in the course of the nineteenth century was shipped south on the Hudson River to the New York metropolitan market and to many ports on the Atlantic seaboard, north and south of New York. A large quantity of anthracite coal was also shipped up the Hudson River to Albany, and shipped through the Erie Canal to the American Midwest.

The mining, manufacturing, and transportation system that became operational on that day between the anthracite mines of the Lackawanna Valley and the retail markets for that coal on the eastern seaboard and in the American Midwest was the product of enlightened entrepreneurial, technological, and managerial thought on the part of the officers, managers, directors, and employees of the Delaware and Hudson Canal Company. That system, the first private sector million-dollar enterprise in American history, was, at the same time, the pioneer expression on this continent of mass production, a mode of production that would thereafter characterize industry in America and around the world.

Mass production, the revolutionary engine that made it possible for the D&H to launch its mining, manufacturing, and transportation system in Carbondale on October 9, 1829, and to perpetuate that system well into the 20th century, came into existence when it did and lasted for as long as it did because a body of employees

and managers, within the context of a community, of which both groups were a part, chose to work together for their mutual benefit and enrichment, to mass produce and market a commodity, and in so doing to implement the clearly articulated production and marketing objectives of "the company," the Delaware and Hudson Canal Company.

In this 24-volume work on the D&H,* we will (1) document the history of that mining, manufacturing, and transportation system, with a special focus on the rail lines of the Delaware and Hudson Canal Company in northeastern Pennsylvania, from the opening of the D&H Gravity Railroad in 1829 to the anthracite coal strike of 1902; and (2) demonstrate that the history of that mining, manufacturing, and transportation system, the D. & H. C. Co., from 1829 to 1902, is, at the same time, not only an illustration of eight decades of fine tuning by the D&H of their mass production procedures and techniques but also a full-bodied expression and record, both from the point of view of the D&H and from the point of view of its employees, of the birth, development, and first maturity of the industrial revolution in America.

This is a success story, directed by America's pioneer urban capitalists, and implemented by them and the tens of thousands of men, women, and children who emigrated from Europe to the coal fields of northeastern Pennsylvania in the nineteenth century to work for and with the D&H and to start their lives over again. This is a success story that is important not only within in the context of local, state, and regional history but also within the context of American history. It is a compelling story.

^{*}The present volume focuses on *Reaching Out: D&H Steam Lines beyond the Lackawanna Valley*. Each of these 24 volumes will focus on one aspect of the history of the Delaware and Hudson railroad, from the opening of the Gravity Railroad in 1829 to the anthracite coal strike of 1902. Each volume will be an autonomous entity and published separately.

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Reaching Out: D&H Steam Lines beyond the Lackawanna Valley

1201

Beyond the Lackawanna Valley

In earlier volumes in this series, we took a close look at the D&H rail lines from (1) Carbondale to Honesdale, (2) Carbondale to Scranton and Wilkes-Barre, and (3) Carbondale to Lanesboro. We will now turn our attention to five additional D&H rail lines to the north and south of the Lackawanna Valley.

Those five rail lines, all steam lines, are: Albany and Susquehanna Rail Road, Rensselaer and Saratoga Railroad, Lackawanna and Susquehanna Railroad, Delaware and Hudson Railroad to Canada, Wilkes-Barre Connecting Railroad.

Here are those five rail lines listed in chronological order of construction and/or acquisition by the D&H:

February 24, 1870: Albany and Susquehanna Rail Road, opened on January 12, 1869; leased to D&H on February 24, 1870. Merged into the D&H on July 2, 1945

May 1, 1871 Rensselaer and Saratoga Railroad, incorporated on April 14, 1832, leased to D&H on May 1, 1871. D&H to Whitehall, via Albany. Merged into the D&H on July 2, 1945.

Lackawanna and Susquehanna Railroad, opened on January 1, 1872. Built by the D&H; 23.3 miles from Jefferson Junction to Nineveh

November 29, 1875 Delaware and Hudson Railroad to Canada (Whitehall to Canada), opened on November 29, 1875

March 29, 1915 Wilkes-Barre Connecting Railroad, 6.65 miles in length, opened in 1915, between Buttonwood and Hudson

On the map of the entire D&H system on page 174 of the June, 1969 edition of *The Official Guide of the Railways and Steam Navigation Lines of the United States, Puerto Rico, Canada, Mexico and Cuba*, all of those rail lines are shown.



1202

Market Development and the D&H

William and Maurice Wurts, knowledgeable and successful entrepreneurs and businessmen in the dry goods business in Philadelphia, and founding fathers of the D&H, made their initial efforts to market anthracite coal in Philadelphia during the winter of 1815. In the absence of an effective and affordable means of transportation from the northern coal fields to Philadelphia, however, their initial efforts, as well as their efforts over the next several years, to market coal in Philadelphia were not successful. Finally, in 1823, they were able to transport 100 tons of coal to Philadelphia, where it sold for \$12 a ton. In that same year, however, the coal operators in the Lehigh and Schuylkill regions of Pennsylvania (closer to Philadelphia than the Wurts' holdings) transported 6,000 tons of anthracite coal, which they were able to sell at one-half the price that the Wurts brothers had to sell theirs for, and still make a profit. It was an easy decision for the Wurts brothers to make: They would have to find other markets if they were to make any money mining and selling anthracite coal. That being the case, they conceived the idea of connecting the Lackawanna River Valley with the valley of the Hudson River by railroad and canal. They went to work. They built the D&H Gravity Railroad and Canal.

On October 9, 1829, the transportation system created by the Wurts brothers to market their anthracite coal in New York became operational.

To attract financial investment in their enterprise, and to create a market for anthracite coal in New York City, the Wurts brothers arranged to have shipped, by the sloop *Tripler*, from Philadelphia, on December 5, 1824, a shipment of coal. Five days later that coal arrived in New York. On January 5, 1825, a grate for burning stone coal was set up in the Tontine Coffee House (NW corner of Wall and Water Streets) in New York. Potential investors in the D&H were invited to demonstrations of the effectiveness of stone coal as a domestic and commercial fuel.

On January 7, 1825, subscription books were opened "for the purpose of receiving subscriptions to the stock of the said Company" at the Tontine Coffee House in New York, Kingston (Middle District Branch Bank), and Goshen (Orange County Bank). "The shares are one hundred dollars each, and five dollars is to be paid upon each share at the time of subscribing." The notice announcing the opening of the subscription books is dated December 2, 1824, and is signed by Philip Hone, Lynde Catlin, Jonathan Thompson, and G. B. Abeel.

On that January day, "\$1.5 million worth of shares were offered at the Tontine Coffee house in New York City. Before two o'clock in the afternoon the entire amount was subscribed. The stock attracted investors not only because of potential profits from the canal but also because the company's charters from Pennsylvania and New York allowed it to purchase coal lands and engage in banking, too. . . The entire enterprise was notable for being 'the largest undertaking that had ever been entered into upon the continent by any [private] corporation.' " (Miller and Sharpless, pp. 36-37, who credit Chester Lloyd Jones, The Economic History of the Anthracite Tidewater Canals, Philadelphia, 1908, pp. 76-78)

From the very beginning of the Delaware and Hudson Canal Company, market development for anthracite coal was a very high priority.

On December 5, 1828 (D&H Canal open; Gravity Railroad not yet open), 10 boats arrived at Rondout with the first shipment of coal from Honesdale via the D&H Canal. Each boat carried 10 tons, a load limit imposed by the un-stabilized condition of the canal with consequent shallow water. The coal had been moved from Carbondale to Honesdale in wagons and was loaded into the boats with wheelbarrows.

In the lead of the 10-boat squadron was the *Superior* in the command of Captain Hickson, followed in order by the *United States* commanded by Captain Kortwright, *Company Boat No.* 2 commanded by Captain Lamerau, the *Oliver H. Perry* commanded by Captain Terwilliger; and *Company Boat No.* 20, commanded by Captain Doll. The names of the other boats and their captains have not come down to us.

The ten tons of coal in one of those boats were transferred to the sloop *Toleration* and taken down to New York. Some of that coal was burned in the grates at the Western Hotel on Cortlandt Street in December 1828. Some of that coal was burned, in January 1829, in the D&H offices on Wall Street. In *Coughtry*, we read:

"A portion of her cargo was burned in the grates of the Western Hotel on Cortlandt Street . . . [another grate was later set up in the D&H's offices on Wall Street] in the latter part of January, 1829, which so demonstrated its free burning qualities that during the cold wave that followed a month later Philip Hone, in his diary, deprecated the fact that there was 'no coal for sale in the city.' " (W. J. Coughtry, "A Century of Anthracite The Hundredth Anniversary of the Arrival of the First Cargo to Reach New York City, *The Delaware and Hudson Company Bulletin*, December 15, 1928, pp. 373-77, 379-380)

The impact on the population growth of New York City in the twenty five years following those initial shipments of anthracite coal there was enormous:

"... the epochal entry [on December 10, 1828] into New York City [of these first 10 tons of Lackawanna anthracite; which had been transferred from the canal at Rondout on December 5, 1828 to the sloop *Toleration*, the same ship which four years before, almost to the day, had brought the first sample of Lackawaxen Anthracite to the City of New York by way of Philadelphia.] opened up a new era which enabled that city to more than double its population in a quarter of a century. (*Coughtry* article cited above)

Not only was anthracite coal sent by the D&H down the Hudson River to New York City in December 1828, fifty tons of coal from that same initial squadron were sent up the Hudson River to Albany, where its effectiveness as a heating fuel was demonstrated in one fireplace in the Senate chamber there. In addition, a ton of that coal was sent to the Governor-elect, Martin Van

Buren. In *Coughtry* we read:

"Fifty tons of the coal transported by the same squadron was forwarded to Albany [The 50 tons that were sent up to Albany were consigned to Messrs. Townsends.] where a portion of the shipment was tried by the clerk of the Senate 'in one fire in that room,' and a ton was sent to the Governor-elect Martin Van Buren."

With those early and successful demonstrations of the use of anthracite coal as a heating fuel to their credit, the D&H, early in 1829, was spurred on to demonstrate to potential customers the effectiveness of anthracite coal as a fuel for cooking purposes. Accordingly, they installed a cooking stove in the company's banking house in New York. In *Coughtry* we read:

"The supremacy of anthracite for hearing purposes now firmly established, the managers of the infant company eagerly sought new outlets for their product [emphasis added]. Early in 1829 the managers had a range installed in the kitchen in the company's office and banking house in New York City to demonstrate its adaptability for cooking purposes."

In 1830, the D&H redoubled their efforts to expand their market for anthracite coal by sending samples to Boston and Providence and, remarkably, New Orleans. In *Coughtry*, we read:

"During the next year [1830], they redoubled their efforts for enlargement of the business [emphasis added] by investigating the Boston market, sending samples to Providence and even shipped several hogsheads containing anthracite to New Orleans."

Upstate New York and New England, with virtually no coal resources, were good markets for D&H coal. An analysis of 51 coal barges leaving Rondout in the last two weeks of October 1831 shows as many bound for Providence as for New York (seven), while four each were destined for Newburgh, Hudson, Troy and Albany, and five for Poughkeepsie.

During the week ending on November 5, 1831, 24 vessels and one canal boat cleared Rondout, loaded with Lackawanna coal. From an article published in the November 25, 1831 issue, p. 3, of the *Dundaff Republican*, and Canal & Rail Road Intelligencer, we learn the destinations for those 24 vessels and one canal boat:

"Coal Trade of the Delaware and Hudson Canal—During the week ending on the 5th instant, twenty four vessels and one canal boat cleared from Rondout, loaded with Lackawanna Coal. Of these, three were bound to New York, and one of them carried 450 tons—one to Yonkers—one to Belleville, N. J.—two to Boston—one to Charleston—one to Nantucket—one to Salem—one to Milton, Mass.—one to Saugerties—one to New Haven—two to Hudson—two to Poughkeepsie—one to Waterford—one to Troy—one to Newburg—one to Athens—one to Albany—one to Providence—one to Elizabethtown, N. J., and one to Rhinebeck."

Coal shipments up and down the Atlantic coast:

1830: 63,000 tons; to Boston 5,000 tons (*Lowenthal*, p. 194)

1848: 9,300 tons to Boston (*Lowenthal*, p. 194)

1850: a million tons (Lowenthal, p. 194)

Aggressive marketing of anthracite coal as a fuel was basic D&H policy. In 1831, it was used in the furnaces of the iron industry. By 1836 it was used to fuel Hudson River steamboats. By 1848, it was used in the manufacture of salt. In *Coughtry* we read:

"In 1831 they extended its use to steam production in the furnaces of the Ulster Iron Company, the steamboat *Viceroy*, and the Walnut Street ferryboat *Experiment*, and employed agents to introduce the use of anthracite in manufactories and other establishments using steam engines and in blacksmith forges. / By 1836 the use of anthracite was extended to the Hudson River steamboats. On June 23 of that year, the *Novelty*, with the managers of the company and a party of distinguished guests, left her wharf in New York at six in the morning and arrived at Albany twelve hours later, the first steamboat propelled by anthracite to make such a voyage. / In 1848 anthracite was successfully used in the manufacture of salt at Syracuse and Salina. By 1868 the company was shipping anthracite down the Susquehanna to Baltimore, where attempts to create interest in it a half century previously were rebuffed with great positiveness, to points on the railroad line between Wilkes-Barre and Jersey City, and two years later entered the rapidly developing markets of the west."

Anthracite coal was advertized for sale in New York newspapers from the beginning of D&H coal shipments there. Shown below is an advertisement for Lackawanna Coal from the Tuesday Evening, September 21, 1841 issue of the *New-York American*. Our thanks to John V. Buberniak for bringing this ad to our attention.

Five sizes of coal were offered for sale: egg, broken, stove, and furnace @ \$7 per ton, delivered; small nut @ \$5 per ton delivered. The D&H coal yards were located at the corner of Beach and West Streets, King and Greenwich Streets, and Monroe and Montgomery Streets, where orders were received. Orders were also received at the D&H Bank at the corner of William and Penn Streets, "where contracts will be made for cargoes delivered at Rondout."

ACKAWANA COAL .- The Delaware and Hudson Canal Company are now receiving into their yards Lackawana Coal, fresh from the mines, which hey offer to their customers and the public at the following reduced prices :-\$7,00 per ton, delivered. Egg and Broken, Stove and Furnace, 7.00 do Small Nut. 5,00 do Yards corner of Beach and West streets. King and Greenwich streets. Monroe and Montgomery. where orders will be received. Also, at their Bank corner of William and Pine streets, where contracts will be made for cargoes delivered at Rondout. 1413

For the period 1843-1849, here is the "Table of All Freights on Lackawanna Coal" shipped from Rondout in Ulster County, NY to New Haven, Providence, Boston, Newburyport, and Portsmouth. Our thanks to John V. Buberniak for making available to us this table on March 5, 2016.

Shipped from Rondo year 1849, both	inclu	sive;	shov	ving t	he hi	ghest	, the	lowes	st, and	d the	actua	ıl ave	rage	rate	of eve	ery ye	ear an	d to	each	place	o th
RONDOUT,	1843.			1844.			IS45.			1846.			1847.			1848.			1849.		
ULSTER COUNTY, STATE OF NEW YORK,	HIGHEST RATE.	LOWEST RATE.	AVERAGE RATE.	HIGHEST RATE,	LOWEST RATE.	AVERAGE RATE.	HIGHEST RATE.	LOWEST RATE.	AVERAGE RATE.	HIGHEST RATE,	LOWEST RATE.	AVERAGE RATE.	HIGHEST RATE.	LOWFST RATE.	AVERAGE RATE.	HIGHEST RATE.	LOWEST RATE.	AVERAGE RATE.	HIGHEST RATE.	LOWEST RATE.	AVERAGE RATE.
PROVIDENCE, R. I	\$1.25 \$1.00 \$1.25 \$1.00	.62 <u>1</u> .80 .87 <u>1</u> 1.00 1.00 1.00	.66½ .84 .88½ 1.08½ 1.00 1.00	.75 1.25 1.31 1.12 <u>4</u> 1.25 1.10	.62½ .80 .87½ 1.00 1.00 1.25	.72 .90% .894 1.07% 1.15% 1.21	.95 .90 1.50 1.374 1.50 1.25	.75 .90 .90 1.20 1.37 <u>4</u> 1.25	.75½ .90 1.00 1.24½ 1.47⅓ 1.25	.90 1.15 1.00 1.27 1.50 1.50	.75 1.00 1.00 1.12 <u>1</u> 1.25 1.25	.78 1.05 1.00 1.23§ 1.36 1.30	1.12½ 1.40 1.50 2.00 2.00 2.00	.75 1.10 1.124 1.50 1.624 1.50	.94 1.22 1.51½ 1.72 1.80 1.90	.80 1.00 1.00 1.25 1.30 1.25	.75 .87 <u>4</u> .90 1.00 1.20 1.124	.77 .90 .93 1.06 1.21 1.18	.87½ .95 1.12½ 1.12½ 1.30 1.12¼	.75 .90 .90 1.06 1.15	.76 .90½ .91 1.10¾ 1.24¾ 1.126

The practicability of anthracite coal as a fuel had now been conclusively demonstrated. The public however, was still reluctant to accept the new fuel and so, ironically enough, the canal, which had been built, primarily for the purpose of hauling anthracite coal, hauled far more cord wood than coal during the first two seasons of operation.

On the question of the growth of the D&H from its modest beginnings in 1823 to the centennial of its existence, we read, in *Century of Progress* (p. 593), the following:

"The growth that has been achieved by the company from such small beginnings seems marvelous. During its earlier years of desperate struggle for successful existence this growth was often interrupted and the final progress was made possible only by consistent courage, indomitable energy, broad vision and persistent economies. The company has enjoyed the maximum of careful management [emphasis added], in aid of which the expanding use of anthracite and the increasing population of the country have played their conspicuous parts."

New markets for coal demanded additional coal-bearing lands and improved transportation facilities.

Under the leadership of George Talbot Olyphant (vice president under John Wurts, Olyphant was elected D&H president on March 15, 1858, served until 1869; his vice president was Robert Soutter) from the spring of 1855 on, the Delaware and Hudson shifted its emphasis away from canal transportation and embarked aggressively on a program to acquire (1) thousands of additional acres of coal-bearing lands and anthracite production facilities in the Lackawanna and Wyoming valleys, and (2) transportation facilities to market that coal. By the time the transformation was complete, the D&H was primarily a railroad company that owned mines.

One of the first steps toward expansion occurred in 1866, when the D&H purchased the Union Coal Company, which owned coal lands, railways, and canal boats.

Shaughnessy describes this period in the D&H history as follows:

"During and following the war the company greatly increased is coal holdings in the Lackawanna and Wyoming valleys, from Carbondale to Wilkes-Barre. By gaining control of such companies as the Plymouth & Wilkes-Barre Railroad and Bridge Co., the Howard Coal & Iron Co., the Union Coal Co. and the Northern Coal & Iron Co., it gained access to thousands of acres of additional coal-bearing lands, many mine openings, company houses and stores, breakers and processing plants, a first-class railroad bridge across the Susquehanna River from Wilkes-Barre to Plymouth, and miles of railroad which were then tied together to form a network of feeder rail lines. Because of franchise limitations in its own charter, the D&H came to use the Northern Coal & Iron Co. as the instrument through which it acquired these vast properties and railroads." (p. 60)

The revisions made to the Gravity system in 1859 and 1868 made it possible for the D&H to ship annually to the East, to the Hudson River, increased quantities of coal.

With the acquisition of new coal lands under Olyphant's leadership, still more markets for anthracite coat were necessary to promote the growth and profitability of the company. The D&H, accordingly, focused on the development of new markets to the North, to the South, and to the West, in Albany and beyond, in Baltimore, in the Great Lakes region.

In July 1866, the managers of the D&H, recognizing the potential usefulness of the A&S in marketing D&H coal at Albany and north of that city (the A&S was completed as far as Unadilla in 1866), negotiated a contract with the A&S, providing for completion of the A&S to Nineveh within a year, and for moving D&H coal over the A&S in A&S cars "from any point where tendered for loading." Under this agreement (which was signed 30 months before the contract with the Erie to build the Jefferson Branch), the D&H bought \$500,000 worth of A&S second mortgage bonds.

An interesting presence, always there, in the history of the D&H, was the Erie Railroad, whose line between the Hudson River (Piermont) and Binghamton was opened on December 28, 1848. If the Erie had not been able to open on schedule that line before January 1, 1849, it would have had to forfeit all its rights, franchises and titles to the state of New York.

The fact that that line was opened five days ahead of schedule (December 28, 1848) was made possible, in part, by the D&H. It's an interesting story, how the D&H Gravity Railroad helped to save the Erie, and one worth re-telling here.

Published on page 4 of the *Carbondale Leader* of September 21, 1878 is the following article that was previously published in *The New York Times* about how the D&H helped to save the Erie:

"Erie's Early Troubles. / HONESDALE, August 28.—The work of narrowing the gauge of the Erie Railway, which is progressing rapidly along the line, recalls one of the most interesting and important incidents in the history of the pioneer of American trunk lines—an incident by which the New York and Erie Railroad Company was enabled to save the forfeiture of its franchises to the state [NY], and by which another struggling corporation was enabled to raise itself from insignificance and probable bankruptcy to be one of the most wealthy and powerful of its kind in the country. / After the great financial revulsion of 1836-7 had compelled the suspension of early operations on the railroad, the sate came to the aid of the company, and in 1838 loaned it its credit to the amount of \$3,000,000. This large amount of money was used up, and only 61 miles of the road in operation in 1845—from Piermont, on the Hudson, to Otisville, the summit of the Shawangunk mountains, in Orange County. Not a small share of the company's means had been spent in carrying out the ridiculous idea of its engineers that rails must be laid on piles from Owego to Hornellsville. For a distance of 90 miles two rows of heavy posts were sunk in the earth. Each one of these posts stood for many a year afterward as a monument to the memory of millions of wasted riches, for they were never used, and the last one disappeared from the side of the present route only a few years ago. / In 1845 the state came again to the aid of the railroad enterprise. By act of the assembly of that year the company was released from the claim of the state against it, and liberal provisions were made as to subscriptions to new stock. This

legislation was accompanied with the proviso, however, that unless rails were laid and the road was in working order between the Hudson and Binghamton by January first, 1849, the company should forfeit all its rights, franchises, and titles to the state. Thus given a new lease of life and another replenished Treasury, the directors of the company again ordered the work to proceed. The rails with which the road was ironed between Piermont and Otisville was of English iron, which cost \$80 a ton. The straitened circumstances of the company, and the time to which it was limited, required the obtaining of iron at a cheaper rate and in a more convenient market. / At that time [1845], the manufacture of railroad iron was an entirely new industry in this country. Up to 1845, no T-rail had been made here, the strap-rail being used. The New York and Erie imported the first T-rail on this side of the ocean. In 1843, Colonel George W. Scranton and Seldon T. Scranton of Oxford, N.J., established a rolling mill and rail factory in the village of Harrison, now the city of Scranton, Pa. It was known as the Lackawanna Iron Works. In 1846 the Scrantons, knowing that the New York and Erie Railroad Company had paid \$80 a ton for the rails imported from England, and that it was necessary for iron to be obtained by the company at a lower price made a contract with the railroad company to furnish it with 10,000 tons at a rate much less than that of the English iron. At the time of making this contract, the iron company had no machinery capable of turning out the rails, and furnaces and all had to be provided. The iron was to be delivered to the New York and Erie Railroad's agents at the mouth of the Lackawaxen river, in Pike county, Pa., during 1847-8. A number of prominent capitalist interested in the success of the railroad loaned the Scrantons \$100,000, with no security but their word. The machinery for the iron mills was drawn 60 miles by teams, and was ready for operations in a few months. As fast as the iron was ready it was drawn by teams nine miles to Archbald, Pa., then the southern terminus of the gravity road of the Delaware & Hudson Canal Company. On this road it was carried over the Moosic Mountains to Honesdale the head of the canal, where it was loaded on boats, which delivered it to the railroad company at Lackawaxen. From there it was carried on the canal to Port Jervis. [emphasis added]/ When 1,500 tons has been thus delivered the rails were laid from Otisville to Port Jervis. Then the difficulties of running the road into Pennsylvania arose. The road was originally intended to cross the Delaware at the village of Matamoras, nearly opposite Port Jervis, but the route was impracticable, and it was decided to change the crossing point to Sawmill Rift, three miles further up the river. Injunctions were served on the company forbidding this change of route, until it had agreed to construct a bridge across the river at Matamoras. The delay attending these proceedings threatened to be fatal to the completion of the road to Binghamton within the stipulated time. [Unless rails were laid and the Erie had the road in working order between the Hudson and Binghamton by January 1, 1849, the Erie would forfeit all its rights, franchises, and titles to the state.] Accordingly a change was made in the terms of the contract with the iron company. Instead of having the balance of the iron delivered at Lackawaxen, the railroad company had it distributed at different points along the Delaware Division. The iron was hauled on wagons over the heavy mountain roads of Northern Pennsylvania to Narrowsburg, Cochecton, Equinunk, Stockport, Deposit, and Lanesboro'. Over 400 mules and horses were employed in the carting. From these points the rails were simultaneously laid, resulting in the completion of the track, according to agreement with the state, five days before the date fixed upon. / The celebration of

the event in Binghamton on the 28th of December, 1848, was one of the greatest demonstrations ever witnessed in this country. The contract was a profitable one to the new iron company, notwithstanding the expense and labor connected with its fulfillment, and it had laid the foundation for the present great city of Scranton, and resulted in taking into the Lackawanna Valley a capital of \$150,000,000. The present gigantic corporation, the Lackawanna Coal and Iron Company, is the direct outcome of that early venture of the men whose great business enterprise saved the New York and Erie Railroad from certain bankruptcy.—*N.Y. Times*."

The D&H saved the Erie, to be sure, but the D&H needed the Erie to help market its anthracite, and in 1868, the D&H contracted with the Erie Railroad for construction of a branch line from their main line to Carbondale, thereby opening the Great Lakes and New York state markets to the D&H.

Recognizing the potential usefulness of Erie in marketing its coal in New York state, especially in the Buffalo area and to the Great Lakes region when the Erie tracks were extended there, the managers of the D&H met with the Erie in 1863 to explore the possibility of the D&H's marketing anthracite via the Erie Railroad via a rail connection between the D&H and the Erie at Lanesboro Junction, Lanesboro, PA.

The talks went well, and in 1864 the D&H (with Olyphant as president and with Thomas Dickson as D&H General Superintendent) met with the Jefferson Railroad Company (which was operated and controlled by the Erie Railroad Company, through its ownership of nearly all the capital stock, from its principal office in Dunmore, PA) to consider their constructing for the D&H a rail line from Carbondale to Lanesboro.

The D&H and the Erie, rivals in the business of anthracite mining and railroading, then, from as early as 1848, were prepared to work with each other, for their mutual benefit.

Given the presence of the Erie Railroad in Binghamton, beginning on December 28, 1848, it is not surprising that the Erie's presence there should stimulate railroad activity in the *Parlor City* (official nickname for Binghamton).

As such, in 1851, the Albany and Susquehanna Railroad was brought into existence and chartered to build a 6-foot gauge rail line from Albany to Binghamton.

To prepare for possible expansion into New York, the D&H, in May 1867, acquired railroad rights in New York state. At the same time, expansion was taking place to the south, and by May 1868, the D&H was sending coal down the Susquehanna to Baltimore and along the railroad lines connecting Wilkes-Barre with Jersey City. In *Century of Progress*, we read:

"President Olyphant said that the company was already sending coal down the Susquehanna

to Baltimore and along the railroad lines connecting Wilkes-Barre with Jersey City and he expected 'next year' to have facilities for putting coal into western markets 'on the most favorable footing.' "(COP, p. 201)

On September 2, 1868, the managers of the D&H (with Olyphant as President and Dickson now Vice President) entered into a very important contract with the Jefferson Railroad/Erie Railroad. There were three very important provisions to this contract. Under this contract, the Erie agreed:

- (1) to build a line from Carbondale to Lanesboro (the Northern Coal and Iron Company, which the D&H now controlled, had previously surveyed such a route, and the route followed by the Erie was essentially the Northern Coal and Iron Company's earlier survey), the line to be built under the existing charter of the Jefferson Railroad Company, with labor and capital provided by the Erie (to finance the line, the D&H agreed to buy at par \$1,500,000 worth of Boston, Hartford & Erie Railroad bonds from the Erie), and with the D&H to have trackage rights over the route to Lanesboro;
- (2) to transport D&H coal to Rochester and Buffalo (On March 11, 1869, the D&H purchased for about \$150,000 a property on Buffalo Creek, Buffalo, for a coal depot.);
- (3) to transport D&H coal to Weehawken and New York City during the winter, as soon as the Erie could extend its line from Hawley to Honesdale.

On January 12, 1869, the **Albany & Susquehanna Railroad** (a 6-foot gauge line, from Albany to Binghamton) opened. On February 24, 1870, the line was leased, for 99 years, to the D&H.

Given below is a copy of the original Indenture of Lease that was made and entered into by and between The Albany and Susquehanna Railroad Company of the first part, and The President, Managers and Company of the Delaware and Hudson Canal Company of the second part.

This copy of the lease is Exhibit A to the bill of complaint that was initiated in 1906 by the Aetna Insurance Company et al., Complainants, against the Albany and Susquehanna Railroad Company and the Delaware & Hudson Company, Defendants.

This case was tried in the Circuit Court of the United States for the Southern District of New York. The final decree of that court was that the Delaware and Hudson Canal Company pay the Albany and Susquehanna Railroad Company the amount of \$1,107,923.24.

A volume of the proceedings of this case in that court is in the collection of the Carbondale D&H Transportation Museum, courtesy of John V. Buberniak. Here is the title page of that volume:

Circuit Court of the United States

For the Southern District of New York.

AETNA INSURANCE COMPANY ET AL., Complainants,

against

IN EQUITY.

ALBANY & SUSQUEHANNA RAILROAD COMPANY and DELAWARE & HUDSON COMPANY,

Defendants.

No. 9475.

RECORD.

HOWLAND, MURRAY & PRENTICE, EDWARD W. SHELDON,

Solicitors for Complainants.

OPDYKE, LADD & BRISTOW,

Solicitors for Defendant

Delaware & Hudson Company.

HERBERT S. BARNES,

Solicitor for Defendant

Abany & Susquehanna Railroad Company.

C. G. BURGOYNE, Walker and Centre Streets, N. Y.

The D&H appealed the final decree of the Circuit Court Southern District of New York and, on a certificate from the United States Circuit Court of Appeals for the Second District, the case (The Delaware and Hudson Company *vs.* The Albany and Susquehanna Railroad Company et al) was heard by the Supreme Court of the United States at the October Term 1908 (No. 416). A volume of the proceedings of this case before the Supreme Court of the United States is in the collection of the Carbondale D&H Transportation Museum, courtesy of John V. Buberniak. Here is the title page of that volume:

CERTIFICATE.
SUPREME COURT OF THE UNITED STATES. OCTOBER TERM, 1908.
No. 416.
THE DELAWARE AND HUDSON COMPANY vs. THE ALBANY AND SUSQUEHANNA RAILROAD COMPANY ET AL.
ON A CERTIFICATE FROM THE UNITED STATES CIRCUIT COURT OF APPEALS FOR THE SECOND CIRCUIT.
FILED MAY 29, 1908.
(21,199.)

Here, then (from pp. 40-57 of the Circuit Court volume) is a copy of the original *Indenture of Lease* that was made and entered into by and between The Albany and Susquehanna Railroad Company of the first part, and The President, Managers and Company of the Delaware and Hudson Canal Company of the second part.

40 157 Exhibit A. Whereas, The Albany and Susquehanna Railroad Company (hereinafter called the Railroad Company), and The President, Managers and Company of The Delaware and Hudson Canal Company, (hereinafter called the Canal Company) on the 24th of February, A. D. 1870, made, entered into, executed and delivered an Indenture of Lease, as follows, to wit :-ORIGINAL LEASE. 158 This Indenture, made and entered into this twentyfourth day of February, one thousand eight hundred and seventy, by and between The Albany and Susquehanna Railroad Company of the first part, and The President, Managers and Company of The Delaware and Hudson Canal Company of the second part, wit-1. That the party of the first part, in consideration Lease of railroad. of the rent, covenants and agreements hereinafter mentioned, reserved and contained on the part and behalf 159 of the party of the second part, its successors and assigns, to be had, kept and performed, has granted, demised and leased, and by these presents does grant, demise and lease unto the said party of the second part, its successors and assigns, the entire railroad of the party of the first part, lying, being and extending from its eastern terminus in the city of Albany, in the State of New York, in a south-westerly direction, through portions of the counties of Albany, Schenectady, Schoharie, Otsego, Delaware, Chenango and Broome, to its western terminus in the City of Binghampton, being in length one hundred and forty-two miles or thereabouts; also all the lands, tenements, heredita-160 ments, appurtenances, buildings, fixtures and machinery appurtenant and belonging to or connected with the said railroad, or in any manner used or employed by the party of the first part in operating said railroad; also all rails, ties, fencings and erections of every kind belonging to, upon, or connected with said railroad; also all the cars, locomotives, engines, Equipment, tools, machinery, equipments and appliances belonging to or connected with said railroad, and the use thereof; also all and every right of way, license, easement, right, privilege and immunity of the party of the first part, conferred by, and possessed and enjoyed under and by virtue of, its charter, and of any and every act passed and to be passed amendatory thereof, and otherwise, howsoever obtained, with the full right and authority to have, hold and use such rights of way, easements, rights, privileges and immunities as fully as the party of the first part might or could if these presents had Habendum. not been executed. To have and to hold all and singular the above demised railroad, premises, property, estate and effects, including the right of way, easements, licenses, privileges and immunities aforesaid, unto the party of the second part, its successors and assigns, from the day of the date hereof, for and during the full end and term of the charter of the party of the first part, or of any and every renewal and continuance thereof, yielding and paying therefor unto the party of the first part, its successors or assigns, Rent accrurent at the rate of four hundred and fifty-five thou-ing on and sand dollars per annum from the date of these pres-after Jan. ents until the first day of January, one thousand eight 1st. 1871, hundred and seventy-one, and thereafter the annual \$490,000 a rent of four hundred and ninety thousand dollars, pay-year. able in the way and manner and at the times hereinafter particularly stated.

2. And the said party of the second part, in consideration of the demise, covenants and agreements herein contained, and to be performed, fulfilled and kept on the part of the party of the first part, its successors and assigns, hereby covenants and agrees that Covenant of it will pay as and for the annual rent aforesaid, semi-lessee to pay annually, the interest accruing and to become due and interest on payable from and after the date of these presents, Albanyloan; upon one million of dollars of six per cent. Albany city bonds, issued to the party of the first part under and by virtue of an act of the Legislature of this State, entitled "An act to authorize the city of Albany to make a loan to the Albany and Susquehanna Railroad Also to pay Company," passed April 10, 1852, together with one into sinking per cent. thereon to the sinking fund, as mentioned fund for and referred to in the aforesaid act, and will also pay, Albany loan. semi-annually, three and one-half per cent. for the interest accruing from and to become due and payable after the date of these presents, upon three and a half millions of dollars of certain mortgage bonds made and executed by the party of the first part, upon the Also to pay aforesaid railroad property and effects, according to interest on the terms and conditions thereof, and will also pay to\$3,500,600 the party of the first part, in equal monthly install-bonds; ments, from the date of these presents until the first day of January, one thousand eight hundred and

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Also to pay dividends on stock :

Also to pay balance to

> Covenant of lessee to repair.

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Covenant of lessee to endorse its guarantee of interest and dividends.

seventy-one, at the rate of one hundred and forty thousand dollars per annum, and will also, from and after the first day of January, one thousand eight hundred and seventy-one, upon the first days of July and January in each year, pay a semi-annual dividend of three and one-half per cent. upon the then existing shares of the capital stock of the said party of the first part, not exceeding twentyfive thousand shares, and if so many shares do not exist at the time of any such payment, or if the sums required for the payments before mentioned, shall not amount to the aforesaid four hundred and ninety thou-166 Susquehanna sand dollars, the balance not applied to the payment of such interest and dividends shall be paid to the said party of the first part. It being understood, however, that the interest and dividends to be paid as aforesaid shall and may be paid directly to the respective bond and stockholders.

3 And the said party of the second part further covenants and agrees in manner aforesaid that during the term hereby granted, it will, at its own cost, risk and expense, maintain, preserve and keep the aforesaid railroad in good working condition and repair, as a firstclass railroad, and so as to be suitable for the transaction of all the business that can be reasonably done thereon, and will also maintain, preserve and keep the side tracks, station houses, fixtures, appurtenances, tools, machinery, rolling stock and equipments, belonging and appertaining to the said railroad, in as good repair, order and condition as the same are in at the date of these presents, and will maintain and keep in order all fences, cattle guards and warning boards which are or shall be necessary or required by law, and will operate, employ and use the said railroad, its fixtures and appurtenances in such manner as to do and perform in a proper manner all the business offered to and which can be reasonably done upon the same, and will perform all and every duty and obligation toward the public which the party of the first part would be legally bound to do and perform under its aforesaid charter if these presents had not been executed.

4. And the party of the second part hereby further covenants and agrees, in manner aforesaid, that, upon the due presentation of the aforesaid Albany city bonds, mortgage bonds, and the scrip or certificates of the stock, the interest and dividends upon which are to be paid as aforesaid, it will, by a proper instrument in writing, to be endorsed or stamped thereon re-

spectively, and duly executed, guarantee to the owner and holder thereof payment of the interest and dividends thereon, as aforesaid, the dividends to be payable semi-annually, on the first days of January and July, as aforesaid, and the interest to be payable according to the terms and conditions of the said bonds; and in case the party of the first part at any time shall desire to use new bonds, either in renewals of, or to obtain means with which to pay, bonds now outstanding, or any portion of them, that the party of the second part will endorse or stamp such new bonds, in the manner aforesaid, and deliver Renewal of the same to the party of the first part; provided that outstanding bonds by the new bonds, so endorsed or stamped, shall be for lessor. the same amount as the bonds then surrendered to the party of the second part for cancellation. And in case the aforesaid bonds, or any of them, shall at any time be converted into stock, that thenceforth the party of the second part will pay, in lieu of the aforesaid interest on said bonds, semi-annual dividends on said stock, at the rate of three and a half per cent., and, upon due presentation of such new stock, will endorse or stamp upon the same a guarantee of the payment of such semi-annual dividends, according to this agreement; and in all cases of new certificates being issued on the return of guaranteed certificates, or transfer of guaranteed shares, or new bonds having been issued in place of guaranteed bonds, the said party of the second part shall endorse or stamp the like guarantee on such new certificates and bonds; provided, however, and it is hereby understood and agreed, that in case the party of the first part shall fail to keep and perform these presents upon its part, that thereupon such guarantee, and each and every of them, shall become and be null and void.

5. And the said party of the second part further Covenant of covenants and agrees, in manner aforesaid, that, during pay taxes. the time of this demise, it will pay, bear and discharge all taxes and assessments of every description assessed, imposed, levied and accruing upon the railroad, property and effects hereby demised, and upon the business done upon the said railroad, from the day of the date hereof, in the same manner and to the same extent as the party of the first part would be liable to pay if these presents had not been executed. And if, by any change of the law, the present tax or duty required of the said party of the first part shall be required of the said stockholders, then the said party of the second party shall pay the same. But the party of the second

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174 To pay for

Lessor's covenant for quiet possession.

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Lessor's covenant for further assurance.

part shall not be required to pay the present income tax upon the aforesaid interest and dividends, or any tax thereon imposed, or hereafter to be imposed, by whatever name the same may be called. And if the law under which tax is or may be levied requires the party of the second part to pay the same, then the amount of tax so paid may be deducted and kept back from and out of the aforesaid interest and dividends. And, further, that the party of the second part will pay all expenses for construction, repairs, salaries, and otherwise, which may be necessarily incurred on account of the railroad and demised premises, from the day of the date hereof; and will also pay and repairs, etc. discharge all damages which may be recovered against the said party of the first part, for injuries to persons or property, or for negligence, or breach of duty as carriers or warehousemen, and in all respects save the said party of the first part harmless and indemnified from all damages, losses and penalties which may be incurred ar arise in or by the conduct, use or operation of said railroad.

6. And the said party of the first part, for itself, its successors and assigns, hereby covenants and agrees to and with the said party of the second part, its successors and assigns, that it has full power and lawful authority to execute these presents, and that the said party of the second part, its successors and assigns, paying the said yearly rent above reserved, and performing the covenants and agreements herein contained on its part, shall and may at all times, during the continuance of this lease, peaceably and quietly have, hold, possess and enjoy the railroad property, rights and effects hereby demised, and every part thereof, without any manner of let, suit, trouble or hindrance, of or from the party of the first part, its successors or assigns, or any other person or persons whomsoever, lawfully claiming or to claim the same.

7. And the said party of the first part further covenants and agrees in manner aforesaid, that it shall and will from time to time, during the continuance of this lease, make, execute, and deliver to the party of the second part, its successors and assigns, all and every such further and other leases, deeds, transfers, instruments in writing and assurances as by said party of the second part, its successors or assigns, shall be reasonably desired or required for fully effectuating the objects, intents and purposes of this lease, and of leasing the railroad and other property hereinbefore mentioned, and hereby leased or intended so to be, and for more fully confirming and securing unto the said party of the second part, its successors and assigns, all the rights and privileges hereinbefore mentioned, and

granted and secured, or intended so to be.

8. And the said party of the first part further cove- Lessor's nants and agrees that it will, during the term of this organization demise, keep and continue, at the cost and expense of to be mainthe said party of the second part, not exceeding, how- expense of ever, the annual sum of one thousand dollars, its legal lessee. organization, and also at all times, when thereunto reasonably required by said party of the second part, do and perform, at the expense of the said party of the second part, all such reasonable acts, matters and things as may be proper for the due protection, preservation and enjoyment of the property hereby demised, and to carry into effect the true intent and meaning of this instrument, so far as the same may be done consistently with the rights of the said party of the first part, and in default thereof, the same may be done by, but at the expense of, the said party of the second part, and therein the said party of the second part may use the name, power and authority of said party of the first part; and further, that the said party of the second part may, at any time, and in all places, at its own expense, use the corporate name of the party of the first part in any suits or proceedings wherein it shall be necessary or proper for the enforcement of its rights against the third parties.

9. And the said party of the first part hereby further Assignment covenants and agrees, in manner aforesaid, that it will of certain and does hereby assign and transfer to the party of contracts, the second part, its successors and assigns, by a proper instrument in writing to be duly executed, all its right, title and interest of, in and to a certain contract made and executed by and between the said party of the first part and the Cherry Valley and Mohawk River Railroad Company, now known as the Cherry Valley, Sharon and Albany Railroad Company, and also in and to a certain contract made with the Fall Creek Coal Company, in relation to operating the railroad belonging to the last aforesaid Railroad Company, and the carrying coal thereon for the said Company, and also the contract for additional land at Albany, made with the Van Rensselaer estate; and also the contract for the erection of a joint depot at Binghamton, made with the Erie Railway Company and with the Syracuse, Binghamton and New York Railroad Company; and also all pending contracts for supplies, materials and equipments not yet delivered,

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and which contracts, respectively, the party of the second part agrees to perform, and carry out according to the tenor and effect thereof, as fully as the party of the first part would be bound to do if these presents were not executed; and the property and interests resulting from said contracts and the performance thereof shall be subject to this lease, and shall form a part of the demised property.

Stock and bond register to be kept.

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 It is hereby mutually covenanted and agreed, by and between the parties hereto, that there shall be kept, at the office of the party of the second part, a book or register containing a record in proper form of the aforesaid stock, and of each and every of the aforesaid bonds, and of the bonds which may be issued in substitution therefor, which register shall be open at all reasonable and proper times to the inspection of the party of the first part, its successors or assigns.

Renewals of bonds by

outstanding 183 lessee.

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Payment of stock subscriptions.

11. And it is further mutually covenanted and agreed, that when the bonds, the interest upon which is to be paid by the party of the second part, as above provided, arrive at maturity, if the same be not exchanged by the holders thereof for new bonds, and payment of the principal be demanded and be not paid by the party of the first part, then the party of the first part in order to protect the property hereby leased from foreclosure and sale, shall, if so required by the party of the second part, issue in due form new bonds, payable at such times and in such manner as the party of the second part shall require, or shall, at the option of the party of the second part, issue stock therefor, and shall deliver the said bonds or stock to the party of the second part to be negotiated and sold, and the net proceeds arising therefrom shall be applied to the payment of such maturing bonds; such new bonds or stock to be sufficient in amount to provide, by their net proceeds, for the payment of such maturing bonds, and the payment of the interests or dividends upon such new bonds or stock shall be guaranteed in the same manner as have been the interest upon such maturing bonds.

12. And it is further mutually covenanted and agreed, that the said party of the first part will, whenever requested by the said party of the second part, require the payment, in installments, of the balance of the nine thousand five hundred shares of stock which has been subscribed, and on which an installment of ten per centum has been paid, and will pay over the said installments, when collected or received, to the said party of the second part, and the said party of the second part will apply the same to the laying of additional track, the purchase of additional equipment, and other necessary improvements of said road, and will also pay an additional rental, semi-annually, at the rate of seven per centum per annum on such installments, as may from time to time be paid as aforesaid; and said stock when fully paid shall have the like guarantee as is hereinbefore provided for the twenty-five thousand shares of full paid stock, issued and to be issued as aforesaid, to the extent of the ninety per cent. so to be received by the party of the

second part as aforesaid.

13. And it is hereby further mutually covenanted Issue of and agreed, that in case the party of the second part additional shall at any time hereafter desire to enlarge the capa- stocks or bonds of city of the said railroad by building a double track, lessor for extending the length thereof, or otherwise, that then, additional and in every such case, the party of the first part, its facilities. successors or assigns, shall and will make, execute and deliver to the party of the second part, its successors or assigns, additional stock or bonds, in such form, for such amount, and payable at such times, as the party of the second part may require, which stock and bonds shall be delivered to and negotiated by the party of the second part, and the proceeds and money realized therefrom shall be received by it, and be used and applied in building such double track, enlargements and improvements, and in equipping and stocking the same; and the party of the second part shall and will thereafter pay the holders of such bonds or stock an interest or dividend thereon at the rate of seven per cent. per annum, in equal semi-annual payments, as hereinbefore provided in respect of the present bonds and stock, and shall and will also endorse or stamp thereon a guarantee of the payment of such in-Aggregate of terest and dividend in the way and manner above pro-stock and vided; but the total amount of all such additional bonds not to bonds shall never be such that the aggregate liabilities exceed of the party of the first part in steel all liabilities \$15,000,000 of the party of the first part, in stock and bonds, shall exceed fifteen millions of dollars. But other than this the party of the first part, its successors or assigns, shall not nor will make or issue any bonds or stock, for any purpose whatever, beyond what have been already issued and are now in existence, except as hereinbefore provided, without the consent in writing of the party of the second part, its successors or assigns.

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189 Lessee may make improvements.

14. It is hereby further mutually covenanted and agreed, that the said party of the second part may, from time to time, at its own expense, make all such alterations, improvements and additions in, upon or to the property hereby demised, as may be proper for its full enjoyment for railroad purposes. And in case said party of the second part shall at any time desire, for its more advantageous use of said demised property, to acquire, obtain or enjoy any additional lands, rights of way or other property, said party of the first part, when thereto reasonably required and indemnified against all expense and damage therefrom, shall aid therein, with its name, power and authority and in default thereof, such name, power and authority may be used by said party of the second part, after fully indemnifying the said party of the first part as aforesaid, and all lands, rights of way or other property which shall be so acquired or obtained, shall immediately be and become part of the property covered by this demise.

15. And it is hereby further mutually covenanted

Lessee to have exclusive control of leased property.

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and agreed, that the party of the second part, its successors and assigns, shall at all times, during the continuance of this lease, have the exclusive right to manage and control the railroad and premises, and to regulate and determine the rates of passage money, tolls, freights and charges for all the transportation over the whole or any part of said railroad and premises, and shall also have full, free and exclusive right to charge and collect all the passage money, rents, tolls, freights and charges, and to appropriate the same to its use; and shall have, use, exercise and enjoy all the rights, powers and authority aforesaid, and all other corporate powers and privileges which can or may be lawfully exercised and enjoyed on or about said demised railroad and premises, as fully, amply and entirely as the same might or could have been used, exercised and enjoyed by the party of the first part if this instrument had not been made, and as exclusively, fully, amply and entirely as the party of the first have or shall acquire authority by law to grant the same, subject, nevertheless, at all

Default by lessee. by law.

16. And it is hereby further mutually covenanted and agreed, that in case of default in payment of interest and dividends on the bonds, stock and the sinking fund aforesaid, and if the same or any part thereof shall remain unpaid for the space of sixty

times to the restrictions and regulations imposed

days from and after the time when the same shall become due and payable, then the said party of the first part shall have the right to enter upon and take possession of all the property hereby leased, and all depots, shops, buildings, tracks and other permanent property or rolling stock added thereto, and that the party of the second part will not unlawfully hinder or prevent such entry, nor the taking possession and using of all the said property by the party of the first part for its own benefit and use, and that this lease shall terminate upon the party of the first part so taking possession of the demised premises, Provided, however, that in case the party of the second part shall be unavoidably restrained by injunction or by law (not obtained by or upon their own action) from paying the aforesaid rent and performing the covenants herein contained, or any of them, then and in that case, the time during which the party of the second part shall be so restrained shall not be taken or considered as any part of the aforesaid sixty days. And further, that this lease shall not be assigned by the party of the second part without the consent in writing of the party of the first part first had and

17. It is further mutually covenanted and agreed, that in case of foreclosure and sale of either of the in case of aforesaid class of bonds now existing, or which may foreclosure. hereafter be created, then the seven millions of dollars of stock and bonds named in this instrument shall be first fully paid and discharged, from and out of the proceeds of said sale, and the balance of such proceeds shall be applied to the payment of the

remaining bonds, in the order of their priority.

18. And it is hereby further mutually covenanted and agreed, that the guarantee to be endorsed on the afore- Form of said bonds and stock, shall be in the following form, merely changing the same so far as may be necessary to make the same applicable to the said stock, that is to say: The President, Managers, and Company of The Delaware and Hudson Canal Company hereby guarantee to the holder hereof the payment of the interest on this bond, according to the terms thereof, subject to the conditions and provisions of a certain lease, made between the said Canal Company and The Albany and Susquehanna Railroad Company, bearing date the twenty-fourth day of February, one thousand eight hundred and seventy, and duly recorded in Albany and other counties, such guarantee to be duly executed

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197 under the corporate seal of the party of the second

part.

19. And it is further mutually covenanted and agreed, that all the provisions of this instrument, shall extend to, and bind the respective successors and assigns of the parties hereto, and wherever mention is hereinbefore made of either party hereto, the successors and assigns of such party shall be deemed to be comprehended.

198 U. S. Int. In Witness Whereof each of the parties hereto has affixed his corporate seal and caused these presents to be duly countersigned by its proper officer, the day and year herein first above written.

[L. s.] THE PRESIDENT, MANAGERS, AND COMPANY OF THE DELAWARE & HUDSON CANAL COMPANY. THOMAS DICKSON, President.

Attest,

CHARLES P. HART, Treasurer.

THE ALBANY & SUSQUEHANNA RAILROAD COM-

199 [L. S.]

J. H. RAMSEY, President.

Attest,

W. L. M. PHELPS, Secretary.

PROVISIONS OF SUPPLEMENTAL CONTRACT.

And whereas questions have arisen as to the proper construction of some of the provisions of said Indenture, and as to the rights and liabilities of the said parties thereunder, and as to whether the certificates issued by the said Railroad Company to the said Canal Company, dated the 26th day of July, A. D. 1872, for fifteen thousand nine hundred and fifty shares of the capital stock of said Railroad Company, were properly authorized and issued, or now are valid certificates.

And whereas both of the parties to said Indenture desire to now have the said questions and the rights and liabilities of the said parties respectively under the said Indenture definitely settled.

Now, therefore, in consideration of the premises and of one dollar by each of the said parties to the other, in hand paid before the execution hereof, the receipt whereof is hereby by the said parties respectively acknowledged, and of the mutual agreements herein contained, they

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DO HEREBY AGREE.

FIRST.—That the total amount of the capital stock Capital stock of the said Railroad Company shall be, and the same of lessor is hereby fixed at and limited to the sum of three fixed at millions five hundred thousand dollars, to be repre-\$8,500,000. sented by thirty-five thousand shares of stock, of one hundred dollars each, upon which the said Canal Company shall, upon the first days of January and July in each year, pay a semi-annual dividend of three and one-half per cent. And the provision in the aforesaid recited Indenture contained, giving to twenty-five thousand shares of stock a preference in case of fore-Certain closure, shall be and the same is hereby abrogated and preference to made of none effect; and in consideration thereof, \$2,500,000 of after the one million dollars of the Albany City Bonds, tioned in issued to the said Railroad Company, shall, by the lease operation of the sinking fund provided for the pay-abolished. ment of said bonds, be paid, the amount to wit: The sum of seventy thousand dollars a year, which the said Canal Company will thus be relieved from paying into Addition to the sinking fund and upon said Bonds, shall thereafter dividends be added to the dividends to be paid to the stock-when Canal holders, thus making the dividends to be paid by soid by holders, thus making the dividends to be paid by said relieved of Canal Company after the said Albany City Bonds are paying into paid as aforesaid, upon the said thirty-five thousand sinking fund shares of stock hereinbefore provided for nine instead interest upon of seven per cent. per annum, to be paid semi-annually, Albany loan. on the days hereinbefore mentioned, for the payment of the semi-annual dividends of three and onehalf per cent. And upon the execution and delivery of this Agreement, the said Canal Company shall, upon receiving the bonds hereinafter agreed to be issued to the amount, at par, of one million five hundred and ninety-five thousand dollars, cancel and surrender up to the said Railroad Company the aforesaid certificates issued for fifteen thousand nine hundred and fifty shares of stock.

Second.—That the said Railroad Company, in order Issue of new to provide for the payment of the Mortgage Bonds by mortgage for it heretofore issued, and to pay the said Canal Com-\$10,000,000, pany for the cancelling and surrender of the aforesaid include fifteen thousand nine hundred and fifty shares of stock, former and also such further sums as the said Canal Company, mortgage of \$3,500,000.

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Provisions as to issue of bonds.

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now or hereafter, may be entitled to for expenditures made upon said demised Railroad and property, shall, in such due and legal form as the counsel of the respective parties hereto may advise, make and execute to the Farmers' Loan and Trust Company of the City of New York, (hereinafter called the Trust Company,) a mortgage to be designated as a "First Consolidated Mortgage," bearing date the first day of April, in the year of our Lord, one thousand eight hundred and seventy-six, upon all and singular the railroads, rolling stock, property, rights and franchises covered by and included in the aforesaid recited Indenture, or intended so to be, together with the betterments, improvements and additions thereto made since the making of said Indenture, and together with all the betterments, improvements and additions thereto hereafter made, and including all property hereafter acquired in anywise appertaining to the said Railroads or property, excepting money, shares, securities and things in action, to secure the payment of ten thousand bonds of one thousand dollars each, to be, as hereinafter provided, made, executed and delivered by the said Railroad Company, together with the interest thereon; which mortgage thus made and executed the said Railroad Company shall acknowledge and deliver to the said Trust Company.

And the said Railroad Company shall also for the hereinbefore recited purposes, in like due and legal form, from time to time, as hereinafter mentioned, make, execute and deliver to the said Trust Company ten thousand of its Bonds, payable to the said Trust Company or Bearer for one thousand dollars each, all to bear even date with said mortgage and to be made payable on the first day of April, A. D. 1906. Of which ten thousand Bonds, three thousand shall upon the execution of this agreement, be made, executed and delivered by the said Railroad Company to the said Trust Company, and shall bear interest at the rate of seven per centum per annum, to be paid semiannually; and the principal and interest thereof shall be made payable in lawful money of the United States of America, at the office of the said Canal Company in the City of New York or at such other place in the said City as the said Canal Company may designate.

And the residue of the said ten thousand Bonds shall be, by the said Railroad Company, from time to time, as requested by the said Canal Company, made,

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executed and delivered to the said Trust Company so far and to such extent as the same may be required for the purposes hereinbefore mentioned, which Bonds shall bear interest, at the option of said Canal Company, either at the rate of seven per centum per annum, payable semi-annually in lawful money of the United States of America, or at the rate of six per centum per annum, payable semi-annually in gold, or at the rate of six per centum per annum, payable semi-annually in sterling; and the principal of the Bonds that bear interest at the rate of seven per cent. in currency, or six per cent. in gold, shall be made payable at the office of the said Canal Company in the City of New York or at such other place in the said City as the said Canal Company may designate, in lawful money of the United States of America, and the interest thereon shall be made payable at the same place; and the principal of as well as the interest upon such of the said Bonds as bear interest, payable in sterling, shall be made payable in sterling, at such place in the City of London, in England, as the said Canal Company may designate, and the payment of the principal of and interest upon all of the said Bonds which are issued by the said Trust Company for the purposes aforesaid shall be guaranteed by the said Canal Company; and the said Trust Company shall upon the cancellation and surrender by said Canal Company of the said certificates to be issued for 15,950 shares of the stock of said Railroad Company, certify and issue to the said Canal Company 1,595 of the said three thousand Bonds which are now to be made and shall also then certify and issue to the said Canal Company as many more of the said three thousand bonds as may be required to reimburse the said Canal Company; the balance to it then due for the expenditures by it made upon the railroads and property by the hereinbefore recited Indenture demised.

And the said Trust Company shall also, from time Outstanding to time, certify and issue any of the said Bonds, tobonds to be them delivered, in exchange at par with an equal ad-exchanged justment of interest, for the existing outstanding Mort-for new gage Bonds of the said Railroad Company, which amounts in the aggregate to three million, four hundred and fifty thousand dollars (\$3,450,000).

And the said Trust Company shall, as fast and as soon as any of the said existing Mortgage Bonds are thus taken up, cancel them and certify the fact to said Canal Company. And the said Trust Company shall also,

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215 Surrender of stock and issue of new certificates hereunder.

Form of guarantee.

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from time to time, certify and issue to the said Canal Company, any of the said Bonds, to them delivered by said Railroad Company for such amount as may be due to the said Canal Company for moneys expended pursuant to the provisions of the hereinbefore recited Indenture; and such bonds so to be issued to said Canal Company, shall be taken by said Canal Company at par, and all coupons thereof then past due shall be cut off and cancelled before such issue; and it is further provided and agreed that in every case of doubt or disagreement as to the amount of bonds so to be delivered by said Trust Company to said Canal Company, either of the parties hereto or the said Trust Company may bring the question before any competent Court of adjudication, and until such adjudication shall be finally made neither of said parties hereto shall be liable to any forfeitures, nor shall these presents or anything herein contained be impaired or made void by any omission to make, or cause to be made, such delivery or such acceptance of bonds.

And it is further provided and agreed that there shall, at all times, be retained by the said Trust Company, an amount of said Bonds equal to the then outstanding amount of Mortgage Bonds heretofore issued.

Third.—That all the certificates for stock heretofore issued by said Railroad Company and now outstanding except the said certificates for 15,950 shares, shall be called in, and as fast as they are received new certificates shall be issued subject to the provisions of this agreement, but the said Railroad Company shall not be bound to guarantee or insure a compliance with such call.

FOURTH.—That the said Canal Company shall indorse or stamp upon each Bond so as aforesaid, to be issued by the said Trust Company, a guarantee duly executed under its common seal, in the form following.

The President, Managers and Company of The Delaware and Hudson Canal Company, Hereby, for Value Received, guarantee the payment of the principal of and the interest upon the within bond.

And the said Canal Company shall also endorse or stamp upon the new certificate of stock, hereinbefore provided for a guarantee, duly executed under its common seal, in the form following:

The President, Managers and Company of The Delaware and Hudson Canal Company, HEREBY, FOR VALUE RECEIVED, guarantees the payment of a semi-annual

dividend of three and a half per cent. upon the stock represented by the within certificate, to be paid on the first days of January and July in each year, until the Albany City Bonds, which were loaned to the Albany and Susquehanna Railroad Company, to the amount of one million of dollars, shall be paid, by the operation of the sinking fund provided for their payment, and thereafter the said Canal Company guarantees to pay Guaranty to upon said stock four and a half per cent. semi-annually be subject to on the same days, subject to the conditions and pro- provisions of visions of the lease made by said Railroad Company lease. to said Canal Company, dated February 24th, 1870, and of an agreement supplemental thereto, dated the 7th day of March, A. D. 1876.

FIFTH.—That the total amount of stocks and bonds Total to be issued by said Railroad Company shall never ex- amount of ceed in the aggregate thirteen millions, five hundred be issued by thousand dollars-but nothing in this agreement con-lessor limited tained shall be construed as a waiver of the said Canal Company to have a new mortgage made and new bonds \$18,500,000. issued, in case of the maturity of the bonds by this agreement provided for, as provided in the hereinbefore recited Indenture.

SIXTH .- That all the charges of said Trust Company Payment of and other expenses of administering the trust herein-Trust before provided for, shall be paid by said Canal Com-charges. pany.

SEVENTH .- That all and singular the covenants, Original agreements and provisions in the aforesaid recited In-lease to denture of February 24, 1870, contained, except so far stand. and to such extent as they are changed, altered or modified by this agreement, shall be and remain in full force the same to all intents and purposes as though this agreement had not been made.

Eighth.—That all and singular the covenants, agreements and undertakings hereinbefore contained, shall inure to the benefit of and be binding upon the successors and assigns of the respective parties hereto.

In Witness Whereof, this agreement is signed on behalf of The Albany and Susquehanna Railroad Company by its President, and its Common Seal is hereto affixed and attested by its Secretary, pursuant to a Resolution of its Board of Directors. And this agreement is also signed in behalf of The President, Managers and Company of The Delaware and Hudson Canal Company by its President, and its Common Seal is hereto affixed and attested by its Treasurer, pursuant to a Resolution of its Board of Managers,

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221 this, the seventh day of March, in the year of our Lord, one thousand eight hundred and seventy-six.

THE ALBANY AND SUSQUEHANNA RAIL-ROAD COMPANY,

The A. & S. R. R. Co. Seal.

By

J. H. RAMSEY, President.

Attest:

W. L. M. Phelps, Secretary.

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D. & H. Canal Company. Seal. THE PRESIDENT, MANAGERS AND COM-PANY OF THE DELAWARE AND HUD-SON CANAL COMPANY, By

> Thos. Dickson, President.

Attest:

J. C. HARTT, Treasurer.

COUNTY OF ALBANY, SS. :

On the 7th day of March, A. D. 1876, before me came Joseph H. Ramsey, to me known and known to me to be the President of the Albany and Susquehanna Railroad Company, and the same person described in and who executed the foregoing instrument, and acknowledged to me that he executed the same as such President, and being by me duly sworn, did depose and say that he is such President, and that the seal to said instrument affixed is the common seal of said Company, and that he set the same thereto by authority of said Company and by like authority he signed the same; also before me came William L. M.

Phelps, to me known and known to me to be the Secretary of said Company and the same person described in and who executed said instrument, and acknowledged to me that he attested and signed said instrument as such Secretary, and being by me duly sworn did depose and say that he resides in the City of Albany, and is such Secretary, and that he so attested and signed said instrument by authority of said Company.

JACOB G. RUNKEL,
Commissioner of Deeds,
Albany,
N. Y.

STATE, COUNTY AND CITY Ss. : OF NEW YORK.

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Be it remembered that on this 10th day of March, in the year of our Lord, one thousand eight hundred and seventy-six, before me personally came Thomas Dickson, who being by me first duly sworn, did depose and say, that he is the President of The President, Managers and Company of The Delaware and Hudson Canal Company, and that as such he signed the foregoing instrument of writing for and on behalf of said Company, and that the seal thereto affixed is the common seal of said Company, and that J. C. Hartt is the Treasurer 226 of said Company, and that the said seal was affixed to the foregoing instrument, and attested by the said Treasurer in deponent's presence, and that the said instrument was thus executed and is by this deponent acknowledged for and on behalf of said Company, as and for its act and deed for the purposes therein expressed, pursuant to a resolution of the Board of Managers of said Company.

THOS. DICKSON.

Sworn to and subscribed before me) the day and year above written, witness my hand and official seal,)

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SEAL.

JOHN A. PATTISON, Notary Public, Kings Co.

STATE OF NEW YORK, City and County of New York, ss. :

I, WILLIAM WALSH, Clerk of the City and County of New York, and also Clerk of the Supreme Court for the said City and County, being a Court of Record, do hereby certify that John A. Pattison has filed in the Clerk's office of the County of New York, a certified copy of his appointment as Notary Public for the 228 County of Kings, with his autograph signature, and was at the time of taking the annexed, duly authorized to take the same, and that I am well acquainted with the handwriting of said Notary Public, and verily believe that the signature to the annexed certificate is genuine.

IN TESTIMONY WHEREOF, I have hereunto set my hand and affixed the seal of the said Court and County the SEAL. 10th day of March, 1876.

Clerk.

WM. WALSH,

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Albany and Susquehanna Rail Road Company

The history of the A&S from 1851 up to the opening of the line on January 12, 1869 and the leasing of the line on February 24, 1870 by the D&H is interesting (the A&S was merged into the D&H on July 2, 1945). Here are some of the highlights in that story.

Important texts / sources of information about the A&S include:

- 1. Century of Progress, pp. 631-653: "Albany and Susquehanna Rail Road Company"
- 2. Shaughnessy's *Delaware & Hudson*, Chapter 4, pp. 71-88: "Checkmate Mr. Gould (The Albany & Susquehanna)
- 3. August 2015 issue of the *Bridge Line Historical Society Bulletin*, Volume 25, Number 8. It's a "White Flags Issue" in which there are two substantial articles about the A&S by Gary R. Schermerhorn:

"The D&H to Albany—Part One Yesterday on the Albany & Susquehanna," pp. 4-13

"CP Rail on the Albany & Susquehanna The D&H to Albany—Part Two," pp. 14-21.

4. Stray Poems and Early History of the Albany and Susquehanna Railroad by Harley Tuttle Dana, published by Anstadt &Sons, York, PA in 1903.

In the present volume, we will supplement the history of the A&S presented in those texts.

The Albany and Susquehanna Rail Road Company was incorporated on April 19, 1851, with Edward C. Delevan, an Albany hotel owner and businessman, elected as president.

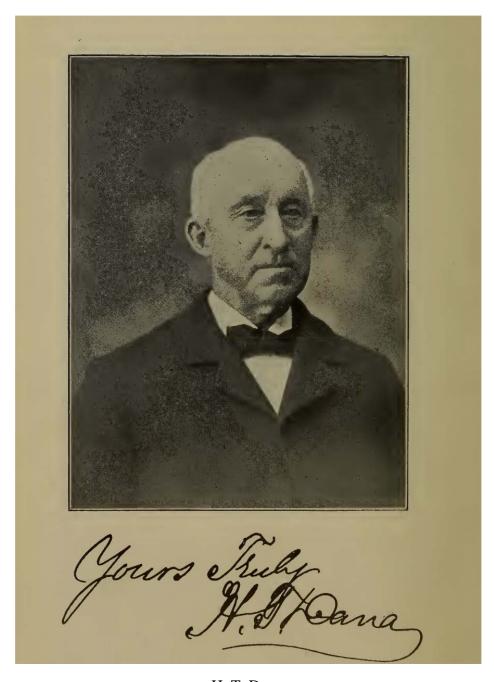
A preliminary survey of the proposed route of the 143-mile long line was completed by Martin B. Inches and published on January 28, 1852. James P. Kirkwood,* the builder of the Lanesboro Viaduct (1847-1848), was appointed chief engineer of the A&S in July 1852, and surveys were resumed and the road located.

*Kirkwood, a Scot, came to America in 1832. The Lanesboro Viaduct was designed by Julius Walker Adams, who was born in Boston on October 12, 1812. A very good article on the bridge, titled "The Starrucca Viaduct," was reprinted on pp. 43-44 of the December 2015 issue of the *Bridge Line Historical Society Bulletin*.

Funding to build the A&S was lined up, and in November 1852 proposals for bids were published. On May 30, 1853, a contract was signed for the complete building of the line at a cost of \$6.3 million. For the next ten years, the enterprise was plagued by funding and legal problems, which are described well in *Shaughnessy* (p. 71, pp. 73-74), which prevented construction of the line to move forward.

In 1903, Harley Tuttle Dana, who worked for the Albany and Susquehanna Railroad, published a book titled *Stray Poems and Early History of the Albany and Susquehanna Railroad*. The book was published by Anstadt & Sons, York, PA.

Here is the portrait of Dana that is given facing the title page of his book:



H. T. Dana

One of the many remarkable "stray poems" in that volume is one (pp. 111-114) that Dana wrote in 1856 titled *Lines in Favor of Building the Albany and Susquehanna Railroad*.

Here is that remarkable poem that Dana, an enthusiastic promoter of the Albany & Susquehanna, wrote in 1856, during the difficult and problematic years between incorporation of the A&S in 1851 and the formal opening of the line in 1869:

Lines in Favor of Building the Albany and Susquehanna Railroad.

by Harley Tuttle Dana

Men and brethren list I pray, This is an important day; Rising up in thinking minds, Truth with brilliant lustre shines. Yet through error's darkened screen Dreadful ills by some are seen. The cobwebs that the spiders strung, While Rip Van Winkle yet was young, Still cling around the hazy brows Of some who should our cause espouse. Men with vision closely shrouded, And with reason sadly clouded, Now would raise the mist of ages, And yet claim to be our sages. 'Tis this that bids me raise my voice In token of my settled choice. Behold the good which has been done, By railroads, and by them alone. See yonder west in verdure dressed, Speaks volumes in their praise; It was but rich to say the best, Before its railroad days.

"Behold the good which has been done, / By Railroads, and by them alone. . ."

Where wild the beasts roamed o'er the land, The cabin and the farm-house stand: Where once the cabin, now instead A mansion rears its stately head; Cities as if by magic raised Where once the deer in quiet grazed, Now stand to show the speedy change, Of all within a railroad's range. Where e'er these roads were ever built, Their power for good is seen and felt. Behold, beyond the seas afar, Beneath the sway of Russia's Czar, Or yonder where the English Isle, Is blessed by Queen Victoria's smile, They bring progression on its way, And usher in a brighter day. Then why should we not have one too? It would for us the same thing do; Here where nature has done much. But art scarce felt us with its touch; Our lands are rich-- our crops are fair--Our herds are fat-- yet here we are. In blissful nothingness we dwell. But build the railroad, and we'll claim A better fate-- a brighter name.

"Wher e'er these roads were ever built, / Their power for good is seen and felt. . . "

"Within our old and rusty shell, / In blissful nothingness we dwell, / But build the railroad, and we'll claim / A better fate—a brighter name.

Close at our backs we'll have the west, ◆ With all its verdure richly dressed; New York and Boston at our feet, And Albany we'll hourly greet. Thus joined unto the business world, Progression's flag will be unfurled; And men will prize the railroad's sway That now upbraid its cause today. How shall we do it? some may ask, And 'tis indeed a heavy task. Go, sirs, and sign the railroad bill, • It will not cause you any ill; But then some say it is not just That they be taxed to raise the "dust," While yet they are in truth confessing The road would be to them a blessing. Why then not pay your honest part, And do it with a cheerful heart? But if there is a single soul So lost to reason's right control, As not to prize a railroad's sway, To him I would most humbly say, Go seek some dark sequestered glade, Beneath some lonely mountain's shade, And with some moss beneath your head, Make beech leaves answer for your bed. Rest on, ye sloths in quiet sleep, While tree toads 'round you vigil keep!

"Close at our backs we'll have the west, / With all its verdure richly dressed; / New York and Boston at our feet, / And Albany we'll hourly greet. ."

"Go, sirs, and sign the railroad bill, / It will not cause you any ill. . ."

* * * * * * *

In 1862, Joseph Kirchner ("Grew Up With the A. & S.," *The Delaware and Hudson Company Bulletin,* January 1, 1927, pp. 3-4), who, in 1917, at the age of 80, was living at 109 Alexander Street in Albany, began working for the A&S as a water boy under R. B. Patterson. At that time the A&S was under construction and the end of the road was at Madison Avenue, Albany, where a station was located.

Kirchner was soon promoted to work in the engine house, "a small wooden affair, that stood at the foot of Green Street, across from the present roundhouse, on the site of the old freight house close by the docks. Two locomotives, the *E. C. Delevan* and the *E. P. Prentice*, were all that the Company owned at that time; others arrived later on on river barges and each arrival was an event of considerable local importance."

Not surprisingly, Kirchner remembered the Erie War at the tunnel (see below pp. 61-63): "Of the Erie War at the tunnel, north of Binghamton, he has a most vivid recollection. He was working as a car inspector at the Madison Avenue station, in Albany, when Jim Fisk and his Erie followers, threatened to seize the road. Excitement was rife as preparations were made to repulse the invasion. A special train was made ready and while an 'army' was being recruited from the Connaught block, between Madison Avenue and South Ferry street and extending from Broadway to the Hudson river, he was detailed to gather up all available sledge handles and similar articles that might be used as combat weapons. The crowd that eventually left the city was a daring, devil-may-care lot of fellows, who, wherever the train stopped, made it a part of their business to raid local saloons until, at last, precautions to thwart their marauding and to keep the party intact became necessary. Stations at which stops were planned were notified in advance and saloons closed before the arrival of the train."

Kirchner, who was born in Buffalo, NY, November 25, 1847, was pensioned by the D&H on September 1, 1913. At the time of his retirement, he was a car inspector at the Union station in Albany.

On September 16, 1863, the line was opened from Albany to Central Bridge.

In 1864, four years before the completion of the line, Joseph H. Ramsey was elected president of the A&S. In 1868, Jay Gould and James Fisk of the Erie Railroad began an initiative to gain controlling interest on the A&S Board of Directors.

Two years after the road was extended to Central Bridge, on January 2, 1865, the A&S was completed and opened as far as Cobleskill; as far as Oneonta on August 29.

On October 22, 1866, the line reached Sidney.

George L. Ferguson, who was pensioned by the D&H on November 1, 1924, after forty-eight years and seven months of service as baggageman in the D&H Sidney station, remembered well the opening of the A&S to Sidney, even though he was just past eleven years old at the time.

In the biographical portrait of the man ("Found His Work Always New") in the September 15, 1925 issue of *The Delaware and Hudson Company Bulletin* (pp. 3-4, 13) we read:

"Although he was just past eleven years old when the Albany and Susquehanna railroad was opened to Sidney, October 22, 1866, his recollection of the heights to which public sentiment rose is most vivid; how there was a public demonstration joined in by the folks of the village and their neighbors in the surrounding countryside and a dinner that was served in the station in honor of the occasion."

He also remembers the first locomotive he ever saw, as well as the original configuration of the railroad buildings in Sidney. In Ferguson's biographical portrait we read:

"The first locomotive he ever saw was the *E. C. Delevan* (No. 1), then being used on a construction train and quite naturally he was much awed by the performances of the little wood burner. It was then working in the vicinity of the bridge that spans the Susquehanna river a mile north of the Sidney station. / . . . The first station building, combining also the freight house, stood some distance south of the present building and close to the point where the tracks of the New York, Ontario and Western cross those of our road. Later it was moved nearer to the Main street crossing and since the erection of the present building, has served as a shop for the Car department at the opposite side of the tracks."(pp. 3-4)

In the spring of 1876, George Ferguson, at the age of 21, began working in the Sidney station:

"The station force then comprised, beside the agent, a day operator who also did the billing, and a night operator. His [Ferguson's] day, beginning at 7 a. m., and not being concluded until after the last evening train had arrived and departed, usually covered from fourteen to fifteen hours and paid him \$1.25. During the time, his duties were many and varied, but for the greater part were devoted to the loading and unloading of freight, and to the handling of baggage to and from passenger trains. . . In June, 1880, he was appointed yardmaster and had desk room in the ticket office. At that time there had never been a switch engine stationed at Sidney and it became necessary to get 'everyone (meaning the crews) who came along,' he says, 'to do some part of it.' Later, an engine with one man, a brakeman, was sent from Nineveh every other day. It became the duty of the brakeman to protect the switching operations with a flag and the work of placing

the cars and doing the necessary coupling and uncoupling fell upon Mr. Ferguson with the result that he acquired considerable experience with the link and pin of that time. Incidentally, he had many narrow escapes and at last decided to relinquish the position [of yardmaster] to return to his former work as the station." (p. 4)

Baggage handling was what Mr. Ferguson liked best:

"He recalls a record month when some 3,600 pieces of baggage, nearly every one of which was in excess of the weight governing its free transportation and handling, were handled. . . [H]e declares that he liked to do nothing better than to get in a car and check up its contents. . . . During his time, twenty-two years of which were passed in charge of the freight house, he worked under five agents, William Wood (deceased) was the first; B . S. Fox, later sent to Rome as agent for the Company when it operated a line out of that city that has since been leased to the Ontario and Western; H. W. Clark (deceased); George Firman, now living in Fulton, and J. W. McCullough, the present agent. The first man to hold that position was F. A. Harrington, whose father was the baggageman for several years."

Remarkably, in 1866, before the Jefferson Branch of the Erie from Carbondale to Lanesboro was built, and three years before the A&S from Albany to Binghamton was completed, the D&H entered into a contract with the A&S to transport to Albany, as soon as the A&S was opened, 500,000 tons of coal per year.

Remarkably, in addition, the D&H, as early as 1866, had plans to erect at Albany an immense coal depot and to make Albany a second Rondout in the way of anthracite coal movements. Those facts we know from an article that was published in the *Albany Evening Post* and reprinted in the March 24, 1866 issue of the *Carbondale Advance*. Here is that remarkable article:

"We find many items floating upon the surface of our exchanges, in regard to the construction of a Railroad from our Coal Field, to the North [the Jefferson Branch]. They all indicate that it is the purpose of the Del. & Hud. C. Co. to build the road at once. Instance the following: / A few days since certain gentlemen connected with the Delaware & Hudson Canal Company, contracted with the Directors of the Albany and Susquehanna Railroad to bring to Albany as soon as the road is opened to Binghamton 500,000 tons of coal per year [emphasis added]. This is an immense contract, and will give the road an amount of freight, that will soon force them to put down a double track the whole length of the road. / To carry out the designs of the Coal Company, an immense coal depot is to be erected at this city, one of the largest in the State, for it is the intention of those interested, to make Albany a second Rondout in the way of anthracite coal movements. [emphasis added]. Albany Eve Post." (Carbondale Advance, Saturday, March 24, 1866, p. 2)

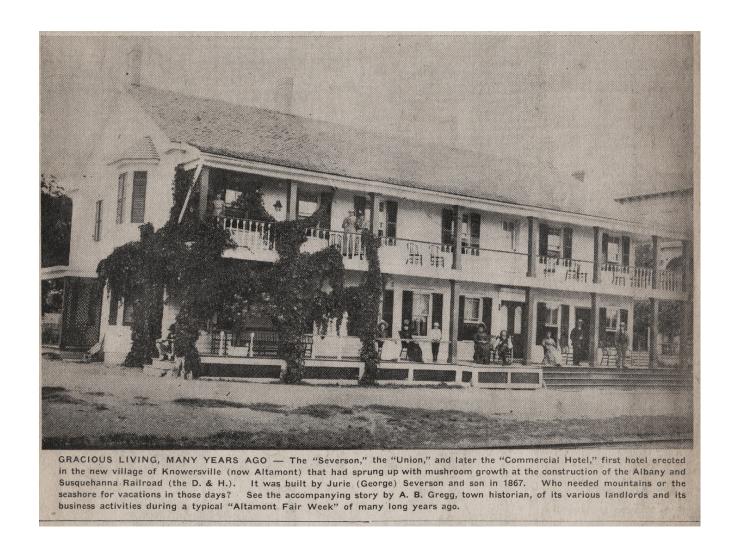
The A&S was completed and opened as far as Harpursville on December 25, 1867.

The construction of the A&S had a major impact on all of the communities all along the line as the construction of the line from East to West proceeded. In 1867, at Knowersville (later to become known as Altamont) the Severson Hotel was built. That we know from an article in a copy of the July 5, 1963 edition of the *Altamont Enterprise and Albany County Post* in the Alan G. Dustin Collection in the holdings of the Carbondale D&H Transportation Museum. That copy of that newspaper was in the collection of Arthur B. Gregg, the town historian of Guilderland, and it was given by Gregg to Alan G. Dustin (or to someone who gave it to Dustin).





In Knowersville, in 1867, Jurie (George) Severson and son built the Severson Hotel. In the caption on the photo given below of the photo of the "Severson" in Knowersville, we read: "The 'Severson,' the 'Union,' and later the 'Commercial Hotel', first hotel erected in the new village of Knowersville (now Altamont) that had sprung up with mushroom growth at the construction of the Albany and Susquehanna Railroad (the D. & H.)..."



The history of this hotel, which was built with the coming of the A&S to Knowersville, is given below. It was published in the *Altamont Enterprise and Albany County Post*, July 5, 1963:

THE STORY OF ALTAMONT'S FIRST HOTEL: BUILT IN 1867 WITH THE COMING OF THE RAILROAD, IT PRCSPERED FOR 50 YEARS

By A. B. GREGG, Town Historian

cient homestead at the foot of the ity, he felt again the urge to belong, steep climb up the Helder- come "Mine Host." But the new berg, had watched the construction world opening before his eyes deof the Albany and Susquehanna served something better than the Railroad with great satisfaction. old Wayside Inn. Thus it was, lands had paid him handsomely, railroad had only reached Oneon Now that it was actually running ta, old June and his son, since for as tar as Central Bridge, he had June, outly the impressive structure. other plans. Back in 1849 he had ture pictured on this page. It was seen the Schoharie and Albany Plank Road bypass the Wayside Tavern which has father had built at the close of the Revolution, relegating it to a mere farm house. Here he had been born, and after the slaves and everything else had been auctioned off that cold winter's day of 1813, to settle his fa-ther's estate, he had profitably managed the famous stage stop and first post office at West Guilderland, until the plank road had veered to the west a half-mile below him.

Now after 14 barren years, he would have his revenge. Already the plank road company was feeling the killing competition from the railroad and was trying des-perately to abandon its right of way and terminate its obligations with honor. A brand new village was springing up almost over night near the tracks, with carpenters and masons at a premium. New stage coach lines were meeting trains from the east and west to carry mail, passengers and freight to the Helderberg lakes and moun-

With memories of the old days, when his spacious barns and yards Houck, Mrs. Houck and Clarence were crowded at night with herds Houck, and Mr. Knickerbocker." of transient cattle, swine, and flocks of turkeys, and his dining

Old Jurie Severson, from his an- room and bar bustling with activ-The right of way across his farm that in the year 1867, when the located just south of the Main St. crossing, and directly opposite the box car that would serve as a depot for the railroad until more elaborate quarters could be construct-

> Although the patronage of the new hotel was phenomenal, old Jurie, after a year of personal operation, decided he would be better off to lease the property. Andrew Warner lasted a year. The next manager was John Stafford, who took over and operated the "Union Hotel," as it was then called, for 28 years. If one can judge from the fashions worn by the guests on the spacious upper and lower verandas, it was sometime during the early years of his management that this photograph of the hotel was taken.

> Stafford's popularity can be seen from the following quotation from an Albany Journal of early 1880's: "At the Union House where Honest John Stafford puts the wayfarer at ease, and whose table has been made famous throughout Albany county by his better half, we find quartered this week, Ex-Sheriff James A.

During all these years the spa-(Continued on Page 2)

Historian Tells Story Of First Hotel Here

(Continued from Page 1)

cious ball room at the rear of the building did a lively business for dances, political caucuses, parties, and band concerts.

From 1897 to 1900, the owner and manager was a Mrs. Eggleston; then, for a year, a Mr. Moore of Troy. In 1901, as the "Commercial Hotel," the property was purchased by W. H. K. Cornelius, familiarly known as "Dutch." Many in the Capital District will remember this colorful character who was destined to be the final operator of the property as a hotelooperator of the property as a hoteloo

large five-cent beers, many a patron made out a hearty meal at the serve-self free lunch table. The profit on beer in those days must have been enormous.

But it was during "Fair We.." that this hotel was a beehive of activity. Days had been spent in preparation for this annual event. The pool tables were pushed back against the wall, an an extra bar installed. Eleven extra bartenders were employed. On the ran-road siding stood a freight car loaded with beer from the brewery. At five cents for a big glass of beer, five cents for a glass of white whiskey, ten cents for a "shot" of the best whiskey, the cash register showed over \$2,000 a day during "Fair Week." In addition, the dining room, crowded to capacity, served a full course dinner for 25 cents. Since there was no night fair in those days, business continued briskly until 12 o'clock midnight, the closing hour strictly observed. Then, "all out" was announced, the lamps were extinguished, and the Conmercial Hotel slept.
Not long after World War I, the

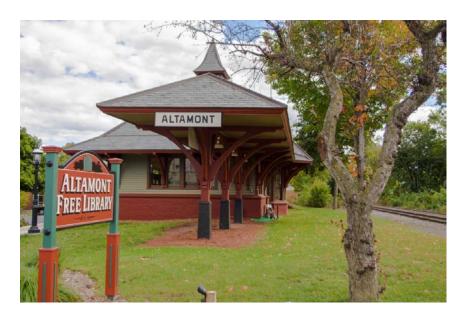
Not long after World War I, the hotel closed, never to be re-opened. For a time the building was turned into apartments and small stores. Finally, after the erection of a large garage behind it (now the Altamont Super Market), the old "Union" or "Commercial" Hotel was demolished. It had supely had its day.

The construction of the A&S from Albany to Harpursville was completed on December 25, 1867.

This post card view of the "Railroad Depot, Altamont, N. Y." was reproduced in the *Altamont Enterprise and Albany County Post*, July 5, 1963 (the same paper in which the history of the Severson Hotel, given above, was published):



Here is the same depot, now the Altamont Free Library, as seen here in a photograph downloaded from the Altamont website:



In 1868, the A&S entered into an agreement with the Cherry Valley & Mohawk Railroad Co. Under this agreement, the A&S would built and operate a 23-mile branch from Cobleskill to Cherry Valley.

On May12, 1868, the D&H asked the consent of its stockholders to double its capital stock in order to expand markets for anthracite coal:

"DELAWARE & HUDSON CO.--The Delaware & Hudson Canal Company proposes to double its capital stock, making it \$20,000,000. A special meeting has been called for May 12th, to obtain the consent of the shareholders. If the authority be given, 50,000 shares, or one share for every two now held by the stockholders, will be issued immediately, and which 60 per cent. will be called for in installments during the present year; and the remaining 40 per cent, or \$2,000,000, will be credited as paid to represent that amount of undivided earnings expended in improvements, purchases of lands, and in providing a necessary increase of working capital. The amount to be called in (\$3,000,000) is required to pay for the recent large purchase of coal properties in the neighborhood of Wilkes-Barre. The purchases will lay a solid basis for an important and needed extension of the company's operations, and the board feel the strongest confidence that they will contribute largely to the future prosperity of the company. The remaining 50,000 shares, if the issue be authorized, will be disposed of hereafter, from time to time, as, in the judgment of the board, the interests of the company and the attainment of the objects of its charter may require.--*Philadelphia Press." (Carbondale Advance*, Saturday, May 16, 1868, p. 3)

In 1868, on December 31, the A&S reached Binghamton.

The last rail in the portion of the line between Harpursville and Binghamton was laid on December 31, 1868, and the entire line was formally opened on January 12, 1869 with a commemorative excursion; the line was opened for public travel between Albany and Binghamton on January 14, 1869.

Published at the top of the front page of the issue of the *Altamont Enterprise and Albany County Post* referenced above is a facsimile of "Albany & Susquehanna R. R. / Time Table No. 2. Takes effect April 26, 1869."

This extremely rare original copy of the A&S April 26, 1869 timetable was made available to Arthur Gregg by Charles Lainhart of Altamont. Gregg then had the timetable published in the Altamont paper.

Here is "ALBANY & SUSQUEHANNA R. R. / Time Table No. 2. Takes effect April 26, 1869," as reproduced in the *Altamont Enterprise and Albany County Post*, July 5, 1963, page 1:

			akes effe EASTV							WEST'		No. 7.	No. 9
		No. 2.	No. 4.			No. 10.	and the state of t	- 28		Mrn.Ex.			
STATIONS.	÷	F.A. Pas				1 & Pas.			A. M.	A. M.	P. M.	P. M.	P. M.
2.5		A. M.	A. M.	P. M.	PM	A. M.	Albany Visitas dila		7.00	8 (9)	1.30	6.00	9.0
nghamton		8.15	7,30	1.25	5.20			- 6 - 7	7, 22 7, 28	*5 16 *8 20	*1.44	6.33	#9 : #10 0
ri Crane bora Hollow.	-6	4.03	17, 17	71.11	5.50		New Scotland	11	1.44	18,30	2.00	6.44	+10.2
mael	15	4.27	8.16	2.10	6.10				37,57	48.38	72.08	96.56	110.4
arpersymile	22	4.57	8,35	2.28	7.10		Amount	17	8.13	8.46	2.17	7.10	10.5
ton inbridge	200	5,38	8.50	2.43	7.48			91	8.46	99 BS	*2.36	7.15	Hi v
dney	339	6.15	9.10	3.12	8 10 8 33		Onakor Strong		4.54	49 19	-2.13	1.50	H1 4
eadilla	43	6.38	9.37	3.00	8.54		E. Paparatice		0,20	9.35	12.53	8.07	*12.10
ells' Bridge	50	0.53	19.48	10.34	9.18		Beperative		9.35				
conta	60	1.50	10.03	2.47 4.10	30.40	Taus .			9.55	9.38	0.07	8 25	12.0
reonta					10.30							8.45	
time ne	100	8.04	10.33	4.17	110.45	18.58	THE RESERVE	39 45	10.45	19,45	14.10	8.46 9.66	*12.5
divers arriand	- 10	8.13	30,42	4.40	11.00	4.10	 To throughout a 	50	31.39	10.18	23, 43	9.26	11.49
Control of the Contro	13	9.00	11.10	4.50	111 28 111 43	11.35	List Worder	0.8	11.10	10.40	4.04	9.49	19.9
Lance of the Control	80	9.23	11.26	5.05	112.06	5.10	and the same of th		12.00	10.51	4.00	10.07	12 9 13 0
et Worrester chmondville		10.18	11.40	-5.18	112.23	5.40	All Marylands	300	12,45	11,20	4,40	110,37	13.3
bleskill	- 60	10.45	10.10	5.87	1.21	6.10	A South	- 36	1,38	11.10	1.58	1.23000	4.16
ower-Cavella	100	11,35	112.27	16.07	11.46	16.41		79 82	135	11.18	5 15	11.20	1.8
	1486	11.32	12 35	6,15	2.00	0.50	64 mounts		9.60	12.00			
heharie perance	444	12 (0)	12,50	6 25	+2 23	7 to	The street of th	.90	2,40	33,30	7) 20 5 16		5.11
inker Street	115	12.24	*1.17	6,50	48 19	4.30	Nells Birthe		3.23	112.40	74 HO 44 H2		5.3
danéshurgh		12.38	91,24	16.58	12,53	3.11			0.15	12.55	6.23		6.1
poversville didentant		1.8	1.43	7.15	3,26	5.12	The substitution of the		1.13	1.20	6.37		6.4
	131	1.55	18.00	HT 50	48.52	8.30	Hainbřidge						6.5
School and		2.00					Mark Street	114	4.40	1.45	2.10		1.9
Confluence.		2.16	92,00	41.49	24.10	8.10	S Property of		9.12	2.10			8.1
scowlle		2.22	92.19 2.25	*7.45 8.00	*1.13	15.11	John Hollow		6.10	2,25	100		8.0
bany		0.00	2.00	0.181	3.40	9.00	of Courses		16,30	3.00	87, 52 8, 10	200	9.5
ales y Figu	res (lenote i	nothings.	* Tm	ns do r	iot stop.	singlamion	142	7 (8)	9.107	0.40		
description and									long to 1	distinus	* Tan	ns do r	at sta
		J. W. V	7AN VA	LKENI	SURGH	Supt.		al or	ds = T t	ilu_No	9 will n	of rus	Saturd
								ve Su	udas mi	ONE .			
-									J. W.	VAN V	ALKE	ARAKO	H, Sup
RAILROAD TI	MET	ABLE,	A CENTL	RY AGO	- Rep	roduced abo	ve is the second tin	netable	eissued	by the A	lbany an	d Susque	hanna
Railroad (D. &	& H.)	after th	e road h	ad finally	been co	ompleted be	tween Albany and I	Bingha	mton in	January	1869.	These rer	roduc-
	n f.	m both	sides of	114410 1	allana and	d washington	the only one in ex	3.10	+1.	41			3000

Detail of the timetable shown above, *Trains Eastward:*

	TR	AINS	EASTV	VARD.		
STATIONS.	É	No. 2.	No. 4.	No. 6.	No. 8.	No. 10.
	-	F.& Pas		Affu Ex.		
Ringhamton		A, M 3, 15	$\Delta_{\pi}M_{\pi}$	P. M.	P. M.	A. M.
tori t rune	6	3.44	7,30 47,47	1 23	5.50	
Sborn Hollow.	10	4 03	18.00	*1.53	6.10	
Finnel	13	4.27	8.16	2.10	6.38	******
Marpersville	22	4.57	8.85	2.28	7 10	l III.
Setton	228	5 (2)	8.59	2.43	7.38	
Painbridge	33	5.30	9 10	2.38	8 10	
laney hadila	89 43	6.13	9,21	3,12	8,33	
della Bridge.	17	6.545	9,37	13.4	9.18	
Olego	52	7 17	10.03	3.6	9.40	2
g)conta	600	7.70	10.27	4.10	10.20	3 48
diffeonta					10.30	
Sman ne	161	8 04	10.33	4.17	110.35	18.58
Coldinary	66	8.17	10.42	4 25	11.00	4.10
reastland	33	8.45	33.00	4.40	111.28	14,85
Lanevus	137	59, (8)	11.10	3.50	*111.43	4,50
A Vorcelet	80	9,23	11.26	5.05	112.06	5.10
Dichmondville .	9.2	10.18	11.58	5.18 5.87	+12.27 +12.57	5.30
obleskill	90	10.45	12 12 1	5.50	1.31	6.19
Gowell Cavessia		11.35	412.91	46.07	11.46	165 43
Schüharie	100	f1 32	12,25	6,15	2.00	6.53
Scheharie			12,50	6 25		7.40
Esperance	111	12 (0)	*1.05	6.40	12 23	9.27
	100	12.24	*1.17	6,50	46.45	7.40
t unaesburgh	108	12.8	11.34	16.55	12.53	3.30
lemas ers ville attrice tand	125	1.81	1 48 #1 52	7 - 105	3.26	8.13
	398	1.55	3.00	WT 389	***	8.30
as Sections.		2.00				
arriverbands	1008	2 111	49.00	#1 42	*4.10	8.0
Control of the Control	1000	9.99	09.19	+1.45	44.17	
Many	3 300		9.25			9 (8)

"Heavy Figures denote meetings.

^{*} Trains do not stop.

⁺ Stop on signal only.

J. W. VAN VALKENBURGH, Supt.

	ľR	AINS	WEST	WARD.		
STATISTICS.		No. 1.	No. 3.	No. 5.	No. 7.	No. 9.
				Afters.		Faire.
i 11		A. M.	A. M. 8 (#)	P. M.	P. M.	15 M
Chany National Inc.	6	7,22	*8.16	*1.40	6.33	#0.55
Singerhands	7	1.08	48.20	21 IN	6.30	+10.07
New Scotland	H.	7.44	78,30	2.00	6.44	*10.27
anddedand,	14	7.57	18,38	23.08	6.06	110 40
Knowersville	17	8.13	8.46	2.17	7.10	10.58
Khowersville					7:15	
Dianesburgh	24	8.46	79, 95	22.06	7.41	111.35
Disker Street Experience	87	9.58	90.35	12.10	7,50 8,07	111 47 112 10
	43.3	9.23				
100	286	9.75	9.38	3.97	8 25	12.06
Schologic					8.00	
water the	300	10015	19.45	10.16	8.46	*12.50
1.00	45	10.45	10.01	8,30	9.16	1,24
Ridinionijette,	50	31.39	10.18	33, 433	9 (26)	11.48
Earl Workering		11.40	10.40	4,04	9,49	12.95
346	102	11.0	11.10	4.00	10.25	13.16
Maryland	800	12.45	11.00	4.40	410.00	13.81
softers.	363	1.38	31.40	1.58	11,000	4.10
Emmons.	700	1.83	11.48	5.00	711.41	14.20
Chronta	882	1.35	11 0	5 5 15	11.20	4,35
(meonth		9.800	12.10	2.45		8 40
11 House 12	99	3.49	142.0	5.067		5.12
II. Bridge	100	3.22	13.55			0.54
* disev		8.45	1.485	6,331		6.15
Balabeldge	108	1.33	1.20	0.37		6,40
Buildhilde						6.50
Affordance	114	1.41	1.0			7.39
Carpersville		5 12 5 13	2.10	2,10		8.16
Trainel	127	6.10	2.25	10, 10		8.00
Maria Salah	136	6,30	2.10	87,02		18.85
Cinghamton	112	2.00	3.00	8.10		9.20

"Heavy Figures denote Meetings.

Train No. 9 will not run Saturday nights; will leave Sunday nights.

J. W. VAN VALKENBURGH, Supt.''

^{*} Trains do not stop.

⁺ Stop on Signal only.

From the "ALBANY & SUSQUEHANNA R. R. / Time Table No. 2 Takes effect April 26, 1869," Trains Westward, shown immediately above, we learn (the two columns on the left below) that these were the station stops for A&S trains beginning April 26, 1869.

Data in the two columns on the right on this page from *D&H Summer Time Table*, *June 27 to September 8, 1937 inclusive*

<u>Station</u>	Miles from Albany	<u>Station</u>	Miles from Albany
Albany		Albany	
Adamsville	6	Delmar	5.5
Slingerlands	7		
New Scotland	11	Voorhesville	10.8
Guilderland	14		
Knowersville	17	Altamont	17.2
Duanesburg	24		
Quaker Street	27	Delanson	26.8
Esperance	31	Schoharie Junction	35.3
Schoharie	36	Central Bridge	36.2
Howe's Cave	39	Howe's Cave	39.2
Cobleskill	45	Cobleskill	44.7
Richmondville	50	Richmondville	49.9
East Worcester	57	East Worcester	56.9
Worcester	62	Worcester	61.5
Schenevus	67	Schenevus	66.5
Maryland	70		
Colliers	76	Colliers	76.3
Emmons	79		
Oneonta	82	Oneonta	81.7
Otego	90	Otego	89.6
Wells' Bridge	95	Wells Bridge	94
Unadilla	99	Unadilla	98.8
Sidney	103	Sidney	103.3
Bainbridge	108	Bainbridge	108.4
Afton	114	Afton	114.3; Nineveh* 118.8
Harpersville	120	Harpursville	120.3
Tunnel	127	Tunnel	127
Osborn Hollow	132	Sanitaria Springs	132.5
Port Crane	136		
Binghamton	142	Binghamton	142.5

^{*}The L&S from Jefferson Junction to Nineveh had not yet been finished when the A&S opened in 1869.

"Albany & Susquehanna R. R. Time Table No. 2. Takes effect April 26, 1869."

--there were 31 station stops between Albany and Binghamton: Albany, Adamsville, Slingerlands, New Scotland, Guilderland, Knowersville, Duanesburg, Quaker Street, Esperance, Schoharie, Howe's Cave, Cobleskill, Richmondville, East Worcester, Worcester, Schenevus, Maryland, Colliers, Emmons, Oneonta, Otego, Wells' Bridge, Unadilla, Sidney, Bainbridge, Afton, Harpersville, Tunnel, Osborn Hollow, Port Crane, Binghamton

--there were five trains East and West, daily: **Trains Westward** (No. 1, freight and passenger; No. 3, morning express; No. 5, afternoon express; No. 7, freight and passenger—Albany to Oneonta; No. 9, freight and passenger; **Trains Eastward** (No. 2, freight and passenger; No. 4, morning express; No. 6, afternoon express; No.8, freight and passenger; No. 9, freight and passenger—Oneonta to Binghamton

--the express trains made the trip in seven hours (more or less) and the regular trains in twelve hours (more or less). The 7:30 A. M. morning express train from Binghamton, which arrived in Albany at 2:25 P. M., did not stop at Esperance, Quaker Street, Duanesburg, Guilderland, Slingerlands, or Adamsville; it stopped on signal only at Port Crane, Osborn Hollow, and Howe's Cave. The 1:30 P. M. afternoon express train from Albany, which arrived in Binghamton at 8:10 P. M., did not stop at Adamsville, Slingerlands, Guilderland, Duanesburg, Wells Bridge, Tunnel, Osborn Hollow, or Port Crane; it stopped on signal only at Quaker Street, Esperance, and Howe's Cave.

--given the fact that the A&S was single-tracked in 1869, trains traveling East always met trains going West, and trains traveling West always met trains going East. The 7:00 A. M. train from Albany for Binghamton, for example, met a train traveling East at Knowersville, Esperance, Cobleskill, East Worcester, Unadilla, and Osborn Hollow.

--J. W. Van Valkenburg was the Superintendent of the A&S in April 1869. "To J. W. Van Valkenburgh the superintendent during that heroic year, 1869, goes the credit of defending the railroad from the 'Erie Robber Barons' and saving it for the Delaware and Hudson in February of the following year." (from the caption on the timetables given above)

Time Table No. 1 was in effect for the period January 14, 1869—April 25, 1869; facsimile on page 80 of *Shaughnessy* of *Trains Westward*. The station stops in Time Table No. 1 were as follows (those in bold face were not in Time Table No. 2):

Albany, Adamsville, Slingerlands, New Scotland, Guilderland, Knowersville, **Knox**, Duanesburg, Quaker Street, Esperance, Schoharie, Howe's Cave, Cobleskill, Richmondville, **Carlyville**, East Worcester, Worcester, Schenevus, **Chaseville**, Maryland, Colliers, Emmons, Oneonta, Otego, Wells' Bridge, Unadilla, Sidney, Bainbridge, Afton, Harpersville, Tunnel, Osborn Hollow, Port Crane, Binghamton

One of the most well known stories in the history of railroads in the northeastern United States is that of the celebrated "Battle at the Tunnel" that took place at Belden Hill in August 1869, seven months after the A&S was formally opened on January 12, 1869.

Jay Gould and James Fisk, representing the Erie Railroad, attempted to take over the A&S. Joseph H. Ramsey, representing the A&S, opposed the Erie takeover.

At the root of the scandal were two well-known scoundrels, Jay Gould and Jim Fisk. Half of the A&S Board of Directors of the A&S were supporters of Ramsey, the other half of the Board were supporters of Jay Gould and James Fisk. A two-year long proxy fight ensued (Shaughnessy's account, pp. 76-83, of this proxy fight is excellent), followed by the celebrated confrontation at the tunnel in Tunnel, NY (near Harpursville) between the Ramsey supporters and the Gould/Fisk supporters on August 10, 1869. (*Shaughnessy*, on page 81, reprints the *Harper's Weekly* engraving showing the "Erie war" at the tunnel.)

The Joseph H. Ramsey supporters ultimately blocked the Gould/Fisk takeover effort of the A&S.

"The Battle at the Tunnel" has been reported in many railroad histories. It is a story that was told by Arthur B. Gregg, the town historian of Guilderland in 1963, and published on page one of the July 5, 1963 edition of the *Altamont Enterprise and Albany County Post*.

Given the possibility that Gregg reports in his article data or information that has not been reported in any other account of that famous battle, we present here a typescript of Greggs's page one article about the famous battle at Belden Hill:

"'THE BATTLE OF THE TUNNEL' - STORY OF RAILROAD 'WAR' FOR CONTROL OF THE A&S AFTER ITS COMPLETION IN 1869/ By A. B. Gregg, Town Historian / Many people, on reading that the Albany and Susquehanna Railroad (D & H) passed through Knowersville (Altamont) in 1863 assume that the line reached its terminus at Binghamton shortly afterward. This was not the case. It had taken from 1852 to 1863 to secure the funds from individual subscriptions, from the towns along the line, the cities of Albany and Binghamton, and the State of New York—to begin the undertaking. / From Central Bridge, reached in 1863, the railroad got no farther than Richmondville by the spring of 1865, and Oneonta by the summer of that year. Not until the month of January 1869, after the tunnel of 2,260 feet was bored through Cole's Hill near Harpursville, was the road competed to Binghamton. Thus, after a period of nearly 16 years of constant toil and strength and financial embarrassment, the heroic men who led in the enterprise saw the realization of their faith and hopes. / It was in April of that memorable year (1869), only a few months after the first through train had run from Albany to Binghamton, that the Time Table No. 2, pictured on this page, was issued. Through the kindness of Charles Lainhart of Altamont, we are able to reproduce both sides of the little yellow card on which the schedule was printed. Note the names of the stations, later changed: Adamsville

(Delmar), New Scotland (Voorheesville), Guilderland (Meadowdale), Knowersville (Altamont), Quaker Street (Delanson), and Schoharie (Central Bridge). Note also that all but one train of the five each way daily were combination freight and passenger. / Scarcely had the road been completed when Jay Gould and Jim Fisk of the Erie Railroad, that had long since swept west through Binghamton, cast their covetous eyes on the new railroad, and quietly started acquiring A. and S. stock, to obtain control of the approaching annual election. The 'Erie' party, through friendly Judge Barnard of New York city, secured an order to restrain the duly elected president of the A. and S., Joseph H. Ramsey of Cobleskill, from the discharge of his duties. Then Judge Rufus W. Peckham of Albany modified this order, as to allow Mr. Ramsey to appear and show cause why this should NOT be done. While these legal skirmishes were proceeding, the rival factions with their rival Supreme Court judges, succeeded in having the railroad placed in the hands of rival receivers. / Fisk sent out a notice Aug. 6th, 1869, for all reports and remittances of station agents to be sent to him. The next day, J W. Van Valkenburgh, whose name as superintendent appears on the above time table No. 2, sent out a countermanding order, saying that Robert H. Pruyn had been first appointed receiver by Judge Peckham, and had possession of the office, books and property of the company and had continued him (Van Valkenburgh) as superintendent, and all transactions must be with him. / Transferring their operation from Albany to Binghamton, the Fisk or 'Erie' party, backed by the order of rival Judge Barnard, determined to take the road by force. On Aug. 10, 1869, Van Valkenburgh sent out this message to all station agents: 'Have all the men you can find in the station so as to defend your station and the road.['] This was followed by another of the same date: "Train No. 7 (see timetable above) will go through to the front. Have all the men you can get. Drum them up. J. W. Van Valkenburgh, Supt.' / The next day, R. C. Blackall, master mechanic of the A. & S., commanding the Ramsey party at Harpursville, sent this message to Supt. Van Valkenburgh: 'The Erie folks are preparing to move upon us. They have the militia with them, encouraged by the sheriff. Our retreat is cut off. We must resort to desperation if they attack us tonight. What are we to do? We must have reinforcements. Their force is estimated at 1500. Our forces are about 300. Send reinforcements immediately. I will burn the high bridge if they move on us tonight. R. C. B.' / The forces met near the tunnel, 17 miles from Binghamton. Gould attempted to run an Erie locomotive to Albany with employees of the Erie to take possession of the depots along the road. Blackall had warning of its approach and, side-tracking his engine at Bainbridge, with the lights turned out, he placed a pair of patent 'frogs'—such as were used for replacing cars on the tracks—reversed upon the rails over which the Erie engine had to pass. / Everything appeared secure, on they came, until they reached the 'frogs,' where they left the rails, becoming easy prey to the Ramsey forces. The engine was later put back on the rails, and sent on to Albany, while the Erie employees were paroled by Blackall. / Meanwhile, to avoid bloodshed, Governor Hoffman of New York sent out a message, commanding the belligerent forces to lay down their arms while he himself took charge of the road. He appointed his adjutant general, James McQuade, superintendent of the A. & S. Shortly afterward, Robert L. Banks of Albany was appointed sole receiver for the Governor. / With the road in the hands of the state, both parties were active in the purchase of stock in preparation to the holding of the annual election Sept. 7, 1869. Fisk with his army of

supporters, chartered a boat from New York to Albany. The meeting room was well guarded by the Ramsey party. J. W. Van Valkenburgh, with flashing eyes and knit brow, strode through the hall to see that the election was honest and fair. Each party organized; each elected its own directors. / Not until after another long trial before a judge in Rochester were the Ramsey directors, finally in November, declared the legal ones, and the Gould and Fisk efforts to grab the road forever thwarted. 'The Battle of the Tunnel' forms an important episode in Edna Ferber's popular novel, 'Saratoga Trunk.' / Our little yellow timetable pictured with this article, was no doubt in force during the entire 'Erie War,' and as such becomes an important tie between the stormy days of '69 and the present."

What else was going on in Binghamton in 1869 at the time the A&S opened? Horse races. And the railroads were offering reduced fares and extra trains for the convenience of race enthusiasts. Reprinted in the *Carbondale Advance* of September 11, 1869 is the following article from the *Binghamton Daily Democrat* about the upcoming Binghamton Driving Park Races:

"From the Binghamton Daily Democrat. Binghamton Driving Park Races. —The entries for the great races have been made, and judging from them the meeting promises a decided success. American Girl, Lucy, Goldsmith Maid, and a long list of more or less distinguished trotters, some of them recently large winners at Buffalo, are booked to try their mettle on Binghamton soil next week. Certainly such horses cannot meet in contest without exciting great interest and drawing together thousands of people. We paid a visit to the grounds yesterday and were surprised at the extent of the preparations and alterations the Association are making. New stands have been erected, new fences made, the track widened, its bed greatly improved, new reporters and members stands, &c. Every preparation is being made to accommodate strangers in the city and already rooms are being engaged and parties are hunting out comfortable quarters for the fast horses. The railroads too are contributing to the success by reducing their rates of fare, most of them running extra trains and others holding regular trains each day until the conclusion of the races. / May the weather prove propitious." (Carbondale Advance, September 11, 1869, p. 2)

A month after the Battle of the Tunnel, Gould and Fisk were still causing trouble—this time on the New York Gold Exchange, and on September 24, 1869 ("Black Friday") they orchestrated the collapse of the U. S. gold market.





Jay Gould James Fisk

Gould was irrepressible and very successful. Following Black Friday, he went on to control the Union Pacific Railroad, the Western Union Telegraph Company, and the Manhattan Elevated Railroad. Fisk, on the other hand, was less fortunate. In 1872, after arguments over money and a Broadway showgirl named Josie Mansfield, a fellow financier named Edward Stokes shot Fisk dead.

On February 24, 1870, Ramsey and the A&S Board of Directors leased in perpetuity to the D&H "the property and franchises of the Albany & Susquehanna Railway Company at an annual rent of \$490,000 or 7 per cent upon its capital, and bonded debt of \$7,000,000." (Hollister, p. 179) The 143-mile long line between Binghamton and Albany would become known as the Susquehanna Division of the D&H.

At the time of the leasing of the A&S by the D&H, the A&S had twenty-five locomotives, a full complement of modern passenger coaches, freight cars, and coal cars.

D&H Coal from Carbondale to Binghamton and to Albany: 1848-1872

With a lease in perpetuity on the A&S by the D&H, the door was open, so to speak, to aggressive marketing of anthracite coal by the D&H in the state of New York, New England, and Canada. Before this lease agreement, marketing coal in upstate New York by the D&H was a moderately complicated process.

In the period from December 1848/January 1849, when the Erie main line from Piermont to Binghamton was opened, up to June 23, 1868, when the Erie rail line from Hawley to East Honesdale was opened, to market coal in Binghamton and north, D&H coal had to be sent to Lackawaxen via the D&H Canal and then transferred to Erie rail cars and sent by Erie rail to Binghamton.

In the period from June 23, 1868, when the Erie rail line from Hawley to Honesdale opened, to January 14, 1869, when the A&S was opened, D&H coal, in Erie Cars, could go from Honesdale to Lackawaxen and be transferred there to the Erie for the trip to Binghamton and onto the A&S.

In the period from January 14, 1869, when he A&S was opened, up to February 24, 1870, when the A&S was leased to the D&H, D&H coal could be sent by rail from Honesdale to Lackawaxen by rail and be transferred there to the Erie main line at Lackawaxen and sent to Binghamton, and then over the A&S to Albany.

February 24, 1870, when the A&S was leased to the D&H, up to October 1870, when the Jefferson Branch of Erie was opened, D&H coal could be sent by rail from Honesdale to Lackawaxen by rail and be transferred to the Erie main line at Lackawaxen and sent to Binghamton, and then over the D&H (former A&S) to Albany.

With the opening of the Jefferson Branch of the Erie in October 1870 up to the opening of the L&S (Jefferson Junction to Nineveh) on January 1, 1872, D&H coal went from Carbondale to Lanesboro Junction, where it was then moved over the Erie to Binghamton, and then onto the D&H (former A&S) for shipment North.

With the opening of the L&S on January 1, 1872, D&H coal could then go from Carbondale to Jefferson Junction to Nineveh to Albany on D&H tracks.

About the February 24, 1870 leasing of the A&S by the D&H, there were two articles in the March 5, 1870 issue (p. 2) of the *Carbondale Advance*:

1. "A GREAT ACHIEVEMENT. / The Delaware & Hudson Canal Co. Leases the Albany & Susquehanna Railroad. / Negotiations were concluded at Albany, N. Y., at the close of last week, between the two corporations mentioned above, by which the Albany & Susquehanna Railroad (with all its connections, present and prospective) was leased for the full period of its charter, ninety-nine years, we believe, to the Delaware & Hudson Canal Company. It went into effect immediately, and on Saturday last engines passed over the road bearing the new name of the combined corporation, 'A. & S. R. R. Dpt,' 'D. & H. C. Co.' This is one of the most important railroad consolidations, which has yet taken place, even in this age of concentration [emphasis added]. Its results will be of the most magnificent character to the D. & H. C. C., as it will give it vast outlets for the coal mines, which could be had in no other way. For the present it will ship coal via Honesdale to Lackawaxen [the third rail was laid on the Jefferson Branch from Honesdale to Lackawaxen in 1881], then via the Erie to Binghamton, and thence by the A. & S. to Albany. After July next, when the Jefferson R. R. will be completed, [emphasis added] its coal will go via Carbondale to Lanesboro, thence by the Erie and on as above. After this latter road shall have been completed, a branch will then be built from Lanesboro to Nineveh, on the A. & S., twenty miles, when transportation via the Erie will cease. This being done, it is said the distance to Albany by the new route will be four miles less than via Scranton. The A. & S. railroad has twenty-five locomotives, a full complement of modern passenger coaches, freight cars, coal cars &c., and is otherwise well appointed.—Morning Republican, Feb. 28. / The above we copy from the *Morning Republican*. We find elsewhere the terms of the lease thus stated: / The Delaware and Hudson Canal Company is to pay an interest of 7 per cent, upon \$7,000,000 made up as follows: / On paid stock......\$2,500,000/ On the Albany city bonds.......1,000,000 / On first mortgage bonds.......1,000,000 / On second mortgage bonds......2,000,000 / On equipment bonds.......500,000 / [total] \$7,000,000 / To meet this interest will require an annual outlay of \$490,000. / The Delaware & Hudson Company assume the interest on all the bonds from the 1st of March ensuing, but the interest on the \$2,500,000 of stock is not to be assumed until the 1st day of January next. / The 9,500 shares of subscribed stock, upon which per cent. has been paid is to be paid in full as the necessities of the road may require. The shares will only bear interest as, and to the extent that they are paid for. / The road will continue to be run under its present management, the lessee, of course, having the power to make changes when and as they lease, in the employes. The stock holders will continue to control the Director-ship of the road. / All existing contracts with other railroads and coal companies to be carried out, in good faith by the lessee. / In commenting upon the transfer, the Albany Argus says: / The lease has been made, we understand, with the approval of the Jay Gould party, and with it we suppose will terminate the long conflict between the direction of the road, and that of the Erie Railroad Company.' / 'The object aimed at on all sides... [one line of original newspaper article not legible in SRP copy] will be thus accomplished, with full justice to the stockholders and bondholders of the leased road." (Carbondale Advance, March 5, 1870, p. 2)

2. "Grand Achievement. / In our columns this week we found the announcement of the recent master stroke of policy on the part of the management of the Del. & Hud. C. Co., in the lease of the Albany & Susquehanna Railroad. This will not only be productive of great results to the company, but will be largely beneficial to our valley and its business interests, and to the Susquehanna Valley, through which the road passes. / An arrangement has also, it is stated, been consummated between the Del. & H. C. Co., and the Northern Central R. R. Co., for the shipment of coal over that road from Northumberland to Baltimore. Thus it will be seen that the Del. & Hud. C. Co., has opened up the way for a brilliant future, and a business of immense magnitude [emphasis added]. It will have in its prospective business both a Northern and a Southern aspect, and that great advantage which it has hitherto lacked, the choice of markets at all seasons of the year." (Carbondale Advance, March 5, 1870, p. 3)

'No; look out we don't acquire the Erie next.'

From a copy of a newspaper clipping, dated February 28, 1922, in the holdings of the Carbondale D&H Transportation Museum, titled *Living Representative of Old D. & H. Gravity Canal*, we learn that Z. Jessup Lord (nephew of Russel F. Lord, chief engineer of the D&H Canal) ZJL retired, age 76, on a pension, on December 1, 1921, after 59 years of continuous service to the D&H. He began his D&H career as a telegrapher and clerk on the D&H Canal in 1862, later being transferred to the Gravity Railroad in Carbondale where he worked initially as a clerk and then as a telegrapher. At the time of his retirement he was a telegrapher at Wilkes-Barre.

In that newspaper article, Lord speaks of a telegraphic exchange that took place between Thomas Dickson and Jay Gould at the time that the D&H out-maneuvered the Erie and leased the A&S for 99 years. This exchange between Dickson and Gould is surely one of those "don't-tell-anyone-you-heard-it-from-me-but" reports that was passed from one telegrapher to another over the years, even though at the time there were surely confidentiality requirements which all telegraphers pledged to uphold. Given his 59 years of service to the D&H, almost all of them as a telegrapher, Jessup could have easily learned of this remarkable telegraphic exchange between Dickson and Gould from a telegrapher/colleague.

That Dickson/Gould telegraphic exchange is reported in Lord's reflections on his career as a telegrapher for the D&H, as follows:

"Well remembered is a telegram received by Thomas Dixon [sic], then president of the Delaware and Hudson Canal Company, from Jay Gould, then president of the Erie railroad, which read, 'Now that you have acquired the Albany and Susquehanna railroad, I suppose you will want us to run it." Mr. Dixon's reply was, 'No; look out we don't acquire the Erie next.'

October 1870:

Jefferson Branch of the Erie Railroad, opened in October 1870. D&H: Carbondale to Lanesboro Junction (first coal train October 28, 1870). We examined in detail this rail line in Volume XI in this series. This line was crucial to all D&H lines north of Carbondale, as it provided the rail link north. As such its importance in the history of the D&H can not be overstated.

With a lease arrangement on the A&S in place, the D&H then took an important step to make that rail line user-friendly: a third rail was laid in the line (by December 14, 1871, the third rail had been laid and brought into use from Albany to Nineveh, a distance of one hundred and twenty miles) so that the D&H's standard-gauge cars could move over the line.

In COP (pp. 225-26), we read: "... having entered the railroad field on a large scale, it was at once necessary [for the D&H] to make costly improvements and alterations in order to bring the acquired properties into more effective use. Among the principal improvements was laying a third rail on the Albany and Susquehanna. On June 8, 1871, President Dickson submitted a letter on this subject to the Managers in which he declared that placing a third rail on the line, so as to permit the handling of equipment of the standard gauge, was of the highest importance as would place the railroad 'in direct and close communication with the whole railroad system of the country.' [emphasis added]. The desirability of producing this result had been greatly enhanced, he added, by the recent acquisition of the Rensselaer and Saratoga system. Although the Erie was at this time a broad gauge road, President Dickson assumed that it was 'the policy of the company to reach a uniform gauge of four feet eight and one-half inches as soon as practicable,' and \$985,000 was at once appropriated to do the work. By December 14, 1871, the third rail had been laid and brought into use from Albany to Nineveh, a distance of one hundred and twenty miles."

May 1, 1871

Rensselaer and Saratoga Railroad, incorporated on April 14, 1832, leased to D&H on May 1, 1871. D&H to Whitehall, via Albany. Merged into the D&H on July 2, 1945. For the history of this sprawling line (about 181 miles in length), see *Century of Progress*, pp. 654-702; also *Shaughnessy*, Chapter 5, An Empire in the North (The Rennselaer & Saratoga), pp.89-114. We will take a closer look at this rail line in Section 1205 below.

In 1871, the D&H appropriated \$200,000 for use on the line leased from the A&S.

During 1873, 8,473 tons of steel rail were laid on the A&S by the D&H. Also in 1873, the D&H introduced a plan to heat the cars on the A&S with steam from the locomotive. In the May 3, 1873 issue of the *Carbondale Leader*, we read:

"The D. & H. C. Co. is about to introduce upon the Albany & Susquehanna Railroad, the plan of heating the cars with steam from the locomotive." (*Carbondale Leader*, May 3, 1873, p. 3)

January 1, 1872 Lackawanna and Susquehanna Railroad, opened on January 1, 1872.

Built by the D&H; 23.3 miles from Jefferson Junction to Nineveh. We will have a look at this line below in Section 1206.

Effective January 1, 1873, said the D&H, the Rensselaer & Saratoga and Albany & Susquehanna Railroads and their branches would be operated as one department, with H. A. Fonda, Esq., as General Superintendent of Transportation, and C. W. Wentz, Esq. in charge of the track maintenance, and repairs, with the title of Chief Engineer. Here is the announcement that was published in the *Rondout Freeman* and reprinted in the *Carbondale Advance* of January 4, 1873:

"The D. & H. Company. / The Delaware and Hudson Canal Company have made the following changes, to go into effect on the 1st day of January, 1873: / . . . 3d, The Rensselaer & Saratoga and Albany & Susquehanna Railroads and their branches will be operated as one department, with H. A. Fonda, Esq., as General Superintendent of Transportation, and C. W. Wentz, Esq. will be placed in charge of the track maintenance, and repairs, with the title of Chief Engineer.--*Rondout Freeman.*" (Carbondale Advance, January 4, 1873, p. 3)

In June, 1876, David Bond, a D&H trackman working on the A&S, was struck by an engine on the road near Afton and killed:

"David Bond, a trackman in the employ of the D. & H. C. Co. on the A. & S. Railroad, was struck by an engine and fatally injured on the road near Afton on Monday morning. Bond was standing outside the track, bending over, and in the act of drawing a spike when the front of the engine struck him. He was thrown into a ditch, and died in the course of an hour afterwards." (Carbondale Leader, June 3, 1876, p. 3)

In September 1876, the A&S ticket office at Afton was broken into. Money was taken and two trunks in the baggage room were rifled of items useful to the burglars:

"The Albany & Susquehanna Railroad ticket office at Afton was broken into by burglars on Thursday night of last week and robbed of three dollars. They also broke into the baggage room and stole two trunks which they carried a short distance away and rifled of such contents as were of use to them." (*Carbondale Leader*, September 9, 1876, p. 3)

On March 21, 1877, Joseph H. Ramsey liquidated 50 shares of the capital stock of the Albany and Susquehanna Railroad (stock certificate No. 194) that he owned. Here is the receipt, signed by Ramsey, for those 50 shares. This receipt was donated to the Carbondale Historical Society and Museum by John V. Buberniak.

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Albany March 21st 1877
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Agelroad Company by the hounds of W. Lo. M.
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of the Capital Stock of said Company in ac-
Cardance with the agreement between myself
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Joseph H. Rambey
Received, of the ALBANY AND SUSQUEHANNA RAILROAD COMPANY, by thehands of
W. L. M. PHELPS, Treasurer,
To 0 Dollars in full for the above account.
Dated,
Witness

At the request of the Managers of the D&H, a committee was named on May 8, 1877, to examine the business, operations, history, policies, prospects, and equipment of the D&H. In their report, dated August 11, 1877, the committee said the following about the A&S:

"The road, its structures and equipment were in bad condition. The gauge, which was six feet, isolated the line from the roads of the Delaware and Hudson and all connecting corporations, except the Erie. It was necessary, therefore, to lay a third rail in order to secure proper connections. This, and the gradual replacement of equipment suited to the new gauge of four feet eight and one-half inches, and relaying the road with steel rails, involved heavy expenditures for a number of years."

With regard to the R&S, the committee reported that the D&H had spent \$308,491.40 in the period 1874-76 for improvement of track by laying steel rails.

The loss reported on those leased lines (the A&S, R&S, New York and Canada) in 1875 was \$623,662, the profit from coal sold upon those lines, the Committee reported, was \$1,080,061. The important fact that was recognized by the committee, a fact that was recognized by the D&H leadership in the years previous to the leasing of those lines, was that through those leased lines the D&H had access to significant new markets for the company's coal, and that those markets would become increasingly profitable in the years ahead. The Committee said:

"When the Delaware and Hudson Company leased the Albany and Susquehanna and the Rensselaer and Saratoga railroads, it was not merely the control of these roads and their revenues which were thereby secured; for it was certain that, for several years at least, their aggregate incomes would not suffice to meet rentals which, in the form of interest upon bonds and dividends upon stock, were assumed by the Delaware and Hudson Canal Company. It was certain, too, that large expenditures for improvements and equipment would be necessary, and that the moneys therefor must be furnished by the Delaware and Hudson Company, for which that corporation would receive only the stock and bonds of the companies leasing the several lines. / What was really secured was an extended market for the company's coal—a market not subject to competition—that is certain consequently to pay profitable prices—a market that already takes more than one-third of the company's product, and which is constantly increasing in importance [emphasis added]. Thus, in 1875, although the loss on leased lines, including the southern division of the New York and Canada, was \$623,662, yet the profit on coal sold upon those lines and their connections, and which could not have been realized but for their possession, was \$1,080,061."

An interesting fact about the management of the D&H was noted by the Committee: the D&H were very good record keepers. In their report, the Committee said:

"There can be no other corporation in which exact results are kept more constantly in view than are those of the Delaware and Hudson Canal Company, by its President and General Manager. The mining day closes at four o'clock P. M. By 4:30 P. M. on each day, it is known exactly how many car loads, tons and hundred-weight of coal have been produced on that day at each colliery; where each car load has been sent; how much coal has been received at Honesdale; the number of car loads received, and empty cars returned; the number of canal boats loaded-in transitu—arrived at Rondout, and of light boats returning; the quantity, size and location of all coal on hand; the vessels in waiting for cargoes; where and for what sizes of coal. An abstract of all this is sent every day by telegraph, and a detail copy by mail, to the President and General Manager. The mileage of freight, passenger, mixed and construction trains is sent daily by the railroad superintendents to the President and General Manager. The receipts from passengers, freight and miscellaneous sources are sent weekly. Each department head makes a monthly report to the general manager, from which he prepares his report to the president."

On the question of record keeping, Frank A. Yokey, who fired and drove D&H locomotives in upstate New York for 50 years and who retired on pension on March 1, 1937, kept highly detailed records of his D&H work experience. Here is the opening paragraph of his biographical portrait ("Rode Locomotives 1,556,407 Miles") as published in *The Delaware and Hudson Bulletin*, July 1, 1937, pp. 99-100:

"In his 50-year railroading career retired Engineer Frank A Yokey fired and drove Delaware and Hudson locomotives a total of 1,556,407 miles, an average of 31,128 miles per year, his greatest annual mileage having been 43,659 accumulated in 1927. He can make this positive statement because, beginning with his first trip with engineer Michael Carroll, on No. 82, December 6, 1887, he kept a record in four huge volumes of every run he made, listing the date, train, engine, miles, pay, engineer or fireman, conductor, terminals, and times of arrival and departure. At the end of each year he computed his total hours worked, earnings, and mileage, setting those data down in the books with all the care of the most methodical bookkeeper."

Facts are facts. Bravo, Frank A. Yokey.

One hopes that those four volumes of data recorded by Frank A. Yokey have been preserved.

In November 1880, the Binghamton *Daily Republican* reported that (1) the A&S was going to be extended to the southwest, to the coal fields of northern Pennsylvania, with a preliminary survey of the route west of Binghamton then in progress, and (2) that this new D&H line would form a through trunk line from Boston to Pittsburgh.

In a public statement that was published in the November 24, 1880 issue of the *New York Tribune*, D&H president Thomas Dickson declared "that the published report was the first he had heard of the project, and that the company [the D&H] was concerned in no way in it." Here is the account of this "new railroad project" that was published in November 27, 1880 issue of the *Carbondale Leader:*

"A NEW RAILROAD PROJECT. / The Delaware & Hudson Canal Company does not mean to be behind its great rival, the D. L. & W., in projects for extending their transportation facilities. And there is no reason why it should be. In many respects it has the advantage of all other coal carrying companies in supplying the trade, and this is especially true of the northern and eastern markets. It is most complete and thorough in its organization, is well officered, and its management combines wisdom and enterprise in a large degree. Everybody in this region will be glad to see the 'old company' stretch outs its arms to all parts of the country in a grand net work of public improvements. The Binghamton Republican gives some of the details of the project, as follows: / The project of building an extension of the Albany and Susquehanna Railroad to southwest, piercing Northern Pennsylvania coal fields and forming a through trunk line from Boston to Pittsburgh, is being revived. A preliminary survey is already being made. The proposed route will extend west from this city [Binghamton] and it is thought will run parallel with the new Delaware, Lackawanna & Western extension line to near Apalachin, where it will branch off, following the line of the old Dushore survey made in 1868. / The enterprise is in the hands of the Delaware & Hudson Canal Company, and it is proposed to make the road an extension of the Albany & Susquehanna. Connections will probably be made with the Lehigh Valley railroad and branches of the Pennsylvania Central, thus virtually establishing another through line from Boston to the West, and at the same time open up the coal fields of Northern Pennsylvania and provide through coal and freight transportation to the East over the company's roads. With the proposed road constructed the Delaware & Hudson Canal Company will control two leading lines, piercing the coal fields of Pennsylvania in the north and northeast, and thus giving them superior advantages for securing the coal trade in Eastern States. / --Since writing the foregoing, the N. Y. *Tribune*, of the 24th inst., has come to hand, in which the following authoritative statement is found—denying the facts set forth: / It was stated in the Binghamton Daily Republican, on Monday, that the Delaware & Hudson Canal Company was contemplating the extension of the Albany & Susquehanna Railroad to the coal fields of Northern Pennsylvania. A preliminary survey of the route west of Binghamton was reported to be in progress. Thomas Dickson, president of the Delaware & Hudson Canal Company, declared yesterday that the published report was the first he had heard of the project, and that the company was concerned in no way in it." (Carbondale Leader, November 27, 1880, p.2)

The D&H appropriated \$525,000 on January 14, 1881, for double-tracking the former A&S:

"\$525,000 was appropriated for double-tracking the Albany and Susquehanna and during the year [1881] some double-tracking of the railroad in Pennsylvania, between Scranton and Pittston, was accomplished." (COP, p. 232)

Important renovations were made to D&H depots in 1885:

"The Delaware and Hudson Canal Company is remodeling the remainder of its depots along the entire road by additions which will make larger waiting rooms and afford a separate office to each one. This will be a good improvement and will afford much convenience, as the platform will be on a grade with the track. It will dispense with climbing up steps as of old. So says the *Scranton Republican.*" (*Carbondale Advance*, March 7, 1885, p. 3)

Not only was the D&H extending its facilities to the North, it was, at the same time, reaching out to the South. In 1885, a link was established between the D&H and the Lehigh Valley Railroad at Wilkes-Barre:

"SHIPMENTS OF COAL. / The connection of the D. & H. C. Co. R. R. with the Lehigh Valley at Wilkes-Barre is likely to divert a large amount of coal tonnage which now passes through this city and Honesdale. It will manifestly be to the advantage of the former company to ship much of their coal product by the Lehigh Valley route, as the distance to tide water from their mines in Luzerne county is much less than via Carbondale and Honesdale. This will be one of the many changes which the new connection will make in the policy of the D. & H. C. Co., and it will be to some extent injurious to our local interests—much more so to those of Honesdale. In other respects, some advantages may accrue to us, and will possibly more than compensate for the injury. As we have said in former articles on this subject, the next few years will bring great and surprising changes in the face of matters throughout the region, and we can only hope that our locality will come in for its full share of the benefits that are to be realized." (Carbondale Leader, December 29, 1885, p. 2)

In 1887, the D&H promoted its passenger service to Summer resorts. Among those resorts were surely resorts to be reached by the D&H lines in New York.

"The *D. & H. Manual* for 1887 has been issued by the Passenger Department at Albany, N. Y. It contains a list of Summer resorts and places of interest on the line of the Delaware and Hudson railroad, the excursion fares to those points, the names of hotels and boarding-houses, together with rates for board. This little book should be in the hands of all tourists and those seeking to escape the discomforts of town life in Summer, for whose information it has been printed. The D. & H. railroad is really what it is claimed to be, the favorite tourist route." (*The Journal*, June 2, 1887, p. 3)

A car thief by the name of Verne Wilson, who was formerly in conductor Benedict's crew on the Susquehanna division of the A. & S. R. R., was caught on September 5, 1887, on Conductor Johnson's train in the Carbondale D&H yard. Here are the amazing details on this criminal behavior:

1887: "A CAR THIEF CAPTURED. / He Will Be Given a Hearing this Evening by the Alderman. / At about 2 o'clock this morning, as Conductor Johnson's train was being made up in the D. & H. yard, a man was discovered in one of the merchandise cars, who failed to account satisfactorily for his presence there. The crew turned their 'capture' over to watchman Wills and he was secured in a gravity freight car until morning, when constable Tracy introduced the gentleman to alderman Baker, by whom he was committed to the local 'cooler' to remain until 7 o'clock p. m., when he will be accorded a hearing. / The man is Verne Wilson, formerly in conductor Benedict's crew of the Susquehanna division of the A. & S. R. and it is suspected he belongs to the gang that have been operating on the A. & S. R. R. whose methods were to conceal one of the gang in a car, and while the train was in motion, and at given points, packages of merchandise would be thrown from the car and gathered by other members of the gang under arrangement." (*Carbondale Leader*, September 5, 1887, p. 4)

"Fast time" on the A&S at Nineveh was made by engineer Robert Copeland as he was making up a train for the return trip to Carbondale in October 1892. Here's the story of this "railroad rumble" that was published in *Carbondale Leader* of October 18, 1892:

"RAILROAD RUMBLE. / Notes Concerning the Different Railways in This Vicinity. / The fast time made by express trains is always an interesting topic among railroad men and the engineer who does more than a sixty mile an hour rate, whether the run be short or long is

counted an exceedingly nervy throttle puller. Occasionally an emergency run is made that fairly discounts all schedule fast time but the engineer never speaks of it. An instance of this kind occurred a few nights since on the Albany & Susquehanna division. Engineer Robert Copeland of this city was making up the train for the return trip at Nineveh and backed up to couple on three cars that stood on the side track. The first car was coupled to the engine, but as the brakeman stepped back he saw the other cars move down the switch and into the main line and he had to make a brisk run to board the nearest car. By the time he reached the brake the runaways were getting well under way and though he twisted the brake with all the strength he had, the cars continued to bowl along. Then he tried the brake on the other car but that too proved useless and the brakeman realizing that he was helpless signalled the engineer to come on. / Engineer Copeland responded to the signal promptly and then the race between runaway gondolas and a locomotive began. The night was very dark and the engineer was an entire stranger to the road but the brakeman continued to signal 'come ahead' and on he went. At first the race was all in favor of the runaways but when Rob got his big engine down to steady work the ponderous machine fairly flew along the track and he soon had the satisfaction of knowing that he was gaining. For two and a half miles the race was kept up before the runaways were overhauled and made fast. Then there was the run back to Nineveh and it was made in lightning time, just how fast no one will ever know but according to the flagman's time the five mile run was made in considerably less than four minutes. The brakeman on the runaway gondolas is sure the run both ways occurred in less than three minutes and he says if he was roadmaster that Engineer Copeland should have the best engine on the road for he is sure there is not another throttle puller in the land that could have won such a race." (Carbondale Leader, October 18, 1892, p. 4)

In 1903, as we mentioned at the head of this section, Harley Tuttle Dana published a history of the early years of the A&S in his book *Stray Poems and Early History of the Albany and Susquehanna Railroad*. Here, for the record, is the complete text (pp. 208-32) of that remarkable history:

Early History of the Albany and Susquehanna Railroad Its Early Financial Struggles, First Board of Directors, and First Officers. Some of the Early D. & H. Company's Men.

By Harley Tuttle Dana

Looking out over the valley today, watching the ponderous engines of the D. & H. Co. pulling the heavy trains eastward and westward, my mind has been carried back to the days of my youth, and the unpleasant experiences attendant upon a journey from Cobleskill to Albany, prior to the building of the Albany and Susquehanna Railroad have been the subject of my thought.

Then a journey to Albany with a load of farm produce, was an event of as much importance, and required more elaborate preparation than a journey to Omaha, Nebraska, does now. The good farmers' wives of those days went bustling about the house the day before the journey was to be made, frying doughnuts, cooking sausages, and baking bread, whilst the farmer and his sons were putting up the grain or apples for [209] market. In the evening the food prepared for luncheon for the three days' trip to Albany and back, was packed by the good wife into the dinner box, whilst "the man of the house" was busy at the barn packing a huge bundle of hay, and binding it with strong bands made by twisting rye straw into coils for rope. This done, and secured upon the load to avoid the purchase of feed for the horses whilst upon the trip, the wagon greased, and the dinner box placed safely on board, all was ready for a start in the morning before daylight. I will not recount the horrible details of a three days' pilgrimage to Albany and back, through mud and rain, slush and snow, trudging part way on foot or seated upon the soft side of a barrel of apples. There are those living yet, to whom this allusion will bring unpleasant recollections of their youth and young manhood. But the time was approaching when all this was to be changed. For years, the iron horse, steam-fed and harnessed, had been hauling long trains of cars eastward through the Mohawk valley, laden with the produce of the fertile prairies, and bearing the merchandise and manufactures of the east westward.

Farm lands which had ranged from \$25.00 to \$40.00 per acre prior to the building of the New York Central railroad, had increased in value to \$75.00 and \$100.00 per acre, and there was not lacking in the valley of the Susquehanna men with the vigilance to [210] note the change, and the business sagacity to see that what had been done in the Mohawk, could also be done in the Susquehanna valley. With the trains of the Erie railroad sweeping past Binghamton on the west, and those of the New York Central gliding through Albany on the east, with the shrill blasts of the locomotives resounding up the valley at each end, the years for the sway of the lumbering farm wagon and slow stage coach in this part of the state were being numbered. Yet long years of unappreciated toil, persevering energy and tireless work were to be performed by some men,

before the battle was fought and victory won. Prominent among the men of that day with the necessary brain to appreciate the importance of constructing a railroad from Albany to Binghamton, were Edward C. Delevan and Robert H. Pruyn, of Albany, Geo. W. Chase, of Maryland, Elakim R. Ford, of Oneonta, Arnold B. Watson, of Unadilla, Edward Thompson and Senator Dominick, of Binghamton, Jared Goodyear, of Colliersville, John Cook, of Worcester, John Westover, of Richmondville, Minard Harder, Charles Courter and Joseph H. Ramsey, of Cobleskill. Doubtless there are other names which should be inserted upon this roll of honor, but they do not occur to me now. All of these early railroad pioneers have gone from this world to the next, but the most of them lived to see the creation of their faith [211] and brain a reality, and to receive the praise and blessing of some who opposed them in their labors. The company was first organized in the year 1852, at a meeting held in Oneonta, and Edward C. Delevan chosen president. From this time may be dated the birth of the Albany and Susquehanna Railroad Company. With a board of directors chosen, officers selected and subscription books opened, the company was in position to send its representatives among the people soliciting subscriptions to the stock. This work was vigorously prosecuted by the holding of public meetings at which the merits of the enterprise were discussed, and by personal application and appeal at the homes of the people, by the directors and agents of the company, and fair progress made, but the magnitude and cost of the work was to be so great, that it soon became apparent that sufficient money to build and equip the road could not be raised by individual subscription. Accordingly the friends of the enterprise formulated a bill and presented it to the Assembly of the State at the session of 1855 authorizing the towns along the line of the proposed road to subscribe to the stock of the company. This bill passed the Assembly but was defeated in the Senate. The next year a bill passed both branches of the legislature, requiring as a condition to the subscription, the consent in writing of two-thirds of the taxpayers of the town. The next year, 1857, the bill was amended, requiring [212] the consent of a majority of the taxpayers, representing a majority of the taxable property of the towns, as the bill had been at first drafted. This bill was signed by Governor John A. King. By thorough canvassing and earnest work by the friends of the road, the necessary consent was obtained, and the towns became subscribers to the stock of the company. After the subscription of the towns, and that of the city of Binghamton to the stock of the company to the extent of one million dollars was obtained, the construction of the road was commenced, and an application made to the legislature of the State for aid, and a bill authorizing it passed by both branches. This bill was vetoed by Governor Morgan in 1859, but the friends of the road were not disheartened and at the session of 1863 a bill making an appropriation from the State of a half million dollars to complete the road to Oneonta was passed, and approved by that friend of the farmer and internal improvements. Governor Horatio Seymour.

Let the present and all future generations, who enjoy the pleasures and advantages of a well equipped and managed railroad, revere his memory.

Subsequently another appropriation was made by the State of \$250,000 to complete the road to Broome county, and the bill authorizing it signed by Governor Reuben K. Fenton. My recollection is that in addition to these great financial aids the company secured [213] a loan of one million dollars from the city of Albany. It is impossible for me to trace the company through all its financial embarrassments, but one thing I must say to the young men of this day, that the struggle made by the pioneers of this enterprise in overcoming individual and organized opposition, in obtaining the right of way, the consent of the taxpayers to the bonding of the towns, and state aid, was a work which required all the energy, the perseverance and the patience of a Courter, a Harder and a Ramsey on this part of the line, and their compeers and equals along the route.

In the obtaining of aid from the State, Joseph H. Ramsey as a member of the legislature, likely contributed more than any other man, but to those champions and co-workers in a noble cause, let equal honor be given:

Hail, men of noble thought and deed, Your work supplied our greatest need; Accept the homage which we bring. Not thoughtless, light, or trifling. But sincere praise which swells and starts. From the deep recess of thankful hearts.

The road was completed to Central Bridge, and the first passenger train run to that place, on the 12th day of September, 1863. Engineer Thomas Harden was in charge of the locomotive pulling the train, broad shouldered, brusque McCormick baggageman, the [214] lively little Johnny Clow, express messenger, and that prince of conductors, "Billy" Gardner, the conductor in charge of the train.

The author of this article commenced his service with the company in the month of October succeeding, and from that time forward will speak from personal recollection. Matters relating to the organization of the company and financial projects in its aid have been stated upon the authority of one of the pioneers of the road, now dead, but one who when living stood at the helm of affairs, and entirely reliable. Among the prominent characters connected with the track department of the road at that time, rises up before me Chief Engineer C. W. Wentz, who had been the engineer in charge of the work from its inception. Who, that ever looked upon his massive head and powerful physique, would have dreamed that his intellect would ever become clouded and weak? Yet such was the case at the close of his career, until death, or rather birth

into the world eternal, gave him clear intellect and mental vision. Next in this department loom up the stalwart figures of Mike Dorsey and R. H. Lane. The first with his mild gray eyes, stolid but friendly countenance still swaying the rod of authority over the northern division of the Delaware & Hudson with headquarters at Saratoga. The latter for a long time a resident of Cobleskill, died in the hospital at Albany a few years ago. True, time and exposure to [215] storm and wind during his long years of service, had wrinkled his face and dimmed his eye, but always true to the company he served, he ranked in life among its most efficient and trusted track men.

With them come visions of Col. N. Coryell, at one time paymaster on the Albany & Susquehanna and bridge superintendent; a strict disciplinarian, but always the courtly gentleman, and of huge H. Patterson, who still lives in Albany and has charge of the D. & H. company's track material. Only a few days ago I had the pleasure of grasping the hand of this "old timer" and its warm pressure took me back in memory to 1863.

W. L. M. Phelps, Esq., was the first treasurer of the company and still remains so. Indeed, I believe he has been their secretary and treasurer continuously from the organization of the company, with the exception of a brief period during which the road was in the hands of Robt. L, Banks, of Albany, as receiver. Among my early friends on the road, one who cheered me on in my way, and always spoke kind words to me, comes the thought of Geo. W. Church, our first General Freight Agent. A poor boy himself, he had worked his way from ordinary freight clerk, up to the head of the freight department of this new road, and he looked down in sympathy to the toilers below who would faithfully work and strive to win. S. K. Mayo was appointed the first General Passenger Agent of [216] the road, and superintendent of telegraph. Blessed be his memory, and dear to every employe's heart who worked under him. For several seasons he spent his evenings in summer in Cobleskill.

The friendly grasp of his hand and beaming light of his eye will be remembered by some of our residents and others who see these lines. He, too, for many years has been gone to the home "beyond the river." Gone but not forgotten.

James M. Foster was the first station agent at Central Bridge, and a more genial and capable man than he is rarely if ever found. He retained his position until the spring of 1865, when the road having been completed to Richmondville, he was transferred to that station, and the writer of this article made his successor. I think I am paying him only a deserved compliment, when I say, that notwithstanding all the perplexities of his position at the terminal station of the road, during the year and a half I labored under him, he only gave me one cross word. He subsequently engaged in banking business at Richmondville, and after an honorable and successful career as a banker, died at that place. Cobleskill Station was opened for business on the first day of January, 1865, and our respected fellow townsman, Colonel Alonzo Ferguson, appointed the agent. This

gentleman retained the place until the succeeding October, when he resigned, and the author of this article was sent on from Central Bridge to take charge, and remained in charge until the 1st day of April, 1901, a period of nearly 36 years. For his assistant in the office Col. Ferguson had Allison Kibbee. For his helpers in the freight house, German Boyce [217] and Asa W. Phelps. My readers will remember, Mr. Boyce died from sickness while a resident of this village, and poor Phelps fell a victim to his own carelessness, being crushed by the cars whilst attempting to cross the track underneath the train.

Allison Kibbee went to New York City. He died in Brooklyn, and his remains were brought to Cobleskill, and laid to rest in the village cemetery.

Among the superintendents of the Albany and Susquehanna Railroad, I recall, first, Jonathan R. Herrick, at one time a merchant at Lawyersville. Subsequently he removed to Albany, where he died. I hold his memory sacred and gratefully remember, that as a member of the executive committee of the board of directors, he gave the casting ballot which secured my promotion to the position of station agent at Central Bridge. Next in order came Geo. R. Skinner, of Oswego, who granted my request to be transferred to Cobleskill upon the resignation of Col. Ferguson. Well do I remember Superintendent Skinner's salutation to me one morning at Central Bridge. I had thoughtlessly dumped a pan of coal ashes upon the track in front of the station, at the time the eastward and westward bound passenger trains passed there, and a lady dressed in black silk, having occasion to pass from one train to another swept her magnificent robe right through that pile of ashes. Skinner stood upon the platform of one of the coaches and saw it all. "Who dumped that pile of ashes there?" was the morning salute which rang in my ears. "I did, sir," was my reply. "It is a confounded slovenly trick, sir, and don't you do it again," was his warning answer, and I never did. The next superintendent was [218] J. W. Vanvalkenburgh. Heavy eyebrowed, with piercing eyes and massive, overhanging forehead, short neck, deep chest, broad shoulders, and limbs as supple and wiry as those of an athlete, he looked the very man who could run a railroad, ride a wild mustang, or throw Jim Fisk over the railing and down stairs, and rumor says he did it during the Erie war.

His successor was H. A. Fonda, the "Mohawk Dutchman," as he was called, with long, lank, lean body, and sallow countenance, looking more like a sickly Methodist clergyman than a railroad superintendent, but a man with a warmer heart, a truer friend, or more economical manager of a railroad, I never knew. With him came the friendly, honorable, Julien Clark as his assistant, and the genial and magnetic H. S. Morse, as superintendent of telegraph and train dispatcher. This gentleman became our next superintendent, and continued in the position until succeeded by C. D. Hammond, the present superintendent of the D. & H. Company's Susquehanna and Northern divisions of road. Of H. S. Morse let me say he was kind to his employes, and courteous to the public; in short, a gentleman. After leaving the road he went South and became interested in the

construction and management of railroads in Georgia for several years, and then came North and retired upon his farm near Westfield in Chautauqua county, N. Y. Here he died. Blessed be his memory. A nobler man, or truer friend never lived. Julien Clark, after battling for years with that fatal disease, consumption, died almost at his post and passed "over the river."

The successors to Mr. Mayo in the passenger department of the road have been, I think in order: H. [219] M. Watson, Joseph Angell, the late D. M. Kendrick and the present incumbent, J. W. Burdick.

In the freight department Geo. W. Church was succeeded by Mr. H. S. Marcey, of the Rensselaer and Saratoga division, with Dudley Farlin as his assistant. Mr. Marcey was subsequently made general traffic manager of the company, and Mr. Farlin the general freight agent. Both of these men have passed on to join the great majority in the world eternal.

Mr. Farlin was succeeded by James Colhoun, with Paul Wadsworth, the present efficient general freight agent of the D. & H., as his assistant.

Looking back to 1863, I see a galaxy of stalwart, alert and energetic men, as the directors and officers of the Albany & Susquehanna Railroad Company. Looking about me today, I see only a quartette of those I then knew.

W. L. M. Phelps, R. C. Blackall, Mike Dorsey and H. Patterson are only left of the men prominently identified with the management of the road at that time.

O robber, old time, of the friends who were mine, You've taken full many a score, And I feel I'm bereft so few are there left, Of the friends and comrades of yore.

But I must hasten. Let us turn to the year 1869. This was an important era in the history of the road. The road had been completed to Oneonta in the summer of 1865, and was vigorously carried forward. The tunnel of 2,260 feet at Coles Hill, near Harpersville [sic], was being bored, and the month of January, 1869, found the road completed and trains running from Albany to Binghamton. [220] Thus after a period of nearly sixteen years of constant toil and struggle, hampered by financial embarrassment, and opposed by some who should have been ready to aid, the heroic men who led in the enterprise saw the realization of their faith and hopes. *The Albany and Susquehanna railroad was completed*.

With the shrill blasts of the locomotive whistle reverberating from mountain side to mountain side, throughout the whole valley of the Susquehanna, the Rip Van Winkle spell which had bound the "sequestered region" had long been broken, and it needed not the eye of a railroad seer to perceive, that the Albany and Susquehanna railroad formed the missing link in the Erie chain, necessary to unite it with the New England roads, New England seaboards, and the great hub of the country, Boston. What wonder then that the sagacious Jay Gould, the great railroad magnate of that day, and James Fisk, Jr., the genial, dashing, audacious spirit of Wall street and the Erie railway, should have looked with longing and wistful eyes over into our valley, at the young railroad lamb and desired to bring it into their fold. Such was the case, and so in 1869 we find them quietly securing the stock of the Albany and Susquehanna railroad, with the intent of securing the control of the road at the approaching election. The different steps, legal and otherwise, taken by them to secure this object, and means employed by those opposed to them to prevent it, are too voluminous and minute to be recited in this article, and the reader is referred to Roscoe's History of Schoharie County, page 439, for a succinct account of the preliminary steps taken by the two parties. [221] The year 1869 was fraught with important events, and a season of conflict and turmoil within the board of Directors. Indeed, it was a house divided against itself, and the faction known as the Erie party commenced an action to remove Mr. Ramsey from the presidency of the road, and obtained an order from Judge Barnard, of New York, restraining him from the discharge of his official duties. Then Judge Rufus W. Peckham, of Albany, vacated, or so modified this order as to allow him to appear, and show cause why this should not be done. During the pendency of these proceedings, and the legal skirmishing growing out of them, the rival factions, with their rival Supreme Court Judges, succeeded in having the road placed in the hands of rival receivers, and as the military phrase is, there was skirmishing "all along the line." As a result counter orders were being constantly sent out over the telegraph wires, and conveyed by trains to the employes of the road.

One now before me reads:

"To the Officers and Employes of the Albany and Susquehanna Railroad,

"Take Notice. — By an order of the Supreme Court, made August 6th, 1869, we are duly appointed receivers of the property, franchises and effects of said road, and by the same are authorized to take possession of, and run the said road. On and after this date you will make all reports and remittances to James Fisk, Jr., and Charles Courter, Office of the Albany and Susquehanna Railroad.

"James Fisk, Jr., Charles Courter, Receivers."

Albany, August 6th, 1869.

Another without date but received August 7th, 1869, reads:

"To the Officers and Employes of the Albany and Susquehanna Railroad Company.

"Take Notice. — Robert H. Pruyn was first appointed receiver by Judge Peckham and has possession of the office, books and property of the Company. He has continued me as superintendent, and all transactions of the company must be with him, and my orders obeyed. J. W. Van Valkenburgh, "Superintendent.

A message received from the treasurer's office at the time reads:

"To all Agents of the A. & S. Railroad, "Make your money remittances and continue all your business transactions with me as usual.

"W. L. M. Phelps, Treasurer."

Copies of these documents are not given in any sense as personal reminiscences, but as showing the conflict of authority on the road at the time. Of course such a state of things could not long exist, without breeding bad blood and culminated in the Erie war. Baffled if not beaten in their legal maneuvering, the Erie party sought by force to get possession of the road, and with true military tact, chose Binghamton as the base of their operations. Here they were not only removed from the fiery J. W. Van Valkenburgh, and invulnerable R. C. Blackall, but were near [223] their source of supplies, and in close communication with Judge Bernard, of New York.

Thus under show of legal authority, not like Ethan Allen at Ticonderoga, "In the name of the Great Jehovah and the Continental Congress," but in the name of Judge Bernard, of New York, in the month of August, in the year of our Lord 1869, the Erie hosts swept down the western end of the line, intent upon taking possession of the road. O, then there was hurrying to and fro in the office at Albany. The eagle eye of Van Valkenburgh blazed with indignation. Blackall, with short speech and quick command, ordered the engines fired up and forces aboard, and away flew the fiery iron horses for the point of attack. The writer of this history was one of the "home guard" on this occasion, and so cannot speak of the military incidents from personal observation. The general nature of the times, can be judged by the communications given herewith. One directed to all agents under date of August 10th, 1869, is as follows:

"Have all the men you can find to the station, so as to defend your station and the road.

Another under the same date reads:

[&]quot;Albany, August 9th, 1869.

[&]quot;J. W. Van Valkenburgh, "Superintendent."

"Superintendent's Office, August 10, 1869.

Train No. 7 will go through to the front. Have all the men you can get to go with it. Drum them up.

"J. W. Vanvalkenberg, Superintendent."

From documents now in my possession I conclude that the "Erie war" reached its zenith on the 11th day of August, 1869. I know that the proclamation [224] of Governor Hoffman, commanding the belligerent forces to lay down their arms and retire peacefully to their homes, was flashed over the wires about that time, and messages now before me lead me to believe it was on the evening of that day. The desperate condition of things that evening can be judged by message of R. C. Blackall commanding the forces of the Ramsey party. It is as follows:

"Harpersville, August 11, 1869.

To J. W. VanValkenberg, Supt.

The Erie folks are preparing to move upon us. They have the militia with them encouraged by the sheriff. Our retreat is cut off. We must resort to desperation if they attack us today or tonight. What are we to do? We must have reinforcements. Their force is estimated at 1500. Our forces are about 300. Send reinforcements immediately. I will burn the high bridge if they move on us tonight. We have scouting parties now watching them. J. M. Bailey will return soon. He is now out reconnoitering.

R. C. B."

This shows determination and desperation, but soon the scene changes, for under date of August 11th, 1869, 8:30 P. M., I hold a message which does not show to whom it was addressed, but it is one I took from the wires that night and my recollection is, it was addressed to R. C. Blackall and reads: "Gen'l James McQuade will leave here tonight to stop further disturbances. The governor has charge of the road, and orders from James McQuade must be obeyed and regarded.

J. W. Van Valkenburgh, Sup't." [225]

Another reads: "Albany, August 11th, 1869, 11:30 P. M.

Gen'l McQuade is superintendent of the A. & S. R. R., appointed by the governor of the State of New York. All orders by him will be obeyed until further orders.

McQuade, Superintendent.

J. W. Van, Acting Sup' t."

With the road in the control of the State, the question of rival receiverships was settled, and shortly thereafter Robert L. Banks, of Albany was appointed sole receiver by the governor, and I

believe held that position until the decision of the courts in favor of the election of the Ramsey directors, and the re-instatement of Mr. Ramsey as president of the company. Robert L. Banks and General James McQuade were both courtly gentlemen, and I gratefully remember their kindness to me as their agent. One of the most pleasing episodes of the Erie war to the Ramsey party, was the capture at Bainbridge by R. C. Blackall and his men, of the Erie engine, which was moving down over the line with a force on board, turning out our operators, and installing theirs.

Blackall had warning of their approach, and side tracking his engine, with the lights all turned out, he placed a pair of patent frogs, such as are used for replacing cars on the track, reversed upon the rails over which the Erie engine had to pass. Then placing his men in ambush he patiently awaited the coming of the Erie party. Everything appearing secure, on they came, until they reached the frogs, when lo, and behold, their engine was soon on "terra firma" instead of the iron rails, and became an easy prey to Mr. Blackall and party, and was confiscated as the "spoils [216] of war" and for a time was run upon the road as such. Thirty-three years have passed since that night, but well do I remember the morning when that engine was run into Albany. By orders of the company, I had been on duty all night, supported by John Dunn, then section foreman, and his men, with instructions to" hold the fort" against the Erie raiders. About 5 A.M. I was awakened from my reverie, by the shrill blasts of a locomotive whistle near Richmondville station. Onward it came, "nearer, clearer, deadlier than before," making the welkin ring, until the engine was stopped in the Cobleskill yard. Tired and sleepy from the night's vigils, I rubbed my eyes, and peering out of the window of the freight office beheld "Erie Engine," "The Roswell McNeil." Surely, I said the enemy are upon us, but the second look revealed the form of burly "Abe" Ottman, one of the A. & S. conductors, and of Royal Cornwall, one of our early locomotive engineers. Royal Cornwall has been dead many years. Ottman died but a few years ago, at the home of his son in New Jersey. John Dunn still lives at Green Island, N. Y., serving in the track department under Mike Dorsey. Among the "old timers" I have thus far failed to mention in the clerical department are "Billy" Robinson in the return ticket department and E. R Willerton in the general passenger office. These persons have held their positions for very many years, and are among the company's most trusted servants. "Jimmy" Loughren, head clerk under Geo. W. Church, our first general freight agent, I have lost track of.

Among the early freight handlers, I recall at Central Bridge, Charles Eldredge, Esq., now living at [227] Sharon; Philip Dietz now in retirement at Central Bridge, and Geo. S. Lynes at that time a resident of Middleburgh, nor must I forget to mention "Yankee" Sullivan who for a long time guided the freight truck at Richmondville, nor at Central Bridge, John F. Bradt, the father of "Billy" Bradt, our popular passenger train conductor.

With the road in the hands of the State, and the Erie warriors disbanded, neither party rested quietly on their oars, but were active in the purchase of the company's stock, preparatory to the holding of the annual election, on the 7th day of September, 1869. By this time the strife to gain the ascendancy in the board of directors to be elected, had waxed to blood heat.

Fisk with his army of heelers prepared with proxies to vote, came up the river from New York in a boat chartered for the purpose, and landing took possession of the room where the election was to be held. R. C. Blackall, then our master mechanic, with John L. Cory his assistant, with men of "broth and brawn" from the shops were on hand to see that there was "fair play" if any scrimmage occurred. Superintendent J. W. Van Valkenburgh with flashing eyes and knit blow strode through the hall, a very king among men, ready for any honorable strategy, or even to fight for the good of the road if necessary. J. H. Ramsey, with his legal knowledge and tact, with the Hon. Henry Smith as counsellor, were there to secure if possible an honest election and a "fair count." It is proper to say to the present generation, that one of the contending parties at this election was known as the Ramsey party; the other as the Gould and Fisk, or [228] Erie party. Now between them there was no more affinity than between oil and water, and the result was, that at this meeting each party organized, and elected a board of directors, each claiming to be duly and lawfully elected, and each claiming to be entitled to the control of the road, and out of this controversy grew the long legal trial at Rochester before Judge E. Darwin Smith in November of the same year, at which trial it was decided that the election of the Ramsey directors was legal, and that of the Gould and Fisk directors illegal and void. But I must close; yet before closing will digress and say that the Cherry Valley Sharon and Albany railroad Company was chartered in 1860. The road was completed and opened for business in June 1870. The master spirits in its construction were Dewit. C. Bates and James A. Young, of Cherry Valley. The principal if not the sole contractors for its construction were the Weed Bros. Thos. H. Giles, the father of Harry Giles, now firemen on the engine running on that branch of the D. & H. Co., during its construction, was engineer and conductor combined, in charge of the work train. Here the name of Giles brings to my mind another "old timer" Gill Giles, now in charge of the D. & H. water supply. He was with the Susquehanna Company from its commencement, and has been with D. & H. Co., since the lease by them of the road, so that his doings have become a part of the history of the line. One more chapter in this hurried history and my work is done. Whilst the two parties which have been mentioned were struggling for the mastership, down among the coal fields of Pennsylvania, and in the marts of New York City, were men, not of the dash [229] of James Fisk, Jr., nor the adroitness with which Jay Gould is always credited, but men, reserved, solid, cautious, deliberate, and far seeing; who were looking up through from Honesdale, and Carbondale to Nineveh, thence down through the valley to Albany, away along the shores of the Champlain to Canada, and across New England to Boston; and they said, what an outlet for our coal. What an inlet for the iron ore up at Port Henry and Plattsburgh. Eureka, we have found it. Let us lease the Albany and Susquehanna Railroad for 99 years. Let us do so also with the Rensselaer and Saratoga Railroad. Let us build a new road along the bank of the Champlain and instead of putting out our coal to market through the muddy waters of our canal, we will have a network of railways over which our black diamonds shall pour in incessant streams, and they did it. The Albany and Susquehanna Railroad was leased to the Delaware and Hudson Canal Company on the 24th day of February, 1870. The Rennselaer and the Saratoga

later. At this time Thos. Dixon [sic] was president of the D. & H. Co., and Coe F. Young its general manager. Who that ever saw these gentlemen at Cobleskill station, asking to have some trifling service rendered with the modesty of the most common patron, could fail to recognize the solid greatness of the men. Both have been gone from earth for many years, but their mantels have fallen upon worthy shoulders. Among the early conductors on the line who are living I recall the blue eyed pleasant Gunnison, now a resident of Binghamton, and W. E. Bartlett, now in the hardware business at Jefferson, Oklahoma territory. D. S. Fero also remains with us living at Newburgh, N. Y. [230] Among the dead, the brusque but whole-souled McCormick, first baggagemaster, then conductor, whose light went out in Albany many years ago. The venerable H. P. Dorr, the sedate "Ham" Evarts, the impassive, quiet, military Captain Merrehew, and "Billy" Gardner, whose smile faded from his face and blue eyes closed in death about fourteen years ago; and now during the last summer the chivalrous and genial Coe F. Young, who went down to his grave lamented by all, and honored for his bravery and devotion to his country during the war of the rebellion. I must bring this history to a close; but there is a class of men on all railroads who, in this article, have received but little notice. Men with soiled hands, begrimed faces and dirty vests, but beneath whose rough exteriors there are warm hearts, loyal hands and heroic souls. I mean the locomotive engineers.

Of those first upon the road I recall Charley Delany and Thos. Harden, whom I have mentioned as engineer in charge of the engine drawing the first regular train from Albany to Central Bridge. Next Wallace Blake I believe in charge of the engine hauling the construction train when the road was being built through Cobleskill, and I think in charge of the engine which brought the first passenger train to Cobleskill. Nor must I forget to mention engineer Frank Eaton. Coming early upon the road, if not among the first, he still handles the lever and ranks among the most trusted of the D. & H. Co.'s engineers, with headquarters at Sidney. Delany and Blake have been dead a score of years. The last I knew of Harden he had retired from railroading and settled upon a farm, likely he, too, ere this has passed on to join the great majority. [231] "Charley" Jones, at one time our master mechanic, with headquarters at Oneonta, was also early on the road. It is only of recent date that he, too, passed on to the world immortal. No one who ever knew him will fail to bear testimony to his care and skill as an engineer, and his solid worth as a Christian gentleman. And "Charley" Stickles too, he of long arms, ruddy but spare countenance and hawk eyes, with his tall form bent in the cab, neck and head extending forward, eye scanning the track with a keenness that would detect a fly upon the rails four rods ahead, who would have supposed that he would have run engine tender and baggage car all off the end of the rails of the siding at Cobleskill, yet he did it. I had relieved the switchman for the night, and promised to be on hand and change the switch, so as to let the train out of the yard in safety. At the time I was freight agent, ticket agent and telegraph operator, and in the rush of caers [?cars, ?cares] I forgot my promise to the switchman, and was therefore responsible for the accident. As the train pulled out I closed the ticket office window, and was crossing the yard to the freight office, when I heard the sharp, quick signal for "down brakes," and, glancing towards the train, saw what happened.

Going down to the train I soon came face to face with the then superintendent, J. H. Fonda. With a squeaky, rasping voice, he demanded, "Whose place was it to be here and turn this switch?" Without bravado, but with respectful firmness, I replied, "It was mine, sir. I relieved the switchman, and agreed to be here and turn this switch myself." "You better quit railroading then" was his rejoinder; to which 1 answered, "That is for you to say, Mr. Fonda." As I [232] was not discharged, I concluded that, although irritable and sometimes angry, Mr. Fonda had beneath his rough exterior a tender heart, and in that particular case appreciated truth-telling as compared to equivocation and falsehood.

Of the later engineers comes the thought of poor S. G. Cook, killed by a collision on the road. Always affable and kind, notwithstanding the hardships and exposures of an engineer's life, I loved the man and revere his memory. And young "Jud" Mann, who lost his life in a similar manner when nearing Oneonta station. His literary tastes and affectionate nature made him a general favorite, and his sad death fell like a pall upon the hearts of all classes upon the road. There were doubtless many others in all departments of the road who deserve special mention, but they do not occur to me now. To one who, during a service of thirty-seven years on the road, received only kindness from these men and the officers of the old Susquehanna Company and from its successor, the D. & H. Co., the friendship of the living is precious, and the memory of the dead rises up like the fragrance of crushed flowers. To the living I say, here is my heart and hand. Sad indeed will be the hour to me when the time of parting comes.

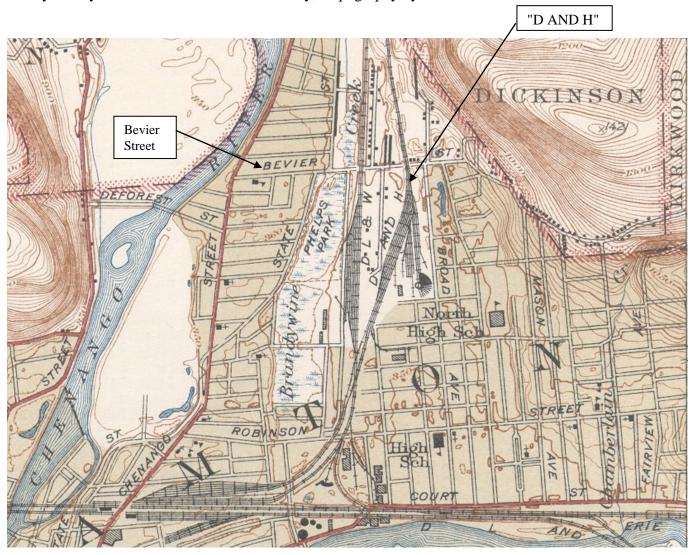
'Tis hard to part when friends are dear, Perhaps 'twill cause a sigh or tear; Then steal away, give not much warning, Say not "good night," but in some brighter world, Bid me, "Good morning."

H. T. DANA, Cobleskill, N. Y. January 4, 1903 Let's now take a look at the A&S rail line from Binghamton to Albany (keeping in mind that construction of the line began in Albany and ended in Binghamton).

Binghamton to Nineveh Junction:

Binghamton, 1891

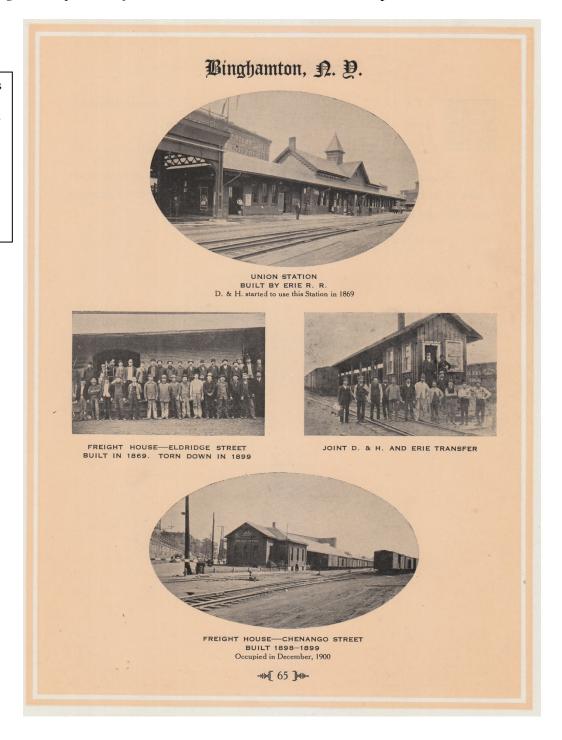
Detail from U. S. Geological Survey, *Binghamton Quadrangle*, Surveyed in 1891. Henry Gannett, Chief Geographer; H. M. Wilson, Geographer in charge; Triangulation by N. Y. State Survey and by U. S. Coast and Geodetic Survey; Topography by Frank Sutton.



In 1863, two of the Directors of the A&S were from Binghamton: Alonzo Everts and Sherman D. Phelps.

The photographs of the Binghamton passenger station and freight houses are from *Passenger* and Freight Stations Delaware & Hudson. The Delaware and Hudson Company / Board Of Managers / Inspection of Lines::. June 7th to June 10th, 1928, p. 65.

Binghamton was incorporated as a village in 1813 and as a city in 1867. It was named after William Bingham, a Philadelphia merchant.



In 1903, James Downs ("Racing a Runaway Freight," *The Delaware and Hudson Railroad Bulletin*, March 1, 1936, pp. 35-36) went to work on a D&H express passenger train, known as the *Cannon Ball*, from Binghamton to Albany. The train left Binghamton at 4:20 P. M. and arrived at Albany at 8:40 P. M., stopping only at Nineveh, Sidney, Oneonta, Cooperstown Junction, and Cobleskill en route.

On January 19, 1891, Bert J. Loomis went to work for the D&H as a fireman. His first trip was with Engineman Potter on "the 212" from Oneonta to Nineveh. Two weeks later, we learn from his biographical portrait ("Engineer's Biggest Thrill") that was published in the January 15, 1932 issue (pp. 19-20, 30) of *The Delaware and Hudson Railroad Bulletin*, he was sent to Binghamton, where Charles O. Vedder was Foreman and Patrick Shea, later Assistant Superintendent, was Yardmaster. The Binghamton yard and railroad operations there at the time are described in his biographical portrait as follows:

"The Delaware and Hudson layout at Binghamton was then considerably different from the present [1932] plan. The roundhouse was at Liberty Street—part of it is still used as the yard office. In addition to the twelve locomotive stalls there was a blacksmith shop, carpenter shop, oil room, supply shed for storing lumber and tools, and the foreman's office. The freight house was located about half way between Chenango Street and the roundhouse. / . . . When Mr. Loomis first came to Binghamton the train crews made up their own trains, averaging 22 cars each. All the lead engines were Moguls; Consolidation type engines 179, 180, and 181 were in pusher service. When a crew began to make up its train the conductor spotted all the light loads and did his best to include them in his train, provided, of course, a yardmaster wasn't looking. Those cars averaged about 40 tons each; when 80-ton cars appeared the crews were dismayed, although heavier motive power removed that source of concern to some extent." (pp. 20, 30)

The crossing of the D&H and the Delaware, Lackawanna and Western at Binghamton at the time was governed by a red and green ball system:

"Our line crossed the Delaware, Lackawanna and Western at the same place then as at present [1932], although a red and green ball signal was the only device in use at that point. There was a red indicator on one end of a cross arm at the top of a high pole and a green one on the other. When the Delaware and Hudson indication was green the Lackawanna showed red and vice versa. The same type of signal was used at the Sidney O. & W. – D. & H. crossing and the West Shore intersections at Voorheesville and South Schenectady until replaced by more modern equipment." (p. 20)

More about the Binghamton Yards from the biographical portrait of William L. Parsons, who worked there for more than forty-eight years ("Veteran of Binghamton Yards," *The Delaware and Hudson Company Bulletin*, January 1, 1930, pp. 3-4). Therein we read:

"Upon the recommendation of Fred Ronk [a close friend of the Parsons family and a foreman on the Albany and Susquehanna] William [Parsons] secured a job on the transfer at Binghamton. At that time [1880] the Erie Railroad tracks were of broad (6 ft.) gauge while those on the Albany and Susquehanna were standard. For this reason cars could not be interchanged from one line to the other; their contents had to be transferred at every junction point. This was the purpose of the transfer on which Mr. Parsons saw his first service with our [D&H] company. / At that time there were heavy shipments of grain, iron ore, rails, flour, sugar, molasses, and furniture through Binghamton. When transferring grain, a chute was run from the Erie to the Albany and Susquehanna car and the contents ran from one to the other by force of gravity. The rails were handled with derricks. Iron ore, of course, had to be trucked across the platform. Barrels of flour, sugar, and molasses were rolled from one car to another. At times they found a weak barrel of some one of these commodities, and if it was not carefully handled there would be flour, sugar, or molasses all over the platform. At that time the men on the transfer received one dollar per day and worked form 7 A. M. until 6 P. M."

In 1902, Parsons was transferred to the yard force as a car checker, and continued in that line of work until his retirement became effective in 1928. The work of a yard clerk is described in Parsons' biographical portrait as follows: "No position in the yard calls for more care and attention than that of a yard clerk. With the fast movement of freight trains, he must be 'on the spot' when a train is ready to move in order to get the number and initial of each car or he must explain the reason why. Being one minute late may mean that a score of cars has passed without his securing their numbers. Likewise care must be taken to get the right number and initial the first time. By the time he looks the second time the car may be out of sight. / The mistaking of a single letter or figure on a car might mean endless trouble to the yard office and traffic forces. This is particularly true on trains being delivered to connecting lines. By the time it is realized that a mistake has been made, the train may be miles away. While trains arriving from connections may be re-checked if necessary, the motto of Mr. Parsons, as well as every other yard clerk is, 'Get the number right the first time and you won't have to worry.' " (p. 4)

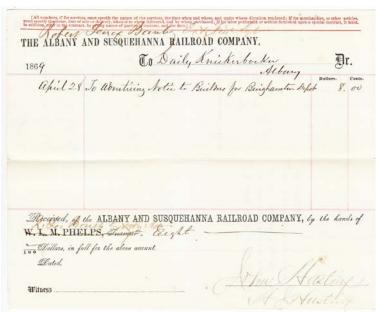
Railroading on the old A&S:

In the biographical portrait of Patrick J. Connors ("Conductor No. 1 Retires After 55 Years On Susquehanna Division," *The Delaware and Hudson Railroad Bulletin*, July 1, 1935, pp. 99-100, we read: "Railroading on the old single-track Albany and Susquehanna was so uncertain a business that a man leaving Binghamton had no idea when he would be back. Delays occasioned by engine and car failures, meeting and passing other trains, picking up train orders on the signalless line, switching, loading and unloading freight at stations consumed more time than was spent running. Mr. Connors' crew was 26 hours making his first trip from Binghamton to Oneonta and return. Furthermore, they were subject to call at any time after reaching home, sometimes when business was brisk, starting out again an hour after completing the previous trip. / Conductors, in addition to hiring their brakemen, also recommended them for promotion.

When, in 1882, Conductor T. F. Sullivan informed Superintendent C. D. Hammond that Brakeman Connors was qualified to handle a train, he was placed in charge of a crew. / Mr. Connors was given Caboose No. 40, which was his until he entered passenger service in 1899. It was much smaller than those now in use, having one window on each side, and two at each end, and there was no cupola from which to look out over the train. In one end was a small heating stove, while along each side was a tool locker, the hinged top of which formed a cushioned seat. The lockers contained coupling links and pins, journal brasses, 'dope,' a car jack, chains for hitching cars when the couplers broke, etc." (pp. 99-100)

Building the Binghamton A&S Depot:

Advertizing, April 28, 1869, in the Albany *Daily Knickerbocker* for proposals to build the Binghamton Depot. Invoice donated to the Carbondale D&H Transportation Museum by John V. Buberniak.



Advertizing, April 29, 1869, in the *Elmira Daily & Weekly Advertiser* for proposals to build the Binghamton Depot. Invoice, shown on the following page, donated to the Carbondale D&H Transportation Museum by John V. Buberniak.

1869 Ahiil 2	To adritising Pr		ly Weekly A com & Theirs	n n		Cent
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Received	the ALBANY AND S'	USQUEHANNA	RAILROAD COM	IPANY, by	he hands	9
W. L. M. PH	ELPS, Theasurer,	right -				

In Howard Hontz's column ("From the Top") in the February 2016 issue of the *Bridge Line Historical Society Bulletin ("A Brief history of The D&H's tables and wyes"*, pp. 10, 11, we read the following about pushers (on Belden Hill out of Binghamton, and elsewhere) on the Susquehanna Division:

Operations on the Susquehanna Subdivision

A tonnage train leaving Binghamton faced a grade for 15 miles to climb to the Belden Hill tunnel, which is at the top of the grade. The train would have, say, a 1500-series Challenger class "J" on the head end, and the same for a pusher, with a 5000-ton train. The pusher would push to the tunnel, cut off, and follow the train downgrade (north) to the wye at Grover's.

At Grover's, the pusher would pass the north switch of the wye, back into the wye's north leg past the switch at the wye throat, and head out on the south leg. This would head the engine south, where it would wait for a Binghamton-bound southbound train. Once the southbound arrived and stopped south of the wye, the pusher would come out, couple to the rear, push the train about 12 miles to the top of the Belden grade, and follow the train to Binghamton. If it was needed for another northbound out of Binghamton, the pusher would be turned on the turntable at the Binghamton engine terminal. However, if it was needed for another southbound, it would have to back north to Grover's again to wait for the next southbound.

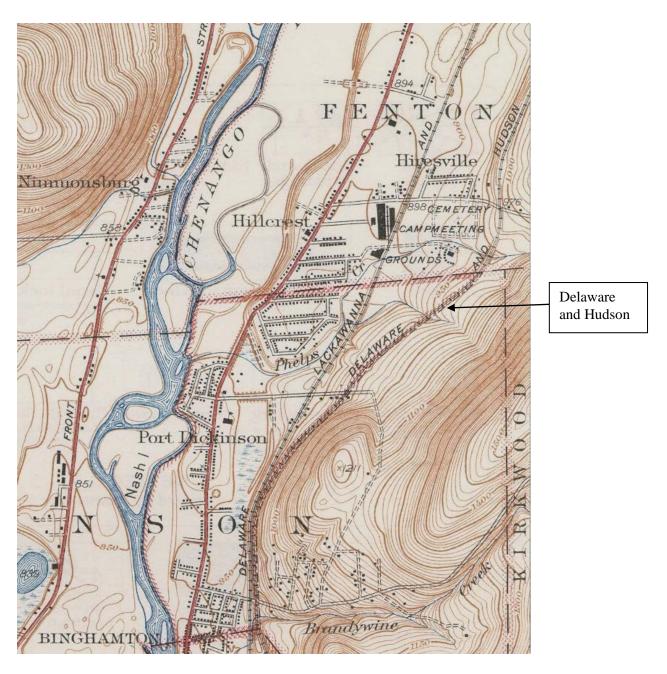
Cabooses used from and to Binghamton were built to be pushed on, and had strong underframes.

Pushers were often needed for southbound trains facing the Esperance, Howes Cave and Richmondville grades, for about 44 miles from Schenectady. These pushers usually originated usually at Mohawk Yard, but the engines mostly came from Mechanicville. The pushers would shove from Mohawk to the top of Richmondville and then return to Mechanicville, making a reverse move, running backward, as there was no wye in that distance to turn the engine. Usually the pusher would make only one push due to the time and distance involved. Sometimes these trains would be double-headed, with two engines on the head end to avoid a pusher being required.

Castle Creek

Detail from U.S. Geological Survey, *Castle Creek Quadrangle*, Surveyed in 1934; Topography by H.R. Kilmer and J. L. Watkins; Edition of 1942.

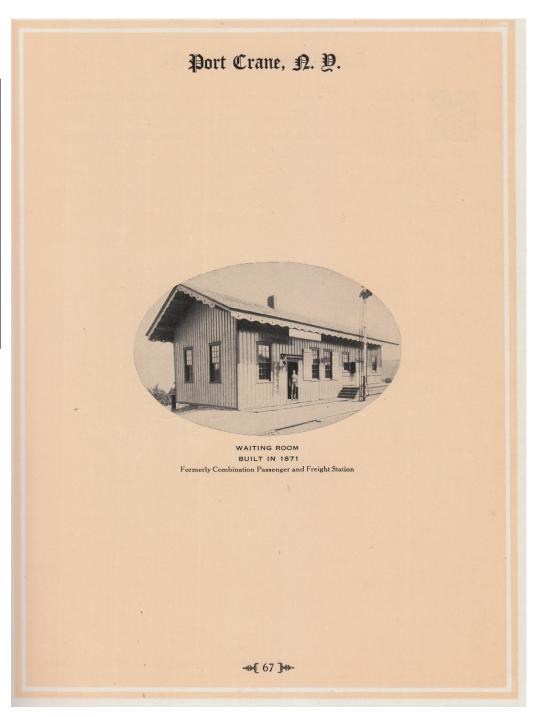
Binghamton to Port Dickinson to Fenton:



The photograph of the D&H Port Crane waiting room that is given below is from Passenger and Freight Stations Delaware & Hudson. The Delaware and Hudson Company / Board Of Managers / Inspection of Lines::. June 7th to June 10th, 1928, p. 67.

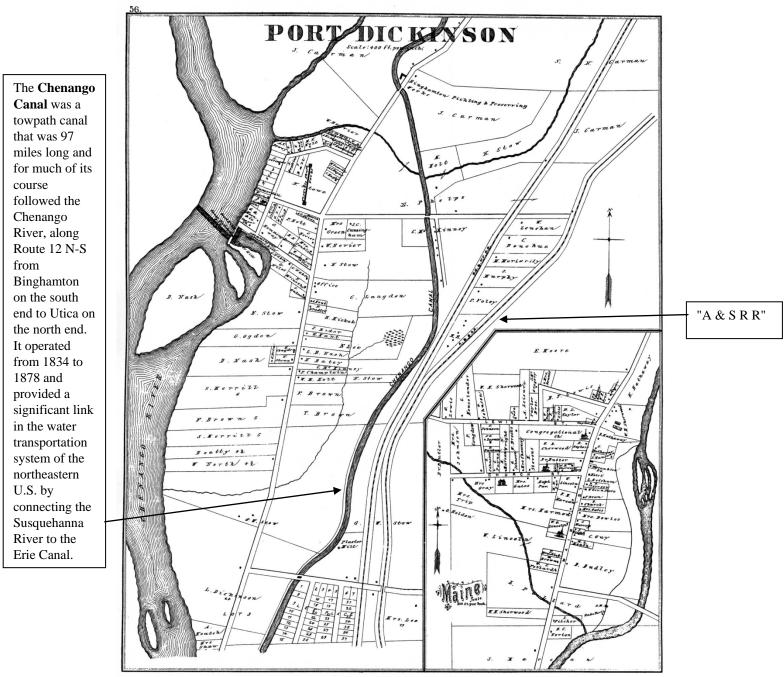
Port Crane:

Elisa Pease was one of the original settlers of Port Crane in 1788. When the Chenango Canal; was constructed, the village was made a port of call and named Port Crane after Jason Crane, one of the engineers who built the canal.



Port Dickinson

Port Dickinson, as shown on Combination Atlas Map of Broome County New York, 1876, p. 56:



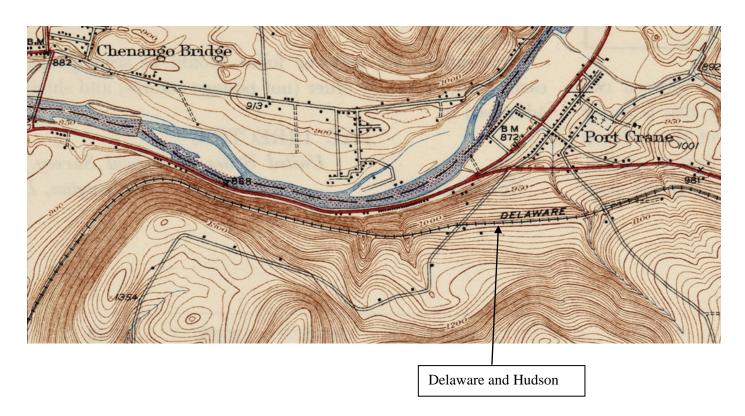
Source: Combination Atlas Map of Broome County New York. Philadelphia: Everts, Ensign & Everts, 1876, p. 56.

Chenango Forks

(1)

Detail from U. S. Geological Survey, *Chenango Forks Quadrangle*, Surveyed in 1935; Topography by H. R. Kilmer and C. W. Birdseye.

Chenango Bridge to Port Crane (now called Fenton):



In 1855, Port Crane was formed from the town of Chenango. In 1867 the name of the town was changed to Fenton, after Reuben Fenton, a governor of New York. The Chenango Canal passed through the west side of the town. The A&S passed Port Crane on the east.

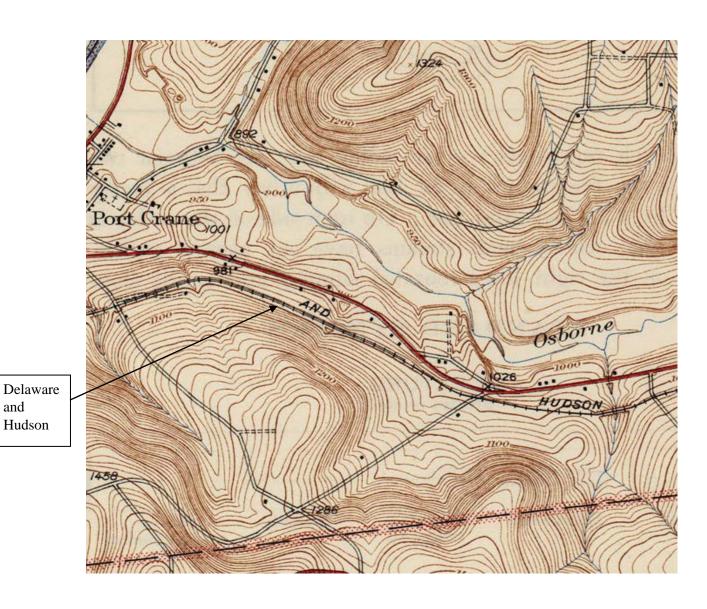
Chenango Forks

(2)

and

Detail from U. S. Geological Survey, *Chenango Forks Quadrangle*, Surveyed in 1935; Topography by H. R. Kilmer and C. W. Birdseye.

Port Crane to Osborne:

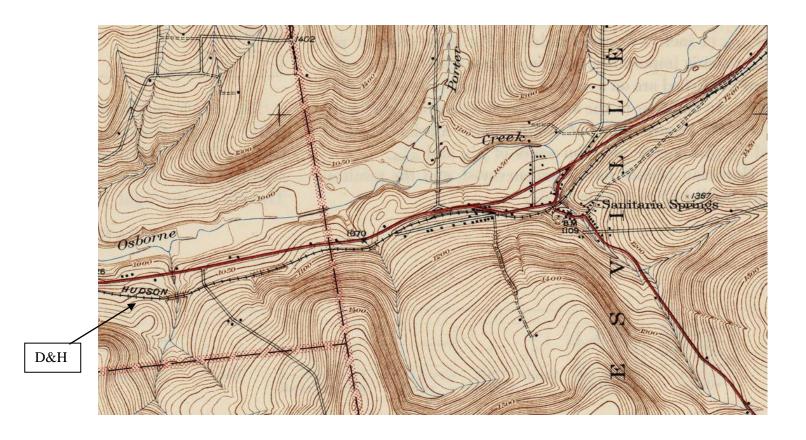


Chenango Forks

(3)

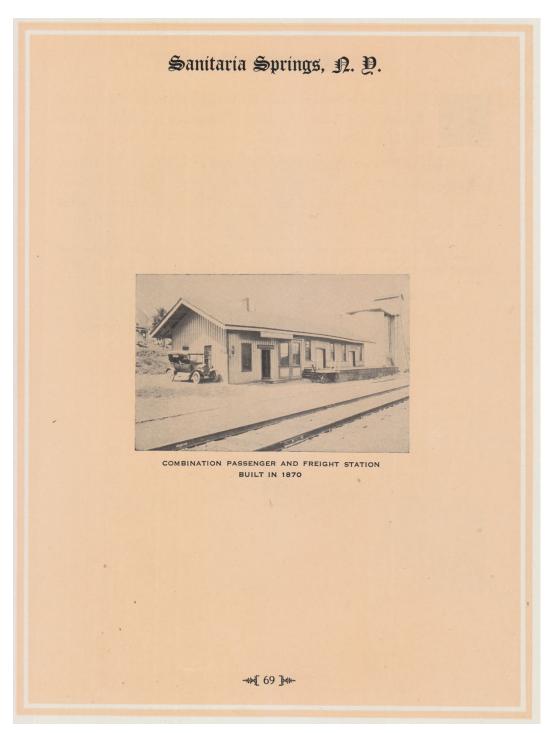
Detail from U. S. Geological Survey, *Chenango Forks Quadrangle*, Surveyed in 1935; Topography by H. R. Kilmer and C. W. Birdseye.

Osborne to Sanitaria Springs:



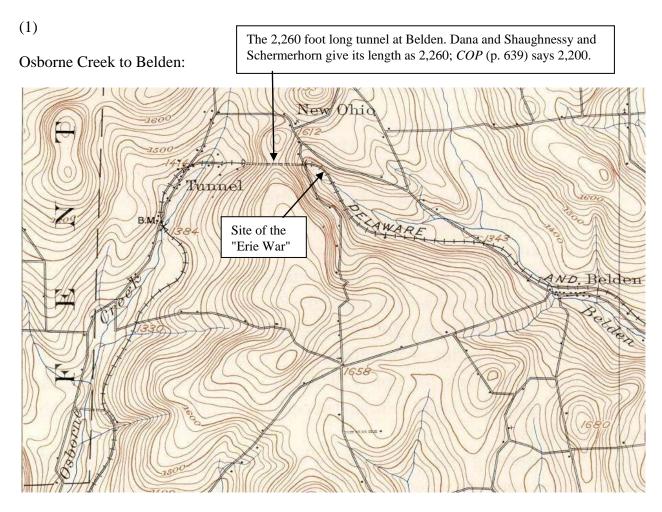
The hamlet of Sanitaria Springs in the Town of Colesville was originally called Osborne Hollow after Eli Osborne, who settled in the region in 1790. The name was changed after S. Andral Kilmer, inventor of Swamproot medicine, opened a curative spa based on the mineral spring waters that flow in the area.

The photograph of the combination passenger and freight station at Sanitaria Springs is from Passenger and Freight Stations Delaware & Hudson. The Delaware and Hudson Company / Board Of Managers / Inspection of Lines::. June 7th to June 10th, 1928, p. 69.



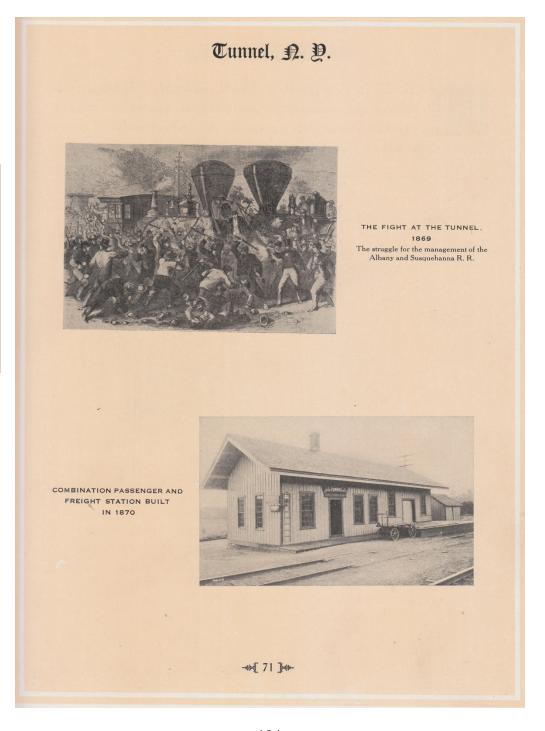
Nineveh

Detail from U. S. Geological Survey, *Nineveh Quadrangle*, Surveyed in 1902-1903. H. M. Wilson, Geographer in charge; Topography by C. C. Bassett; Control by N. Y. State Survey, J. H. Jennings and E. G. Hamilton.



On a curve in the track, just east of the Tunnel, on August 10, 1869, took place the celebrated "Erie War" between the Ramsey interests and the Jay Gould/James Fisk interests.

The photograph of the combination passenger and freight station at Tunnel, NY, that is shown below is from *Passenger and Freight Stations Delaware & Hudson. The Delaware and Hudson Company / Board Of Managers / Inspection of Lines : : . June 7th to June 10th, 1928, p. 71.*



Tunnel: The village of Tunnel is located at the southern portal of the tunnel. The settlement grew up as the result o the construction forces that lived here to project the tunnel through the hill.

Constructing Section 43 (Tunnel) of the A&S:

Monthly Estimate for May 1867, Section 43 (Tunnel) Neilsen & Brown, contractors. \$6,990.00

Invoice donated to the Carbondale D&H Transportation Museum by John V. Buberniak.

	→ CONSTRUCTION.
	CONSTRUCTION.
	Recorded as No. 9800
MON	THLY ESTIMATE for Mean 186
/	For Section No. 43 (Fund
11	For Section No. 43 (Fund contractor)
Retu	n No. made ellary 3/ 1867 al. P. Green Rest ENGINEER
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Total	Amount of present Estimate \$ 2/3, 578.
	ct previous Estimate\$206,528.
Ection	ate for May 1867, \$ 6,990.0
125(111	
,,,	Payable, as per Contract, as follows, viz:
Dedu	et percentage retained \$ 349.60
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-	per cent in stock 7.8
	per cent retained . 8
Amt	of payment for May 1867, \$ 6.640
7	o be Charged in Record Book, as follows:
	encing
	raduation
	ridges
-	
	CERTIFY that I have examined the within
	aly Estimates, from which the foregoing state are made, and believe the same to be correct
	DATED ENGINEER'S OFFICE,

Work done in May 1867 by Neilson and Brown, Contractors on the A&S. Original document in the collection of the Carbondale D&H Transportation Museum, courtesy of John V. Buberniak:

	Estimate of Work Done, and of certain Materials Delivered, on Section 43 during the Month of May 1867, by Neilson + Prown Contractors.								
				Amounts.		Amounts.			
	ITEMS.	Quantities.	Prices.	Dolls.	Cts.	Dolls.	Cts.		
	1. Clearing and Grubbing, Section,			/ 200					
	2. Exeavation of Earth, Cubic Yard,	- 40.53	0.75	30.397	501				
	3. do Loose Rock, do			, '					
"Enlarge-	4. do Solid Rock, do Enlargement in Sannel Lin It	84	1.40	/ 117	60 V	30.5-15	10		
ment in		1.744	63.00	109.872	001				
Tunnel"	5. First Class Masonry, laid in Hydraulic Cement,	584	12.00	1.008	001				
	6. Second Class do do do	-				/			
	7. Third Class do laid dry, do	27	6.00	162	00 V	117.042	00		
	8. Fourth Class do do do								
	9. Fifth Class do do do								
	10. Rip-rap Protection of Brush and Stones, do								
	11. Paving in Foundations, &c., do								
	12. Piling in Foundations, Lineal Feet,								
imber for	13. Timber in Foundations, do								
porting	14. Plank and Boards in Foundations, Feet B. M.	,		10		28.942			
nnel"	- Jumber for sufferting Junel Linds	144,710	0.20	20.942	00 V	20.942	00		
	15. Timber and Plank in Bridges of 50 feet span, &c., do								
	16. Wrought Iron do do Pounds.								
	17. Cast Iron do do do					1			
	18. Spikes and Nails, do do do								

(continuation of document given above)

17. Cast Iron do do18. Spikes and Nails, do do							
24. Fencing, including Gates, &c.,	Rods.	171	1.65	282	15 x	V 1282	.15
MATERIALS DELIVERED Heuding in advance of Enla	irgement In H	298	16.00	4.768	00 V	4.768	00
Add Estimate A Duly 1 503 In It of heading Estima	, st, 865 less ted to fine 1 st 1860	}				31.969.	65
2		otal Amount,		Antonia		213.518	
Deduct amount of for Amount of estimates	for May	1867			. 1	6.990	
on account of the above estimate.		ND SUSQUEH	ANNA R	1 6		NY, the sum Dolla	ers,
Albany, June	1867	C	1,0,				

I CERTIFY that I have measured and estimated the items of work embraced in the foregoing estimate, and believe the quantities therein stated are true and accurate amounts of the several kinds of work done as therein set forth.

Pambridge May 31 1867.

Resident Engineer.

In *D&H Time Table No. 3, Effective 0201 hrs. E.S.T., Sunday, April 29, 1973*, the restrictions and instructions on the movement of trains through the Belden Hill Tunnel are given on p. 47, as follows:

Tunnel

A falling rock detector system has been installed which, when activated, will cause block signals to display an aspect indicating "STOP".

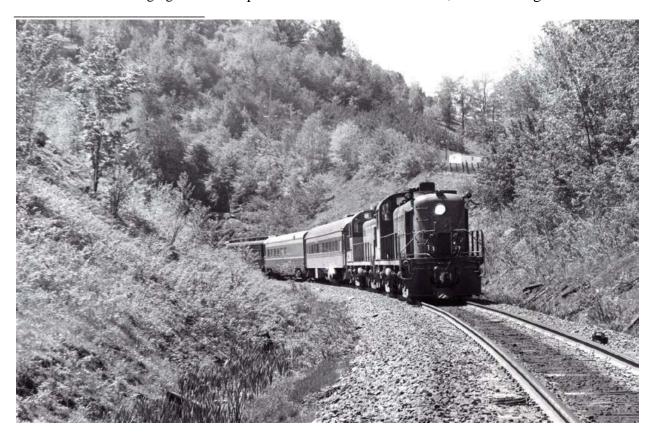
Signals governing movement through the tunnel will be observed as follows:

Trains finding northward home signals 126.1 or 126.1A, or southward home signal 125.2 at the tunnel, displaying an aspect indicating "STOP", must stop and communicate with the Train Dispatcher for instructions. Trains receiving permission to pass one of these signals must proceed at restricted speed to the next signal and must flag through the tunnel if so instructed by the Dispatcher. In case of failure of means of communications it may proceed when preceded by a flagman to the next signal displaying a proceed indication.

Telephones are located on signals 126.1 and 125.2 for use of flagmen in reporting back to their trains.

When night signals are not in use, all trains will display a lighted lantern on the rear when passing through tunnel.

1970: D&H train emerging from north portal of the Belden Hill tunnel, north of Binghamton.



"In the spring of 1970, the Delaware and Hudson ran a special passenger train for senior officers of the railroad. The train, behind a pair of first generation Alco roadswitchers, is seen just emerging beyond the north portal of the Belden Hill tunnel, north of Binghamton. The train had passed slowly through the tunnel, as members of the official party detrained on the south side and walked the entire length to inspect the condition of the bore." Photo and caption from Arthur House on Delaware and Hudson Railroad, *Facebook*, September 10, 2015

Another Belden Hill photo, downloaded from *Facebook*, January 9, 2016: "Thirty years ago this month [1986] the first train ran through the enlarged Belden Hill tunnel. J. J. Young, Jr. (on left), Mike Overlander (on right) and myself [Sam Botts] were present to catch B&M #200 lead train NWED out of the North portal." Photo by Sam Botts

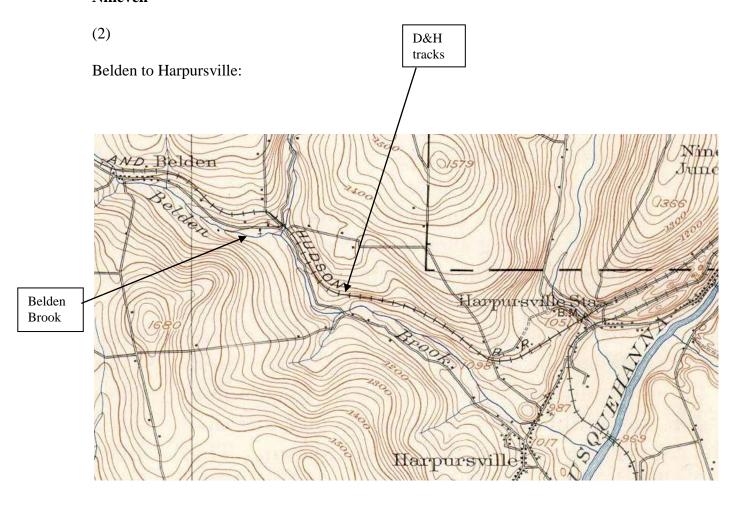


While the Belden Hill tunnel was being enlarged trains were run via Lanesboro and the remaining portion of the D&H's Pennsylvania Division.

Facebook, January 9, 2016: Sam Botts: "The second day of service for the enlarged Belden Hill tunnel found D&H GP38-2 # 7315 leading NWED." Photo by Sam Botts.



Nineveh



Robert Harpur (1731-1825) was an American teacher, politician, pioneer, and landowner who settled in the Binghamton area. He came to the American colonies in 1761 via Scotland. He was a teacher of mathematics at Kings College (now Columbia University). One of his students there, in 1874, was Alexander Hamilton. Harpur served in various capacities in the New York government during the American Revolution. He was a member of the New York State Assembly from 1777 to 1784. He was Deputy Secretary of State under John Morin Scott and Lewis Allaire Scott from 1778 to 1795. In the spring of 1795 Robert Harpur with his 2nd wife Myra and family moved west along the Susquehanna River settling near Belden Brook on his Warren Patent which is near present-day Harpursville, NY. Harpursville, New York was named after him, as was Harpur College (the arts and sciences component of present-day Binghamton University).

The photograph of the Harpursville D&H combination passenger and freight station that is shown below is from *Passenger and Freight Stations Delaware & Hudson. The Delaware and Hudson Company / Board Of Managers / Inspection of Lines : : . June 7th to June 10th, 1928, p. 73.*

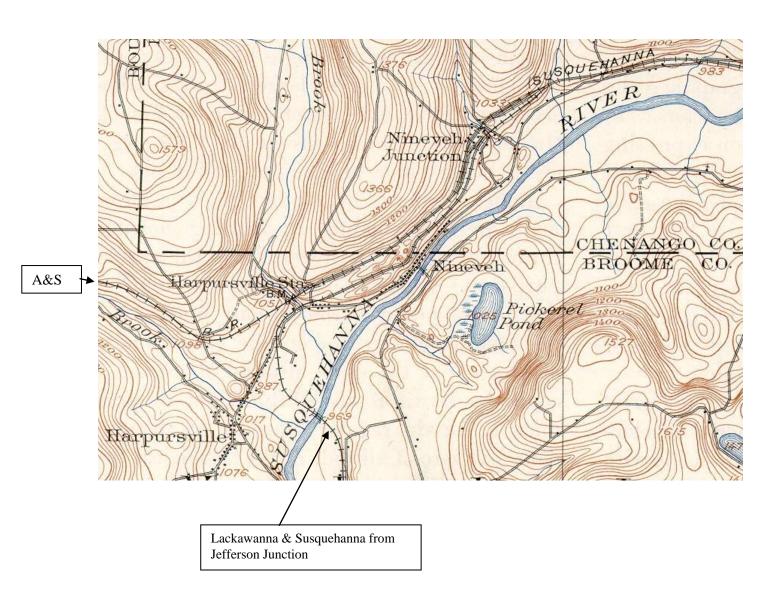


Nineveh

(3)

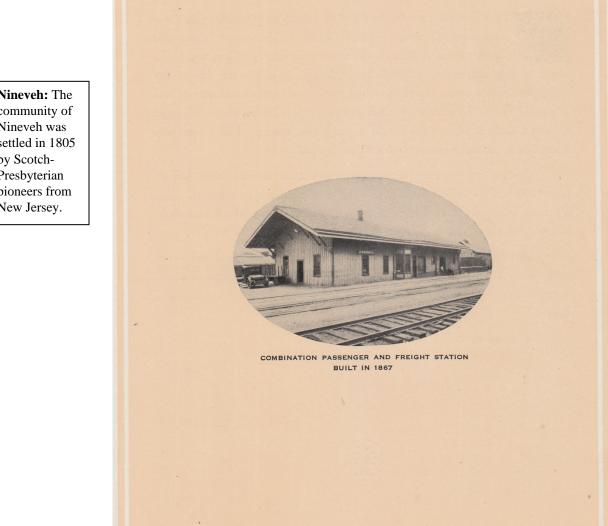
Harpursville to Nineveh Junction:

The A&S from Afton to Harpursville was completed and opened to the public on December 25, 1867.



The photograph of the Nineveh D&H combination passenger and freight station that is shown below is from Passenger and Freight Stations Delaware & Hudson. The Delaware and Hudson Company / Board Of Managers / Inspection of Lines::. June 7th to June 10th, 1928, p. 75.

Nineveh, N. Y.



Nineveh: The community of Nineveh was settled in 1805 by Scotch-Presbyterian pioneers from New Jersey.

-W[75]-

On December 30, 1890, Elmer E. Kerr became a D&H engineer, in which capacity he continued for 42 years prior to his retirement on pension, February 1, 1932. In his biographical portrait ("Locomotives Had Names," *The Delaware and Hudson Railroad Bulletin*, December 1, 1935, pp. 179-180) we read: "Mr. Kerr saw service in fast freight, yard, passenger, and pusher service. His favorite, however, was pusher service on Harpursville Hill, where he was working at the time of his retirement." (p. 180)

At Nineveh Junction, the L&S coming North from Jefferson Junction, merged with the A&S coming East from Binghamton for Albany.

Up to November 1874, A&S crews ran into Carbondale. Beginning in November 1874, the Nineveh Branch was included in the Pennsylvania Division. In the 1890s summary of the Valley Road, we read:

"In November, 1874, (up to which time Albany & Susquehanna crews ran into Carbondale, and were independent of the Pennsylvania division) the Nineveh Branch was included in the Pennsylvania division system, under the supervision of Superintendent Manville, and the second passenger train was put on between Carbondale and Nineveh, in charge of Conductor Daley. Conductor Skeels then became conductor of the train running between Carbondale and Scranton, and has been running a passenger train ever since."

The biographical portrait of James Downs ("Racing a Runaway Freight," *The Delaware and Hudson Railroad Bulletin*, March 1, 1936, pp. 35-36) begins with the following account of a runaway freight train on Harpursville Hill:

"Train No. 6, Binghamton to Albany, was standing at the Nineveh station at dusk one day in the early 1900s awaiting the arrival of its connection from the Pennsylvania Division, when Flagman James Downs heard a series of sharp blasts of a locomotive whistle. Presently he saw the flash of a headlight coming rapidly down Harpursville Hill. Instantly he guessed the explanation—a 40-car freight which they had passed on the siding at East End after emerging from the tunnel, was out of control. / Running forward, he shouted to the engineer to get under way. A glance over his shoulder and the latter clambered into the cab, released the brakes, and opened the throttle. For an instant which seemed eternity the drivers slipped, spun, then caught hold and the train rapidly gained speed. / Meanwhile, Mr. Downs, catching the rear platform, was watching the approaching runaway when someone screamed, 'Jump for your lives—here comes a train!' Mr. Downs blocked the passageway, holding the terrified passengers in check, until the headlight, at one time only a short distance behind, gradually disappeared in the distance. Stopping at Afton for orders, they were told to go back to Nineveh as the runaway was safe on a siding. The incident was later mentioned in an official bulletin which commended Mr. Downs and the engineer." (p. 35)

Nineveh was an ancient Mesopotamian city on the eastern bank of the Tigris River. Located in a central position on the great highway between the Mediterranean Sea and the Indian Ocean, it was the capital of the Neo-Assyrian Empire. It was sacked by an unusual coalition of former subject peoples, the Babylonians, Medes, Persians, Chaldeans, Scythians and Cimmerians in 612 BC. Its ruins are across the river from the modern-day major city of Mosul, in the Ninawa Governorate of Iraq.

Ancient Nineveh was surrounded by a massive stone and mudbrick wall, about 12 kilometers in length. The wall system consisted of an ashlar stone retaining wall about twenty feet high surmounted by a mudbrick wall about thirty-three feet high and forty-nine feet thick. The stone retaining wall had projecting stone towers spaced about every fifty-nine feet. The stone wall and towers were topped by three-step merlons. The city wall was fitted with fifteen monumental gateways. In addition to serving as checkpoints on entering and exiting the city, these structures were probably used as barracks and armories.





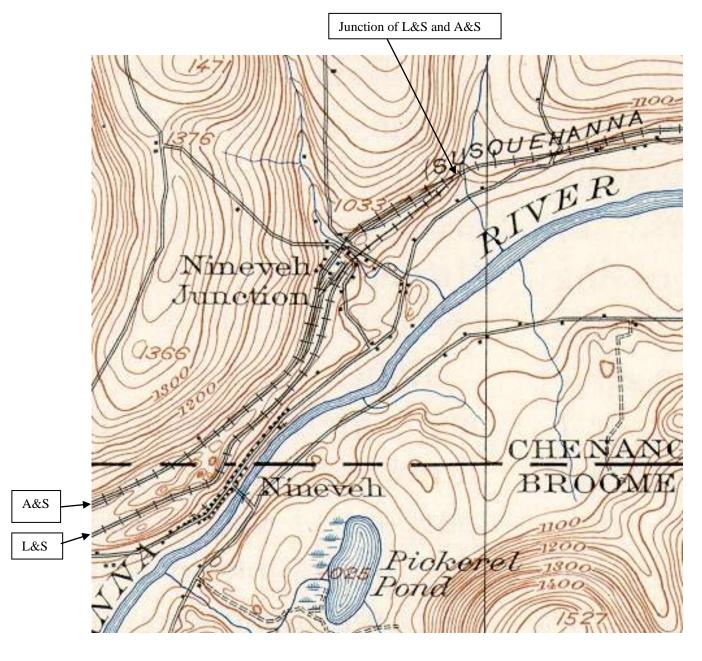
Nineveh Junction Depot. Post card sold on E-Bay on July 15, 2016. Thanks to John V. Buberniak for bringing to our attention this post card.

Nineveh

(4)

Nineveh Junction:

The junction of A&S and L&S:



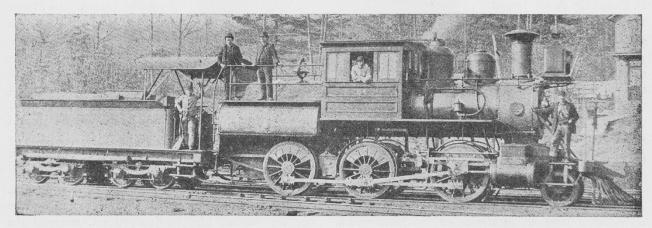


Nineveh, October 1926. Photograph in the collection of the Carbondale D&H Transportation Museum.



Northbound Delaware & Hudson Fast Freight with 1500 class 4-6-6-4 at speed near Nineveh, N. Y. in 1948. Photo by Edward Baumgardner, 26 West End Avenue, Oneonta, NY. Photo in the Alan G. Dustin collection in the archives of the Carbondale D&H Transportation Museum.

On a Carbondale-Nineveh Run in 1888



Left to right: E. R. Sampson, Verne Adams, Tom Kirkbridge, John E. Mack, and Jack Smith.

Delaware and Hudson Railroad Bulletin, April 1, 1934, p.52



Post card in the collection of the Carbondale D&H Transportation Museum. The engine shown here is a D&H engine (see "D&H" on cab of engine) but we have not yet learned its number. The card was sent by "R. C. B." (see below) to Marion Munn, Nineveh, N.Y.

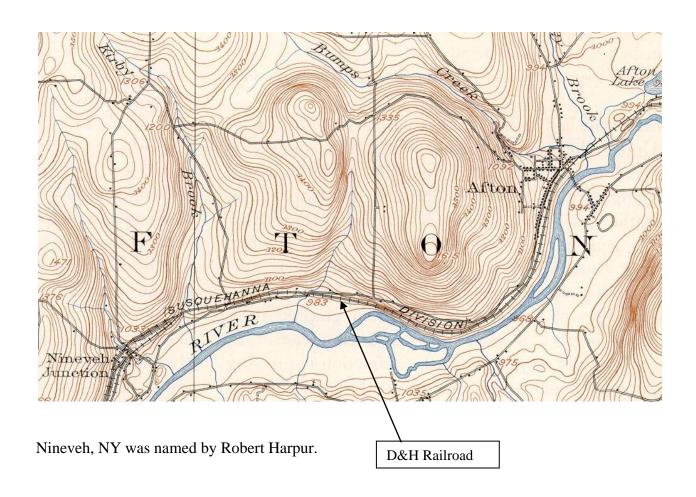


A&S from Nineveh Junction to Albany

Nineveh

(5)

Nineveh Junction to Afton:

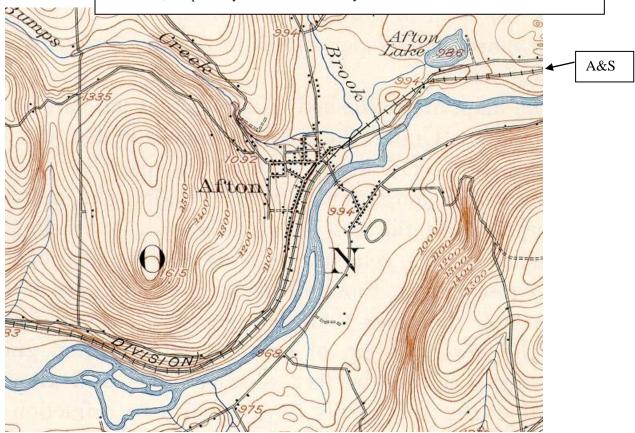


Nineveh

(6)

Afton:

There was a wye at Grover's, near Afton. See text in boxed note in Volume 11, p. 152. In the February 2016 issue of the *Bridge Line Historical Society Bulletin*, p. 29, there is a photo of a Mallet with the following caption: "A 1600-series D&H Class H 0-8-8-0 Mallet (Alco Schenectady 1912) has coupled onto its train in Johnson City, NY. June 1936 photo from H. K. Vollrath. BLHS Archives, MacDonald collection. Marked as "pushing backward", and probably headed toward the wye at Grover's. "

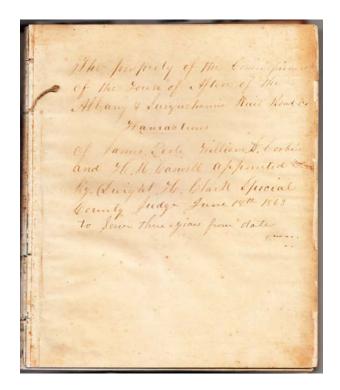


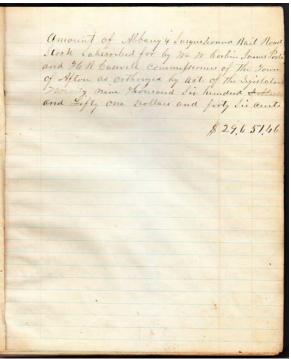
The A&S from Bainbridge to Afton was completed and opened to the public on November 11, 1867.

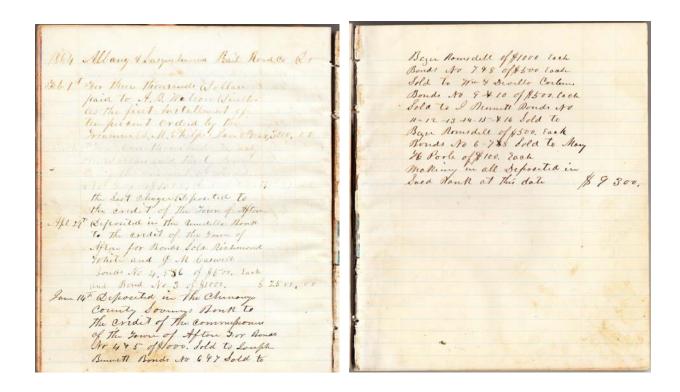
Afton, NY, was named after Afton Water, a small river in the parish of New Cumnock, Ayrshire, Scotland. The Afton was immortalized by Robert Burns in the lyrical poem, *Sweet Afton*, written in 1791. The poem begins: "Flow gently, sweet Afton, among thy green braes,/ Flow gently, I'll sing thee a song in thy praise / My Mary's asleep by thy murmuring stream / Flow gently, sweet Afton, disturb not her dream . . "

A&S Bonds: Afton

The A&S issued bonds to help finance the construction of the rail line. The communities along the route of the road purchased those bonds (in denominations of \$1,000, \$500, and \$100), not only as a financial investment but also to demonstrate their support of the project and to help make the project a reality. The individual communities then re-sold those bonds (in denominations of \$1,000, \$500, and \$100) to individuals in their communities. In Afton, three commissioners were appointed on June 18, 1863 (for a three-year term) to take charge of the sale of these A&S bonds to the members of the community. Shown below are four pages of the register of those three commissioners of the town of Afton of the Albany & Susquehanna Rail Road Co. This original Afton register, thanks to John V. Buberniak, is now in the collection of the Carbondale D&H Transportation Museum.







Given below are details from those four pages:

Detail No. 1:

of the Found of the Commencer of the Found of After of the Found of After of the Allany & Surguehania Mail Road Co Fransactions

Of James Perle Villiam H. Corbin and H. M. Caswell appointed to My Quight He Clark Africial County Judge June 18th 1863

To Serve three years from date

"The Property of the Commissioners / of the Town of Afton of the / Albany & Susquehanna Rail Road Co. / <u>Transactions</u> / of James Poole William W. Corbin / and H. R. Caswell appointed / by Dwight H. Clark Special / County Judge June 18th 1863 / to Serve three years from date."

Detail No. 2:

Amount of Abbary & Surguehouna Nail Nood Stock subscribed for by We to learbin James Pooli and Ho A Caswell Commissioners of The Form of Afton as orthoryed by acts of The Significant Twenty nine Thousand Six hundred tottact and Fifty one bollow and forty Six Cents

"Amount of Albany & Susquehanna Rail Road / Stock Subscribed for by Wm W. Corbin James Poole / and H. R. Caswell Commissioners of The Town / of Afton as authorized by Act of the Legislature / Twenty Nine Thousand Six hundred / and Fifty one Dollars and forty Six cents. / \$29,651.46."

Detail No. 3:

Istof Albany & Surperhance Rail Road Co & r

San 14" Deposited in the Chinoup

Country Sovening Down to

The Credit of the Commeponer

of the Grove of After For Romas

No 4 # 5 of Hove. Sold to Lough

Benutt Bonds No 6 4 y Sold to

"1864 Albany & Susquehanna Rail Road Co. / June 14^{th} Deposited in the Chenango / County Savings Bank to / the credit of the Commissioners / of The Town of Afton For Bonds / No 4 & 5 of \$1000. Sold to Joseph / Bennett Bonds No 6 & 7 Sold to "/

Detail No. 4:

Bonds Ao 748 of \$500 Each

Sold To 7700 & Devillo Corlem.

Bonds No 9 × 10 of \$500. Cach

Sold to I Demuts Donds Ao

11-12-13-14-15-× 16 Sold to

Bayer Romedele of \$500. Each

Bonds No 6-788 Sold to Many

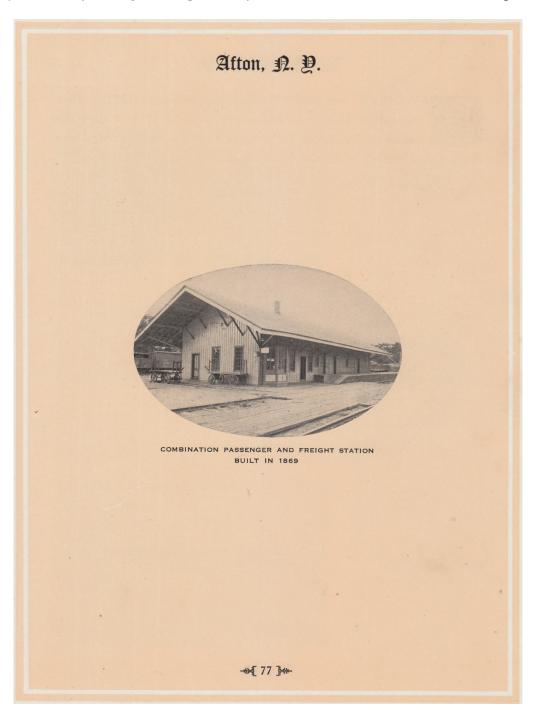
46 Porle of \$100. Each

Making in all Seposited in

Soid Nanh at This date \$9300.

"Beyer Romsdell of \$1000 Each / Bonds No 7 & 8 of \$500 Each / Sold to Wm & Devillo Cortena / Bonds No 9 & 10 of \$500 Each / Sold to J Bennett Bonds No. / 11-12-13-14-15 & 16 Sold to / Beyer Romsdell of \$500. Each / Bonds No 6 -7-& 8 Sold to Mary / H Poole of \$100. Each / Making in all Deposited in / Said Bank at this date \$9,300."

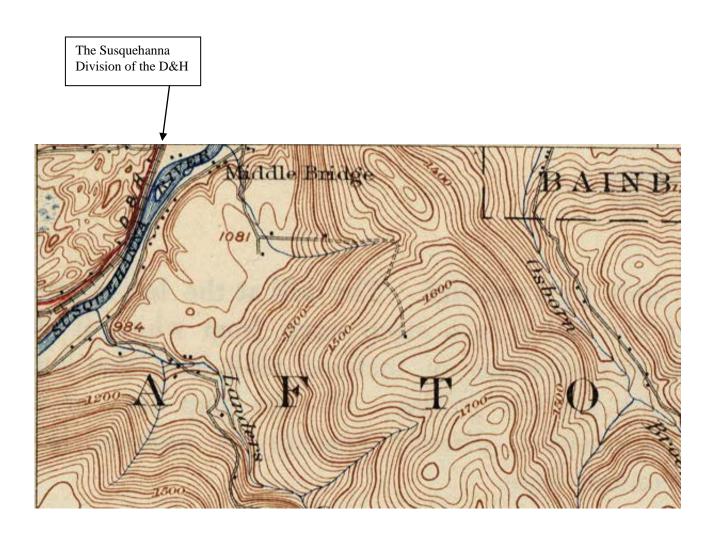
The photograph of the Afton D&H combination passenger and freight station that is shown below is from *Passenger and Freight Stations Delaware & Hudson. The Delaware and Hudson Company / Board Of Managers / Inspection of Lines : : . June 7th to June 10th, 1928, p. 71.*



Middle Bridge

Deposit:

U. S. Geological Survey, *Deposit Quadrangle*, Surveyed in 1924. Topography by J. I. Gayetty, D. H. Watson, and H. A. Bean.



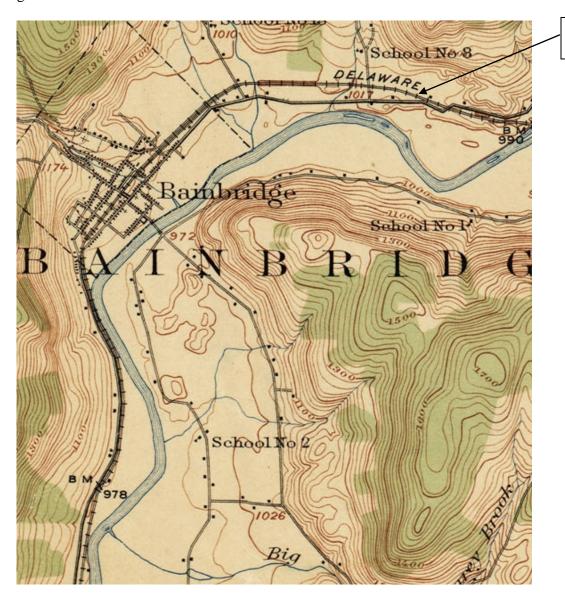
Unadilla:

(1)

U. S. Geological Survey, *Unadilla Quadrangle*, Surveyed in 1915. R. B. Marshall, Chief Geographer; Frank Sutton, Geographer in charge; Topography by Glenn S. Smith, J. I. Gayetty, J. B. Metcalfe, Jr., C. S. Wells, and C. H. Davey. Control by Geo T. Hawkins, E. L. McNair, D. H. Baldwin, C. H. Semper, K. E. Schlachter, and F. L. Shalibo.

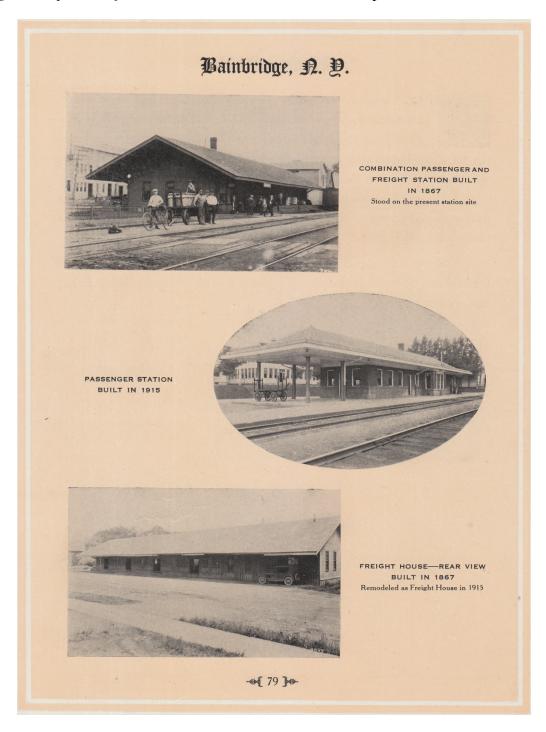
Delaware & Hudson

Bainbridge:



The A&S from Sydney to Bainbridge was completed and opened to traffic on July 10, 1867.

The photograph of the Bainbridge D&H stations and freight houses is from *Passenger and Freight Stations Delaware & Hudson. The Delaware and Hudson Company / Board Of Managers / Inspection of Lines : : . June 7th to June 10th, 1928, p. 79.*



Ralph A. Mead: In the 1870s and 1880s, hundreds of wandering train and engine service men, called "boomers," went from railroad to railroad, remaining with a road only as long as business was brisk. Richard A. Mead was a boomer who switched back and forth between the Erie and the D&H six times in nine years, returning to the D&H on January 1, 1887, and staying with the D&H for the following 47 years.

In his biographical portrait in *The Delaware and Hudson Railroad Bulletin* ("Double-tracking the A. & S.," March 1, 1937, pp. 35-36), we read the following about the first double-track on the A. & S.:

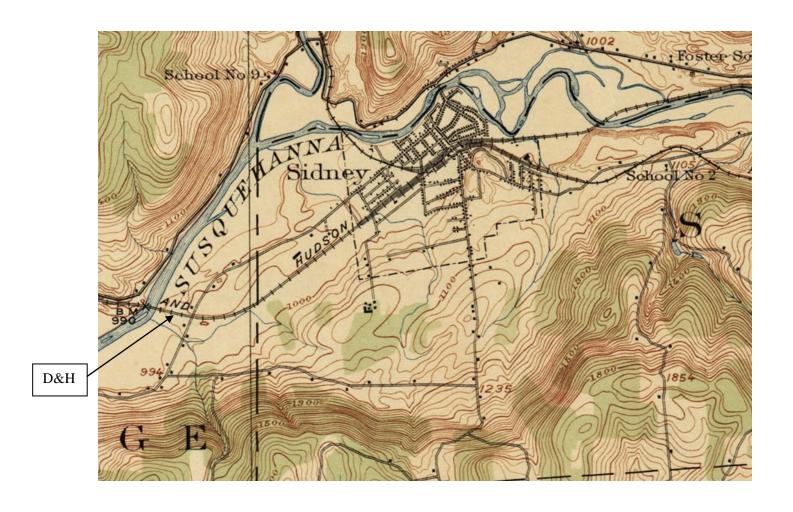
"His [Ralph A. Mead] previous railroading experience gained him promotion to the rank of conductor in the remarkably short time of four months after entering the company's employ. The first double track on the division was then being laid between Afton and Bainbridge and Mr. Mead was placed in charge of the gravel train used on that project. Lacking power shovels and other equipment, all the work of making cuts and fills was done by a gang of from 80 to 100 laborers equipped with picks and shovels. In making fills, dirt from the nearest cut or gravel bank was loaded on the string of ten small flat cars, hauled to the cut, and shoveled off, all by slow manual labor. On a particularly good day they might finish a ten-car length of track; at other times they spent several days in one spot. Later that year his train was moved to Sidney, where they worked for three months, then to Oneonta, where the double-tracking was continued toward Colliers until halted by cold weather." (pp. 35-36)

Bainbridge NY Freight Station. Station now torn down. Photo posted on *Facebook* on February 12, 2016 by Dennis Gohl:



(2)

Sidney:

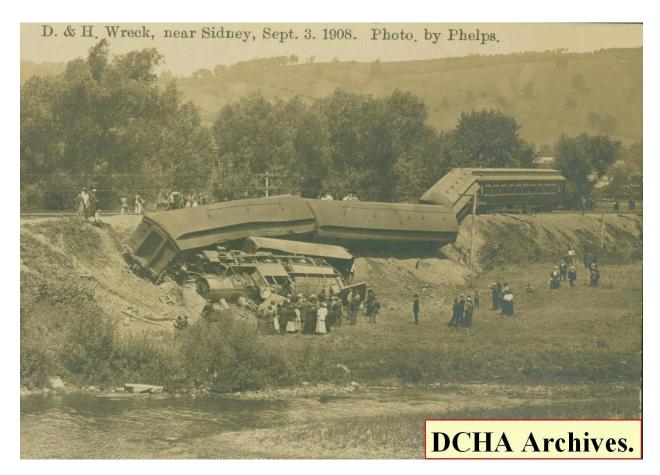


The A&S from Unadilla to Sidney was completed and opened to the public on October 22, 1866.

The photographs of the Sidney D&H passenger stations and freight buildings that are given below are from *Passenger and Freight Stations Delaware & Hudson. The Delaware and Hudson Company / Board Of Managers / Inspection of Lines : : . June 7th to June 10th, 1928, p. 81.*



The photo by Phelps given below ("D. & H. Wreck, near Sidney, Sept. 3 1908") was posted on *Facebook* on November 21, 2015 by the Delaware County (NY) Historical Association:

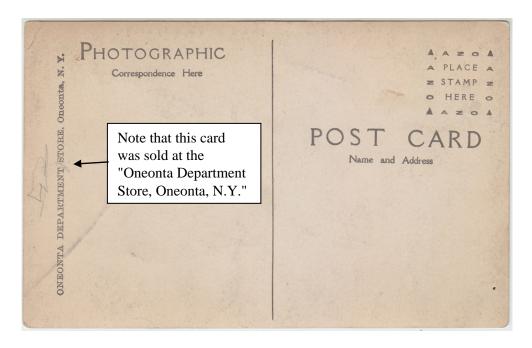


"D. & H. Wreck, near Sidney, Sept. 3, 1908."

Sidney was first settled by Timothy Beach of Fairfield County, Connecticut, who at the close of the Revolutionary War proceeded up the Hudson River to Catskill. Beach named the place after his son. Sidney was a junction point of the Delaware and Hudson and the Ontario and Western Railroads.

Another post card view of the same wreck, near Sidney, on September 3, 1908. This photo is also by Phelps. This post card is in the collection of the Carbondale D&H Transportation Museum.





John D. Salsbury, Car Inspector:

On January 15, 1887, John Salsbury was hired at Sidney, NY, by William Barker, Car Foreman, as a Car Inspector. In his biographical portrait ("Inspected Our First Steel Cars") that was published (pp. 131-132) in the May 1, 1929 issue of *The Delaware and Hudson Company Bulletin*, Salsbury remembered the car shop at Sidney at the time as follows:

"There were fifteen men in the department at Sidney in that early day, the heavy car repair work being done on a single cripple track, and the rest in the transportation yard. The shop itself stood just south of the passenger station and freight house, which was adjacent to the sidewalk of Main Street. When the new station was built south of the original buildings, the car shop was moved down on the other side of the track." (p. 132)

As a car inspector, Salsbury was called upon to inspect the many new cars, with steel underframes that were purchased by the D&H from the American Car and Foundry at Berwick, PA during 1906 and 1907. In Salsbury's biographical portrait we read:

"With the advent of the air brakes and other devices on both passenger and freight cars, the old wooden underframe cars became obsolete and the company began to replace them with cars with steel underframes. During the years 1906 and 1907, 1,420 cars were purchased from the American Car and Foundry at Berwick, Pa. This number included stock, automobile, gondola, produce, and refrigerator cars. The refrigerator car trucks and frames were then sent to Chicago where the body structure work was completed. During the time of construction of these cars, Mr. Salsbury remained at Berwick to inspect the cars as they were turned out by the manufacturer. He was away for fourteen months at one time and for a shorter period when a subsequent order was being filled." (p. 132.)

Salsbury continued to work in car inspection until his retirement became effective on June 1, 1927.

October 18, 1922 wreck at Sidney, NY:

"Collision on D. & H. RR. Oct. 18, 1922 Photo by H. (?) C. (?) Earl Sidney, NY"

Photo in the collection of the Carbondale D&H Transportation Museum.



Written on note that accompanies photograph:

"Southbound train was rammed / Head-on collision on D&H RR / Oct 18, 1922 / Sidney, N. Y./ Myron Downs saw freight coming - / was putting freight on Sidney south / Firemen Ralph Kennedy / Arthur Stevenson / engineer on N. bound killed / firemen jumped, only bruised / Courtesy of / Lester Dix / C'dale"

See photo note on the following page from Lester Dix.

Here is the note from Lester Dix that accompanies this photograph in the collection of the Carbondale D&H Transportation Museum:

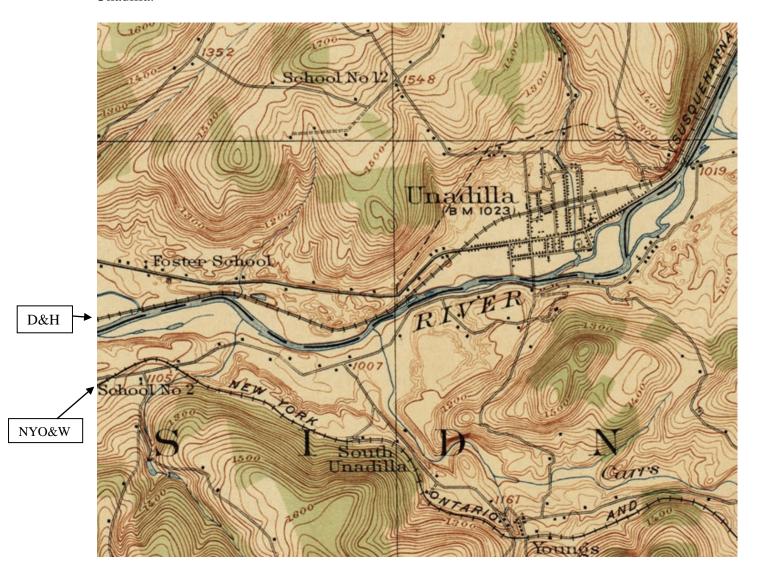
Southbound toin with remned Head-on collision on D+H RR Oct 18, 1922 Sidney, N.y nuxon Downs san freight coming was putting freight or Sidney suite frene Relph Kennedy Arthur Stevenson engueer Nound Willed fremen jumped, only buisel Courtes of Caster Dix Calle Two more photos, in the collection of the Carbondale D&H Transportation Museum, of the Sidney head-on collision on October 18, 1922.





(3)

Unadilla:



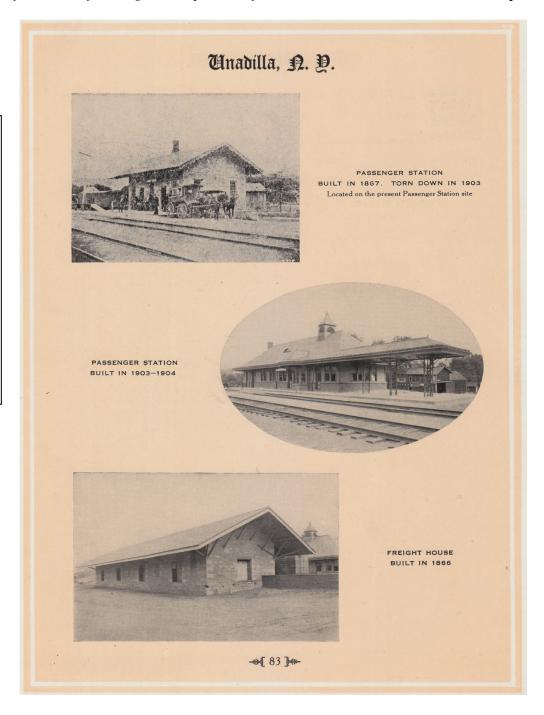
The A&S from Otego to Unadilla was completed and opened to traffic on March 21, 1866.

The name *Unadilla* is derived from an Iroquois word for "meeting place". In 1863, one of the thirteen Directors of the A&S was from Unadilla: Arnold B. Watson.

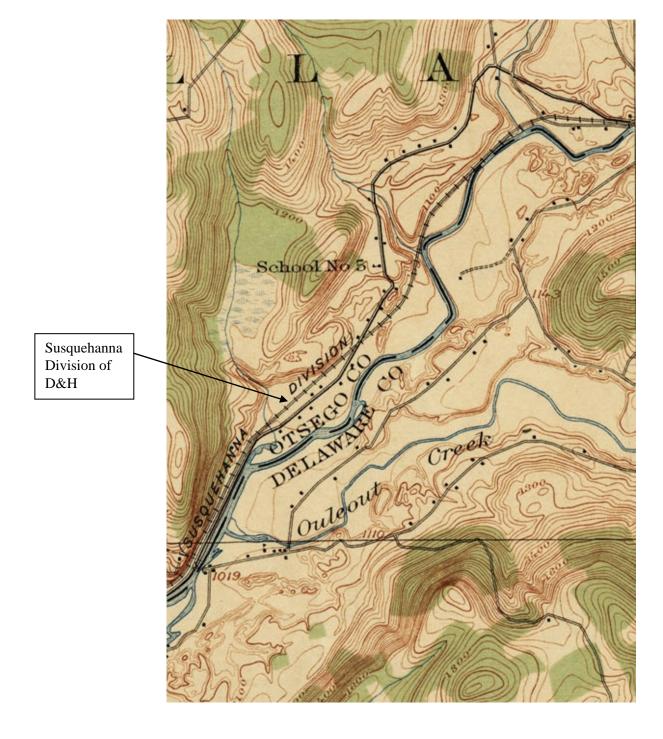
In 1863, one of the thirteen Directors of the A&S was from Milford: Jared Goodyear. Milford, NY was formed from the Town of Unadilla in 1796.

The photographs of the Unadilla D&H passenger stations and freight house that are shown below are from *Passenger and Freight Stations Delaware & Hudson. The Delaware and Hudson Company / Board Of Managers / Inspection of Lines : : . June 7th to June 10th, 1928, p. 83.*

Unadilla: The growth of the settlement was due to the construction in 1802 of the Catskill and Susquehanna Turnpike, which terminated at "Wattles Ferry," named after Sluman Wattles, one of the first settlers.

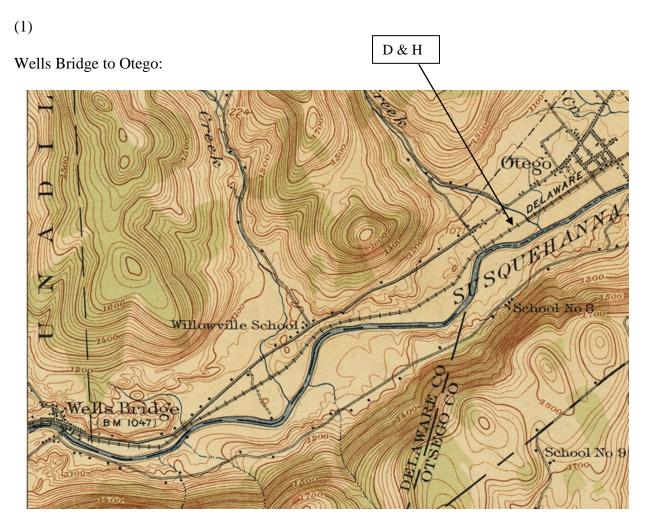


School No. 5 area:



Oneonta:

U. S. Geological Survey, *Oneonta Quadrangle*, Surveyed in 1915. R. B. Marshall, Chief Geographer; Frank Sutton, Geographer in charge; Topography by J. M. Whitman, J. B. Metcalfe, Jr., and C. H. Davey. Control by W. T. Griswold, Geo. T. Hawkins, E. L. McNair, D. H. Baldwin, K. E. Schlachter, and F. L. Shalibo.



The A&S from Oneonta to Otego was completed and opened for traffic on January 23, 1866.

Colonel Coryell:

At the age of 13, James Downs ("Racing a Runaway Freight," *The Delaware and Hudson Railroad Bulletin*, March 1, 1936, pp. 35-36) began his 63-year railroading career by carrying water for the 12-member section gang of the A&S at Otego of which his father was foreman.

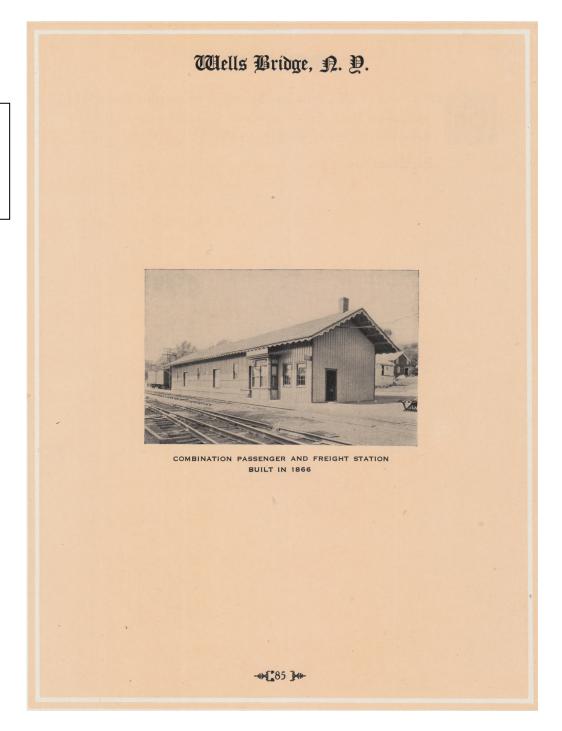
"Mr. Downs recalls, with a smile, his first pay day. Col. Coryell, who paid them, had seen service in the Civil War, and sometimes wore his uniform coat on the road. He greatly admired Col. Coryell, a fine soldier and every inch a gentleman." (p. 35)

In late March 1891, a bad accident took place on the D&H about a mile east of Wells Bridge. Fireman Fred Pierce was crushed to death. Here are the details, as reported in the *Carbondale Leader* of March 28, 1891:

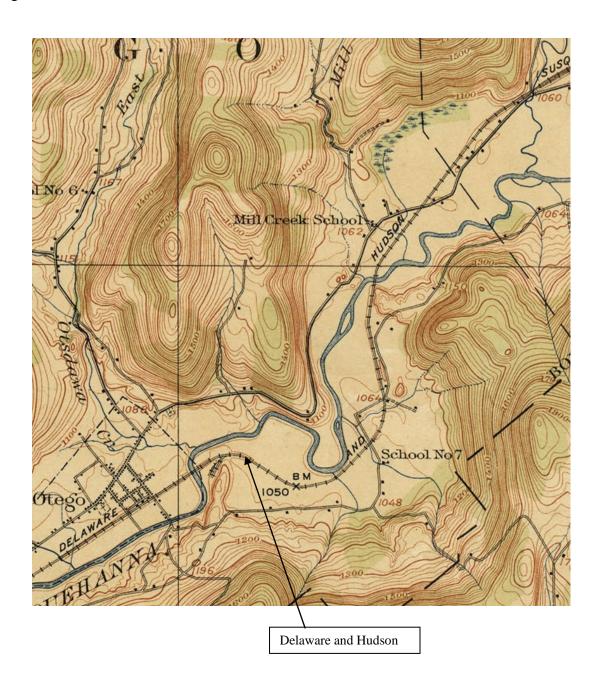
"CRASH ON THE D. & H. / Fireman Fred Pierce Killed and Several Persons Injured. / A Binghamton special says: Passenger train No. 4 on the Delaware and Hudson railroad, which left this city at 12:10 p. m. to-day for Albany, crashed into accommodation train No. 17 about a mile east of Wells Bridge. Conductor Frank Vincent in charge of train 4, had been telegraphed orders to side track his train at Wells Bridge, but for some reason ran by with above results. Engineer Frank White, of train 4, had an arm broken and fireman Fred Pierce was caught between the engine and tender and crushed to death. Engineer William La Grange and fireman Fred Cooper, of train 17, were both injured, the former perhaps fatally. All these men resided in Binghamton, where Pearce leaves a wife and two children. The wreck took fire and several freight cars were burned. No passengers were injured. (Carbondale Leader, March 28, 1891, p. 3)

The photograph of the D&H Wells Bridge combination passenger and freight station that is given below is from *Passenger and Freight Stations Delaware & Hudson. The Delaware and Hudson Company / Board Of Managers / Inspection of Lines : : . June 7th to June 10th, 1928, p. 85.*

Wells Bridge: The community was formerly known as Sandy Hill.

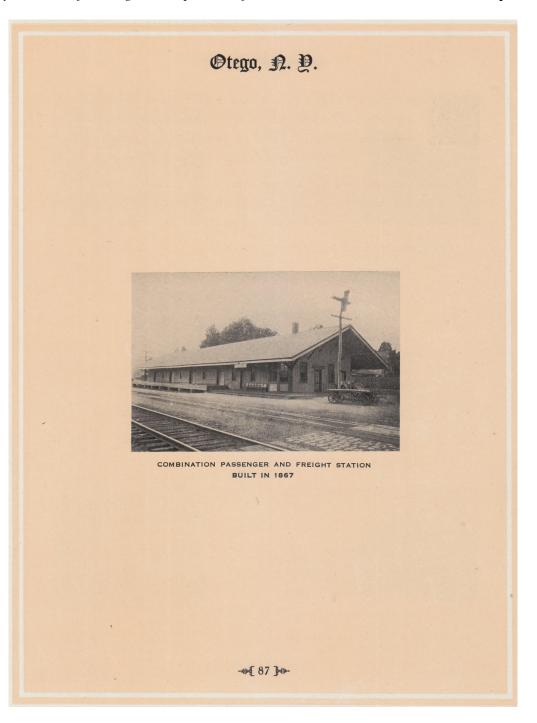


Otego to Mill Creek School:



The photograph of the D&H Otego combination passenger and freight station that is given below is from *Passenger and Freight Stations Delaware & Hudson. The Delaware and Hudson Company / Board Of Managers / Inspection of Lines : : . June 7th to June 10th, 1928, p. 87.*

Otego: Until 1830, Otego was known as Huntsville (named after Ranson Hunt, who, about 1772, settled here with six others on the east branch of the Otsdawa, near the mouth of the Otego Creek).

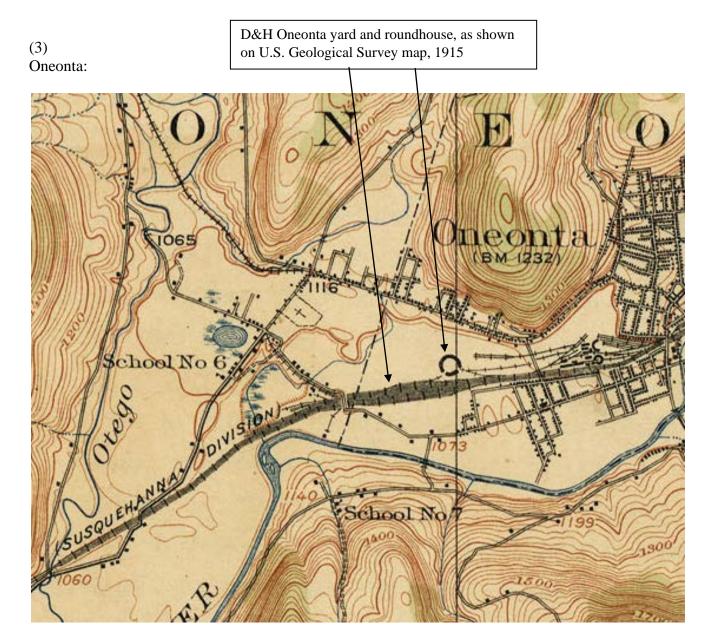


Orrin A. Rogers:

Born at Unadilla, October 2, 1863, Orrin A. Rogers began working for the D&H in June 1874 in a section gang that was responsible for the maintenance of five miles of single track, laid with 44-pound iron rail. "At that time," we read in his biographical portrait in the December 1, 1936 issue of *The Delaware and Hudson Railroad Bulletin* (pp. 179-180), "they had neither tie-plates nor rail joint plates: at the end of each 20-foot length of rail were four slots to receive the spikes intended to fasten the rail and keep it from creeping. Despite their best efforts the resulting track was very rough and an approaching train set up such a clatter that when running at 15 miles per hour it could be heard for a mile and sounded as if it was making a mile a minute. . . Because of the track's condition, the section gang had two trackwalkers, each working a twelve-hour shift. Beginning in 1877, Mr. Rogers was put on the night patrol, making two round trips nightly over the five-mile section, a total of 20 miles. Equipped with a sperm oil lantern, an axe, a pocketful of spikes, and a hammer, he made his rounds, frequently discovering broken or spread rails." (p. 180)

In 1889 Orrin Rogers was sent to Otego as foreman of the five-mile section which passed through that village. "After three years at Otego, he was sent to Nineveh as foreman of the yard tracks as well as four miles of the main line. By this time he had gained a reputation as a foreman who could take a poor section of track and quickly put it into first class condition. He was therefore chosen, in 1900, for the difficult job of improving conditions in the Wilkes-Barre yard where derailments, due to faulty tracks, were an almost daily occurrence. In addition to maintaining the 16 miles of track in the yard, his gang had to transfer the contents of crippled cars refused by connecting lines at Wilkes-Barre. Frequently there were as many as 70 car-loads of iron alone awaiting transfer at the same time. Despite these handicaps the condition of the yard tracks improved steadily until 1928 and 1931 when his section won prize awards." (p. 180)

On March 1, 1933, after sixty years of service to the D&H, and with one of the longest service records ever made in the D&H Maintenance of Way Department, Orrin Rogers retired on pension.



Oneonta, the City of the Hills, is popularly believed to mean "place of open rocks" in the Mohawk language. In 1863, one of the thirteen Directors of the A&S was from Oneonta: Eliakim R. Ford.

The A&S from Schenevus to Oneonta was completed and opened to traffic on August 28, 1865.

In the biographical portrait of J. G. McKinnon ("Back in the Good Old Days," pp. 271-72, 382) that was published in December 15, 1928 issue of *The Delaware and Hudson Company Bulletin*, we read the following about the arrival of the A&S in Oneonta:

"Mr. McKinnon has faint recollections of the days of the Civil War. His family moved from Sidney to Otego, N. Y., when he was but as small boy. While he was only five years of age when the war began he remembers the 'training days' when men and boys from the surrounding country gathered to drill under the supervision of men who had returned from the battlefields. Some of his parents' friends returned bearing the marks of battle. / Then the railroad appeared. On August 28, 1865, the construction work reached Oneonta and a gala celebration was held when the first official passenger train entered Oneonta. Two arches were constructed over the track, just north of the present site of the passenger station, on which flowers were strung, and as the train passed through the railroad was officially welcomed by the citizens of Oneonta and vicinity. This was a red letter day in the life of a future Delaware and Hudson man. Soon the line had reached and passed his home just south of Otego passenger station."

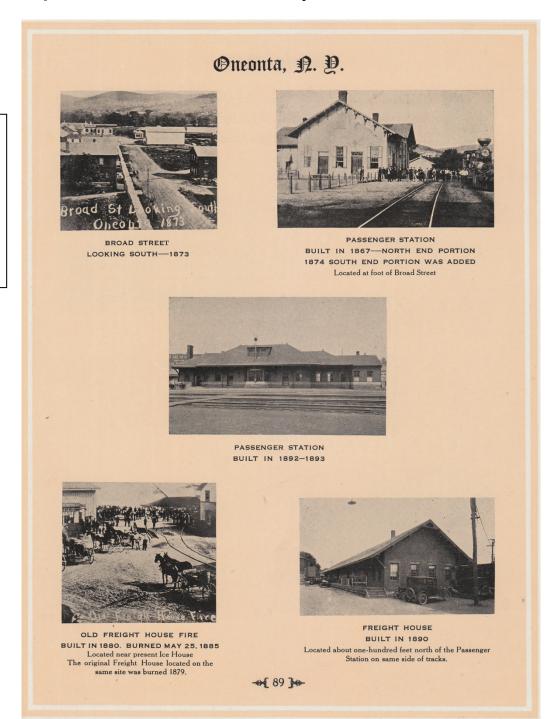
"Oneonta was the principal terminal of the Albany and Susquehanna Railroad." (The Delaware and Hudson Company INSPECTION of LINES:: JUNE 7, 8, 9, 1929, p. 22)

The photographs of the Oneonta D&H buildings shown below are from *Passenger and Freight Stations Delaware & Hudson. The Delaware and Hudson Company / Board Of Managers / Inspection of Lines : : . June 7th to June 10th, 1928, p. 89.*

Oneonta: Incorporated as a town in 1848 and as a city in 1909, the Susquehanna Division offices of the D&H

were located

here.



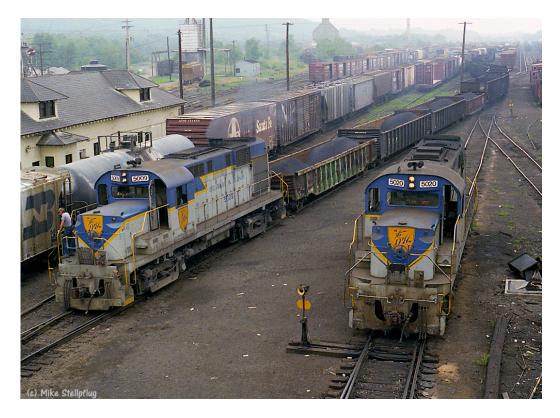


Photo downloaded from *Facebook* of Drew Guild, Delaware and Hudson Railroad, July 22, 2015, Mike Stellpflug photo, with this caption: "Oneonta was the heart of the old D&H. A pair of RS-36's are working the yard in front of the yard office. Sadly, there is almost nothing left in this scene today." *Facebook* comment from Greg Flynn: "Oneonta was the definite center of D&H operations for many years. At one time, it was the location of the first complex of shops on the old A&S, had a 52 stall roundhouse that handled all road engines on the Susquehanna and Penn Divisions, at one time had the largest yard on the D&H with 2 humps, and was the location of the system car shops. Other than miners, probably more D&H employees worked out of Oneonta than any other D&H terminal."

Henry B. Watson and the Machine Shop at Oneonta, circa 1895:

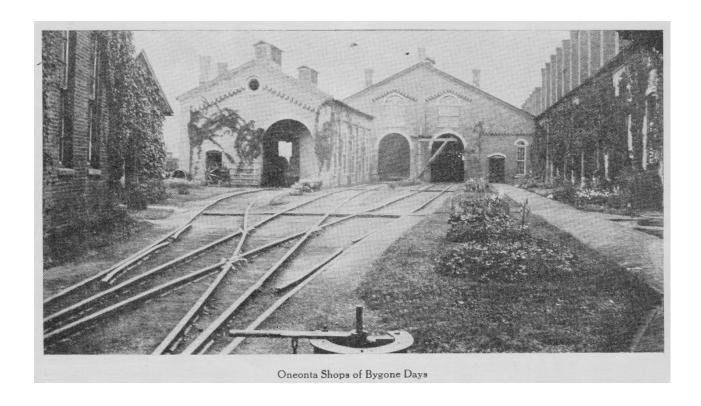
After having worked in several different locomotive shops in America, Henry B. Watson, who was born in Burnley, Lancashire, England on February 21, 1848, began working, in 1895, for the D&H in the machine shop at Oneonta, working as a machinist's helper under Master Mechanic Smith. He then worked continuously for the D&H until his pension became effective in 1922. About that machine shop, we read the following in the biographical portrait of Henry Watson ("Learned His Trade in England") in the April 15, 1929 issue (pp. 115-116, 126) of *The Delaware and Hudson Company Bulletin:*

"At that time the shop was located in what is now [1929] the main car shop at Oneonta. All of the shops of both Car and Motive Power Departments were grouped on the location of the present car shops. Some of the Supervisors at the Oneonta shops during the early part of his term of service were: J. R. Skinner, Master Mechanic; Horace Card, Machine Shop Foreman; J. Reynolds, Tool Room Foreman; D. Down, Foreman Painter; Mr. Wing, Foreman of the Car Department shops; S. Disbrow, Wood Mill Foreman; F. I. Helm, Storekeeper; Mr. Powell, Foreman of Blacksmiths; Mr. Harris, Foreman Boilermaker; James Gerling, Chief Clerk to the Master Mechanic; and Carry Massett, Secretary to Mr. Skinner." (pp. 116, 126)

Given below is a photograph titled "Oneonta Shops of Bygone Days" that accompanies (p. 116) Henry Watson's biographical portrait. About the Machine Shop and the other buildings in this photo, we read the following in Watson's biographical portrait:

"When Mr. Watson came to work in the machine shop, the building in the center of the accompanying illustration, there were no drop pits in the shop. When it was necessary to change or repair driving wheels, the entire engine was picked up, after the binders had been removed, and the wheels remained on the rails. Shortly after this time two drop pits were provided, one at each end of the two entrances to the shop. The Master Mechanic's and other offices were located in the building on the extreme left. The building on the left of the machine shop housed the boiler shop while the machine shop office was located at about the center on the left side of the buildings. South of the office was the tool room, and next to it were various machines, including Mr. Watson's lathe. The extreme southern end of the building contained the coach shop. Across from Mr. Watson were other machines and a door leading to the engine room which housed two stationary engines furnishing power to operate the machinery in the shop." (p. 126)

For identification of all these buildings, see the preceding page.

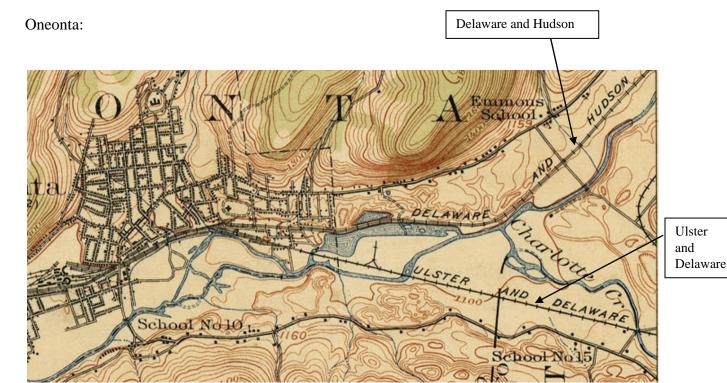


Delaware and Hudson, D-3 No. 506, at Oneonta, NY on October 9, 1947. Photograph in the collection of the Carbondale D&H Transportation Museum.



Delaware and Hudson, D-3 No. 506, at Oneonta, NY on October 9, 1947

(4)



In the biographical portrait ("Telephones Used in 1884," published in the December 1, 1937 issue, pp. 179-180, 182, of *The Delaware and Hudson Railroad Bulletin*) Morey J. Hall, who at the time of his retirement in 1937 was crew dispatcher at Oneonta, we read the following about telephone train dispatching systems on American railroads:

"Telephone train dispatching systems are a relatively recent innovation on American railroads, the first modern installation of this type on The Delaware and Hudson having been placed in service on the Susquehanna Division between Oneonta and Mechanicville in 1918. With this equipment the dispatcher can call any train order station or tower by simply turning the proper lever on a panel before him, the machine automatically ringing the point called. Agents, telegraphers, and towermen call the dispatcher by picking up the receiver and speaking the names of their stations, in this respect following the practice of the old-fashioned 'party-lines' once so common in rural districts in this country." (p. 179)

The Oneonta Yard in 1877:

Don F. Firman began working for the D&H on September 20, 1877, as a trainman at Oneonta. In his biographical portrait ("Stopped Runaway Engine") that was published in the February 1, 1932 issue (pp.35-36) of *The Delaware and Hudson Railroad Bulletin*, we read:

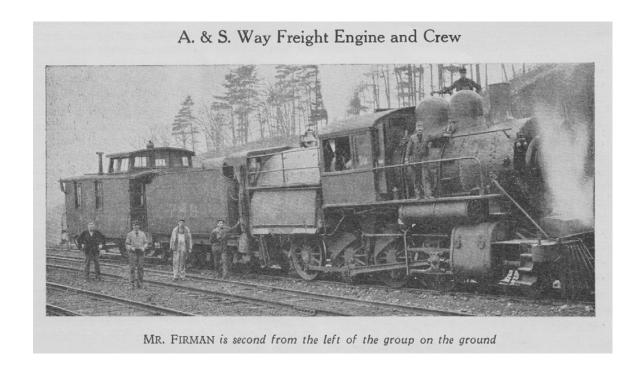
"Superintendent C. D. Hammond gave Mr. Firman his first position on the Delaware and Hudson as a trainman at Oneonta. He saw Mr. Hammond on September 19, 1877, and went to work the following day, making his first trip with engineman M. I. Dimmock on the 88, a Mogul, from Oneonta to Nineveh and return." (p. 36)

The Oneonta Yard and railroading in general at that time are described in Firman's biographical portrait as follows:

"In those days there were only two tracks in the Oneonta yard for southbound and three for northbound cars, in addition to a single freight house track. Yard switch engines were unknown; the crews made up their own trains. The largest cars were eight-wheel low-side gondolas carrying from 10 to 12 tons. Diminutive four-wheel 'Jimmy' cars carried only six tons, and 26 cars made a full train. / The 'Jimmies' had brake handles on the side, the brakes being set by pressing down on the lever. When the brake was set, a ratchet would catch. However, it took a wary brakeman to release the brakes without getting hit by the handle. / Mr. Firman was in charge of the first train to be double-headed northbound out of Oneonta. The regular conductor, when he heard that two engines were to be used, laid off. Although many railroad men doubted the practicability of double-heading, Mr. Firman's crew experienced no difficulty in handling the train. Today double-heading* is the rule rather than the exception. Furthermore, ten modern cars will carry as much as 40 old-time 'Jimmies.' " (p. 36)

* In the biographical portrait of William H. Mickle ("Lonesome Away From Engine") published in the February 15, 1926 issue (pp. 3-4) of *The Delaware and Hudson Company Bulletin* we read: "The practice of 'double-heading' did not come into vogue, to the best of his recollection, until 1878 or 1879."

Here is the photograph that accompanies the biographical portrait of Don Firman that is published in *The Delaware and Hudson Company Bulletin:*



The two roundhouses at Oneonta in the 1880s:

Elmer E. Kerr, a Susquehanna Division engineer, who retired on pension on February 1, 1932, in his biographical portrait ("Locomotives Had Names") in the December 1, 1935 issue of *The Delaware and Hudson Railroad Bulletin* (pp. 179-80) said the following about the two roundhouses at Oneonta in the 1880s: "There were two roundhouses at Oneonta then, one near the main track and another back toward the swamp. To reach the latter they had to go up the main track and back in over a long lead track; later it was arranged so they could run on the table of the big house, cross it, and run over a short connecting track to the other house." (p. 180)

Thomas Monahan, Sr., a Susquehanna Division section foreman who worked at one time or another on practically every section on the Susquehanna Division and who at the time of his retirement on pension on February 1, 1933 was foreman of a section extending from Oneonta two miles north to Emmons Crossing, remembered (biographical portrait, "Sixty Years a Railroader," *The Delaware and Hudson Railroad Bulletin*, March 1, 1934, pp. 35-36) the old Oneonta yard and the two roundhouses there as follows: "When Mr. Monahan went to Oneonta to take over a gang, his section, the entire Oneonta yard, extended from about the site of the new yard office to what is now Fonda Avenue, forming but a small mid-section of the present yard. Aside from the shop tracks and a few sidings, there were no yard tracks north of Fonda Avenue.

Two roundhouses and the car shops stood where only the latter are located today. The two roundhouses were so situated that to reach the second an engine had to be run on the table of the first, turned at right angle, run across an intervening track and over the turntable of the second, thence to the desired stall in the house. Both of these landmarks of the past have been removed since the more modern structure was built some distance south of the car shops." (p. 36)

In 1884, David A. Brimmer began working for the D&H as a wiper in the Oneonta roundhouse under Master Mechanic Thomas Howard. In Brimmer's biographical portrait ("He Wiped the Teekaharaway") published in the August 1, 1930 issue (pp.227-228, 237) of The Delaware and Hudson Railroad Bulletin, we read the following about the Oneonta yard: "At that time there were two roundhouses at Oneonta on the approximate location of the present Car Department repair shops. To get into the 'new' roundhouse, as the one most recently erected was called, the engines had to be run on the turntable of the old roundhouse, make a half turn, and then run on the table of the new house. / When David went to work at Oneonta there were about 100 engines on the Susquehanna Division, each of which had both a name and number. They never ran off their home divisions; Oneonta crews turned at either Binghamton, Nineveh, or Delanson./ . . . The locomotives of that day, most of which were Moguls, (2-6-0 type) would only haul twentytwo cars from Oneonta to Delanson—cars which were scarcely half the size of the ordinary rolling stock of today [1930]. Some of the locomotives then stationed at Oneonta were the Binghamton, Phillip Hone, John Wentz, John Westover, Albany, Oneonta, Otego, Unadilla, Schoharie, Richmondville, Maryland, C. D. Hammond, R. C. Blackall, and several with Indian names including the famous *Teekaharaway*." (p. 227-228)

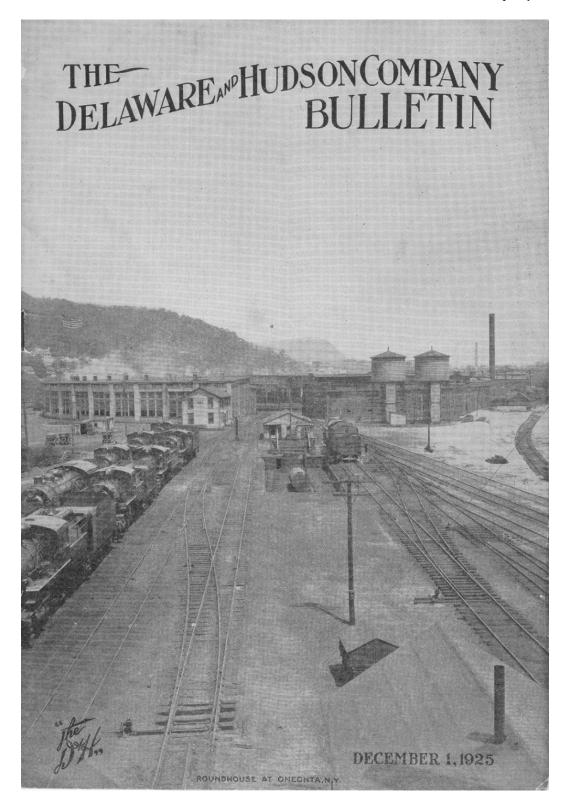
The duties of a locomotive wiper in the 1880s are described in Brimmer's biographical portrait as follows: "The life of a wiper was anything but pleasant in that day of the brass covered locomotive. He had to fill the sand boxes, fill and polish the kerosene headlight, and, 'shove down' the fire. By this is meant that, when a locomotive came into the house, half of the fire was pulled out, leaving just enough to keep steam on the engine. Then, when the locomotive was marked for its run, he would spread it out again and get the steam up on it so it would be ready to leave the house when the engine crew arrived. / Each wiper was equipped with a wad of white waste and a can of kerosene oil. The oldest wiper cleaned and polished the bright work—the guides, cylinders, and steam chest. The other wipers, ranging downward in order of seniority, were placed three on each side, and one underneath. Each of the wipers on the sides wiped one pair of drivers and the parts over them up to the running board. The man underneath cleaned the eccentrics, the inside of the wheels, the tank trucks, and the pilot. When the wipers had finished the foreman came around to inspect their work. Not infrequently he put his hand behind the wheels to make sure they were clean. If he found any black grease or dust the entire engine had to be wiped over again. The wipers took care of only those parts below the running board; the fireman cleaned the upper half of the boiler and the other parts on top. For this work the wipers received \$31 per month for twelve hours a day, with one day off a month." (pp. 227-228)

Oneonta Yard In 1884

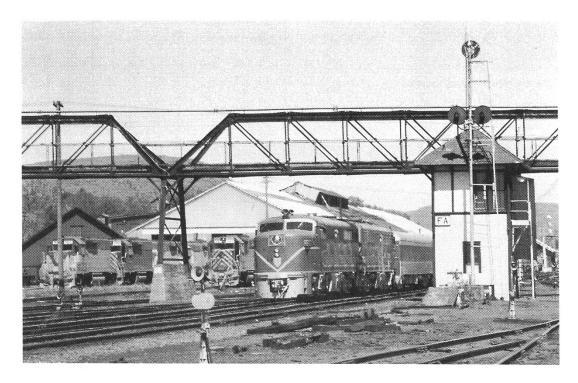


COURTESY CHARLES R. HARP, RETIRED CONDUCTOR

Oneonta Roundhouse, cover of December 1, 1925 The Delaware and Hudson Company Bulletin.



The photo of the Oneonta yard given below is reproduced here from the February 2016 issue of the *Bridge Line Historical Society Bulletin*, p. 21. Here is the caption given there on this photo: "Two of the four D&H PA's roll past Oneonta FA tower with an inspection train (or maybe a fan excursion) on June 4, 1977. Photo by Roger Pugh, courtesy of Ben Martin."



"Oneonta's New Yard Office," *The Delaware and Hudson Company Bulletin*, April 15, 1927, pp. 119, 124-125.

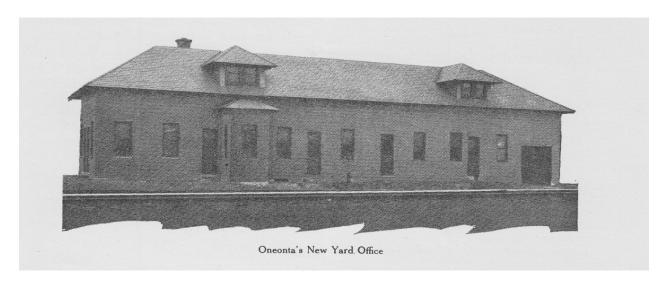


Photo of the Oneonta Yard office staff in 1927 (p. 125 of the article referenced on the preceding page):



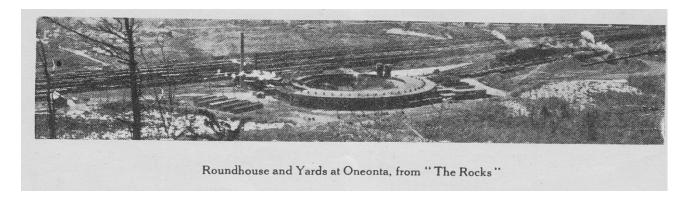
The Happy Force

Front row (left to right)—Wallace A. Mahon, day yardmaster (north bound); Frank J. Nolan, record clerk; Lee L. Firman, location clerk; Roscoe Burdick, bill clerk; Edward J. Stack, yardmaster (Hog's Back); John W. Nolan, general yardmaster; S. Howard Sexton, night yardmaster (Hog's Back).

Second row (left to right)—J. Floyd Baldwin, day yardmaster (Fonda Avenue); Willard H. James, night general yardmaster; Joseph P. Gibbons, clerk; Harlow Bender, bill clerk; Carlos V. Perry, day yardmaster (south bound); and Fred Sinstack and Walter Perry, car checkers.

Back row (left to right)—James D. Johnson, night yardmaster (south bound); Floyd Cummings, reconsigning clerk; Herbert W. Schoonmaker, bill clerk; J. Henry Martial, telegrapher; Louis S. Darling, bill clerk; Kenneth D. Shaw, car checker; Winfred E. Brown, chief clerk; J. F. Boland, night yardmaster (Fonda Avenue); Thomas F. Buston, night yardmaster (north bound); Hammond Parish and Edward Chappell, bill clerks; and Jesse F. Collins, car checker.

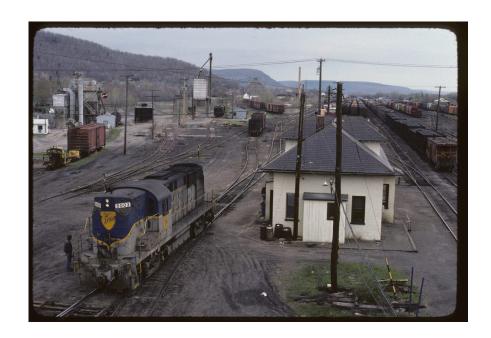
The Delaware and Hudson Company Bulletin, July 1, 1927, p. 210:

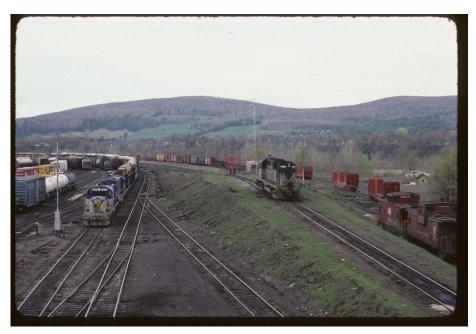


Oneonta D&H Coach Shop: Frank Clark, who served as the Oneonta Coach Shop foreman for 27 years prior to his retirement on pension, June 1, 1936, worked, when a boy, as a farm hand in the Schoharie Valley on hops farms. In his biographical portrait in *The Delaware and Hudson Railroad Bulletin* ("Passenger Car Expert," November 1, 1936, pp. 163-64) we read:

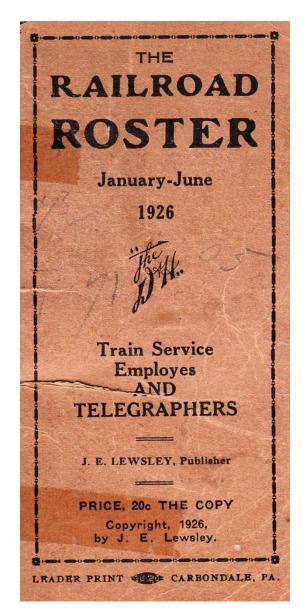
"There was plenty of work for farm hands in the Schoharie Valley during the seventies and eighties. When Frank could be spared at home he hired out to plant, hoe, or hill corn; or to grubhoe, plow, hill, or train hop vines. Hops, it may be explained, were 'trained' to grow to the top of 20-foot spruce, oak, cedar, or tamarack poles, as well as along twine strung between the poles which stood 4 feet apart each way. In September when the hops—leaf covered, burr-like growths—formed at the top of the vines, they were picked, dried, and packed in 200-pound bales. They were then sold to buyers who canvassed the territory each fall, the local farmers delivering them to the station of the then very busy Schoharie Valley Railroad. Later on the vines were cut from the poles and were burned as they were of no further value, while the poles were pulled up and stacked for the winter. All these operations gave work to many laborers in the hops field during the spring, summer, and fall months."

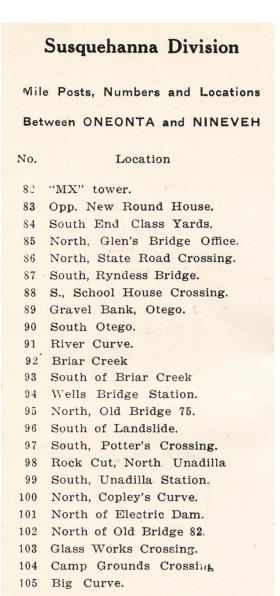
Two photos, downloaded from *Facebook* on February 3, 2015, of Oneonta yard from Richard's Crossing Bridge, taken after 1975:



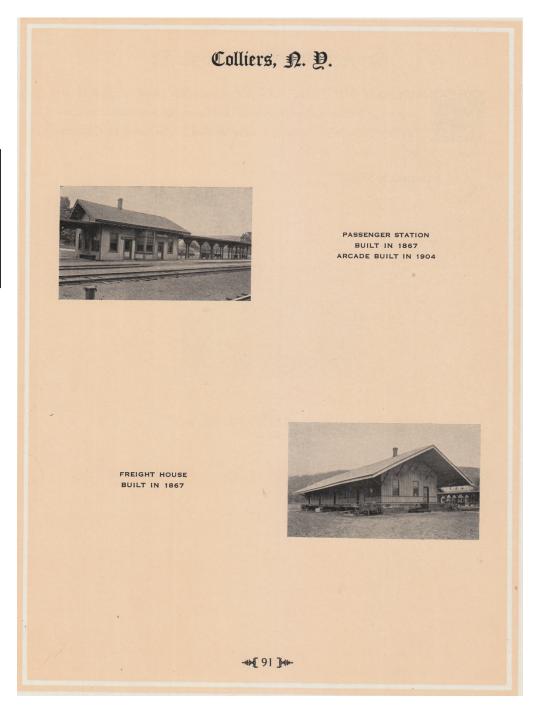


On May 11, 2016, Janet Isger (7900 Quigley Road, Union Dale, PA 18470) donated a copy of "The Railroad Roster /January-June 1926 / The D&H Train Service Employees and TLEGRAPHERS," J. E. Lewsley, Publisher) to the Carbondale D&H Transportation Museum. Her grandfather, Ernest Enslin, and both of her husband Robert's grandfathers (Joseph Isger and Leonard Besecker) were all D&H engineers. She also donated to the museum a D&H VETERAN lapel pin that belonged to Leonard Besecker (lived at 26 Sand Street, Carbondale, phone 1236J). Included in that January-June 1926 Roster is a list of the mile posts on the Susquehanna Division between Oneonta and Nineveh. The title page of that 1926 roster and that list are shown below:





The photographs of the Colliers D&H passenger station and freight house that are shown below are from *Passenger and Freight Stations Delaware & Hudson. The Delaware and Hudson Company / Board Of Managers / Inspection of Lines : : . June 7th to June 10th, 1928, p. 91.*



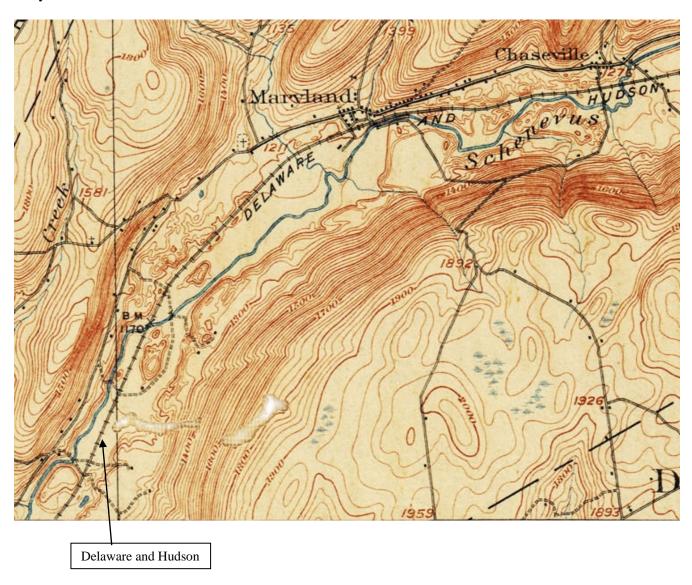
Colliers is the station name for the village of Colliersville, which was settled in 1775 by Isaac Collier.

Cooperstown

U. S. Geological Survey, *Cooperstown Quadrangle*, Surveyed in 1905-1907. Frank Sutton, Geographer in charge; Topography by Chas. E. Cooke; Control by E. L. McNair and J. H. Jennings.

(1)

Maryland / Chaseville:



The photograph of the D&H combination passenger and freight station at Maryland that is shown below is from *Passenger and Freight Stations Delaware & Hudson. The Delaware and Hudson Company / Board Of Managers / Inspection of Lines : : . June 7th to June 10th, 1928, p. 105.*

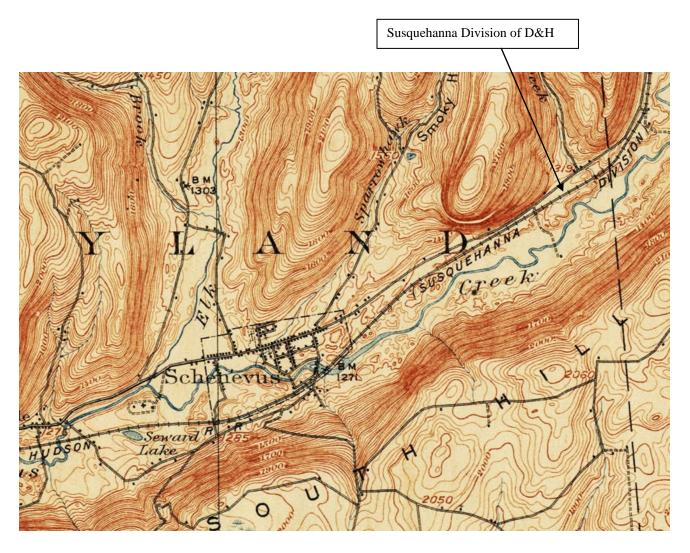
Maryland: This township was formed from the township of Worcester in 1808. It was settled by Elisha Chamberlain and the three brothers Israel, Eliphas, and Phineas Spencer in

1771.



(2)

Schenevus:

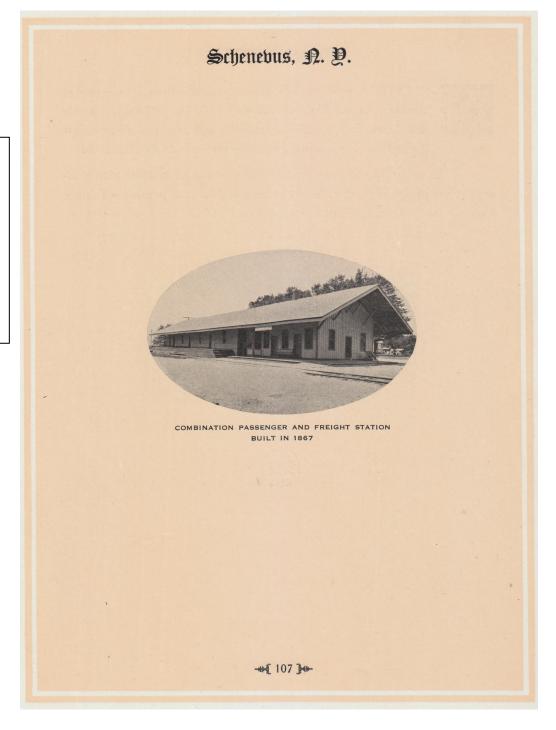


The A&S from Worcester to Schenevus was completed and opened on August 7, 1865.

Schenevus, meaning "speckled fish," is a community rich in Indian lore. Chief Schenevus and his daughter, Manaho, are an important part of its legendary heritage.

The photograph of the D&H Schenevus combination and freight station that is given below is from Passenger and Freight Stations Delaware & Hudson. The Delaware and Hudson Company / Board Of Managers / Inspection of Lines::. June 7th to June 10th, 1928, p. 107.

Schenevus was first settled in 1791. For some time prior to 1840 it was known as Jacksonboro, in honor of President Andrew Jackson. About 1840, Jacksonboro was renamed Schenevus.



In the biographical portrait of Abram D. Cuck ("Would Like to Run Again") published in the July 15, 1930 issue (pp. 21-212) of *The Delaware and Hudson Railroad Bulletin*, Abram D. Cuck is quoted as saying:

"The old A. & S. is home to me; I helped lay the rail when the line was double-tracked from Colliers north to Schenevus, and for 42 years thereafter I worked in the train service out of Oneonta." (p. 211)

In Cuck's biographical portrait, we read:

"Colliers was the center of great activity in the eighties and nineties when the Delaware and Hudson main line was double-tracked, and the Cooperstown and Charlotte Valley Railroad Company, which had leased the Cooperstown and Susquehanna Valley, constructed a road from the 'Hemlocks' eastward to Davenport Center, to connect with the Ulster and Delaware, four and one-quarter miles distant. During the construction of this extension Mr. Cuck was employed by its builders. The line was abandoned some years ago after the Ulster and Delaware was extended into Oneonta." (pp. 211-212)

In 1920 a contract was let to construct a cutoff about thirteen miles in length from Schenevus to East Worcester in order (1) to reduce the grade (from .8% to .5%) between those two points, and (2) to construct a third running track for northward traffic. The work was completed on January 8, 1922, at a cost of \$1,646,368. (The Delaware and Hudson Company BOARD of MANAGERS INSPECTION of LINES: JUNE 7, 8, and 9, 1929, p 20):

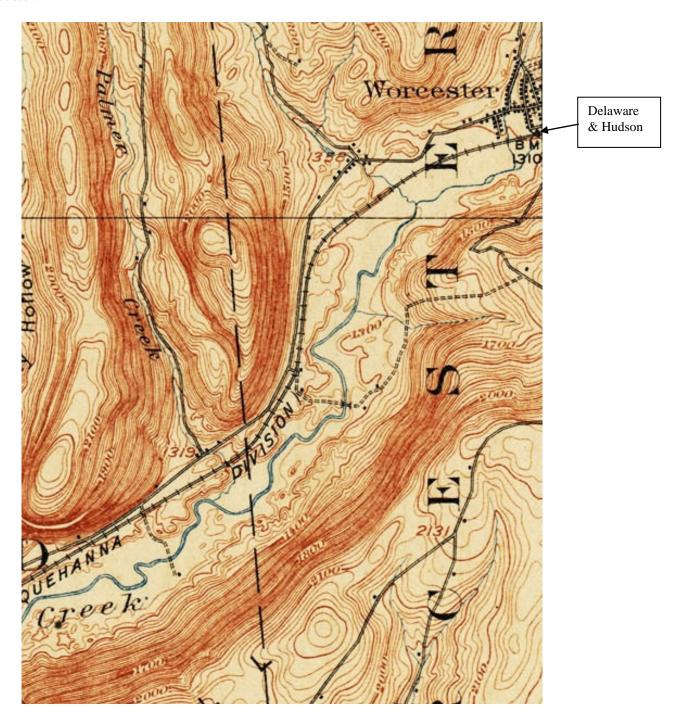
"Other locations where grade reductions have taken place are—Cooperstown Junction to Schenevus, N. Y., nine mile, in the year 1917 grade reduced form a maximum of .8% to .5%. Near East Worcester, 1.05 miles, in the year 1927, from a maximum of .8 to .5%. Between Cobleskill and Barnerville, two miles, in the year 1922, from a maximum of .85% to .5%. The cost of these reductions was upwards of \$300,000." (*The Delaware and Hudson Company BOARD of MANAGERS INSPECTION of LINES*: : JUNE 7, 8, and 9, 1929, p. 20)

John Griswold submitted the following invoice for \$7.00 to the A&S for two months' rent for an engineering office in Schenevus during the construction of the A&S. Here is that invoice:

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Invoice donated to the Carbondale Historical Society by John V. Buberniak.

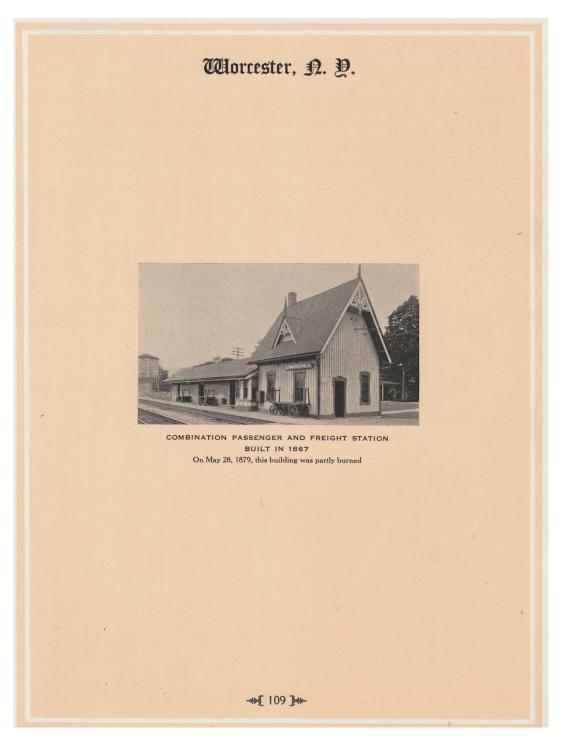
Worcester:



In 1863, one of the thirteen Directors of the A&S was from Worcester: John Cook.

The photograph of the Worcester D&H combination passenger and freight station shown below is from *Passenger and Freight Stations Delaware & Hudson. The Delaware and Hudson Company / Board Of Managers / Inspection of Lines : : . June 7th to June 10th, 1928, p. 109.*

Worcester township was originally settled in 1740. During the Revolutionary War the settlement was abandoned. The township was resettled in 1788 by men from Worcester, MA.



The 1868 school tax bill (\$2.77) to the A&S from the town of Worcester. Invoice donated to the Carbondale D&H Transportation Museum by John V. Buberniak.

	County of Oligo	2	Pr
186	FOR TAXES ASSESSED IN SAID DISTRICT FOR FUEL, REPAIRS OF SCHOOL HOUSE AND APPURTENANCES, &c., FOR YEAR 186: Dotal valuation of peopeety in district, \$ 18,770	Dollars.	Cei
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	of the ALBANY AND SUSQUEHANNA RAILROAD COMPANY, by HELPS, Treasurer, Tro	the hands	of
	n full for the above account		

Wrecked on the A. & S. R. R. Worcester, N. Y., May 13, 1886. Photographed by J. Clow



Wreck on A&S at Worcester, NY, May 13, 1886. Posted on *Facebook* on February 26, 2016 by Steve Brown. This appears to be a stereocard.

Richmondville

U. S. Geological Survey, Richmondville Quadrangle, Edition of November 1904. H. M. Wilson, Geographer in charge; Control by N. Y. State Survey, J. H. Jennings and Gilbert Young; Topography by C. C. Bassett.

Worcester to East Worcester:

Delaware and Hudson

Wordester to East Worcester:

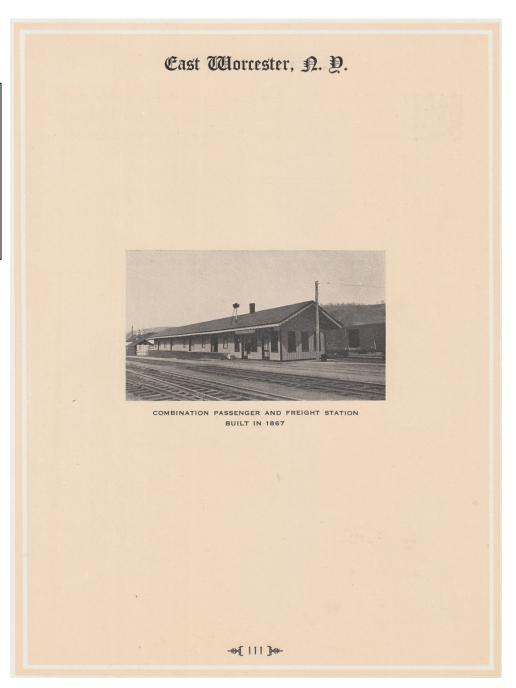
Delaware and Hudson

The A&S from Richmondville to Worcester was completed and opened July 17, 1865.

The photograph of the East Worcester D&H combination passenger and freight station shown below is from *Passenger and Freight Stations Delaware & Hudson. The Delaware and Hudson Company / Board Of Managers / Inspection of Lines : : . June 7th to June 10th, 1928, p. 111.*

East Worcester:

this village was first known as "The Corners". About 1818, Aaron Champion built the first tavern here, known as the Snake Tavern.





Depot Street in East Worcester. Photo posted on Facebook on January 4, 2016 by Steven Brown.



East Worcester, N. Y., Looking North. Post card, posted by Steven Brown on Facebook on January 4, 2016.

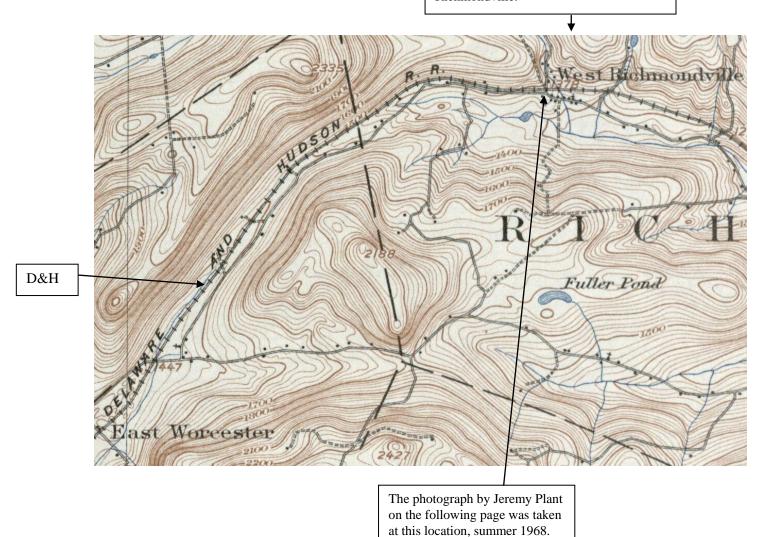


Depot at East Worcester. Photo posted by Steven Brown on Facebook on January 4, 2016.

(2)

East Worcester to West Richmondville:

The steep Richmondville Hill was between Richmondville and the summit at West Richmondville.



The Richmondville Hill grade was 1.31%. This point is on the divide between the Hudson and the Susquehanna watersheds. Gary R. Schermerhorn ("The D&H to Albany—Part One," *BLHS Bulletin*, August 2015, p. 5) says the grade on the Richmondville Hill was 1.35%: "You picture blue and gray Alcos barking furiously up the 1.35% grade out of Cobleskill."

Gary R. Schermerhorn, "The D&H to Albany—Part One," BLHS Bulletin, August 2015, p. 17:

"South of Cobleskill, the single track continues its roller coaster profile as it tackles its most challenging obstacle yet, Richmondville Hill. The ten-mile grueling, twisting 1.4% grade begins immediately past the Cobleskill depot and continues on to West Richmondville, literally cut through the side of the mountain. . . During the D&H period, pushers were used here to assist heavy tonnage moving south over Richmondville Hill."

This wonderful photo was taken by Jeremy Plant and posted on *Facebook* on July 6, 2015. Here is the caption provided by Jeremy Plant: "Summer 1968, a five unit set of Alcos crawling up Richmondville Hill near the West Richmondville grade crossing."

This train is moving West.



Summer 1968, a five unit set of Alcos crawling up Richmondville Hill near the West Richmondville grade crossing. Photo by Jeremy Plant, downloaded from Facebook on July 6, 2015

Clark Thorp:

In the biographical portrait ("Record Clear As a Watchman") of Clark Thorp that is published in the November 1, 1926 issue (pp. 3-4) of *The Delaware and Hudson Company Bulletin*, we read:

"For fifty years and nine months Clark Thorp of Richmondville, N.Y., now retired, worked on our tracks between Esperance and East Worcester, a distance of approximately twenty-one miles, and for forty years of that time was employed either as a gateman or crossing flagman. For more than twenty years he was at the Grand Street crossing in the village of Cobleskill."

He was born on February 14, 1853, and was the oldest son of Cyrus Thorp, a farmer. On April 1, 1874, he began work on the railroad on the section between Central Bridge and Esperance. In his biographical portrait we read:

"Under Tom McGinley, a well known section foreman of that day then assigned to Section No. 7 between Central Bridge and Esperance, he did his first railroad work. . . The road was then but a single track line, newly and, it might be said, poorly laid. As a result, trackmen were confronted by many problems unknown today. The rail, in particular, was a constant source of trouble. It ranged between forty-eight and sixty pounds to the yard in lengths of from twenty-four to twenty-six feet. Under the heat of a summer's sun it would twist as freely as does a snake, while the frost of winter reversed these conditions and left great gaps between the ends of the rails. And it broke easily, too."

These rails on the A&S in the early days were also commented upon by Frederick Miller in his biographical portrait ("Unique Father-Son Record") in the December 1, 1926 issue (pp. 3-4) of *The Delaware and Hudson Company Bulletin*, as follows:

"His experiences with the antics of the light rail used during the earlier days of the road were many and most perplexing. Rail anchors were unknown, and so the rails acquired a habit of creeping that was indeed bothersome. Trains with their brakes set would drag them together until a bad situation would develop at some particular point. It would become necessary then to 'buck' them back in place, with tongs, and not infrequently this slow performance had to be carried on for several miles. On Richmondville hill, in particular, was this embarrassment experienced." (p. 4)

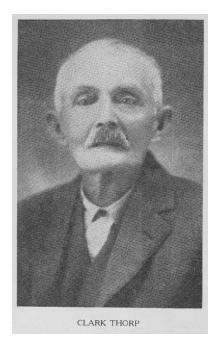
Thomas C. Smith, in his biographical portrait ("Knows Central Bridge History") that was published in the December 1, 1925 issue (pp. 3-4, 12) of *The Delaware and Hudson Company Bulletin* says the following about the early rails on the A&S:

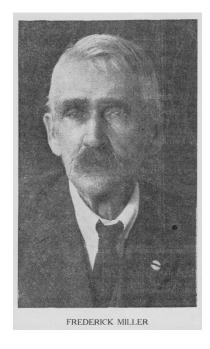
"Considerable of the first rail laid was of English manufacture. Twenty-two years later he had occasion to remove some of this rail, which weighed sixty pounds to the yard, between Esperance and Schoharie Junction and found that it had suffered but very little from the wear of engine and car wheels. That particular rail was manufactured by Brown, Bailey and Dickson of Sheffield. An all-steel rail from the mills of the John A. Griswold Company, was tried at one time but did not prove satisfactory, he says." (p. 12)

Clark Thorp was a member of an unusual repair gang when he was 21. In his biographical portrait (see top of the preceding page for reference), we read:

"In a very interesting way he [Thorp] tells of the loyalty of the men with whom he first worked. He made his home with [Mr. and Mrs. Tom] McGinley [Thorp worked under McGinley at the time], in a house owned by the Company, at Central Bridge. One night the family was awakened by a trainman who came to report a broken rail that was impeding the movement of a freight train. Quickly the repair 'gang'—McGinley, Thorp and Mrs. McGinley—was on the job. Thorp was then only twenty one years old and of rather slight build. A rail was needed and McGinley, the stouter of the two, raised one end to Thorp's shoulder and taking the other end himself they proceeded a quarter of a mile to the scene of the trouble, guided by the light of a lantern carried by Mrs. McGinley, replaced the broken rail, watched the train proceed on its way, and then returned home to resume their broken sleep." (p. 4)

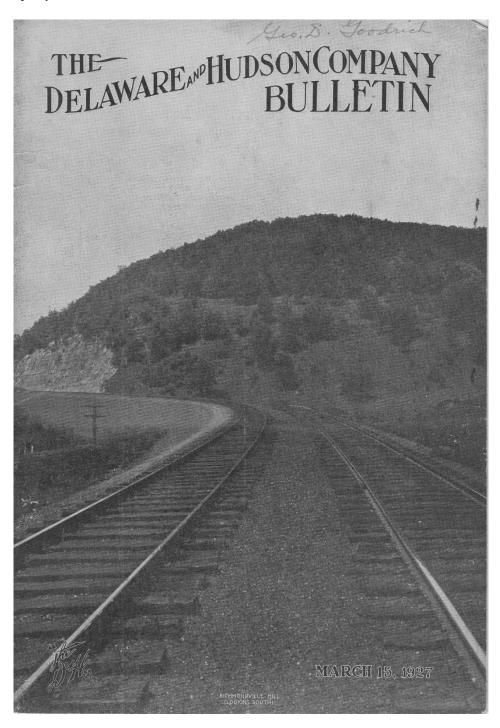
Here are the photographs of Thorp, Miller, and Smith that are presented in their biographical portraits in *The Delaware and Hudson Company Bulletin*:







Richmondville Hill, Looking South. Cover photo of March 15, 1927 issue of The Delaware and Hudson Company Bulletin.



At the time of his retirement from the D&H on January 1, 1935, James J. Conroy ("Dreaded Grade Crossings," *The Delaware and Hudson Railroad Bulletin*, October 1, 1935, pp. 147-148) was asked about his most thrilling experience during his D&H days. In his biographical portrait, we read:

"His most thrilling experience? Roaring down Richmondville Hill on a runaway freight train over 40 years ago. Mr. Conroy was at the throttle of the helper engine when the then-recently adopted air brakes failed to work after they had started down the steep, ten-mile grade. Lead engineer Hall frantically whistled for brakes and although the trainmen strove heroically to halt the train, it had gained such momentum that the fire flew from the screaming wheels without slowing them down. Hurtling down the mountain at terrific speed, the engine truck left the rails on a sharp curve just below Richmondville station and the two locomotives and 21 loaded cars piled up in a mass of twisted wreckage. Fortunately no one was killed; Mr. Conroy suffered two broken ribs which incapacitated him for a month." (p. 148)

Another Richmondville Hill story:

In the biographical portrait of Elmer E. Kerr ("Locomotives Had Names," *The Delaware and Hudson Railroad Bulletin*, December 1, 1935, pp. 179-180), we read:

"Mr. Kerr has an old roster, at the foot of which is recorded one of the most miraculous incidents which ever happened on the A. & S. In 1869 part of a train standing at Richmondville station broke away and raced back down the hill, shot through Cobleskill, negotiating all of the street crossings without hitting a single person or object. It climbed the hill north of the village, finally stopping at the next station, Barnersville, after a runaway trip of over 8 miles."

Posted on *Facebook* on February 15, 2016 by Steven Brown. "A pusher finishing its work at Dante (near West Richmondville)." Comment from Tony Gee: "Is this what would have been the north end of the summit siding? Currently a stub end."



Posted on *Facebook* on February 15, 2016 by Steven Brown, February 15, 2016: "Richmondville Hill." Comment by Michael Eggleston: "I make the number out to be the D&H 536, Class D-3b, 4-6-0 "Ten-Wheeler" with what appears to be a milk train or a local freight with milk cars."



Posted on Facebook on February 15, 2016 by Steven Brown, February 15, 2016: "Richmondville NY July 30 1948 Challenger 1539."



Posted on *Facebook* on February 15, 2016 by Brent Niggli: "Richmondville Summit on the siding" Photo by G. Povall.



The A&S Engineering & Construction invoice shown below, dated June 1864; was donated to the Carbondale D&H Transportation Museum by John V. Buberniak.

Invoices in June 1864, Albany &Susquehanna R. R. Co., Engineering & Construction.

Not known at present which section of the A&S this invoice applies to. The eight men in question were paid a total of \$427.50 for their work in June 1864 in building the A&S.

]	ENGINEERING & CONSTRUCTION
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14.	Water Supply,
	Tools and Shop Machinery, Locomotives,
	Pas. and Baggage Cars,
18.	Freight Cars,
	Gravel and Hand Cars,

The A&S Pay-Roll sheet shown below, dated June 1864; was donated to the Carbondale D&H Transportation Museum by John V. Buberniak.

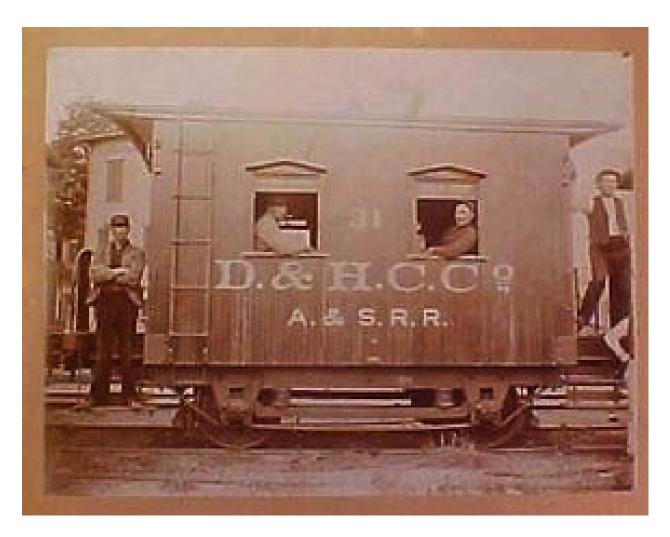
Left side of sheet:

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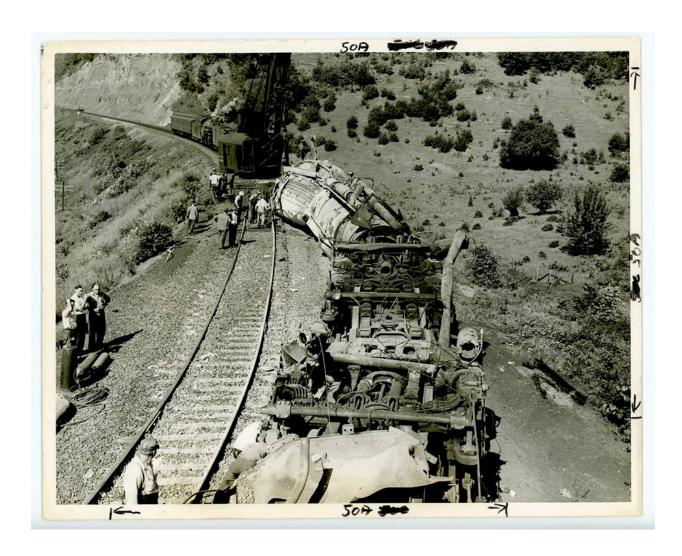
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The photograph of the *D. & H. C. Co. / A. & S. R. R. Caboose No. 31* shown below was offered for sale on E-Bay on January 4, 2016. Our thanks to John V. Buberniak for bringing to our attention this photograph.



D. & H. C. Co. / A. & S. R. R. Caboose No. 31

Posted on *Facebook* on January 29, 2016 by Steven Brown: "July 15, 1941, explosion, Richmondville Hill Challenger 1510 boiler explosion with the note from the back of the photo."



· (12) - 84% 18-45-17

ON JULY 15, 1941 THE TRANQUILITY OF THE VALLEY SOUTH OF COBLESKUL WAS ROCKED BY A VIOLENT EXPLOSION THAT RATTLED WINDOWS AND FRIGHENED WOMEN AND CHILDREN. THE BOILER OF DAH 4-6-6-4 # 1510 HAD BLOWN UP NEAR HENGRY HOLLOW AS IT WAS STARTING UP AKHMONDU, HILL WITH 45 CARS OF BINGHAMTON BOOKD SIMBOL FAEIGHT MB-2. LOW WATER EXPOSED THE CROWN SHEET IN THE BIG 9218 FOOT FIREBOX AND IT CALOPSED WITH A ROAR. EVERY BIT OF WATER IN THE BIG BOILER INSTANTLY TURNED TO STEAM AND WITH THE FORCE OF A MIGHTY ROCKET TORE THE BONGER FROM THE FRAME AS THE THRUST OF THE EXCAPENCE STEAM BLASTED DOWN THROUGHTHE GRATES THE BOILER WAS HURLED OUT OVER THE PILOT 196 FEET DOWN THE TRACKIN FRONT OF THE TRAIN. THE RUNNING GEAR AND TRAIN REPT ON MOVEING CUTHTINE MUMBUTUM IT HAD AT 30 MDH AND CRASHED INTO THE SMOKEBOX OF THE BOILER LAYING ON THE TRACK AHEAD PUSHING THE BOILER 431 FEET ON UP THE TRACK FROM THE EXPLOSION POINT. DERAILING 11 CARS OF THE TRAIN BY THE IMPACT. THE TARGE MEN IN N THE CAB WER INSTANTLY RILLED BEING BLOWN (OVER)

THE CAB WER INSTANTAL HUTTED AS FAR AS FAR AS 130 FEET FROM THE BLAST. ENGINEER CHARLES SMITH, FIREMAN EIBERT PAICE AND HEAD BAAKEMAN CLEARY ALL MET A VIOLENT DEATH IN THE TRAGEDY, TWO MEN WEAR HUALED OUT OF THE CABE COMPLETELY WHILE FIREMAN PIERCES BODY WAS FOUND INSDETHE CAMPLED CAPO THAT WAS HURLED 130 FEG FROM THE SCENE OF THE EXPLOSION. HEAR IN A VIEW FROM THE TRADER THE AUNING GEAR IS SMASHED AGAINST THE OVERTURNED BOILER AHEAD ON THE TRACK WHILE THE ENGONTA WARCKER PAGPARES TO MOVE THE TWINTED BOILER OFFTO THE SIDE. THE 1510 WAS BACK IN SERVICE By DEC 31, 1941 CUTH A NEW BOILER 1281

Our thanks to Steven Brown for sharing on *Facebook* on March 1, 2016 this wonderful photo of the Richmondville, NY D. & H. R. R. Depot.

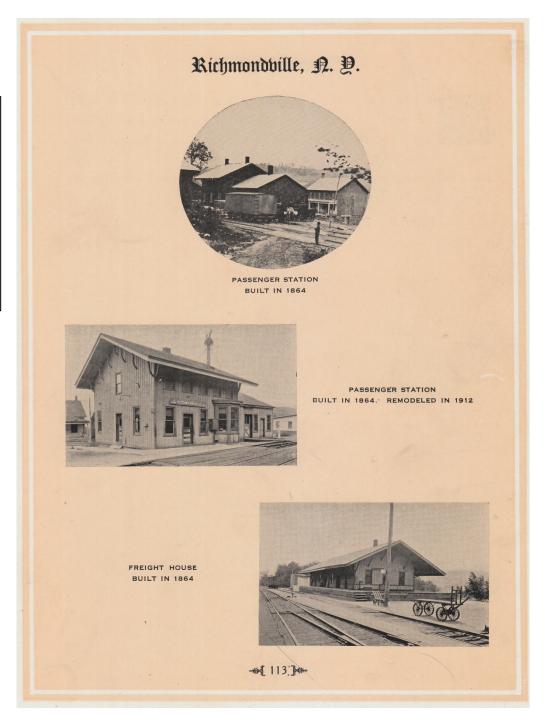
D. & H. R. R. Depot, Richmondville, N. Y.



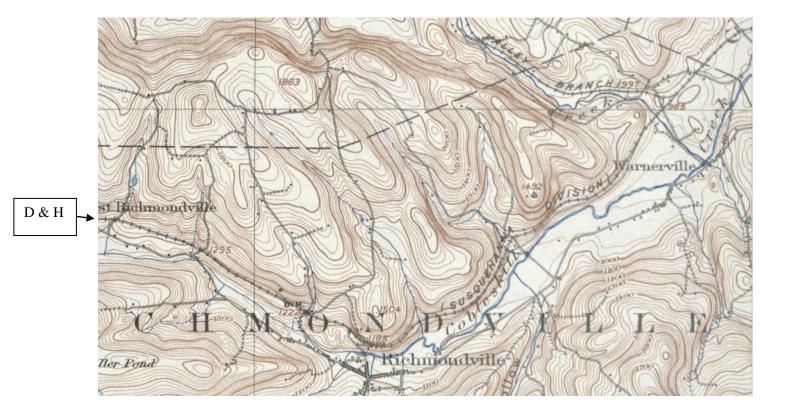
The photograph of the D&H Richmondville passenger stations and freight house shown below are from *Passenger and Freight Stations Delaware & Hudson. The Delaware and Hudson Company / Board Of Managers / Inspection of Lines : : . June 7th to June 10th, 1928, p. 113.*

Richmondville:

The first settlers of Richmond-ville were George Warner and his three sons. George Warner named the place after his native town of Richmond in England.



East Richmondville to Warnerville:

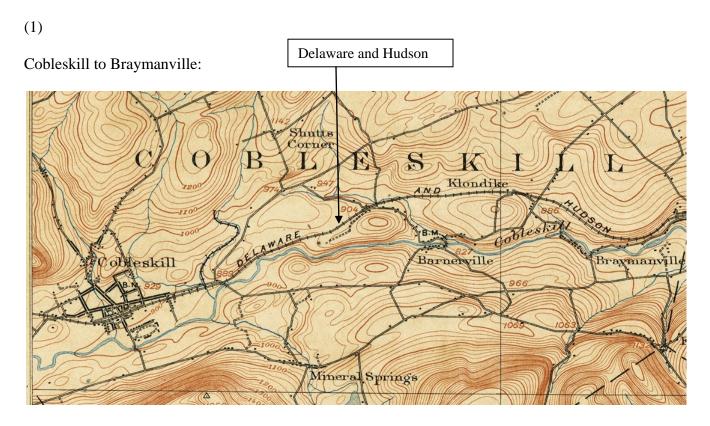


The A&S was completed and opened from Cobleskill to Richmondville on June 1, 1865.

In 1863, one of the thirteen Directors of the A&S was from Richmondville: John Westover.

Schoharie

U. S. Geological Survey, *Schoharie Quadrangle*, Surveyed in 1898. H. M. Wilson, Geographer in charge; Triangulation by N. Y. State Survey; Topography by C. C. Bassett.



The A&S from Central Bridge to Cobleskill was completed and opened on January 2, 1865.

In 1863, one of the thirteen Directors of the A&S was from Cobleskill: Charles Courter.

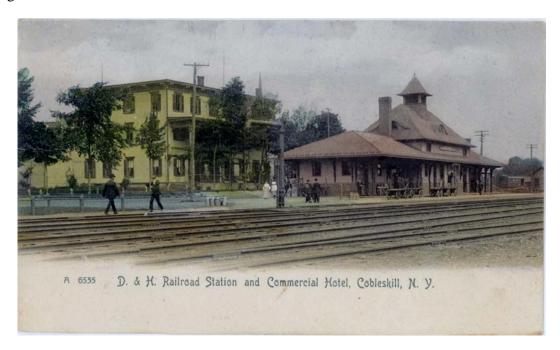
From the biographical portrait of John Bell ("Up the Ladder") that was published in the November 15, 1931 issue (pp. 339-340) of *The Delaware and Hudson Railroad Corporation Bulletin*, we learn two interesting facts about Cobleskill in 1890:

- 1. the Albany way freight and the Oneonta way freight met at Cobleskill
- 2. the crews of those two way freights turned at Cobleskill

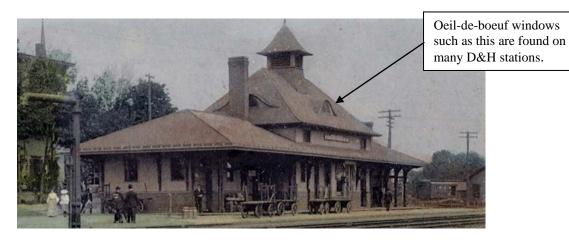
"At that time [1890] the Albany way freight met the Oneonta way freight at Cobleskill at which point both crews turned."

At the time of his retirement on November 1, 1930, John Bell was the second ranking conductor on the Susquehanna Division.

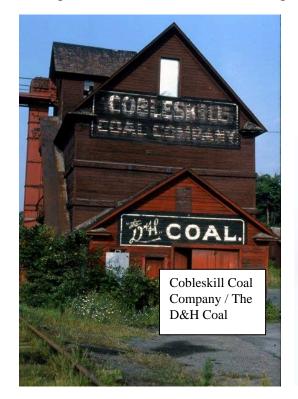
Here is a post card view of the "D. & H. Railroad Station and Commercial Hotel, Cobleskill, N. Y." that was posted on *Facebook* on April 4, 2015 by Steven Brown. Our thanks to him for sharing this fine view of the Cobleskill D&H Station.



Here is a closer look at the Cobleskill D&H Railroad Station:



Three photos of the Cobleskill Coal Company, Inc.:

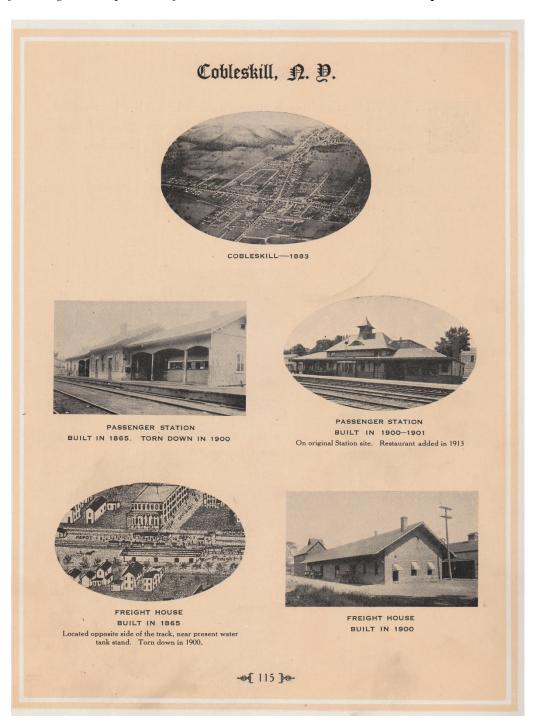


Scatter Tags: The coal companies placed these in the bottom of residential coal bins. When you got down to the scatter tags, it was time to re-order more coal.

Cobleskill Coal Co., Inc.
TELEPHONE:
33



The photos given below of the D&H Cobleskill passenger stations and freight houses are from Passenger and Freight Stations Delaware & Hudson. The Delaware and Hudson Company / Board Of Managers / Inspection of Lines::. June 7th to June 10th, 1928, p. 115.



The town of Cobleskill, which was incorporated in 1868, was named after mill owner Jacob Kobell, plus the word *kil*, which is Dutch for *stream*. Cobleskill Creek, along which hemp grew, was called by the Indians Ots-ga-ra-gee, meaning *hemp creek*.

Wreck at Cobleskill, November 17, 1915:

In Doug Barron's column ("The Receiving Yard") in the February 2016 issue of the *Bridge Line Historical Society's Bulletin*, pp. 19-20, there is the following account of a derailment near Cobleskill on November 17, 1915:

Back in time: Freight derailment rips up roadbed – November 17, 1915 ... A broken rail ditched a northbound extra freight on the D&H railroad near Cobleskill on November 17, and not only piled 27 cars, mostly loaded with coal, in a mass of wreckage, but tore up track for three-quarters of a mile, and the southbound main was pushed into the ditch for 500 feet.

Through traffic was not resumed until 5 o'clock Wednesday afternoon, passengers and mail having to be transferred around the wreck site.

Standing on top of the train when it derailed was Clarence Springer, a trainman from Oneonta. When rescuers sought him in a mass of broken cars, he was found in the midst, protected by the cars themselves, and with only one hand injured. Harry Phillips, another trainman had a shoulder injury.

Altamont Enterprise

"GANG – Signal #71 Nov. 11 1941 Cobleskill NY" Photo in the collection of the Carbondale D&H Transportation Museum.



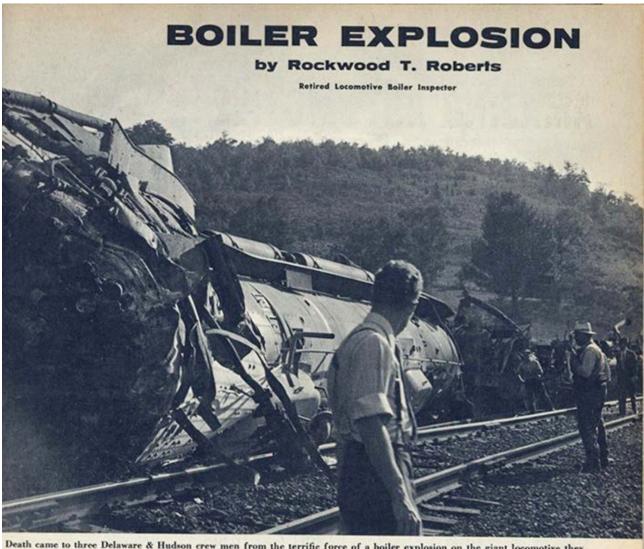
GANG – Signal #71 Nov. 11 1941 Cobleskill NY

Reverse of photo on the preceding page, showing names of persons in photo.

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Our thanks to Steven Brown for posting on *Facebook* a copy of Rockwood T. Roberts' article ("Boiler Explosion," *Railroad Magazine*, February, 1965) about the Wreck of No. 1510 at Cobleskill on July 15, 1941.





Death came to three Delaware & Hudson crew men from the terrific force of a boiler explosion on the giant locomotive they were riding, No. 1510, simple articulated 4-6-6-4, southbound near Cobleskill, N.Y., in early morning of July 15, 1941.

Johnson-Roberts-Breed photo

RARLY that morning of July 15, 1941, people living in and around Cobleskill, N.Y., were awakened by a violent explosion that rattled many windows and frightened the children. Phones buzzed. Men and women dressed quickly, climbed into family automobiles, and followed the state troopers and ambulances with sirens wailing that stormed through the murky pre-dawn.

Residents of that area were used to the distant blasting at nearby Howe's Cavern but not at that time of day. Beset by curiosity and driving fast, they soon caught sight of a freight train stalled on the Delaware & Hudson's main iron, with some of the cars piled high at a crazy angle and the frame and running gear of a giant steam locomotive that weirdly lacked a boiley. Later, they pieced together the grim story of what had happened. A south-bound freight, MB-2, had left Mechanic-ville, N.Y., about two a.m. with a 4-6-4 engine, No. 1510, hauling 16 cars. At Schenectady the crew picked up 29 more cars and filled the 22,000-gallon tender with water from a trackside tank. No more coal was needed to complete the trip to Binghamton, for they had taken on a good 26 tons before starting out.

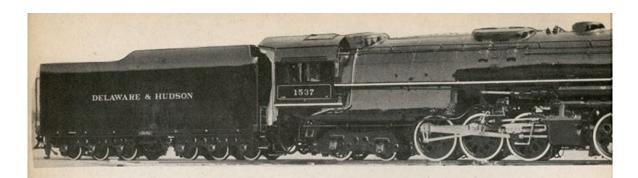
But the huge simple articulated locomotive never reached Binghamton. As MB-2 pulled out of Mohawk Yards the crew could see the sprawling Alco works where the 1510 had been built the previous year. They picked up speed, the stubby stack chopping off its tune in perfect rhythm. Without a click from the rods, the iron monster sped on her way, guided capably by Engineer Charlie Smith. Elbert Price kept the hungry firebox supplied with coal while Brakeman Cleary peered ahead for signal lights.

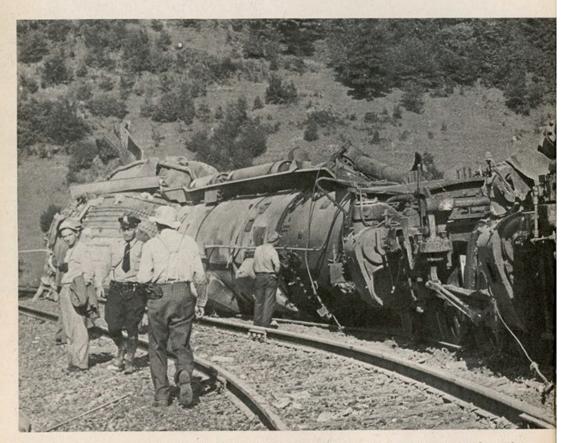
The 1510 was clean and well-groomed, like most D&H power, for Superintendent George Edmunds would have it no other way, and was just sufficiently broken in, having run only 44,000 miles since she left the erecting shop at Schenectady.

Those three men in her cab must have felt proud and satisfied as the tapping at rail-joints quickened. Keen eyes scanned the rails in the illuminated cone from the headlight. All were experienced men who had covered the route hundreds of times. Little did they

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Grotesquely twisted wreckage of the freight-hauler as it looked after coming to rest beside the track after the accident.

making their last trip.

Cobleskill is fifty miles from Mechanicville and the road's physical characteristics vary greatly over that distance. Out of the Mohawk Yards the grade is generally ascending from level

know, as they lifted their voices to talk across the cab, that this time they were it drops for nine miles to as much as it drops for nine miles to as much as 1.16 percent. The hill on which the explosion occurred is eleven miles long, with a maximum grade of 1.32. That stretch has little straight track and it curves 4 degrees at the scene of the accident.

Hundreds of locomotive boilers exploded during the Steam Age and in almost every case the initial cause was low water. Many a time the crews swapped water, Many a time the crews swap-ped water for steam, especially when the going got tough and to prevent stalling, but this could not have been the case with the 1510. The big loco-

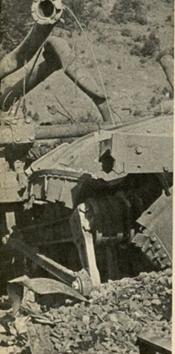
RAILROAD







H.D. Runey, 56 Arnold Ave., Horseheads, N.Y.



motive was equipped with an HT-type stoker and her coal was running good. The boiler, which carried 285 pounds pressure, was a free steamer, mainly because the firebox was as large as an average living room, 9 by 18 feet.

The feedwater appliances were excellent. On the engineer's side, a Hancock non-lifting injector had a capacity of 10,000 gallons an hour. On the left, cylinders to pre-heat the water before forcing it into the boiler. Due to increased efficiency and the

Two D&H engines

great saving in coal, the fireman did the pumping. The engineer, of course, could watch the performance, for the boiler had two water glasses. In addition to one at the left, which he could see, there was one on his own side on a water column with four gage-cocks opened at regular intervals to check with the glass readings. Directly in front of the fireman, on the boilerhead, a large gage indicated exactly how his injector was functioning. Also, a telltale warning from the overflow gave a vivid alarm in case the injector should

an Elesco exhaust steam injector had a

capacity of 12,000 gallons. This injector

was actually a feedwater heater, as it

utilized some of the exhaust from the

"break" and fail to deliver water to the boiler.

Old station at Cobleskill, N.Y., not far from the scene of the boiler explosion.

Exactly what happened as the train nearly topped the grade is anyone's guess. It was learned after the investigation that the water had dropped six inches below the highest point of the crownsheet, causing the plates to be-come reddened, soft, and gradually stretching under high pressure until there was a rapid shearing rupture. Now the thousands of pounds of energy was

set free as the steam escaped.

The powerful steam blast, acting like a jet, shot into the firebox over the grates. This lifted the cab end upward, tearing the entire boiler from the frame. The boiler flew through the air, landing on its nose 196 feet in front of the train, skidding along the rails. The locomotive running gear continued to function,

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striking the smoke-box and pushing the crippled boiler until all came to rest 431 feet from the scene of explosion.

It was estimated the train had been making about 30 miles an hour and, though the brakes immediately went in to emergency with the smashing of brake pipes, the momentum carried the train forward to a point where the collision piled up the first eleven cars. Later, a search was made for all parts. Brake valve and stand, pieces of pipe, castings, and numerous other items were found as far as 475 feet from the blow-up.

The three men in the cab were killed instantly, two bodies being thrown out on the left side of the tracks. What was left of Engineer Smith lay 100 feet from the explosion. Fireman Price's body was found inside of the crumpled cab, which had been hurled 130 feet away. Brakeman Cleary had been blown toward the rear for a distance of 120 feet and into the field 200 feet from the embankment.

Such an explosion is difficult to explain, but some things we do know. If a boiler that size is filled with water or compressed air to a pressure of 285 pounds and if the sheets fail suddenly, the explosion would be a minor one.

the explosion would be a minor one. With steam it is different. When water turns to steam it expands about 1600 times its volume, unless confined in a vessel. When imprisoned and not allowed to expand, it builds up pressure. As the pressures increases so does the temperature of the water and its boiling point. Ordinarily, at sea level, water boils at 212 degrees F., but at 285 pounds pressure the temperature rises to 412 degrees. Now at this temperature, if the fire is dumped from the grates, the engine could be run and as the pressure then drops this water would continue to boil until all pressure was removed and the temperature went back to 212.

In an explosion where the crownsheet ruptures wide open in a matter of seconds, all water immediately turns to steam, due to this stored-up heat. Almost instantly the boiler goes dry as the several thousand gallons of water become steam, expanding 1600 times its volume. And that is what happened to the giant 1510 on the Delaware & Hudson main line near Cobleskill nearly 24 years ago.

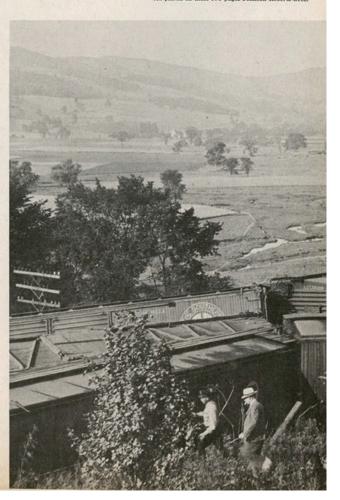
But the morbid curiosity seekers who had been awakened before dawn, cared little for technical details. What fascinated them were the shell of a wrecked locomotive, the bits strewn over the landscape, the piled-up freight cars, and the pathetic-looking bodies of three good men who would never railroad again.



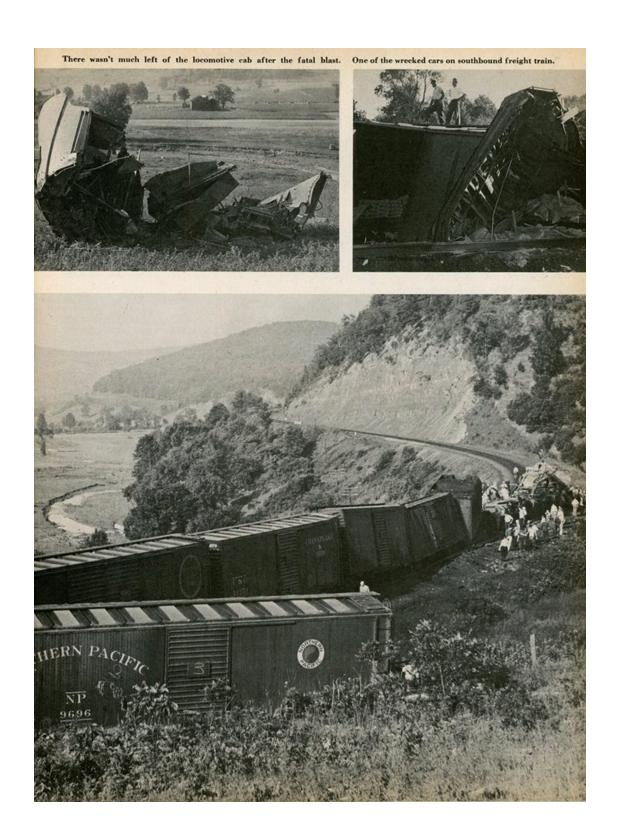
View of the huge boiler after it had rolled down the embankment into a meadow.

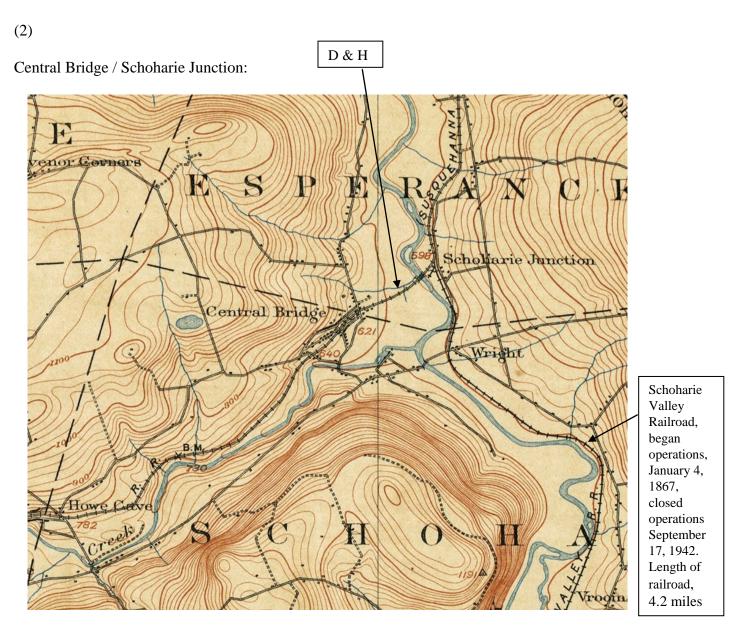
(Big photo) Many shippers were late in receiving freight consigned to the D&H

All photos on these two naces Johnson, Boberts, Brose



16



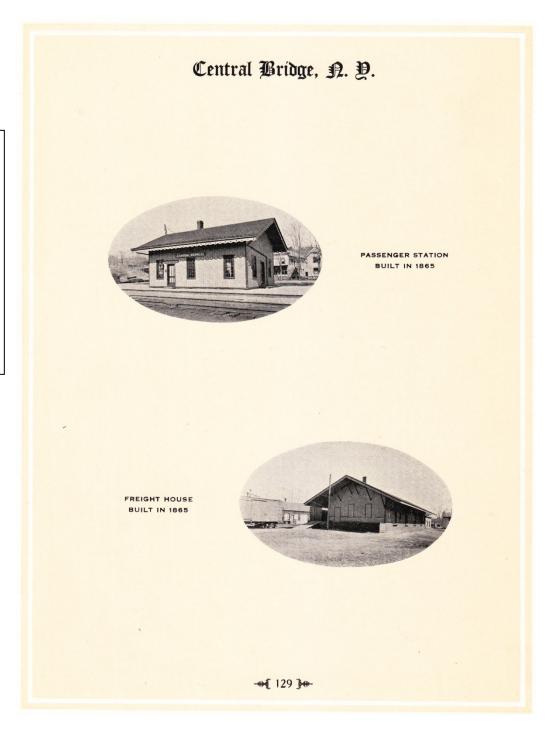


Construction of the A&S started in Albany and moved westward. The road was completed and opened for pubic travel from Albany to Central Bridge on September 16, 1863.

George Westinghouse, who invented and developed the air brake, was born at Central Bridge on October 6, 1846. He was granted a patent on his invention on April 13, 1869. By 1875, the passenger trains of nearly all the principal roads in America were fully equipped with the Westinghouse air brake. His development of the quick-acting brake in 1886, which acted instantaneously on each car upon its application, and upon each part of a train in case of accidental parting, led to its universal application to freight equipment.

The photographs given below of the D&H passenger station and freight house at Central Bridge are from *Passenger and Freight Stations Delaware & Hudson. The Delaware and Hudson Company / Board Of Managers / Inspection of Lines : : . June 7th to June 10th, 1928, p. 129.*

Central Bridge:
Early in the
nineteenth
century the
Schoharie River
was spanned by
three covered
bridges, one at
Esperance, one a
Schoharie, and
the third, midway
between which
became known as
Central Bridge.



Thomas C. Smith of Central Bridge:

From the biographical portrait of Thomas C. Smith ("Knows Central Bridge History") that was published in the December 1, 1925 issue (pp. 3-4, 12) of *The Delaware and Hudson Company Bulletin* we learn that the father of Thomas C. Smith helped build the A&S and was a member of the party that opposed the Fisk forces in the Erie war at the tunnel. Thomas C. Smith, born December 20, 1855, remembers that "At one time there was a large machine shop opposite the station [in Central Bridge] where freight and passenger cars were built and repaired. There was also a turntable and, during the time wood-burners were used, a large wood yard where two men were employed constantly cutting wood to the proper length for fuel and in loading tenders of engines as they passed either north or south." (p. 4)

Smith began working for the railroad in April, 1871, under Flory Mc Carty, a section foreman with 14 men in his gang. In 1879, Smith was promoted to the position of foreman. "[H]e was given charge of Section No. 4, extending from the highway south of Knowersvile (now Altamont) to Knox, where it joined a section over which Hank Cole had supervision. He had four men in his gang—Adam Crounse, track walker, who later became a bookkeeper for W. W. Hickey, bridge and building master; Josiah Schermerhorn and Nick Eldridge. The name of the other man he does not recall." (p. 4)

In Smith's portrait, we read the following about *Blue Cut*:

"Blue Cut, about a mile north of Howe's Cave, was so named because of the bluish color of a hard-pan that was to be found a few feet below the surface of the ground. It never caused any particular trouble although for many years it was customary to keep a man stationed in the vicinity to watch the condition of the banks alongside the track. Here, however, due to a spring freshet, one of the most serious derailments of the road's pioneer days was experienced. With the old No. 24 and twenty-four little coal jimmies, Johnny Whipple went down the bank. He was thrown from the engine and landed in a tree top from which he was rescued, but his fireman was drowned. Wrecking cranes then were unknown and the re-railing of the engine was a most difficult problem. It had to be raised by means of jacks and blocking and tackles."

In 1880, when the work of double tracking the Susquehanna division south of Delanson was undertaken, Smith and his men were a part of the large force employed. A the time of his retirement from the D&H, after fifty-four years and one month of continuous service, Thomas C. Smith was a flagman at the Cameron crossing in Central Bridge.

Schoharie Junction station, photo posted by Brian R. Ward on Facebook on August 18, 2015:



The Albany & Susquehanna Railroad (73 State Street, Albany) in 1863:

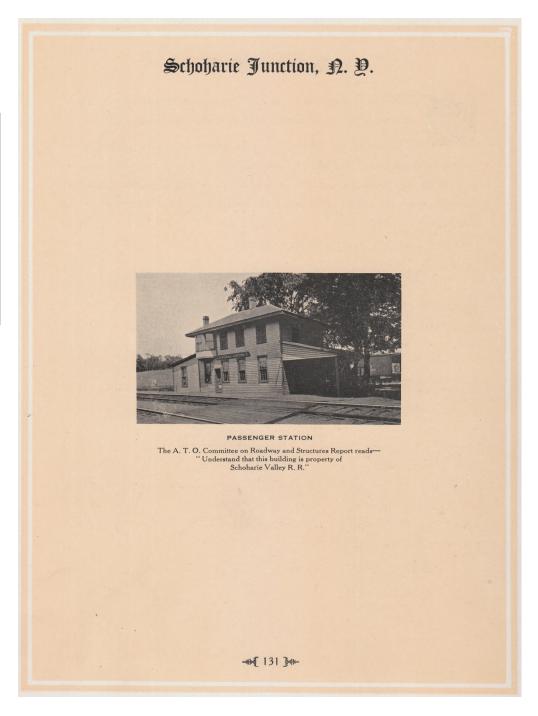
Ezra P. Prentice, President; W. L. M. Phelps, Treasurer and Secretary; C. W. Wentz, Engineer.

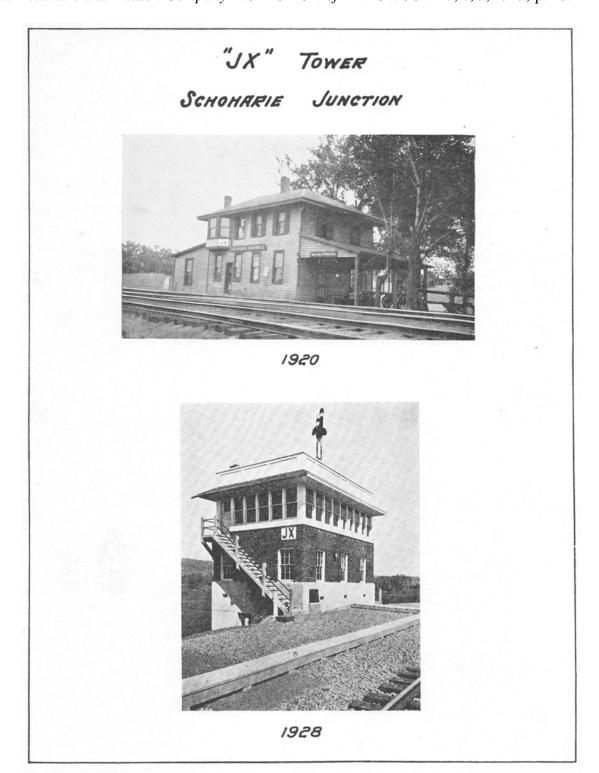
The directors and their place of residence: Ezra P. Prentice, Albany; Joseph H. Ramsey, Albany; Peter Cagger, Albany; Jacob Leonard, Albany; Charles Courter, Cobleskill; John Westover, Richmondville; Jared Goodyear, Milford; Eliakim R. Ford, Oneonta; John Cook, Worcester; Arnold B. Watson, Unadilla; Lewis Northrup, Colesville; Alonzo Everts, Binghamton; Sherman D. Phelps, Binghamton.

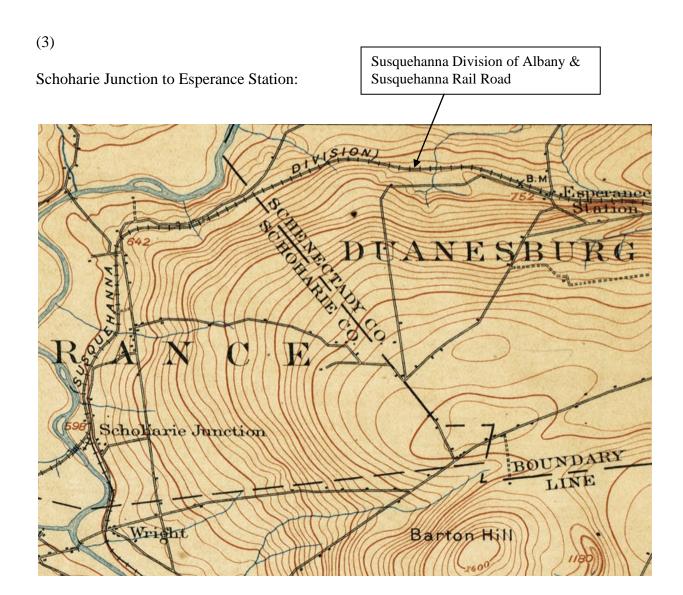
On September 16, 1863, the 35-mile section of the A&S between Albany and Central Bridge was opened and operations were commenced. The A&S at that time: length of double track, including sidings, 1 mile; weight of rail, per yard, on main track, 53 and 54 pounds; number of engine-houses and shops, 1; number of engines, 3; number of first class passenger cars (rated as 8-wheel cars), 4; number of baggage, mail and express cars (rated as 8-wheel cars), 2; number of freight cars (rated as 8-wheel cars), 36; length of line from Albany to Binghamton, 140 miles.

The photograph of the D&H Schoharie Junction passenger station that is given below is from *Passenger and Freight Stations Delaware & Hudson. The Delaware and Hudson Company / Board Of Managers / Inspection of Lines : : . June 7th to June 10th, 1928, p. 131.*

Schoharie
Junction: This
was the junction
point of the
D&H and the
Middleburg and
Schoharie
Railroads.
Schoharie is a
Mohawk word
meaning "drift
wood."

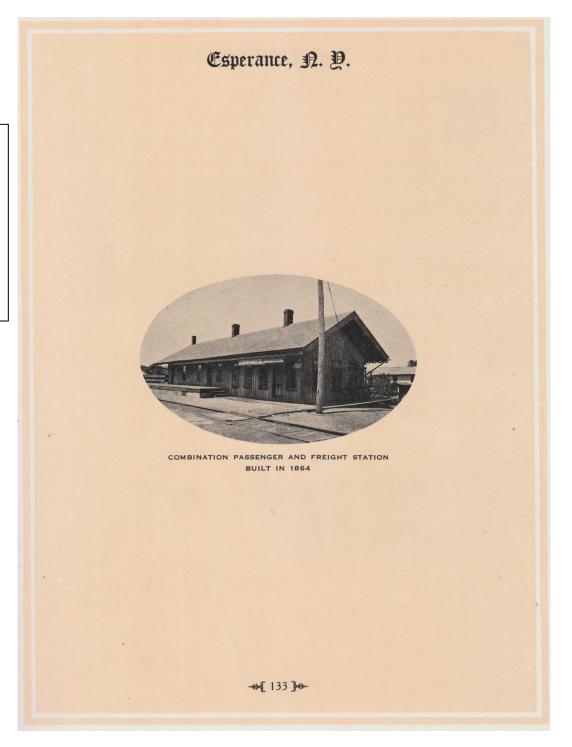






The photograph of the D&H Esperance combination passenger and freight station that is shown below is from *Passenger and Freight Stations Delaware & Hudson. The Delaware and Hudson Company / Board Of Managers / Inspection of Lines : : . June 7th to June 10th, 1928, p. 133.*

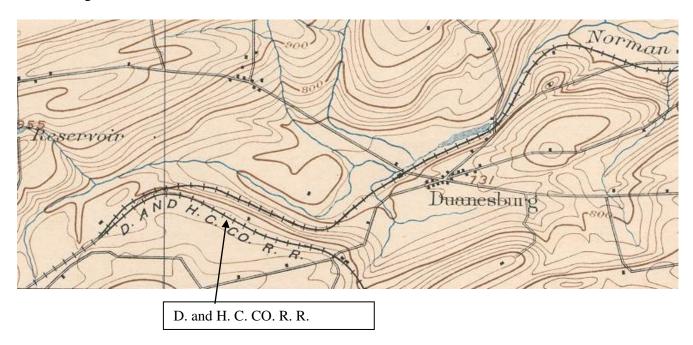
Esperance: The village of Esperance was settled in 1712 by a group of the Palatines bearing such names as Kniskern, Stubrack, Endes, and Houck.



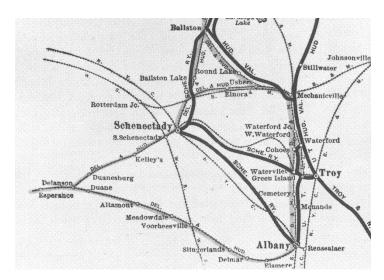
Amsterdam

U. S. Geological Survey, *Amsterdam Quadrangle*, Surveyed in 1893. Henry Gannett, Chief Topographer; H. H. Wilson, Geographer in charge; Triangulation by N. Y. State Survey; Topography by E. B. Clark.

Duanesburg area:



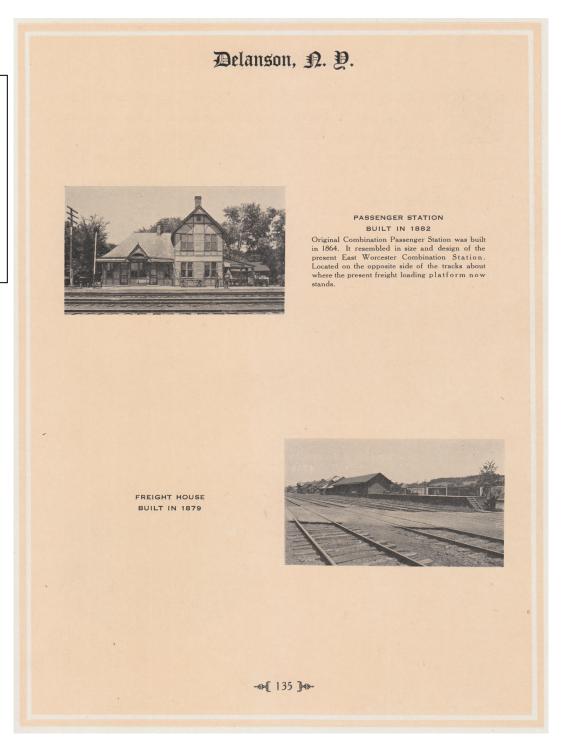
Knowersville (Altamont) was/is between Duane (not Duanesburg, which is on the Schenectady branch) and Meadowdale. (Map detail given below from *Shaughnessy*, p. 220)



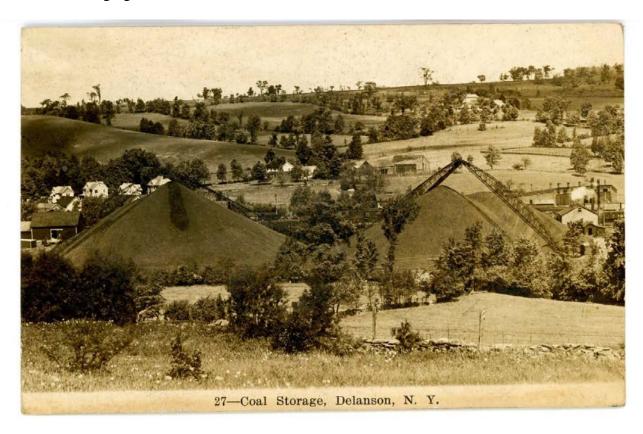
The photographs of the Delanson passenger station and freight house given below are from Passenger and Freight Stations Delaware & Hudson. The Delaware and Hudson Company / Board Of Managers / Inspection of Lines::. June 7th to June 10th, 1928, p. 135.

Delanson:

Formerly known as Quaker Street, the village, in 1893, was renamed Delanson from DELaware AN d hudSON. The old portion of the village, on the hill, is still known as Quaker Street.



27—Coal Storage, Delanson, N. Y.: Post card for sale on E-Bay May 5, 2016. Our thanks to John Buberniak for bringing to our attention this card.



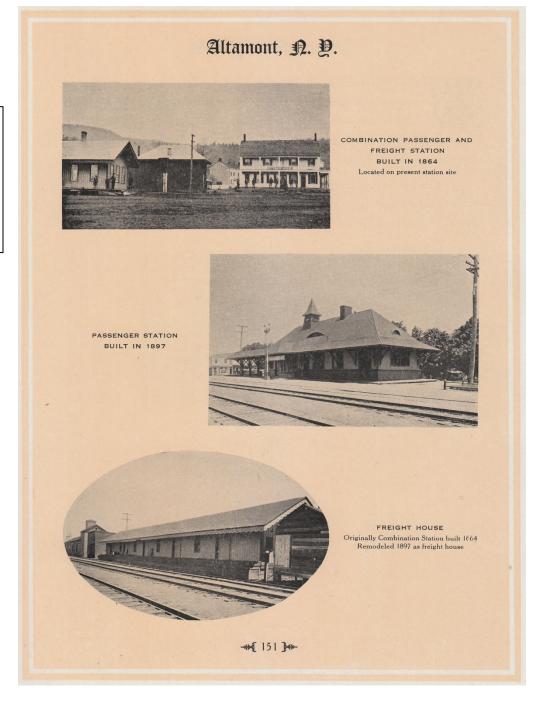
27—Coal Storage, Delanson, N. Y.

The photograph of the Duane D&H combination passenger and freight station that is given on the following page is from *Passenger and Freight Stations Delaware & Hudson. The Delaware and Hudson Company / Board Of Managers / Inspection of Lines : : . June 7th to June 10th, 1928, p. 149.*



The photographs of the Altamont D&H passenger station and freight houses that are given on the following page are from *Passenger and Freight Stations Delaware & Hudson. The Delaware and Hudson Company / Board Of Managers / Inspection of Lines : : . June 7th to June 10th, 1928, p. 151.*

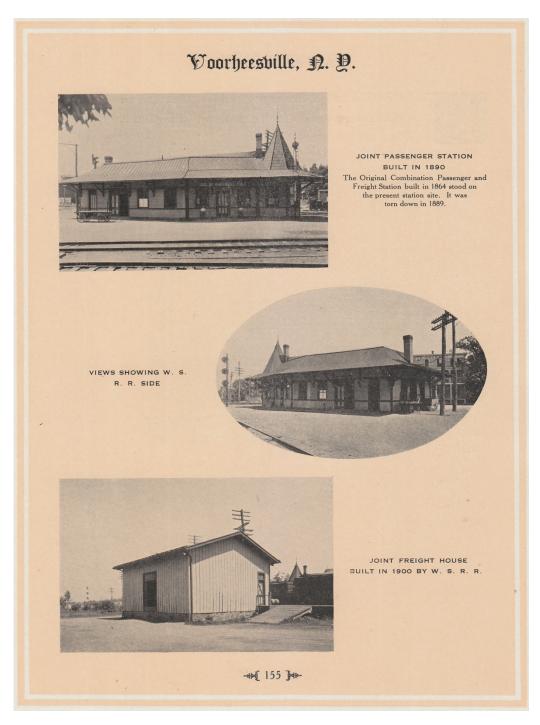
Altamont: On September 16, 1863, the first passenger train from Albany to Central Bridge passed through Altamont.



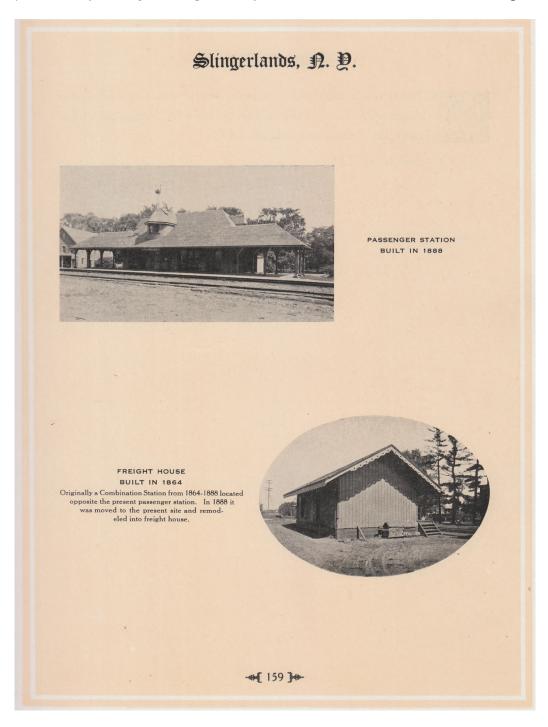
The photograph of the D&H Meadowdale waiting room that is given below is from *Passenger* and Freight Stations Delaware & Hudson. The Delaware and Hudson Company / Board Of Managers / Inspection of Lines::. June 7th to June 10th, 1928, p. 153.



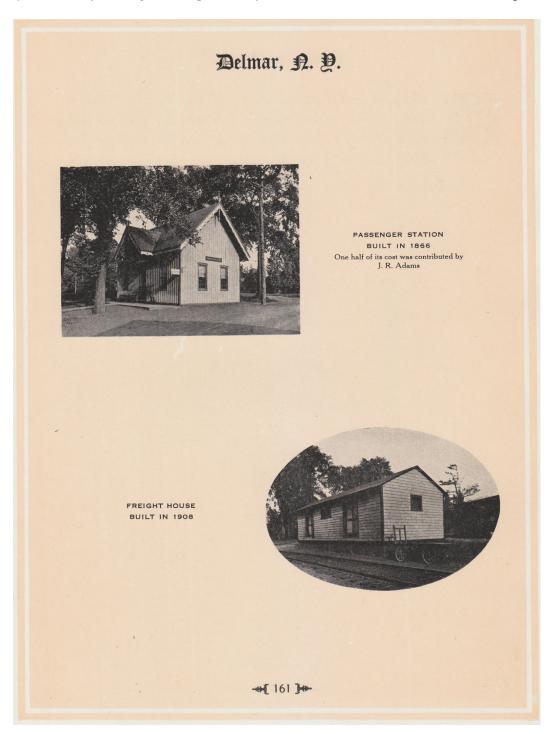
The photographs of the D&H Voorheesville passenger station and freight house that are given below are from *Passenger and Freight Stations Delaware & Hudson. The Delaware and Hudson Company / Board Of Managers / Inspection of Lines : : . June 7th to June 10th, 1928, p. 155.*



The photographs of the Slingerlands D&H passenger station and freight house that are given below are from *Passenger and Freight Stations Delaware & Hudson. The Delaware and Hudson Company / Board Of Managers / Inspection of Lines : : . June 7th to June 10th, 1928, p. 159.*



The photographs of the D&H Delmar passenger station and freight house that are given below are from *Passenger and Freight Stations Delaware & Hudson. The Delaware and Hudson Company / Board Of Managers / Inspection of Lines : : . June 7th to June 10th, 1928, p. 161.*



The photograph of the Elsmere D&H waiting room that is given on the following page is from *Passenger and Freight Stations Delaware & Hudson. The Delaware and Hudson Company / Board Of Managers / Inspection of Lines : : . June 7th to June 10th, 1928, p. 163.*

Elsmere:

The novel "Robert Elsmere" was popular when this village was named, and J. White Sprong suggested that the village be named Elsmere. His suggestion became a reality.

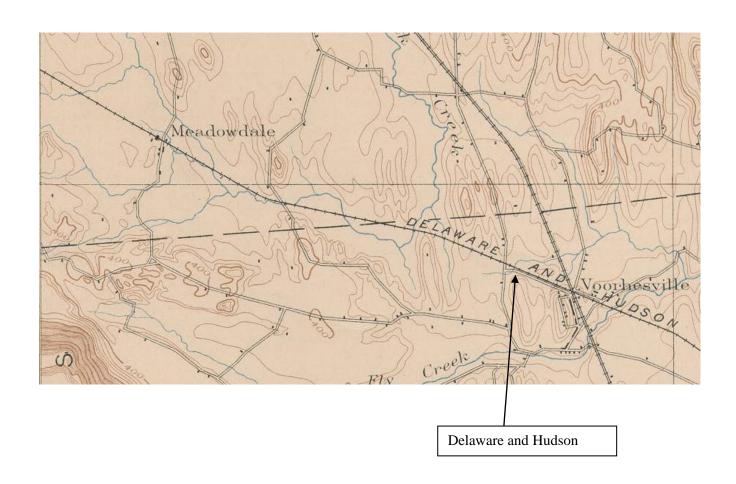


Albany

Detail from U. S. Geological Survey, *Albany Quadrangle*, Surveyed in 1891. H. M. Wilson, Geographer in charge; Triangulation by N. Y. State Survey and by U. S. Cast and Geodetic Survey; Topography by Frank Sutton.

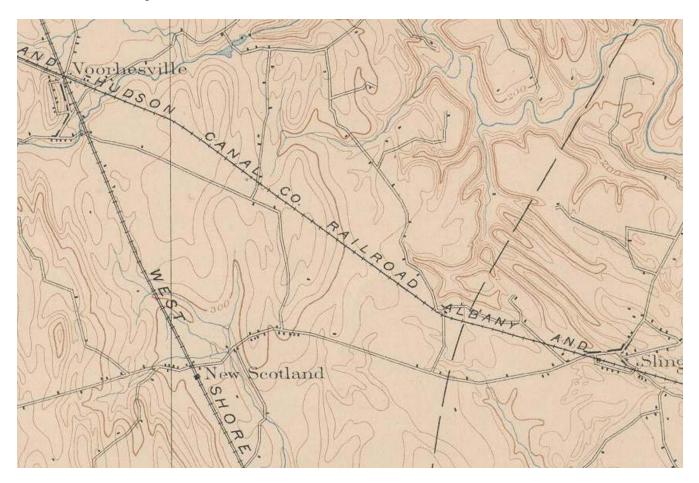
(1)

Meadowdale to Voorhesville:



(2)

Voorhesville to Slingerlands:



In 2003, Canadian Pacific proposed to abandon the 9-mile section of the line from Voorheesville, New York, to Albany. Albany County filed a railbanking application, and purchased the right-of-way in 2009.

On the question of the D&H rail line between Voorheesville and the Kenwood Yard: See Howard Hontz's very informative and interesting column ("From the Top") in the *Bridge Line Historical Society Bulletin* for September 2015 (pp. 7, 245). Given below is that column:



From the Top by Howard Hontz

The Albany Main and the Normanskill bridge

Recently, I was asked to provide some information on the Normanskill bridge, which spans the Normanskill (Norman's Creek in Dutch) on the Albany & Susquehanna main. As is known, this line was closed between Kenwood Yard and Voorheesville, N.Y. and is being reduced to a bike and hiking trail for those 10.5 miles. The remaining 13.9-mile line was retained by a short line switching company to service the Voorheesville Industrial Park.

In the past, this main line was used for passenger train service between Albany and Binghamton, and for freight service between Albany and Binghamton. service included trains such as D&H freight BA-3, which handled traffic for Kenwood. coal trains for the power plant south of the Port of Albany, grain trains for Cargill, and other business on the Albany Port Railroad. BA-3 also handled traffic for North Albany, such as the Menands market perishables, other Albany customers, and traffic for Colonie, including the Watervliet Arsenal and the steel companies, such as Allegheny-Ludlum, Adirondack Steel, and others.

The loss of this 24.4-mile line between Delanson and Kenwood meant the trains had to run through on a longer trip from Delanson to Schenectady to Mechanicville, and then south through Waterford, Watervliet, Cohoes, Colonie and North Albany to Kenwood, a distance of 52.1 miles. This

increased the D&H rail distance 27.7 miles, more than doubling the previous mileage.

I heard that the reason for this abandonment of the Albany main was the bridge over the Normanskill, and further that the bridge needed some expensive repair. Not knowing if this was true and what this involved, I asked Gordon Davids, a BLHS member, who is the most knowledgeable bridge engineer I have ever known. Gordon has an engineering degree from RPI, plus years of engineering experience on bridges from his years working for D&H and other roads and with the FRA.

From Gordon's e-mail:

"I looked and I found that I don't have any detailed information on the bridge structure itself, except for the online record in the D&H Bridge List that Bob Conroy gave me years ago:

"Bridge 2.92 carries the Albany main (Voorheesville Running Track) over Normanskill ('kill' being Dutch for creek, so Normanskill Creek would be redundant). The bridge is a single span, single track, Through Pin Connected Truss with a timber open deck. The span length is 188 feet. It was built in 1884, almost certainly of wrought iron with some cast iron connection parts.

"The D&H did not restrict any locomotive classes on the Albany main, and I know that the 1500's (J-95 Class, 4-6-6-4's) Alco Challengers were permitted on the bridge, so there was, almost certainly, some reinforcing work done on it, probably in the first two decades of the 1900s. The Bridge List shows material of iron and steel. It indicates a Cooper rating of E-64, which is good for any D&H steam or diesel locomotive class, and 100-ton capacity freight cars.

"'Pin-Connected' means that the main structural members in the trusses are connected with pins, eye-bars, and pin plates. The tension members – generally the lower chord and diagonals – are tension members, so they are generally made of slim eye-bars. The eyes are expanded ends of the slim bars, with holes for the pins.

"The compression members are generally the vertical posts inside the trusses, and the inclined end posts. They are of a wider cross section to prevent buckling,

and they are mostly assembled by riveting plates and angle irons to form either a box or an H section.

"During my time at Oneonta, 1970-72, we had a 10 mph speed restriction for all trains on the bridge. I don't recall any reason to restrict the speed due to structural capacity, but I definitely recall a soft subgrade under the track in the south approach to the bridge. That sub-grade, with resulting track geometry problems, was the reason for the speed restriction.

"The bridge is located on a long grade of 1.2% descending northward from Elsmere to Kenwood. That grade caused some serious train handling problems for northward trains, including BA-3 and coal and grain trains. Holding a heavy train to 10 mph on a grade like that requires skillful handling of the air and dynamic brakes. It is bad practice to make a running release of a service application at 10 mph, so I'm sure that the trains had to be stopped, either before the bridge or before the home signal at KN cabin.

"I hope this helps a bit in your article". Thank you, Gordon; your information s a big help.

The cause revealed

The rumors were that the reason for closing the line was due to problems with the bridge, plus the cost to fix it. However, Gordon's information puts the cause on the soft sub-grade in the south approach to the bridge, and the resulting track geometry problems, which caused train handling problems. As Gordon says, there were no bridge structural problems to his knowledge.

The Cooper rating for railroad bridges was created by civil engineer Theodore Cooper, who graduated from what is now RPI in Rensselaer. He created a system of calculations and standards for the safe loading of railroad bridges. In 1880 the rating was E-20, and in 1894 it was raised to E-40. In 1914 it was again raised to E-60, and it remained there until 1990, when it was raised to E72 for concrete and E-80 for steel bridges by the AREA. These ratings were raised as locomotives and car weights swelled, and as train speeds increased. The rating for the Normanskill bridge was E-64, which was above the E-60 rating until 1990.

Train handling problems

As Gordon recalls, and so do I, there continued on page 24

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From the Top from page 7

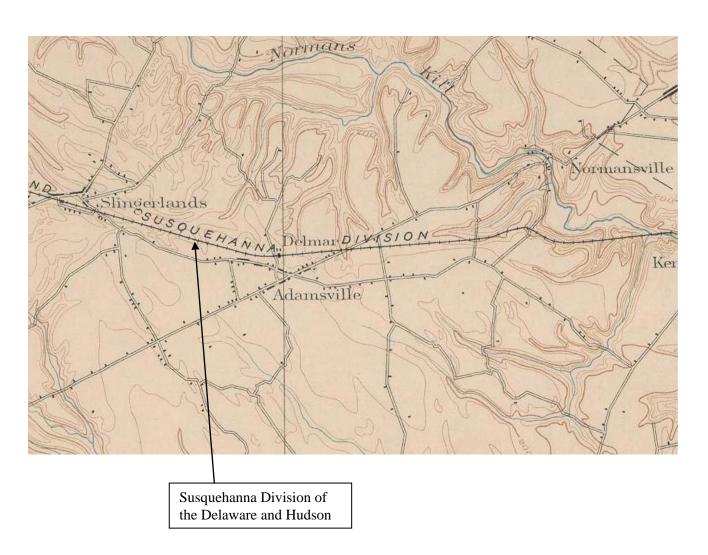
were train handling problems that were a big concern. The coal and grain trains were mostly 100 cars in length. With mostly 100 tons in each car, which made the trains run to 10,000 tons. Handling that tonnage on the 1.2% grade was difficult through a 10 MPH restriction. Different things were tried, such as power rearrangements and splitting the 100-car trains into two 50-car trains. This seemed to work the best, but involved added crews and expense.

In conclusion

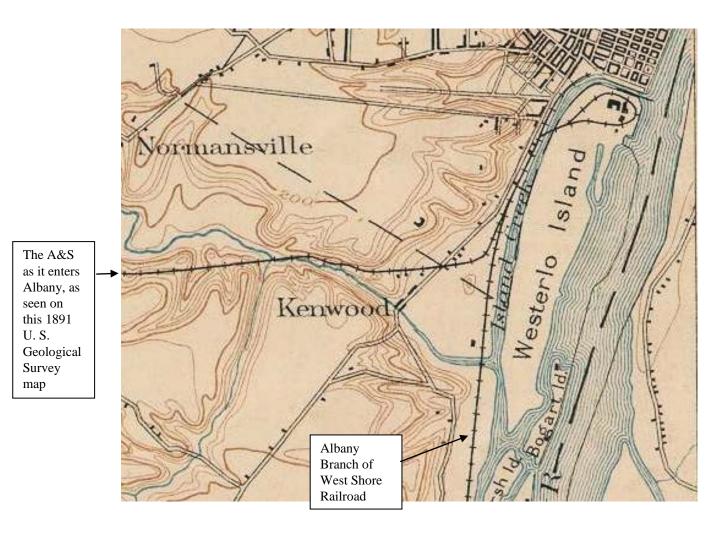
My favorite memory of the Albany main was riding passenger train no. 208 from the lower level at Albany Union Station with engineer Joe Davis, the man who introduced my Dad to my Mother, in the cab of a 600 (4-6-2) steam engine. That was many years ago, but I can still hear the bark of that steam engine as it pulled that train up the 1.2% grade out of Kenwood. That was a sweet sound then, and it still is to my ears.

D&H-BLHS-D&H

Slingerlands to Kenwood:

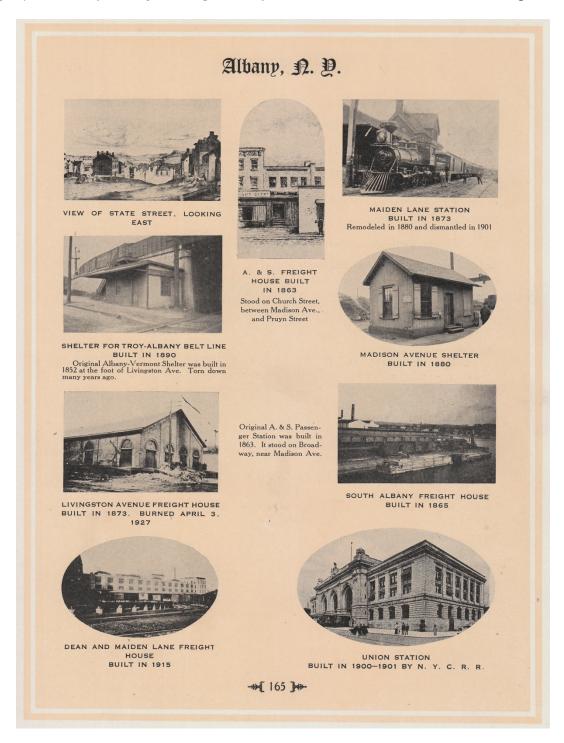


Kenwood to Albany:



Construction of the A&S began on April 19, 1851 at Albany. Twelve years later, the line reached Schoharie Junction. In 1863, four of the Directors of the A&S were from Albany: Ezra P. Prentice, Joseph H. Ramsey, Peter Cagger, and Jacob Leonard. In 1863, the president of the A&S was E. P. Prentice; treasurer and secretary, W. L. M. Phelps; engineer, C. W. Wentz. The company office was at 73 State Street, Albany.

The photographs of the D&H Albany passenger stations and freight houses that are given below are from *Passenger and Freight Stations Delaware & Hudson. The Delaware and Hudson Company / Board Of Managers / Inspection of Lines : : . June 7th to June 10th, 1928, p. 165.*



On July 31, 1926, William J. Keegan retired from the D&H following fifty-two years and one month of continuous service. Between 1878 and 1883 he worked in the Kenwood Yard, braking. In his biographical portrait ("His Retirement Regretted") that was published in the July 15, 1927 issue (pp. 115-116, 226) of *The Delaware and Hudson Company Bulletin*, we read the following about this portion of his career with D&H:

"He worked in the Kenwood yard with link and pin, and the side (shin-cracker) brake—the standard link, the long straight link for drawheads that laid too far back under a car, the crooked link to compensate the difference in the height of couplers, the three-link for 'jimmies' and box cars, and the five-link used between broad and narrow gauge cars. Nick Kearns was yard foreman under Spellman, as the agents in those days were also in charge of yard operations, Johnnie Uhl next, and then George Ellis, now retired and living in Oneonta." (p. 116)

Keegan's biographical portrait includes the following description of the Kenwood Yard:

"The yard proper was very small. All opposite the Spellman track was a swamp, then being filled in. One track extended over a trestle to the Jaeger Iron Works on the 'island,' and there was also the Slaughter House, Ward, Branch, Dock, Patterson, Whiskey, Bourbon, Sharon and Dew Drop tracks, Nos. 1, 2, 3 Coach tracks and Nos. 1, 2, 3 and 4 Hay tracks, in various locations around the docks and in Church Street. All were stub end tracks equipped with various makes of standards, 'flop' and high standards predominating, none of which, however, were fitted with lights." (p. 216)

"During his long career," we read in his biographical portrait in *The Delaware and Hudson Company Bulletin*, "he [Keegan] held at one time or another, nearly every passenger run between Albany and Binghamton. Sperm candles were used for lighting coaches when he began running as a passenger trainmen, and the coaches were heated first by wood fires in stoves located in the ends of the coaches, and later with coal. At first the trains ran only as far as the Company's offices and waiting room in Broadway, Albany. Grady's restaurant was next door, and it was a practice with the proprietor to appear outside his place of business on the arrival of passenger trains and beat a large gong to attract the attention of passengers to his restaurant."

On October 3, 1883, Ambrose Lamphier was hired as extra brakeman and went to work for the D&H on a switch engine in Kenwood Yard. That we know from his biographical portrait ("An Expert Tobacconist") that is published in the February 1, 1938 issue (pp. 19-20, 29) of *The Delaware and Hudson Railroad Bulletin*. From Albany, in 1863, the A&S went west, passing through Kenwood on its way to Adams Station (Delmar), Slingerlands, and ultimately to Binghamton. At Kenwood was Kenwood Junction, the meeting place of the A&S and the Albany Branch of the West Shore Railroad (which would ultimately be leased and then purchased by the D&H). The location of Kenwood, in south Albany, is shown on the map on the preceding page.

In his biographical portrait, published in 1938, Ambrose Lamphier remembered the Kenwood Yard in 1883, when he began work there, as follows:

"The Yard itself consisted of but one main and three yard tracks, with an office at Church Street and Fourth Avenue. A pleasure boat was then operated on a regular schedule from the wharf near the Green Street Crossing to the Abbey near Glenmont. The creek which used to parallel the tracks from Green Street south to the lower end of the yard [Island Creek] was several hundred feet wide and from 8 to 19 feet deep, permitting navigation by large boats to a point below the present site of 'KN' Tower. Equally interesting is the fact that the West Shore main tracks were carried over the marshes south of the yard on a wooden trestle perhaps a quarter of a mile long. Although the yard was small, two yard engines were kept busy day and night switching the hundreds of cars coming from and destined to the Susquehanna Division. There were three regularly assigned crews running south to Delanson, each making two round trips for one day's pay." (p. 20)

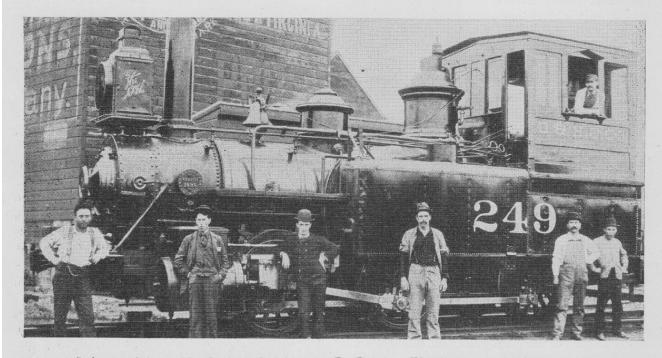
In 1886, after three years' railroading experience, Ambrose Lamphier was promoted to yard conductor by Paul Wadsworth, and for the next 20 years Lamphier was in charge of yard engines at Kenwood.

Kenwood was an important distribution point for D&H anthracite arriving in Albany via the A&S for shipment both North and South. Regarding the shipments South, we read the following in Lamphier's biographical portrait:

"As Albany was then an important coal and grain distributing center, Mr. Lamphier's crew placed hundreds of cars of coal on the trestle at Kenwood for dumping into canal boats, most of the coal moving south to New York and other points along the Hudson River."

Published in the April 1, 1932 issue of *The Delaware and Hudson Railroad Bulletin*, p. 100, is the photograph given below, titled "At Kenwood Yard in 1890."

At Kenwood Yard in 1890



Left to right: M. Cullen, Conductor; F. Dwyer, Timekeeper; P. Kilmartin, Brakeman; P. Burns, Switch Tender; William Singer, Brakeman; J. Hunt, Brakeman. Engineman Wasserback is in the cab window. The photograph was loaned to The Bulletin by Mr. Cullen.

Gary R. Schermerhorn ("The D&H to Albany—Part One," *BLHS Bulletin*, August 2015, p. 15) says the following about the Kenwood Yard in 2015:

"Just north of the village of Delanson is CPF499 (formerly DJ Cabin) where the Schenectady Branch connected with the Albany Main. / The 26.8-mile old Albany Main runs east from Delanson through the villages of Altamont, Voorheesville, Slingerlands and Delmar before descending to the former KN Cabin and into Kenwood Yard at the Port of Albany. Kenwood today continues to be an active and vital port yard for the CP, interchanging intermodal, grain, refinery and freight traffic both with the Port of Albany Railroad and Conrail."

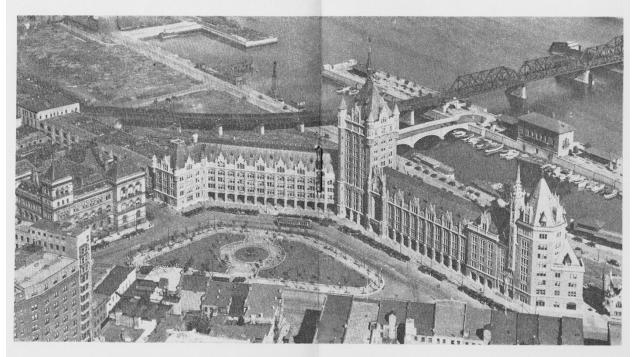
And so we come to the end of our map-trip on the A&S from Binghamton to Albany.

D&H Albany Office Building

In 1889, funds were appropriated for the construction of a D&H office building in Albany (1892-1915) at North Pearl and Steuben Street. A photo of this office building at 58 North Pearl Street is given in *COP* on page 300.

1913: Albany architect, Marcus T. Reynolds, designed the Flemish-Gothic D&H general office building (12-story tower, two 5-story wings) at Albany; completed 1918. Weather vane is a bronze replica of Henry Hudson's ship *Half Moon*.

Delaware and Hudson Building From the Air



At the left of the office building, with its distinctive tower, may be seen the present post office which is to be replaced by the building described on page 331, to be erected north of the present structure. Its convenient location, close to the railroad station and business district is indicated by this aerial photograph.



Tower section of D&H Office Building at Albany.

D&H Building

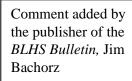
D&H Plaza Building, Albany, NY. Photo posted on Facebook by George Ford, Jr.

Bridge Line Historical Society Bulletin, September 2015, "The Receiving Yard" by Doug Barron, pp. 20-22-24. On page 23, he reprints an item that was in the Albany Business Review, and here titled "Improving a classic. . ."

Improving a classic ... The State University of New York headquarters in Albany, formerly the headquarters of the Delaware & Hudson Railroad, is one of the most distinctive buildings in the city. Opened in 1915, it resembled a Gothic-styled European castle, but appears foreboding and is closed off to everyone except the 600 employees inside.

Chancellor Nancy Zimpher wants to change that and make SUNY HQ more inviting. SUNY took over the building in 1978. The grand historic building is getting a \$1 million makeover designed to make it more accessible to the public and fix decades-old infrastructure problems. Bunkoff General Contractors in Latham, NY has started construction to open up the landscaped plaza in front of the building, to include a circular drop-off area for vehicles and a sidewalk. The circular drop-off also will improve access for emergency vehicles.

The front doors to the building were recently replaced, and are now unlocked during working hours. Tours are provided on an informal basis, and a farmers' market is held on Thursdays. Work is expected to be finished this fall. The project is on budget and ahead of schedule.



Albany Business Review
Actually, the building is modeled after the
Cloth Guild Hall in Ypres, Belgium, and
not after a castle...JB

The Cloth Hall, Ypres, Belgium

The Cloth Hall (Dutch: Lakenhal or Lakenhalle) is a large cloth hall, a medieval commercial building, in Ypres, Belgium. It was one of the largest commercial buildings of the Middle Ages, when it served as the main market and warehouse for the Flemish city's prosperous cloth

industry. The original structure, erected mainly in the 13th century and completed 1304, lay in ruins after artillery fire devastated Ypres in World War I. Between 1933 and 1967, the hall was meticulously reconstructed to its prewar condition, under the guidance of architects J. Coomans and P. A. Pauwels. At 125 metres (410 ft) in breadth, with a 70 metres (230 ft)-high belfry tower, the Cloth Hall recalls the importance and wealth of the medieval trade city.



The Cloth Hall, Ypres, Belgium

D&H Managers and Officers: 1870—May 6, 1906

A&S Directors, Officers, and Finance Committee members: September 6, 1870—October 16, 1906

In 1906, the Aetna Insurance Company et al. initiated a law suit against the Albany and Susquehanna Railroad Company and the Delaware & Hudson Company.

This case was tried in the Circuit Court of the United States for the Southern District of New York. The final decree of that court was that the Delaware and Hudson Canal Company pay the Albany and Susquehanna Railroad Company the amount of \$1,107,923.24. A volume of the proceedings of that case in that court is in the collection of the Carbondale D&H Transportation Museum, courtesy of John V. Buberniak.

The D&H appealed the final decree of the Circuit Court Southern District of New York and, on a certificate from the United States Circuit Court of Appeals for the Second District, the case (The Delaware and Hudson Company *vs.* The Albany and Susquehanna Railroad Company et al) was heard by the Supreme Court of the United States at the October Term 1908 (No. 416). A volume of the proceedings of that case before the Supreme Court of the United States is in the collection of the Carbondale D&H Transportation Museum, courtesy of John V. Buberniak.

At the request of the Complainants (Aetna Insurance Company et al.), when the case was tried in the Circuit Court of the United States for the Southern District of New York, the Defendants (the Albany and Susquehanna Railroad Company and the Delaware & Hudson Company) were asked to provide for the court, and did provide, a complete list of the names of the managers (D&H), directors (A&S), officers (D&H and A&S), and members of the Finance Committee (A&S) of their respective companies from 1870 to 1906. Those lists, which were included in the proceedings of the Circuit Court and in the proceedings of the Supreme Court, are reprinted herebelow. I shouldn't wonder if these lists are the only extant record of the names of the managers (D&H), directors (A&S), officers (D&H and A&S), and members of the Finance Committee (A&S) of the D&H and the A&S for the period 1870 to 1906.

Here, for the record, are those lists.

Stipulation of Facts at Request of Complainants.

UNITED STATES CIRCUIT COURT,

SOUTHERN DISTRICT OF NEW YORK.

ÆTNA INSURANCE COMPANY ET AL., Complainants,

AGAINST

ALBANY & SUSQUEHANNA RAILROAD COMPANY and DELAWARE & HUD-SON COMPANY,

Defendants.

AT THE REQUEST OF COUNSEL FOR THE COMPLAINANTS, IT IS STIPULATED BY COUNSEL FOR ALL PARTIES HEREIN, for the purposes of this action, subject to all objections as to materiality, competency or relevancy, as follows:

That the Board of Managers and officers of The Delaware & Hudson Company, one of the Defendants herein, elected in the year 1870 and in the subsequent years were as follows:

1870

Managers

Charles N. Talbot
Edward J. Woolsey
Geo. Talbot Olyphant
Abiel A. Low
Robert Lenox Kennedy
James M. Halsted
LeGrand B. Cannon
James R. Taylor
Thomas Dickson
John Jacob Astor
Thomas Cornell
W. J. Hoppin
Isaac N. Seymour

745

746

718

188 Officers 749 President, Thomas Dickson Treasurer, Charles P. Hartt Secretary, Daniel Wilson 1871 Managers Charles N. Talbot Edward J. Woolsey Geo. Talbot Olyphant . 750 Abiel A. Low Robert Lenox Kennedy James M. Halsted LeGrand B. Cannon James R. Taylor Thomas Dickson John Jacob Astor Thomas Cornell W. J. Hoppin Isaac N. Seymour 751 Officers President, Thomas Dickson Treasurer, Charles P. Hartt Secretary, Daniel Wilson Managers 1872 Charles N. Talbot Robert Lenox Kennedy James M. Halsted 752 James R. Taylor Thomas Dickson John Jacob Astor LeGrand B. Cannon J. Pierpont Morgan Abiel A. Low

Thomas Cornell W. J. Hoppin Geo. Cabot Ward Robert M. Olyphant

	100	
	189	
	Officers	753
	President, Thomas Dickson	
	Treasurer, James C. Hartt	
	Secretary, George L. Haight	
4	General Manager, Coe F. Young	
1873	Managers	
	Charles N. Talbot	
	Robert Lenox Kennedy	754
	James M. Halsted	101
	James R. Taylor	
	Thomas Dickson	
	John Jacob Astor	
	LeGrand B. Cannon	
	J. Pierpont Morgan	
	Abiel A. Low	
	Thomas Cornell	
	W. J. Hoppin	
	Geo. Cabot Ward	755
	Robert M. Olyphant	199
	Officers	
	President, Thomas Dickson	
	Assistant President, Harwood V. O phant	ly-
	Treasurer, James C. Hartt	
	Secretary, George L. Haight	
	General Manager, Coe F. Young	750
		756
1874	Managers	
	Charles N. Talbot	
	Robert Lenox Kennedy	
	James M. Halsted	
	James R. Taylor	
	Thomas Dickson	
	John Jacob Astor	
	LeGrand B. Cannon	
	J. Pierpont Morgan	

TO THE P	190
757	Abiel A. Low
	Thomas Cornell
	W. J. Hoppin
	Geo. Cabot Ward
	Robert M. Olyphant
	Officers
	President, Thomas Dickson
	Assistant President, Harwood V. Olyphant
758	Treasurer, James C. Hartt
	Secretary, George L. Haight
	General Manager, Coe F. Young
1875	Managers
	Abiel A. Low
	James M. Halsted
	John Jacob Astor
	J. Pierpont Morgan
759	Robert Lenox Kennedy
	LeGrand B. Cannon
	Thomas Cornell
	Geo. Cabot Ward
	Thomas Dickson
	James R. Taylor
	W. J. Hoppin
	Robert S. Hone
	James Roosevelt
760	Officers
	President, Thomas Dickson
	Treasurer, James C. Hartt
	Secretary, George L. Haight
	General Manager, Coe F. Young
1876	Managers
	Abiel A. Low
	James M. Halsted
	John Jacob Astor

J. Pierpont Morgan
Robert Lenox Kennedy
LeGrand B. Cannon
Thomas Cornell
Geo. Cabot Ward
Thomas Dickson
James R. Taylor
W. J. Hoppin
Robert S. Hone
James Roosevelt

762

761

Officers

President, Thomas Dickson Assistant President, Robert M. Olyphant Treasurer, James C. Hartt Secretary, George L. Haight General Manager, Coe F. Young

1877

Managers

Abiel A. Low
Robert Lenox Kennedy
James M. Halsted
James R. Taylor
Geo. Cabot Ward
James Roosevelt
Thomas Dickson
John Jacob Astor
LeGrand B. Cannon
Thomas Cornell
J. Pierpont Morgan
Robert S. Hone
Levi P. Morton

763

764

Officers

President, Thomas Dickson Assistant President, Robert M. Olyphant Treasurer, James C. Hartt Secretary, George L. Haight General Manager, Coe F. Young

	192
765 1878	Managers
	Abiel A. Low
	Robert Lenox Kennedy
	James M. Halsted
	James R. Taylor
	Geo. Cabot Ward
	James Roosevelt
	Thomas Dickson
	John Jacob Astor
766	LeGrand B. Cannon
100	Thomas Cornell
	J. Pierpont Morgan
	Robert S. Hone
	Levi P. Morton
	Officers
	President, Thomas Dickson
	Assistant President, Robert M. Olyphar
7.07	Treasurer, James C. Hartt
767	Secretary, George L. Haight
	General Manager, Coe F. Young
1879	Managers
*	Abiel A. Low
	Robert Lenox Kennedy
	James M. Halsted
	James R. Taylor
	Geo. Cabot Ward
768	James Roosevelt
	Thomas Dickson
	John Jacob Astor
	LeGrand B. Cannon
	Thomas Cornell
	J. Pierpont Morgan
	Robert S. Hone
	Levi P. Morton

Officers

769

President, Thomas Dickson Assistant President, Robert M. Olyphant Treasurer, James C. Hartt Secretary, George L. Haight General Manager, Coe F. Young

1880

Managers

Abiel A. Low
James M. Halsted
LeGrand B. Cannon
James R. Taylor
John Jacob Astor
Thomas Cornell
Thomas Dickson
Geo. Cabot Ward
Robert S. Hone
James Roosevelt
Levi P. Morton
Adolphus Hamilton
Abraham R. Van Nest

Officers

President, Thomas Dickson Assistant President, Robert M. Olyphant Treasurer, James C. Hartt Secretary, George L. Haight General Manager, Coe F. Young

772

1881

Managers

Abiel A. Low
James M. Halsted
LeGrand B. Cannon
John Jacob Astor
James Roosevelt
Abraham R. Van Nest
Thomas Dickson
Robert S. Hone

194 James R. Taylor 773 Thomas Cornell Adolphus Hamilton Hugh J. Jewett David Dows Officers President, Thomas Dickson Assistant President, Robert M. Olyphant 774 Treasurer, James C. Hartt Secretary, George L. Haight General Manager, Coe F. Young 1882 Managers Abiel A. Low James M. Halsted LeGrand B. Cannon John Jacob Astor 775 James Roosevelt Abraham R. Van Nest Thomas Dickson Robert S. Hone James R. Taylor Thomas Cornell Adolphus Hamilton Hugh J. Jewett David Dows 776 Officers President, Thomas Dickson Vice President, Robert M. Olyphant Treasurer, James C. Hartt Secretary, George L. Haight

General Manager, Coe F. Young

	195	
1883	Managers	777
	Abiel A. Low	
	James M. Halsted	
	LeGrand B. Cannon	
	James R. Taylor	7
	Thomas Dickson	
	John Jacob Astor	
	Thomas Cornell	
	Robert S. Hone	
	James Roosevelt	778
	Abraham R. Van Nest	,,,
	Hugh J. Jewett	
	David Dows	
	Levi P. Morton	
	Officers	
	President, Thomas Dickson	
	Vice-President, Robert M. Olyphant	
	Treasurer, James C. Hartt	770
	Secretary, F. Murray Olyphant	779
	General Manager, Coe F. Young	
	Assistant General Manager, Horace G.	
	Young	
1884	Managers	
	Abiel A. Low	
	James M. Halsted	
	LeGrand B. Cannon	2/12/12/12
	James R. Taylor	780
	John Jacob Astor	
	Thomas Cornell	
	Robert S. Hone	
	James Roosevelt	
	Abraham R. Van Nest	
	Hugh J. Jewett	
	David Dows	
1.	Robert M. Olyphant	
	Benjamin H. Bristow	

Cfficers.

President, Robert M. Olyphant
Vice-President & General Manager, Coe
F. Young
Treasurer, James C. Hartt
Assistant Treasurer, Charles A. Walker
Secretary, F. Murray Olyphant
Assistant General Manager, Horace G.
Young

782

783

May 12, 1885

Managers

Abiel A. Low
James M. Halsted
LeGrand B. Cannon
James R. Taylor
John Jacob Astor
Robert S. Hone
James Roosevelt
A. R. Van Nest
Hugh J. Jewett
David Dows
Robert M. Olyphant
Benjamin H. Bristow
John A. Stewart

Officers

784

President, Robert M. Olyphant Vice President, LeGrand B. Cannon General Manager, Horace G. Young Treasurer, James C. Hartt Secretary, F. Murray Olyphant

May 11, 1886.

Managers

Abiel A. Low James M. Halsted LeGrand B. Cannon James R. Taylor John Jacob Astor

James Roosevelt
A. R. Van Nest
David Dows
Robert M. Olyphant
Benjamin H. Bristow
John A. Stewart
Frederick Billings
R. Suydam Grant

Officers

President, Robert M. Olyphant
Vice President, LeGrand B. Cannon
Assistant President and General Manager, Horace G. Young
Treasurer, James C. Hartt
Secretary, F. Murray Olyphant

785

786

787

788

May 10, 1887

Managers

Abiel A. Low
James M. Halsted
LeGrand B. Cannon
James R. Taylor
John Jacob Astor
James Roosevelt
A. R. Van Nest
David Dows
Robert M. Olyphant
Benjamin H. Bristow
John A. Stewart
Frederick Billings
R. Suydam Grant

Officers

President, Robert M. Olyphant Vice-President, LeGrand B. Cannon Assistant President & General Manager, Horace G. Young Treasurer, James C. Hartt Secretary, F. Murray Olyphant

789 May 8, 1888

Managers

Abiel A. Low
William H. Tillinghast
LeGrand B. Cannon
James R. Taylor
John Jacob Astor
James Roosevelt
Johnston Livingston
David Dows
Robert M. Olynbant

790

Robert M. Olyphant Benjamin H. Bristow John A. Stewart Frederick Billings R. Suydam Grant

Officers

791

President, Robert M. Olyphant
Vice-President, LeGrand B. Cannon
2d Vice-President & General Manager,
Horace G. Young
Treasurer, James C. Hartt
Secretary, F. Murray Olyphant

May 14, 1889

Managers

Abiel A. Low
LeGrand B. Cannon
James R. Taylor
James Roosevelt
David Dows
Robert M. Olyphant
Benjamin H. Bristow
John A. Stewart
Frederick Billings
R. Suydam Grant
William H. Tillinghast
Johnston Livingston
John Jacob Astor

792

President, Robert M. Olyphant
Vice-President, LeGrand B. Cannon
2d Vice-President & General Manager,
Horace G. Young
Treasurer, James C. Hartt
Secretary, F. Murray Olyphant

May 8, 1890

Managers

794

Alfred Van Santvoord LeGrand B. Cannon George C. Clark James Roosevelt William W. Astor Robert M. Olyphant Benjamin H. Bristow John A. Stewart James A. Roosevelt R. Suydam Grant William H. Tillinghast Johnston Livingston Frederick Billings

795

Officers

President, Robert M. Olyphant Vice-President, LeGrand B. Cannon 2nd Vice-President, Horace G. Young Treasurer, James C. Hartt Secretary, F. Murray Olyphant

795

May, 1891

Managers

LeGrand B. Cannon James Roosevelt Robert M. Olyphant Benjamin H. Bristow John A. Stewart R. Suydam Grant

797

William H. Tillinghast Johnston Livingston Alfred Van Santvoord William W. Astor James A. Roosevelt

Officers

798

President, Robert M. Olyphant Vice-President, LeGrand B. Cannon 2nd Vice-President, Horace G. Young Treasurer, James C. Hartt Assistant Treasurer, Charles A. Walker Secretary, F. Murray Olyphant

May 10, 1892

Managers

799

LeGrand B. Cannon
James Roosevelt
Robert M. Olyphant
Benjamin H. Bristow
Chauncey M. Depew
R. Suydam Grant
William H. Tillinghast
Cornelius Vanderbilt
Alfred Van Santvoord
Samuel Spencer
James A. Roosevelt
Alexander E. Orr
O. P. C. Billings

800

Officers

President, Robert M. Olyphant Vice-President, LeGrand B. Cannon 2nd Vice-President, Horace G. Young Treasurer, Charles A. Walker Secretary, F. Murray Olyphant

	201	
May 9, 1893	Managers	801
		,
	LeGrand B. Cannon	
	James Řoosevelt	
	Robert M. Olyphant	
	Benjamin H. Bristow	
	Chauncey M. Depew	
	R. Suydam Grant	
	William H. Tillinghast	
	Cornelius Vanderbilt	
	Alfred Van Santvoord	802
	Samuel Spencer	
	James A. Roosevelt	
	Alexander E. Orr	
	Johnston Livingston	
	Officers	
	President, Robert M. Olyphant	-
	Vice-President, LeGrand B. Cannon	
	2nd Vice-President, Horace G. Young	000
	Treasurer, Charles A. Walker	803
	Secretary, F. Murray Olyphant.	
May 8, 1894	Managers	
	James Roosevelt	
	Robert M. Olyphant	
	Benjamin Brewster	
	James W. Alexander	
	Chauncey M. Depew	and a succession of
	John A. Stewart	804
	William H. Tillinghast	
	Cornelius Vanderbilt	
	Alfred Van Santvoord	
	James R. Taylor	
No. of the last	James A. Roosevelt	
	Alexander E. Orr	
	Horace G. Young	,

Officers

President, Robert M. Olyphant Vice-President, James Roosevelt 2nd Vice-President, Horace G. Young Treasurer, Charles A. Walker Secretary, F. Murray Olyphant General Counsel, David Willcox

May 14, 1895

Managers

806

James Roosevelt
Robert M. Olyphant
William H. Tillinghast
Alfred Van Santvoord
James Roosevelt
Alexander E. Orr
Cornelius Vanderbilt
Chauncey M. Depew
John A. Stewart
James W. Alexander
James R. Taylor
Benjamin Brewster
Horace G. Young

807

Officers

808

President, Robert M. Olyphant Vice-President, James Roosevelt 2nd Vice-President, Horace G. Young Treasurer, Charles A. Walker Secretary, F. Murray Olyphant General Counsel, David Willcox

May 12, 1896

Managers

James Roosevelt Robert M. Olyphant Benjamin Brewster James W. Alexander Chauncey M. Depew William H. Tillinghast

203 Cornelius Vanderbilt 809 Alfred Van Santvoord James R. Taylor James A. Roosevelt Alexander E. Orr Horace G. Young John Jacob Astor Officers President, Robert M. Olyphant 810 Vice-President, James Roosevelt 2nd Vice-President, Horace G. Young Treasurer, Charles A. Walker Secretary, F. Murray Olyphant General Counsel, David Willcox May 11, 1897 Managers James Roosevelt Robert M. Olyphant 811 Benjamin Brewster James W. Alexander Chauncey M. Depew William H. Tillinghast R. S. Hayes Alfred Van Santvoord James R. Taylor James A. Roosevelt Alexander E. Orr Horace G. Young 812 John Jacob Astor Officers President, Robert M. Olyphant Vice-President, James Roosevelt 2nd Vice-President, Horace G. Young Treasurer, Charles A. Walker Secretary, F. Murray Olyphant General Counsel, David Willcox

813 May 10, 1898

Managers

James Roosevelt
Robert M. Olyphant
James W. Alexander
Chauncey M. Depew
William H. Tillinghast
R. S. Hayes
Alfred Van Santvoord
James R. Taylor
James A. Roosevelt
Alexander E. Orr
Horace G. Young

814

John Jacob Astor Frederic Cromwell

Officers

815

President, Robert M. Olyphant Vice-President, James Roosevelt 2nd Vice President, Horace G. Young Treasurer, Charles A. Walker Secretary, F. Murray Olyphant General Counsel, David Willcox

May 9, 1899

Managers

816

James Roosevelt
Robert M. Olyphant
James W. Alexander
Chauncey M. Depew
William H. Tillinghast
R. S. Hayes
Alfred Van Santvoord
James R. Taylor
Alexander E. Orr
Horace G. Young
John Jacob Astor
Frederic Cromwell

David Willcox

Officers

817

President, Robert M. Olyphant Vice-President, James Roosevelt 2nd Vice-President, Horace G. Young Treasurer, Charles A. Walker Secretary, F. Murray Olyphant General Counsel, David Willcox

May 8, 1900

Managers

818

James Roosevelt
Robert M. Olyphant
William H. Tillinghast
Alfred Van Santvoord
Alexander E. Orr
Chauncey M. Depew
James W. Alexander
James R. Taylor
Horace G. Young
John Jacob Astor
R. S. Hayes
Frederic Cromwell
David Willcox

819

Officers

President, Robert M. Olyphant Vice-President, James Roosevelt 2nd Vice-President, Horace G. Young Treasurer, Charles A. Walker Secretary, F. Murray Olyphant General Connsel, David Willcox

820

May 14, 1901

Managers '

Robert M. Olyphant William H. Tillinghast Alfred Van Santvoord Alexander E. Orr Chauncey M. Depew

821

James W. Alexander Horace G. Young John Jacob Astor R. S. Hayes Frederic Cromwell David Willcox R. Suydam Grant George I. Wilber

Officers

822

President, Robert M. Olyphant
Vice-President and General Counsel,
David Willcox
2nd Vice-President, Horace G. Young
Treasurer, Charles A. Walker
Secretary, F. Murray Olyphant

May 13, 1902

Managers

823

Robert M. Olyphant
William H. Tillinghast
Charles A. Peabody
Alexander E. Orr
Chauncey M. Depew
James W. Alexander
Horace G. Young
John Jacob Astor
R. S. Hayes
Frederic Cromwell
David Willcox
R. Suydam Grant
George I. Wilber

824

Officers

President, Robert M. Olyphant
Vice-President and General Counsel,
David Willcox
2nd Vice-President, Horace G. Young
Treasurer, Charles A. Walker
Secretary, F. Murray Olyphant

	207	
May 12, 1903	Managers	825
	Robert M. Olyphant	
	C. A. Peabody	
	Alexander E. Orr	
	George I. Wilber	
	Chauncey M. Depew	
	James W. Alexander	
	E. H. Harriman	
	John Jacob Astor	
	R. S. Hayes	826
	Frederic Cromwell	
	David Willcox	
	R. Suydam Grant	
	J. H. Hyde	
	Officers'	
	President, David Willcox	
	Vice-President, Alexander E. Orr	
	2nd Vice-President, Horace G. Young	827
	3rd Vice-President, Abel I. Culver	
	General Counsel, William S. Opdyke	
	Treasurer, Charles A. Walker	
	Secretary, F. Murray Olyphant	
May 10, 1904	Managers	
	Robert M. Olyphant	
	Charles A. Peabody	
	Alexander E. Orr	828
	George I. Wilber	0_0
	Chauncey M. Depew	95
	James W. Alexander	
	E. H. Harriman	
	John Jacob Astor	
	R. S. Hayes	
	Frederic Cromwell	
	David Willeox	
	R. Suydam Grant	
	J. H. Hyde	

	208
829	Officers
	President, David Willcox
	Vice-President, Alexander E. Orr
	2nd Vice-President, Abel I. Culver
	General Counsel, William S. Opdyke
	Treasurer, Charles A. Walker
	Secretary, F. Murray Olyphant
May 1	0, 1905 Managers
830	Robert M. Olyphant
	Alexander E. Orr
	Chauncey M. Depew
	James W. Alexander
	John Jacob Astor
	Frederic Cromwell
	David Willeox
	R. Suydam Grant
	George I. Wilber
001	Charles A. Peabody
831	E. H. Harriman
	James H. Hyde
	William S. Opdyke
	Officers
	President, David Willcox
	Vice-President, Alexander E. Orr
	2nd Vice-President, Abel I. Culver
	General Counsel, William S. Opdyke
832	Treasurer, Charles A. Walker
	Secretary, F. Murray Olyphant
May 6	, 1906 Managers
	Robert M. Olyphant
	Alexander E. Orr
	Chauncey M. Depew
	William S. Opdyke
	John Jacob Astor

Frederic Cromwell
David Willcox
R. Suydam Grant
George I. Wilber
Charles A. Peabody
E. H. Harriman
Dumont Clarke
James A. Linen

Officers

834

836

833

President, David Willcox Vice-President, Alexander E. Orr 2nd Vice-President, Abel I. Culver General Counsel, William S. Opdyke Treasurer, Charles A. Walker Secretary, F. Murray Olyphant

It is further so stipulated, that the Directors and officers of The Albany and Susquehanna Railroad Company, one of the Defendants herein, and the 835 Finance Committee of the Board of Directors of said Company elected in the year 1870 and in the subsequent years were as follows:

Sept. 6, 1870

Directors

Thomas Dickson
Abiel A. Low
J. Pierpont Morgan
Robert H. Pruyn
Thomas Olcott
Joseph H. Ramsay
William A. Rice
Minaid Harder
John Westover
John Cook
Jared Goodyear
Eliakim R. Ford
Ira E. Sherman
John Tracey (Albany City Director)

837

Officers

President, Joseph H. Ramsey Vice-President, J. Pierpont Morgan Secretary and Treasurer, William L. M. Phelps

Finance Committee

Joseph H. Ramsey Robert H. Pruyn J. Pierpont Morgan Thomas Olcott Jared Goodyear

838

Sept. 5, 1871

Directors

Thomas Dickson
George Talbot Olyphant
J. Pierpont Morgan
Robert H. Pruyn
Thomas Olcott
Joseph H. Ramsey
William A. Rice
Minaid Harder
John Westover
John Cook
Jared Goodyear
Eliakim R. Ford
Ira E. Sherman
John Tracey (Albany City Director)

840

839

Officers

President, Joseph H. Ramsey Vice-President, J. Pierpont Morgan Secretary and Treasurer, William L. M. Phelps

Finance Committee

Joseph H. Ramsey Robert H. Pruyn J. Pierpont Morgan

	211	
E 15 127	Thomas Olcott	841
	Jared Goodyear	C
Sept. 3, 1872	Directors	
	Thomas Dickson	
	David Groesbeck	
	J. Pierpont Morgan	
(no) eff	Thomas Olcott	
	Robert H. Pruyn	
	Joseph H. Ramsey	842
	William A. Rice	10
	Minaid Harder	
all all to at	John Westover	
desire an an	John Cook	
	Jared Goodyear	
	Eliakim R. Ford	
0	Ira E. Sherman	
	John Tracey (Albany City Director.)	
	0.5	
	Officers	843
Presid	lent, Joseph H. Ramsey	
	President, David Groesbeck	
	tary and Treasurer, William L. M. Phelps.	
	EA CALL SETT	
	Finance Committee	
46	Joseph H. Ramsey	
	Robert H. Pruyn	
	J. Pierpont Morgan	
	Thomas Olcott	844
	David Groesbeck	NUES!
	Propriet Manual	
Sept. 2, 1873	Directors	
V 4. 1	Thomas Dickson	
	David Groesbeck	
	J. Pierpont Morgan	
(achod)		
(achail)	Joseph H. Ramsey Robert H. Pruyn	

845

William L. M. Phelps
Minaid Harder
John Westover
John Cook
Jared Goodyear
Arnold B. Watson
Ira E. Sherman
John Tracey (Albany City Director.)

Officers

846

President, Joseph H. Ramsey Vice-President, David Groesbeck Secretary and Treasurer, William L. M. Phelps

Finance Committee

Joseph H. Ramsey Robert H. Pruyn J. Pierpont Morgan Samuel C. Thompson David Groesbeck

847

Sept. 1, 1874

Directors

Thomas Dickson
David Groesbeck
J. Pierpont Morgan
Samuel C. Thompson
Joseph H. Ramsey
Robert H. Pruyn
William L. M. Phelps
Minaid Harder

848

William L. M. Phelps
Minaid Harder
John Westover
John Cook
Jared Goodyear
Arnold B. Watson
Ira E. Sherman
John Tracey (Albany City Director.)

Officers.

President, Joseph H. Ramsey

Vice-President, David Groesbeck
Secretary and Treasurer, William L. M. Phelps

Finance Committee

Joseph H. Ramsey Robert H. Pruyn J. Pierpont Morgan Samuel C. Thompson David Groesbeck

Sept. 7, 1875

Directors

850

Thomas Dickson
David Groesbeck
J. Pierpont Morgan
Charles Tracy
Joseph H. Ramsey
Robert H. Pruyn
William L. M. Phelps
Minaid Harder
John Westover

851

John Cook Coe F. Young Arnold B. Watson Ira E. Sherman

Daniel Manning (Albany City Director.)

Officers

President, Joseph H. Ramsey Secretary and Treasurer, William L. M. Phelps 852

Finance Committee

Joseph H. Ramsey Robert H. Pruyn J. Pierpont Morgan Charles Tracy David Groesbeck 853 Sept. 5, 1876

Directors

Thomas Dickson
Coe F. Young
J. Pierpont Morgan
David Groesbeck
Charles Tracy
Joseph H. Ramsey
Robert H. Pruyn
William L. M. Phelps
Minaid Harder
John Westover
John Cook
Arnold B. Watson

854

Ira E. Sherman Daniel Manning (Albany City Director)

Set 1 Ag 24

Officers

855

President, Joseph H. Ramsey Vice-President, David Groesbeck Secretary and Treasurer, William L. M. Phelps

Finance Committee

Robert H. Pruyn J. Pierpont Morgan David Groesbeck Charles Tracy Arnold B. Watson

856 Sept. 4, 1877

Directors

Thomas Dickson
Coe F. Young
David Groesbeck
J. Pierpont Morgan
Charles Tracy
James Roosevelt
Robert H. Pruyn
William L. M. Phelps
Arnold B. Watson
John Cook

John Westover
Minaid Harder
Henry Smith
Daniel Manning (Albany City Director)

Officers

President, Coe F. Young Vice-President, Robert H. Pruyn Secretary and Treasurer, William L. M. Phelps

858

857

Finance Committee

Coe F. Young
Robert H. Pruyn
J. Pierpont Morgan
David Groesbeck
Charles Tracy
Arnold B. Watson

Sept. 3, 1878

Directors

859

860

Thomas Dickson
Charles Tracy
J. Pierpont Morgan
Robert H. Pruyn
Henry Smith
William L. M. Phelps
Coe F. Young
Arnold B. Watson
James Roosevelt
John Westover
Minaid Harder
Robert M. Olyphant
Henry M. Olmsted
Daniel Manning (Albany City Director)

Officers

President, Coe F. Young Vice-President, Robert H. Pruyn Secretary and Treasurer, William L. M. Phelps

Finance Committee

Coe F. Young
Robert H. Pruyn
J. Pierpont Morgan
Charles Tracy
Arnold B. Watson
Henry Smith

Sept. 2, 1879

Directors

862

Thomas Dickson
Coe F. Young
Charles Tracy
J. Pierpont Morgan
Robert H. Pruyn
Henry Smith
William L. M. Phelps
James R. Taylor
James Roosevelt
Robert M. Olyphant
Henry M. Olmsted
John Westover
Minaid Harder

863

Officers

President, Coe F. Young Vice-President, Robert H. Pruyn Secretary and Treasurer, William L. M. Phelps

Daniel Manning (Albany City Director)

864

Finance Committee

Coe F. Young Robert H. Pruyn J. Pierpont Morgan Charles Tracy Daniel Manning Henry Smith

Sept. 7, 1880

Directors

865

Thomas Dickson
Coe F. Young
J. Pierpont Morgan
Charles Tracy
James Roosevelt
Robert M. Olyphant
Henry M. Olmsted
James R. Taylor
Robert H. Pruyn
Robert H. Pruyn
Smith
William L. M. Phelps
Minaid Harder
John Westover
Daniel Manning (Albany City Director)

Officers (elected September 22, 1880)

President, Coe F. Young
Vice-President, Robert H. Pruyn
Secretary and Treasurer, William L. M. Phelps
867

Finance Committee

Coe F. Young
Robert H. Pruyn
J. Pierpont Morgan
Charles Tracy
Daniel Manning
Henry Smith

868

Sept. 6, 1881

Directors

Thomas Dickson
Coe F. Young
Robert H. Pruyn
John Westover
Minaid Harder
J. Pierpont Morgan
William L. M. Phelps
Charles Tracy

	218
869	James Roosevelt
	Henry Smith
	Robert M. Olyphant
	Henry M. Olmsted
	James R. Taylor
	Daniel Manning (Albany City Director)
	Officers
	President, Coe F. Young
870	Vice-President, Robert H. Pruyn
	Secretary and Treasurer, William L. M. Phelps
	Finance Committee
	Coe F. Young
	Robert H. Pruyn
	Henry Smith
	Daniel Manning
	James R. Taylor
871	Henry M. Olmsted
Sept	. 5, 1882 Directors
	Thomas Dickson
	Coe F. Young
	John Westover
	Minaid Harder
	J. Pierpont Morgan
	Charles Tracy
872	Henry Smith
	William L. M. Phelps
	Robert M. Olyphant
	Henry M. Olmsted
	James Roosevelt
	James R. Taylor
	David Dows

1	4	n
.,		u
- 4	1	u

Officers

873

President, Coe F. Young Secretary and Treasurer, William L. M. Phelps

Finance Committee

Coe F. Young
James Roosevelt
Henry Smith
Daniel Manning
James R. Taylor

Henry M. Olmsted

874

Sept. 4, 1883

Directors

Thomas Dickson
Coe F. Young
John Westover
Minaid Harder
Charles Tracy
Henry Smith
William L. M. Phelps
Robert M. Olyphant
Henry M. Olmsted

875

Robert M. Olyphant
Henry M. Olmsted
James Roosevelt
James R. Taylor
David Dows
George I. Wilber
Daniel Manning (Albany City Director)

Officers

876

President, Coe F. Young Secretary and Treasurer, William L. M. Phelps

Finance Committee

Coe F. Young
James Roosevelt
Henry Smith
Daniel Manning
James R. Taylor
Henry M. Olmsted

877 Sept. 2, 1884

878

Directors

Coe F. Young John Westover Minaid Harder

William L. M. Phelps

Charles Tracy
James Roosevelt
Henry Smith

Robert M. Olyphant Henry M. Olmsted James R. Taylor

David Dows
George I. Wilber
Alfred Van Santvoord

Daniel Manning (Albany City Director)

Officers

President, Coe F. Young
Secretary and Treasurer, William L. M. Phelps

Finance Committee

Coe F. Young
James Roosevelt
Henry Smith
James R. Taylor
Henry M. Olmsted
David Dows

880 Sept. 1, 1885

Directors

Coe F. Young
Robert M. Olyphant
John Westover
Minaid Harder
William L. M. Phelps
James Roosevelt
Henry M. Olmsted
James Ř. Taylor
David Dows

George I. Wilber
Alfred Van Santvoord
Benjamin H. Bristow
Horace G. Young
Daniel Manning (Albany City Director)

Officers

President, Coe F. Young Secretary and Treasurer, William L. M. Phelps

882

881

Finance Committee

Coe F. Young
Robert M. Olyphant
Henry M. Olmsted
James R. Taylor
David Dows
James Roosevelt

Sept. 1, 1886

Directors

883

Coe F. Young
Robert M. Olyphant
John Westover
Minaid Harder
William L. M. Phelps
James Roosevelt
Henry M. Olmsted
James R. Taylor.
David Dows
George I. Wilber
Alfred Van Santvoord
Benjamin H. Bristow
Horace G. Young
Daniel Manning (Albany City Director)

Officers

President, Coe F. Young Secretary and Treasurer, William L. M. Phelps

	222
885	Finance Committee
	. Coe F. Young
	Robert M. Olyphant
	Henry M. Olmsted
The A	James R. Taylor
	David Dows
	James Roosevelt
Sept. 6	Directors
886	Coe F. Young
	Robert M. Olyphant
	Minaid Harder
	William L. M. Phelps .
	James Roosevelt
	James R. Taylor
	David Dows
	George I. Wilber
	Alfred Van Santvoord
887	Benjamin H. Bristow
	Horace G. Young
	Robert Olyphant
	Henry M. Olmsted
	Daniel Manning (Albany City Director)
	Officers
	President, Coe F. Young
	Secretary and Treasurer, William L. M. Phelp
888	Finance Committee
	Coe F. Young
	Henry M. Olmsted
	James R. Taylor
	David Dows
	James Roosevelt

Alfred Van Santvoord

223	
Sept. 18, 1888 Directors	889
Robert M. Olyphant	
Coe F. Young	
Minaid Harder	
William L. M. Phelps	
James Roosevelt	
Henry M. Olmsted	
James R. Taylor	
David Dows	
George I. Wilber	890
Alfred Van Santvoord	
Benjamin H. Bristow	
Horace G. Young	
Robert Olyphant	
Jonathan R. Herrick (Albany City Di-	
rector)	· · ·
Officers	
President, Coe F. Young	
Secretary and Treasurer, William L. M. Phelps	891
Societary and Propositor, William D. M. Photps	/
Finance Committee	
Coe F. Young	
George I. Wilber	
Alfred Van Santvoord	
James Roosevelt	
David Dows	
James R. Taylor	
	892
Sept. 17, 1889 Directors	
Robert M. Olyphant	
Horace G. Young	
Minaid Harder	
William L. M. Phelps	
James Roosevelt	
Henry M. Olmsted	
James R. Taylor	
David Dows	
David Dows	

893

George I. Wilber Alfred Van Santvoord Benjamin H. Bristow Robert Olyphant William H. Tillinghast Jonathan R. Herrick (Albany City Director)

Officers

894

President, Robert Olyphant Secretary and Treasurer, William L. M. Phelps

Finance Committee

Robert Olyphant George I. Wilber Alfred Van Santvoord James Roosevelt David Dows James R. Taylor

895

896

Sept. 16, 1890

Directors

Robert M. Olyphant Horace G. Young Minaid Harder William L. M. Phelps James Roosevelt Henry M. Olmsted George I. Wilber Alfred Van Santvoord Benjamin H. Bristow Robert Olyphant William H. Tillinghast Robert C. Pruyn James H. Manning

Jonathan R. Herrick (Albany City Di-

rector)

Officers

897

President, Robert Olyphant • Secretary and Treasurer, William L. M. Phelps

Finance Committee

Robert Olyphant Benjamin H. Bristow James Roosevelt William H. Tillinghast Alfred Van Santvoord George I. Wilber

898

Sept. 15, 1891

Directors

Robert M. Olyphant
Horace G. Young
Minaid Harder
William L. M. Phelps
James Roosevelt
Henry M. Olmsted
George I. Wilber
Alfred Van Santvoord
Benjamin H. Bristow
Robert Olyphant
William H. Tillinghast
Robert C. Pruyn
James H. Manning
Anthony N. Brady (Albany City Director)

Officers

900

899

President, Robert Olyphant Secretary and Treasurer, William L. M. Phelps

Finance Committee

Robert Olyphant
Benjamin H. Bristow
James Roosevelt
William H. Tillinghast
Alfred Van Santvoord
George I. Wilber

901 Oct. 18, 1892

Directors

Robert M. Olyphant
Horace G. Young
Minaid Harder
William L. M. Phelps
James Roosevelt
George I. Wilber
Alfred Van Santvoord
Benjamin H. Bristow
Robert Olyphant
William H. Tillinghast
Robert C. Pruyn
James H. Manning
Charles A. Walker

902

Anthony N. Brady (Albany City Director)

Officers

President, Robert Olyphant Secretary and Treasurer, William L. M. Phelps

903

Finance Committee

Robert Olyphant Benjamin H. Bristow James Roosevelt William H. Tillinghast Alfred Van Santvoord George I. Wilber

904 Oct. 17, 1893

Directors

Robert M. Olyphant Horace G. Young Minaid Harder William L. M. Phelps James Roosevelt George I. Wilber Alfred Van Santvoord Benjamin H. Bristow Robert Olyphant

William H. Tillinghast
Robert C. Pruyn
James H. Manning
Charles A. Walker
Anthony N. Brady (Albany City Director)

Officers

President, Robert Olyphant Secretary and Treasurer, William L. M. Phelps

906

905

Finance Committee

Robert Olyphant Benjamin H. Bristow James Roosevelt William H. Tillinghast Alfred Van Santvoord George I. Wilber

Oct. 16, 1894

Directors

907

Robert M. Olyphant
Horace G. Young
Minaid Harder
William L. M. Phelps
James Roosevelt
George I. Wilber
Alfred Van Santvoord
David Willcox
Robert Olyphant
William H. Tillinghast
Robert C. Pruyn
James H. Manning
Charles A. Walker
Anthony N. Brady (Albany City Director)

Officers

President, Robert Olyphant Secretary and Treasurer, William L. M. Phelps

	228
909	Finance Committee
4-6	Robert Olyphant
	James Roosevelt
	William H. Tillinghast
	Alfred Van Santvoord
	George I. Wilber
	David Willcox
	David Williams
Oct. 18	5, 1895 Directors
910	Robert M. Olyphant
	Horace G. Young
	Minaid Harder
	William L. M. Phelps
	James Roosevelt
	George I. Wilber
	Alfred Van Santvoord
	Robert Olyphant
	William H. Tillinghast
	Robert C. Pruyn
911	James H. Manning
	Charles A. Walker
	David Willeox
	Anthony N. Brady (Albany City Director
	Officers
	President, Robert Olyphant
	Secretary and Treasurer, William L. M. Phelps
912	Finance Committee
	Robert Olyphant
	James Roosevelt
	William H. Tillinghast
	Alfred Van Santvoord
	Charles A. Walker
	David Willcox.

229	
Oct. 20, 1896 Directors	913
	010
Robert M. Olyphant	
Horace G. Young	
Minaid Harder	
William L. M. Phelps	
James Roosevelt	
George I. Wilber	
Alfred Van Santvoord	
William H. Tillinghast	
Robert C. Pruyn	914
James H. Manning	
Charles A. Walker	
Robert Olyphant	
David Willcox	
Anthony N. Brady (Albany City Director)	
Officers	
President, Robert Olyphant	2
Secretary and Treasurer, William L. M. Phelps	915
Finance Committee	
Robert Olyphant	
James Roosevelt	
William H. Tillinghast	
*Alfred Van Santvoord	
George I. Wilber	
David Willcox	
Oct. 19, 1897 Directors	916
Robert M. Olyphant	
Horace G. Young	
Minaid Harder	
William L. M. Phelps	
James Roosevelt	
George I. Wilber	
Alfred Van Santvoord	
Robert Olyphant	

917

Robert C. Pruyn James H. Manning Charles A. Walker David Willcox

Officers

President, Robert Olyphant Secretary and Treasurer, William L. M. Phelps

918

Finance Committee

Robert Olyphant
James Roosevelt
William H. Tillinghast
Alfred Van Santvoord
George I. Wilber
David Willcox

Oct. 18, 1898

Directors

919

Robert M. Olyphant Horace G. Young Minaid Harder William L. M. Phelps James Roosevelt George I. Wilber Alfred Van Santvoord Robert Olyphant William H. Tillinghast Robert C. Pruyn James H. Manning Charles A. Walker David Willcox

920

Officers

President, Robert Olyphant Secretary and Treasurer, William L. M. Phelps

231	
Finance Committee	921
	·
Robert Olyphant	
James Roosevelt	
William H. Tillinghast	
Alfred Van Santvoord	4
George I. Wilber	
David Willcox	
Oct. 17, 1899 Directors	
200000	922
Robert M. Olyphant	
Horace G. Young	
Minaid Harder	
William L. M. Phelps	
James Roosevelt	
George I. Wilber	
Alfred Van Santvoord	
Robert Olyphant	
William H. Tillinghast	
Robert C. Pruyn	923
James H. Manning	020
Charles A. Walker	
David Willcox	
Officers	
President, Robert Olyphant	
Secretary and Treasurer, William L. M. Phelps	
Finance Committee	921
- Robert Olyphant	
James Roosevelt	
William H. Tillinghast	
Alfred Van Santvoord	
Goorge I William	

George I. Wilber David Willcox

925 Oct. 16, 1900

Directors

Robert M. Olyphant
Horace G. Young
Minaid Harder
William L. M. Phelps
James Roosevelt
George I. Wilber
Alfred Van Santvoord
Robert Olyphant
William H. Tillinghast
Robert C. Pruyn
James H. Manning
Charles A. Walker

David Willcox

926

Officers

President, Robert Olyphant Secretary and Treasurer, William L. M. Phelps

927

Finance Committee

Robert Olyphant
James Roosevelt
William H. Tillinghast
Alfred Van Santvoord
George I. Wilber
David Willcox

928 Oct. 15, 1901

Directors

Robert M. Olyphant Horace G. Young Minaid Harder William L. M. Phelps George I. Wilber Robert Olyphant William H. Tillinghast Robert C. Pruyn James H. Manning

Charles A. Walker
David Willcox
James W. Alexander
R. Suydam Grant

929

Officers

President, Robert Olyphant Secretary and Treasurer, William L. M. Phelps

Finance Committee

930

Robert Olyphant George I. Wilber William H. Tillinghast David Willcox James W. Alexander R. Suydam Grant

Oct. 21, 1902

Directors

931

Robert M. Olyphant
Horace G. Young
Charles A. Peabody
William L. M. Phelps
George I. Wilber
Robert Olyphant
William H. Tillinghast
Robert C. Pruyn
James H. Manning
Charles A. Walker
David Willcox
James W. Alexander
R. Suydam Grant

932

Officers

President, Robert Olyphant Secretary and Treasurer, William L. M. Phelps

Finance Committee

Robert Olyphant
William H. Tillinghast
George I. Wilber
David Willcox
James W. Alexander
R. Suydam Grant

Oct. 20, 1903

Directors

934

Robert M. Olyphant
William L. M. Phelps
Robert Olyphant
James H. Manning
R. Suydam Grant
Charles A. Peabody
William S. Opdyke
David Willcox
George I. Wilber
Robert C. Pruyn
Charles A. Walker
James W. Alexander
Abel I. Culver

935

Officers

President, Robert Olyphant Secretary and Treasurer, William L. M. Phelps

936

Finance Committee

Robert Olyphant Charles A. Peabody George I. Wilber David Willcox James W. Alexander R. Suydam Grant

	235	
Oct. 18, 1904	Directors	937
	Robert M. Olyphant	
	William L. M. Phelps	
	Robert Olyphant	
	James H. Manning	
	R. Suydam Grant	
	Charles A. Peabody	
	William S. Opdyke	
	David Willcox	
	George I. Wilber	938
	Robert C. Pruyn	
	Charles A. Walker	
	James W. Alexander	
	Abel I. Culver	
one kinnili	Officers	40
Presid	lent, Robert Olyphant	
Secre	tary and Tressurer, William L. M. P	helps
	Finance Committee	939
	Robert Olyphant	
	Charles A. Peabody	
	David Willcox	
	George I. Wilber	
	James W. Alexander	
	R. Suydam Grant	
Oct. 18, 1905	Directors	
	Robert M. Olymbant	940
	Robert M. Olyphant William I. M. Phalma	
	William L. M. Phelps	
	Robert Olyphant	
	James H. Manning B. Suydam Grent	
	R. Suydam Grant Charles A. Peabody	
	William S. Opdyke David Willcox	
	George I. Wilber	
of party in	Robert C. Pruyn	
	1000tt O. 11dyn	

Charles A. Walker James W. Alexander Abel I. Culver

Officers

President, Robert Olyphant Secretary and Treasurer, William L. M. Phelps

Finance Committee

942

Robert Olyphant Charles A. Peabody David Willcox George I. Wilber James W. Alexander R. Suydam Grant

May 9, 1906 David Willcox, elected Vice-President.

Oct. 16, 1906

Directors

943

John S. Kennedy
John Harsen Rhoades
Adrian H. Joline
George P. Butler
William Stewart Tod
Isaac N. Phelps Stokes
Howard O. Sturges
George L. Shearer
William A. W. Stewart
Charles P. Cooley
John H. McClement
Arthur W. Butler
Henry E. Howland

944

Officers

President, George P. Butler Secretary and Treasurer, William L. M. Phelps.

That in all of the above-mentioned years the by-laws of said company provided that the Board of Directors

of said company should consist of thirteen stock- 945 holders.

That under the provisions of section 8, chapter 195, Laws of New York of 1852, the Common Council of the City of Albany, during the existence of the Ioan herein elsewhere referred to as the Albany loan, appointed annually one director of The Albany and Susquehanna Railroad Company as follows:

In the year 1870 John Tracey, who served as such director until 1875, when Daniel Manning was appointed and continued to be a director of said 946 company until the year 1888, when he was succeeded by Jonathan R. Herrick, who continued a director as aforesaid until 1891, when he was succeeded by Anthony N. Brady, who continued to be a director until the office ceased in 1897 with the payment of the last instalment of the Albany loan.

It is further so stipulated, that for over thirty years last past the by-laws of The Albany and Susquehanna Railroad Company have provided for the 947 election of a Finance Committee of five Directors in addition to the President of the Company, and that the duties of said Finance Committee are stated in the following by-law, which is now and during all the period above mentioned has been in force:

"Section 11. The finance committee shall have the general direction in respect to the collection of instalments, the issue of stock and bonds, and generally the supervision of all receipts and disbursements of the moneys of the company. They shall, at least once in three months, examine the accounts of the treasurer, and report to the board the amount of money on hand, and all other particulars which they may deem proper, connected with the financial concerns of the company, and also direct as to the manner of keeping the books and accounts."

300

Here, also for the record, is a copy of *EMP timetable No. 48 10-28-1962*, *Binghamton / Kenwood / Binghamton*. Posted by George Ford, Jr. on *Facebook* on February 20, 2016.

NORTHWARD—FIRST CLASS						SOUTHWARD—FIRST CLASS						
Distance from Binghamton	STATIONS	Car capacity of Passing sidings	205 DAILY EXCEPT SUNDAY SEE NOTE			Distance from Albany	STATIONS	Car capacity of Passing sidings	DAILY EXCEPT SUNDAY SEE NOTE			
	RINGHAMTON		AM 7.20		1.845	Juliana	ALBANY		PM / 15			
0.9	BINGHAMTON ROBINSON ST. TOWER		7.20			1.5	KENWOOD		4.10			
2.7	IU CABIN		7.27			5.5	DELMAR					
4.7	SA CABIN					1010	VOORHEESVILLE					
12.4	FH CABIN					11.0	CORBIN	27	4.35			
13.9	BL CABIN					17.2	ALTAMONT D	24	8 4.44			
15.6	TUNNEL					26.2	DJ CABIN		4.59			
-	VI CABIN						DELANSON					
22.2	HARPURSVILLE						JX CABIN					
27.5	GR CABIN						CB CABIN					
28.2	AFTON						CENTRAL BRIDGE					
34.1	BAINBRIDGE						WH CABIN					
35.8	LK CABIN					44.7	COBLESKILL					
38.0	TP CABIN						KF CABIN					
39.3	SIDNEY					50.0	RICHMONDVILLE		8 5.48			
43.7	UNADILLA					54.7	DE CABIN					
47.4	UA CABIN					56.9	EAST WORCESTER.		f 6.00			
52.9	UN CABIN		f 8 48				WORCESTER		8 6.07			
54.9	RB CABIN		. 0.40			66.5	SCHENEVUS		8 6.15			
57 2	OG CABIN	2000					WN CABIN					
60.0	FA TOWER D-N		8.58				COOPERSTOWN JCT		f 6.26			
60.9	ONEONTA		8 9.10				MU CABIN		1 6.26			
61.0	CM CABIN						RA CABIN					
61.9	RA CABIN						CM CABIN.					
65.6	MU CABIN					81.7	ONEONTA	3000000	8 6 42	339233333	50000	
67.0	COOPERSTOWN JCT.					82.6	FA TOWERD-N		6.44			
75.2	WN CABIN					85.3	OG CABIN					
76.0	SCHENEVUS					87.6	RB CABIN					
81.0	WORCESTER					89.6	OTEGO		1 6.52			
85.7	EAST WORCESTER					95.1	UN CABIN					
87.8	DE CABIN					98.8	UA CABIN UNADILLA		f 7.04			
92.6	RICHMONDVILLE		8 9.58			103.3	SIDNEY		8 7.16			
96.0	KF CABIN					104.5	TP CABIN					
97.9	COBLESKILL					106.7	LK CABIN					
103.4	HOWES CAVE		810.21			10010	BAINBRIDGE		8 7.25			
106.3	CENTRAL BRIDGE	**	810.30			114.3			8 7.33			
108.0	CB CABIN					118.0						
113.9	JX CABIN						HARPURSVILLE		1 7.41			
115.8	DELANSON		810.45			120.3	VI CARIN	100000	100000	723577530		
116.4	DJ CABIN		10.46			127.0	TUNNEL					
125.4	ALTAMONTD	24	811.01			128.6	BL CABIN	1/80188030				
131.2	CORBIN	37	11.08			130.1	FH CABIN					
131.7	VOORHEESVILLE	1111	111.10			137.8	SA CABIN					
	KENWOOD		f 11.18				YO CABIN		8.16			
-	ALBANY		11.25 A11.30			141.7	ROBINSON ST. TOWER					
.72.0	AUDAN I		AM AM			142.5	BINGHAM ION		A 8.25 PM			
		-	205			-			208		-	
	-		200						200		-	

Jefferson Branch of the Erie Railroad

The Jefferson Branch of the Erie Railroad, opened on October 10, 1870. This was the subject of Volume XI in this series. The Jefferson Branch, it must be kept in mind, was a key rail line in the D&H's expansion northward. Every D&H train that entered the state of New York from Pennsylvania passed over the Jefferson Branch of the Erie.

Construction of the Jefferson Branch was completed on October 10, 1870, at which time trains from Carbondale could connect with the Erie main line at Lanesboro Junction. On October 28, 1870, the first loads of coal left Carbondale for Lanesboro. In 1871-1872, the iron rails on the Jefferson Branch were replaced with heavy steel rails on part of the line.

1205

Rensselaer and Saratoga Railroad

On May 1, 1871, the Rensselaer and Saratoga Railroad, which was incorporated on April 14, 1832, was leased to D&H. The R&S was merged into the D&H on July 2, 1945.

Two important sources for the history of the R&S are:

- 1. Century of Progress, pp. 654-702: The Rensselaer and Saratoga Rail Road Company and Its Constituent and Leased Lines.
- 2. Delaware & Hudson by Jim Shaughnessy, pp. 89-114: Chapter 5. An Empire in the North.

The basic facts on the R&S are presented in those two sources. The material that we present below is intended as a supplement to those two fine sources of information.

It is well to re-state here a few very interesting facts about the R&S.

On April 14, 1832, The Rensselaer and Saratoga Rail Road Company was incorporated by the New York legislature with a capital of \$300,000 and authority to construct a railroad from Troy, via Waterford, to Ballston Spa, for the conveyance of persons and property, its charter to have a life of fifty years.

This charter also permitted the R&S to construct a bridge across the Hudson River (from Troy to Green Island). The cars were drawn by horses from the Troy terminus (10 First Street) through River Street and across the bridge to Green Island where a steam locomotive was waiting to pull them for the remainder of the journey. Richard P. Hart was elected president of the company. The track was constructed of timber rails laid on cedar sleepers and covered with strap-iron rail.

Most interestingly, Philip Hone visited this railroad operation. In his diary, after a trip from Ballston Spa to Troy on Saturday, June 25, 1836, he wrote the following in his diary:

"We came . . . to Troy, in two hours and a quarter, through a very fine country, without interruption, and faster than I have ever traveled on the Mohawk and Hudson road. The enterprise of the Trojans has raised up a powerful rival to the other two lines. The road starts from Troy by a long noble bridge to the Island opposite. . . / The Troy Rail Road has a great advantage over the Mohawk and Hudson, in its having no inclined planes, a most serious difficulty, and one which travelers always look upon with apprehension; several accidents have happened on the road between Honesdale and Carbondale, but that is seldom used by travelers, and a dreadful one on the Quincy road a few years since, by which several persons were killed and my friend, Belknap, seriously injured. This, I am informed, might have been avoided in the case of the Mohawk and Hudson Road, by entering Schenectady at the lower end of the City, but the Directors were tempted to adopt the present route from its affording them a better chance of speculation in the adjacent lands. . . "

From its modest beginnings in 1832, the R&S grew into a sprawling network of rails between Albany and Whitehall, New York.

In *Century of Progress* (pp. 220-221) we read: "... the original Rensselaer & Saratoga railroad, reaching from Troy to Ballston Spa, near Saratoga Springs, had grown by the acquisition of most of the rail facilities north of Albany and Troy, and extending as far as Lake Champlain and Rutland, into a comprehensive and serviceable unit..." *COP*. pp. 220-21.

Just before the Civil War, George H. Cramer was elected president of the R&S. Shown below is a receipt from the Rensselaer and Saratoga Railroad Company, dated January 3, 1868, signed by George H. Cramer, for 50 shares of R&S stock, at \$100 per share, that were purchased by L. G. B. Cannon. The original of this receipt was donated by John V. Buberniak to the Carbondale D&H Transportation Museum.

STATE OF NEW YORK. Shares. Rensselaer & Saratoga Railroad Company. Incorporated 1832. Consolidated Capital Stock, \$2,500,000. SHARES \$100 EACH. It is Hereby Certified that Te J. Dannon W entitled to Shares of one hundred dollars each in the capital stock of the Rensselaer and Saratoga Rail Road Company, transferable only on the books of said Company, by the said stockholder or his attorney, and on the surrender of this certificate. In witness whereof, the said Company has faused this cortificate signed by its President and Secretary at the aty of Troy, this day Danning A. D. 180 8 President.

Who were the leaders of the D&H at the time?

Coe F. Young was General Manager.

During President Dickson's around-the-world tour in 1871, the leadership of the D&H was in the hands of George Talbot Olyphant (temporary president) and Coe F. Young, General Manager of the D&H. In *Hollister* (pp. 183-84) we read the following about Coe F. Young:

"COE F. YOUNG, General Manager of the Delaware & Hudson Canal Company's Railways, living in Honesdale, Pennsylvania, a man of ministered deportment, of good sense and quick perception, thoroughly educated in the art of constructing and managing railways, enjoyed the high consideration then that he retains yet for the possession of qualifications requisite in the successful management of railroads, and was thus enabled to discharge the double duties of President and Manager with becoming tact and discretion. It was during this year that the Company proposed to extend this last acquired railway [R&S] along the western margin of Lake Champlain and nothing but a delay in getting the right of way, postponed work upon it until 1872." (H. Hollister, *History of the Delaware and Hudson Canal Company*, 1879, p. 183).

On December 7, 1870, the D&H presented a lease proposal to the R&S. In *COP* (p. 671), we read:

"A proposal from the Delaware and Hudson Canal Company to lease the property [and franchises] of this company [the R&S] was presented to the Board on December 7, 1870, and the negotiation of a permanent lease was authorized. The result, a lease of the property [most of the rail lines between Albany and the head of Lake Champlain and Rutland, Vermont; steamboats as well] in perpetuity to the Delaware and Hudson Canal Company, became effective on May 1, 1871, and on that day the Rensselaer and Saratoga corporation retired from the field as a transportation company."

The financial terms of the lease were as follows: \$600,000 for the current year, \$700,000 for the next, and \$750,000 a year thereafter.

Under this agreement, the D&H assumed the control of an additional one hundred and eighty-one miles of rail lines. In *COP*, we read:

"As The Rensselaer and Saratoga Rail Road Company operated, at the time of this lease, the road from Albany to Waterford Junction that had been competed in 1853 by the Albany Northern Rail Road, and as Albany is seventy-seven and one-half miles south of Whitehall, the Rensselaer and Saratoga system, operating to Whitehall and thence to Rutland, and returning from Rutland to Eagle Bridge, a comparatively short distance from Troy, embraced a railroad system nearly one hundred and eighty miles in length." (COP, 224)

Hollister, appropriately, referred to this lease of the R&S by the D&H as "another step towards Canada.":

". . . Another step towards Canada was taken by the Company, May 1, 1871 in getting a perpetual lease of the property and rights of Rensselaer and Saratoga Railroad Company. The absence of President Dickson for a year, in his tour around the world in 1871, altered no prearranged measures of development or delayed pulling the latch strings of Canadian doorways."

It's always interesting, in the nineteenth century as now, to take a close look at the boards of directors and officers when corporate acquisitions and mergers take place.

Who were the officers and directors of the D&H at the time that this "step towards Canada" took place?

The officers of the D&H in 1870 were:

President, Thomas Dickson, Scranton, Pa.; Treasurer, Chas. P. Hartt, New York City; Secretary, Richard H. Nodyne, New York City; Sales Agent, James C. Hartt, New York City. / General Superintendent, Coe F. Young, Honesdale, Pa.; Superintendent of Coal Department, E. W. Weston, Providence, Pa.; Superintendent of Railroad Department, R. Manville, Carbondale, Pa.; Superintendent of Canal Department, Asher M. Atkinson, Honesdale, Pa.; Sales Agent Southern and Western Department, James J. Albright, Scranton, Pa.

The managers/directors of the D&H in 1870 were:

Charles N. Talbot, Edward J. Woolsey, George Talbot Olyphant, Abiel A. Low, Robert Lenox Kennedy, James M. Halsted, Le Grand B. Cannon, James R. Taylor, Thomas Dickson, John Jacob Astor, Thomas Cornell, W. J. Hoppin, Isaac N. Seymour

Just before the Civil War, George H. Cramer was elected president of the R&S. His brother-in-law, Le Grand B. Cannon, we note, was a director/manager of the D&H in 1870. LeGrand B. Cannon was also a key figure in The Lake George Steamboat Company and The Champlain Transportation Company, and in the leasing of the R&S by the D&H on May 1, 1871.

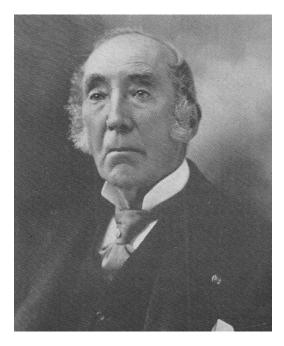
In *Century of Progress*, there are two very important paragraphs about the history of The Lake George Steamboat Company and The Champlain Transportation Company and on the relationship of those two companies to the R&S and to the D&H.

From those two paragraphs, we have learned that beginning in 1872, via LeGrand B. Cannon, The Lake George Steamboat Company and The Champlain Transportation Company were both a part of the D&H transportation system.

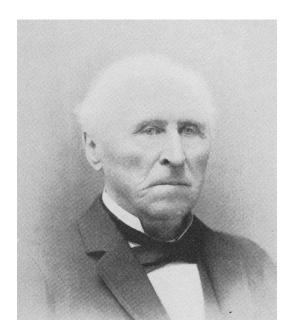
- 1. "In 1866 stock control of The Lake George Steamboat Company was purchased by LeGrand B. Cannon and others connected with The Champlain Transportation Company and The Rensselaer and Saratoga Rail Road Company and Mr. Cannon became president of the company on February 20, 1866. This control passed to The Lake Champlain Company in 1868 but the Lake George Steamboat Company continued to operate the property under its own organization until the termination of its charter on February 15, 1869, when operation was taken over by The Champlain Transportation Company and continued by that corporation until May 15, 1872. On March 2, 1872, articles of association were filed chartering The Lake George Steamboat Company with powers and rights similar to those of its predecessor of the same name which had passed out of existence in 1869. This company organized, on May 17 of the same year, Mr. Cannon being chosen its first president. The new company took over the property of the former and has continued operating the boats on Lake George ever since. All its stock is owned by The Champlain Transportation Company. Through this stock The Lake George Steamboat Company is now [1923] affiliated with The Delaware and Hudson Company as part of its transportation system." (pp. 722-23)
- 2. "Early in 1856, LeGrand B. Cannon purchased control of the company [The Champlain Transportation Company] from Messrs. Drew, Robinson and Kelley and later transferred it to The Rensselaer and Saratoga Rail Road Company. . . In 1866, Mr. Cannon and others interested in the Rensselaer and Saratoga and The Champlain Transportation Company acquired control of The Lake George Steamboat Company which had been incorporated on February 16, 1854, and operated the steamers *Minnehaha* and *Ganouski* on Lake George. In January, 1868, control of The Lake George Steamboat Company passed to The Champlain Transportation Company. Upon the termination of the charter of The Lake George Steamboat Company on February 15, 1869, its property was operated by The Champlain Transportation Company until 1872 when the present The Lake George Steamboat Company was incorporated on March 2* and took over all the property of the former whose charter had expired. The Champlain Transportation Company has since been the owner of the capital stock of the Lake George company." (pp. 713-714)

*On March 2, 1872, Articles of Association were filed by Le Grand B. Cannon and George Talbot Olyphant of the City of New York, George H. Cramer and Isaac V. Baker of the City of Troy, Henry S. March of the Village of Green Island, A. L. Inman of the City of Burlington, Vermont, and Z. V. K. Willson of the Town of Rutland, bringing into existence The Lake George Steamboat Company. Colonel Cannon was elected the first president of the new company, and Isaac V. Baker was elected vice president. Baker, at the time, was president of The New York and Canada Railway and later vice president of the Glens Falls Railroad.

Given below are the photographs of Cannon and Baker that are given in *The Steamboats of Lake George 1817 to 1932*, published in 1932 by The Lake George Steamboat Company:



Colonel Le Grand B. Cannon



Isaac V. Baker

With the R&S now a part of its vast transportation system, the D&H made improvements to and along the former R&S right from the start

October 1871: D&H subscribed to \$25,000 worth of bonds in the new United States Hotel as a gesture in bettering the accommodations at Saratoga Springs and attracting even more summer travelers to former R&S rails.

In 1872, Thomas Dickson reported to the managers that the coal sold over these new lines had made a good profit.

1871-1872: the iron rails between Troy and Saratoga were replaced with heavy steel rails.

In 1873, steel rails were laid on the Rensselaer and Saratoga road, some between Albany and Waterford Junction and the remainder north of Saratoga Springs; double tracking between Ballston Spa and Saratoga Springs was initiated. In *COP*, pp. 227-28, we read:

"In 1873 "2,564 tons of steel rail were laid on the Rensselaer and Saratoga road, some between Albany and Waterford Junction and the remainder north of Saratoga Springs. On December 30 double tracking this road between Ballston Spa and Saratoga Springs was ordered completed whenever, in the opinion of the president, the traffic might require it. As during the busy season of Summer travel as many as one thousand passengers were, at this time, frequently carried on a single train, it is not surprising that the Managers agreed to this improvement." *COP*, pp. 227-28

With the R&S under its control, the D&H now had a rail line from Albany to Whitehall.

Six months after the opening of the L&S (which we will take a look at in the pages that follow), the D&H was able to ship coal in its own cars from the mines at Carbondale to Oswego, NY. At the same time, "flour, lumber and other article of commerce" from Oswego could be shipped south from Oswego "to all places in connection with the line without transshipment."

In the January 13, 1872 issue of the *Carbondale Advance*, we read:

"Direct Shipments of Coal North by the D. & H. Canal Co. / From the Oswego 'Advertiser', we learn that the fast through coal train, direct from the mines of the Delaware & Hudson Canal Company, arrived in that city on the Midland, recently. Hitherto, owning to difference in gauge, all the coal brought over the Midland has been transferred from the broad gauge cars of the

Midland at Sidney Plains. After this the coal will come directly through without trans-shipment from the coal fields to Oswego making a great saving in time and in the quantity of coal brought over the line. This improvement has been brought about by the Delaware & Hudson Canal Company, which has constructed a new road from Nineveh, a point on the Albany & Susquehanna Railroad, to Carbondale and placed a third rail upon the Albany & Susquehanna line, which was formerly broad gauge only. The train which arrived in Oswego was composed entirely of the cars belonging to the Del. & Hud. Canal Co. [emphasis added]. This is an advantage which will work both ways. While it will bring coal to Oswego without interruption, it will permit our flour, lumber and other articles of commerce to be returned to all places in connection with the line without transhipment." (Carbondale Advance, January 13, 1872, p. 3)

With the R&S now in its portfolio, the D&H began to look towards Canada as a market for its coal. In 1872, an initial survey of a railroad route to Rouse's Point was made by the D&H:

"Further Extension: / We learn that an engineer of the Del. & Hud. C. Co. left Albany, last week, to survey a railroad route to Rouse's Point, with the view of opening a route for transporting coal direct to Canada." (*Carbondale Advance*, June 29, 1872, p. 3)

In 1874, the D&H took appropriate action to be able to construct a rail line from Schenectady to Ogdensburgh. In the *Carbondale Leader* of May 2, 1874, we read:

"The D. & H. C. Co., with its vast coal mines for a starting point, is penetrating rapidly northward through the Champlain region, but not content with this, says the *Saratogian*, it has availed itself of the advantages of a clause in a certain statute, passed a few years since, authorizing any company to become possessed of the rights and franchises of a railroad between Schenectady and Ogdensburgh. The condition of possession is the payment into the State treasury of the sum of \$35,000, the expenses involved in making the survey. This sum, it is stated, has been paid or is to be paid by the D. & H. C. Co. As the Company has already leased the roads from Schenectady to its mines, the construction of a road from Schenectady to Ogdensburgh would afford an unbroken line from the mines of the Company to the northern lake ports and cities." (*Carbondale Leader*, May 2, 1874, p. 3)

The D&H leased the R&S on May 1, 1871. Less than a year later, on January 1, 1872, the D&H rail line from Jefferson Junction to Nineveh, the Lackawanna & Susquehanna Railroad, was opened. We will now take a closer look at the L&S, an extraordinarily important link in the D&H's rail lines to the north.

Lackawanna and Susquehanna Railroad

Lackawanna & Susquehanna Railroad, opened on January 1, 1872. Built by the D&H; 23.3 miles from Jefferson Junction to Nineveh.

The Jefferson Branch of the Erie Railroad from Carbondale to Lanesboro Junction opened on October 10, 1870, and five days later, on October 15, 1870, the D&H advertized for proposals to construct the Lackawanna and Susquehanna Railroad from Lanesboro to Nineveh:

"Lanes-Boro & Nineveh R. R. / The Del. & Hud. C. C., advertises for proposals for building the R. R. from Lanes Boro to Nineveh, to be received at their office in Nineveh, until Thursday, October 27th. / This indicates the immediate construction of that important link." (*Carbondale Advance*, October 15, 1870, p. 3)

The D&H was able to move forward quickly on this proposed construction project since authorization from the state of New York to build or operate such a line was already in place, the D&H, in 1867, in preparation for possible expansion into New York, having acquired on May 9, 1867, railroad privileges in the state of New York. As such, from May 9, 1867 on, the D&H was endowed by the state of New York with all the powers and privileges enjoyed by railroad companies incorporated under the general railroad act of April 2, 1850.

To construct the portion of the L&S in Pennsylvania, between the New York state line and Lanesboro, the D&H needed authorization from the Commonwealth of Pennsylvania to do so. That authorization was given by the Pennsylvania legislature on May 12, 1871. Here is the text of that law:

LAWS OF 1871, No. 701.

- AN ACT TO AUTHORIZE THE PRESIDENT, MANAGERS AND COMPANY OF THE DELAWARE AND HUDSON CANAL COMPANY TO CONSTRUCT A RAILROAD FROM THE POINT OF INTERSECTION OF THE LACKAWANNA AND SUSQUEHANNA RAILROAD WITH THE LINE BETWEEN THE STATES OF PENNSYLVANIA AND NEW YORK TO AN INTERSECTION OF THE JEFFERSON RAILROAD, AT OR NEAR THE VILLAGE OF SUSQUEHANNA, IN SUSQUEHANNA COUNTY, IN THE STATE OF PENNSYLVANIA.
- SEC. 1. Be it enacted by the Senate and House of Representatives of the Commonwealth of Pennsylvania, in General Assembly met, and it is hereby enacted by the authority of the same, That the President, Managers and Company of the Delaware and Hudson Canal Company be, and they are hereby authorized to locate and construct a railroad from a point where the Lackawanna and Susquehanna Railroad intersects the State line between the States of Pennsylvania and New York, to an intersection of the Jefferson Railroad at or near the village of Susquehanna, in the County of Susquehanna, in the State of Pennsylvania.
- SEC. 2. That the said President, Managers and Company of the Delaware and Hudson Canal Company, in the location and construction of the said railroad shall have all the power, authority and privileges given in the tenth section of the Act entitled "An Act regulating railroad companies," approved the nineteenth day of February, Anno Domini one thousand eight hundred and forty-nine; and all damages in the location and construction of the said railroad for right of way and land and materials taken, or otherwise, if the parties cannot agree therefor, shall be secured and assessed and paid in the manner provided by the said Act of Assembly, entitled "An Act regulating railroad companies," and the several supplements thereto.

James H. Webb, Speaker of the House of Representatives.

WILLIAM A. WALLACE,

Speaker of the Senate.

Approved—the twelfth day of May, Anno Domini one thousand eight hundred and seventy-one.

INO. W. GEARY.

On October 28, 1870, the bids for the construction of the L&S were opened and the work all allotted. The work of no less than four of these winning contractors was already known to the D&H: Palmer ("familiarly known here as 'Fones Palmer'"), W. F. Rhoads ("who has a section on the new Locomotive extension four miles below town [the Valley Road]"), Patrick Kearney ("a well known and popular contractor in our valley"), and Bryce Blair ("the efficient Chief Engineer of the just completed Jefferson Railroad during its construction."). In the *Carbondale Advance* of November 5, 1870, we read:

"Lackawanna & Susquehanna Railroad. / Proposals were received by the Del. & Hud. C. Co. for the construction of this new Railroad at their office in Nineveh, until Oct. 27th / On the 28th the bids were opened and the work all alloted [sic]. The sections are numbered from the Albany & Susquehanna R. R. at Nineveh, and were declared as follows: / Sec. 1 Andrew Coyne / Sec. 2 M. B. Morgan, with Bridge over the Susquehanna. Sec. 3 & 4 John Abel / Sec 5 & 6 House & Palmer / Sec 7 8 9 & 10 ___ _____ Fleming / Sec 11 & 12 W. F. Rhoads / Sec 13 14 & 15 Patrick Kearney / Sec 16 & 17 Gallagher & Daley / Sec 18 & 19 Bryce R. Blair / Of these several contractors, Mr. Palmer of Section 5 and 6, is familiarly known here as 'Fones Palmer'—Mr. Rhodes has a section on the new Locomotive extension four miles below town, Mr. Kearney is a well known and popular contractor in our valley, and Mr. Blair of sections 18 & 19 is one of our energetic citizens and was the efficient Chief Engineer of the just completed Jefferson Railroad during its construction. / This new Railroad, which is destined to form so important a link, in the Northern extension of the business and influence of the Del. Hud. C. Co., is 21 ½ miles in length, as it forms its connection with the Jefferson R. R., 1 ½ miles on this side of Lanesboro. By this means it passes under the N. Y & E. at the Starrucca Viaduct and secures a good connection and good grade. The distance from Lanesboro to Nineveh is we believe 20 miles, and from Carbondale to Nineveh 56 1/2 miles. / The distance from Scranton to Nineveh via Binghamton, is 84 miles, while from Scranton to Nineveh, via Carbondale and Lanesboro it will be about 74 miles—giving this new road the advantage of 10 miles. / The distance from Carbondale via Scranton to Nineveh is 101 miles, while by the new route which is to be completed in 9 months, it will be, as stated, 56 ½ miles, a saving of almost one half." (Carbondale Advance, November 5, 1870, p. 3)

The newspaper notice given above from the October 15, 1870 issue of the *Carbondale Advance*, in which proposals were requested for building the D&H line from Lanesboro to Nineveh, as well as the newspaper notice given above from the November 5, 1870 issue of the *Carbondale Advance* about the opening of the bids received to build that line and the awarding of the contracts for the construction of the L&S are public documents which we regard as factual.

We say that for the very particular reason that we read in *COP* (p. 216) the following about the construction of the L&S: "Accordingly, on October 5, 1870, a contract was made with General Andrew S. Diven for the construction of the 'Lackawanna and Susquehanna' railroad, the present Nineveh branch. This road was to extend from Jefferson Junction, Pennsylvania, to Nineveh, New York, and to connect at the latter point with the Albany and Susquehanna. . . . By the terms of the contract the road was to be completed by October 1, 1871, and to cost about \$635,000, half of this payable to the contractor in cash and the other half in Albany and Susquehanna second mortgage bonds then held by the company. As frequently happens, and in spite of the fact

that General Diven was a picturesque and forceful man, the date of completion had to be postponed; but that some through coal trains were run over this new route early in December, 1871 appears from the letter books of Coe F. Young, then general superintendent, that have been preserved in the company's archives. The formal opening of the line between Nineveh and Jefferson Junction took place on June 17, 1872..."

Century of Progress says that Andrew S. Diven was awarded the contract for constructing the L&S on October 5, 1870, which is ten days before the newspaper notice, given above, requesting bids was published. On October 28, 1870 (see above notice published in the Carbondale Advance), the contracts for construction were awarded. General Diven, "a picturesque and forceful man," is not named in the public press among the contractors who were awarded contracts for building the L&S. General Diven did not complete the construction of the L&S as his contract required by October 1, 1871. Conflicting statements: Century of Progress and the public press. Possibly General Diven was awarded a contract to build the L&S on October 5, 1870. What probably happened is that for one reason or another the D&H, after signing a contract with General Diven, "a picturesque and forceful man," came to the conclusion that he was the wrong man for the job and cancelled the contract in the twenty-day period following the signing of the contract, and then advertized, on October 25th in the Carbondale Advance for contractors to submit bids to construct the L&S, and on October 28, 1870 awarded the bids given in the newspaper notice published on November 5, 1879 in the Carbondale Advance.

Work on the construction of the L&S, by the contractors who were awarded contracts on October 28, 1870, was underway by mid-March 1871:

"Lackawanna and Susquehanna. / The next link of railroad beyond the Jefferson, in the extension north from our town, now being built, is called the Lackawanna and Susquehanna. It begins one and a half miles on this side of Lanesboro and extends to Nineveh, on the Albany and Susquehanna Railroad." (*Carbondale Advance*, March 18, 1871, p. 3)

Authorization to build the L&S, as we noted above, was in place with the state of New York on May 9, 1867. Authorization to build in Pennsylvania was in place on May 12, 1871. The construction of the line that is referred to in the article from the *Carbondale Advance* of March 18, 1871, given immediately above, must, therefore, have been on the New York section of the line, since authorization to build in Pennsylvania was not in place at the time. The numbering on the sections of the line, at the time that the bids for construction were allotted, we note, began at Nineveh. Construction of the line possibly also began at Nineveh, and by the time construction of the line in Pennsylvania was begun, authorization to do so (May 12, 1871) surely must have been in place.

To construct the L&S, in 1871 the D&H appropriated \$900,000.

The formal opening of the L&S from Nineveh to Lanesboro, the so called "Easy Line," took place on June 17, 1872. It was 22.01 miles long, with three rails laid on the line, making it possible for both D&H standard-gauge cars and Erie six-foot gage cars to travel over the line.

With the completion of both the Jefferson Branch of the Erie (Carbondale to Lanesboro Junction) and the Lackawanna and Susquehanna (Lanesboro to Nineveh), and with a third rail in the A&S for the movement of standard-gauge cars over the line, it was now possible for the D&H to ship coal from the Lackawanna Valley directly to Albany—and beyond. In H. Hollister's *History of the Delaware and Hudson Canal Company*, 1879, we read:

"In 1871 the Lackawanna & Susquehanna Railway running from Susquehanna to Nineveh, a distance of twenty miles, was completed; a third rail for the use of the narrow gauge cars ["narrow" here meaning narrow in relationship to the Erie's 6-foot gauge, that is to say, 4 feet 8 ½"], was laid on the Albany and Susquehanna Road, thus completing a continuous and direct railroad line from Wyoming Valley to Albany. . ." (H. Hollister, *History of the Delaware and Hudson Canal Company*, 1879, p. 183).

In the 1890s Summary, we read the following about the L&S:

"About the year 1870, the Lackawanna & Susquehanna Railroad (Nineveh Branch) was completed. Previous to this, Albany & Susquehanna crews ran into Carbondale over the tracks of the Erie from Binghamton, and the Nineveh Branch was constructed from Nineveh to connect with the Erie at Jefferson Junction to shorten the distance and facilitate the movement of trains."

This through rail line to Nineveh, which went almost due North from Lanesboro to Nineveh, was almost 27 miles shorter than the route from Jefferson Junction to Binghamton to Nineveh.

From *COP* (pp. 216-17) we learn that this "through rail communication with Albany . . . had been desired for six and one-half years, and the precise plan of which had been visualized at least since the Spring of 1869, when President Olyphant prepared the annual report for 1868 and referred in it to the desirability of constructing the 'easy line from Susquehanna to Nineveh, on the Albany and Susquehanna railroad, a distance of 20 miles.' "

In November 1872, the D&H erected a ticket office at the Nineveh Junction of the L&S and the A&S:

"The D. & H. C. Co. is erecting a ticket-office at the Ninevah [sic] Junction of the Lackawanna and Susquehanna and Albany and Susquehanna roads." (*Carbondale Leader*, November 30, 1872, p. 3)

On April 14, 1873, George Talbot Olyphant (former D&H president and still a manager) died.

On May 10, 1873, Robert M. Olyphant, a younger brother of George, was elected a Manager; eleven years later, he became the sixth president of the D&H.

On October 31, 1874, it was announced in the *Binghamton Times* that:

- (1) the Nineveh branch (the L&S) will no longer be under the charge of H. S. Morse, the superintendent of the A&S. Instead, the Nineveh branch will pass under the management of the Carbondale division, R. Manville Superintendent
- (2) the D&H will soon erect of a round house, blacksmith shop, and depot at Nineveh junction, and lay out a yard there, putting down a number of tracks to accommodate their coal trains. A large force of men is now engaged at Nineveh junction in grading, and most of the work of building will be completed by fall.
- (3) Nineveh junction will be, in the future, the terminus of the Carbondale division which heretofore ended at Lanesboro.
- (4) The Nineveh branch will be supplied with twelve engines from the Albany and Susquehanna division in addition to those already on the Carbondale division, of which it will hereafter form a part. (The engine on the turntable at Lanesboro in Volume 11 is A&S No. 3.)

Here is that announcement, as reprinted in the October 31, 1874 issue of the *Carbondale Advance*:

"Delaware and Hudson Canal Company. / The Delaware and Hudson Canal company are making arrangements by which the Nineveh branch will pass under the management of Mr. R. Manville superintendent of the Carbondale division, on the first of November. / Heretofore this branch has been under the charge of Mr. H. S. Morse, the efficient superintendent of the Albany and Susquehanna railroad, whose ability and experience have placed that road in excellent condition and among the safest lines for public travel in the state. The company will soon begin the erection of a round house, blacksmith shop, and depot at Nineveh junction, and lay out a yard there, putting down a number of tracks to accommodate their coal trains. Already a large force of men is engaged in grading, and most of the work of building will be completed this fall. Nineveh junction will be in the future, the terminus of the Carbondale division which heretofore ended at Jefferson junction. The branch will be supplied with twelve engines from the Albany and Susquehanna division in addition to those already on the Carbondale division, of which it will hereafter form a part.—Binghamton Times." (Carbondale Advance, October 31, 1874, p. 3)

In March 1875, the D&H was compelled to withdraw the train that it ran from Carbondale to Nineveh, which passed over the Jefferson Branch from Carbondale to Jefferson Junction, and from there over the Lackawanna and Susquehanna to Nineveh. They had to do so because the Erie Railroad did not consider the arrangement profitable to their company. In the March 27, 1875 issue of the *Carbondale Leader*, we read:

"The D&H C. Co. has been compelled to take the train which it formerly ran from Carbondale to Nineveh off of the Jefferson Branch. The Erie Company was paid for the passengers who were carried over the branch by the D&H train, but we understand that the Erie authorities did not consider the arrangement profitable to their company, and therefore the D&H had to haul the train off. . ." (*Carbondale Leader*, March 27, 1875, p. 3)

The D&H continued to run a passenger train to Nineveh, but that train started at Jefferson Junction (where the Jefferson Branch and the Lackawanna and Susquehanna connected). This train from Jefferson Junction to Nineveh was nicknamed the "Buckwheat Accommodation" by railroad workers. In that same article in the March 27, 1875 issue of the *Carbondale Leader* that we cited immediately above, we read:

"The same train [the train to Nineveh] is now run from the junction in the woods under the rocks [Jefferson Junction] to Nineveh, and makes connection with the 'Peanut Express' on the Jefferson [between Carbondale and Jefferson Junction], and trains on the A. & S. [The "Peanut Express" made no stops on the Jefferson Branch, which meant, presumably, that the D&H did not have to pay the Erie for any passengers who were carried on it over the Jefferson Branch.] The train on the Nineveh branch, we are informed, has received the appropriate name of the 'Buckwheat Accommodation;' and the residents of that part of the Susquehanna valley through which it flies like lightning, are said to patronize it liberally. We are sorry to hear of this change, as the abandoned train was a great accommodation to a few people who are regular patrons at this season of the year, and to a large number of tourists and pleasure-seekers in the summer. When summer comes again, there will undoubtedly be another Saratoga express* put upon the road. Travellers who wish to go to any point between Nineveh and Albany do not care to 'lay off' any length of time in the woods at that romantic place called Jefferson Junction. It is a cool place, but altogether too quiet." (Carbondale Leader, March 27, 1875, p. 3)

* The headquarters of this train in 1886 was "at Green Ridge, as heretofore." (see p. 602 below)

In October 1876 a remarkable event took place on the L&S. The engineer of an L&S train believed that three men who were standing on the L&S tracks in front of his engine were waiting on the road to rob the pay car that was attached to his engine. The engineer, therefore, tried to run over one of the men, W. J. Lee, a printer "who was addicted to the use of liquor," and succeeded in doing so. The D&H engineer, or at least the L&S, is now being prosecuted by the Typographical Union of Scranton:

"The engineer who ran over and killed W. J. Lee, a printer, on the L. & S. road the other day, is to be prosecuted by the Typographical Union, of Scranton, or at least the railroad company is. Lee is said to have been a first class workman although, as is the case with too many of the craft, he was addicted to the use of liquor. The engineer tried to run over the printer and succeeded. His excuse for doing so that he thought the man in company with two others, was waiting on the road to rob the pay-car which was attached to the engine." (Carbondale Leader, October 7, 1876, p. 3)

It would be interesting to hear the legal arguments that were presented in this case, if it went to trial, and to know the outcome of the suit.

On April 3, 1888, a fireman on engine No. 24 of the D&H on the Nineveh branch, Bernard Carpenter, was seriously injured by a collision between engines 24 and 37. In the April 5, 1888 edition of *The Journal*, we read:

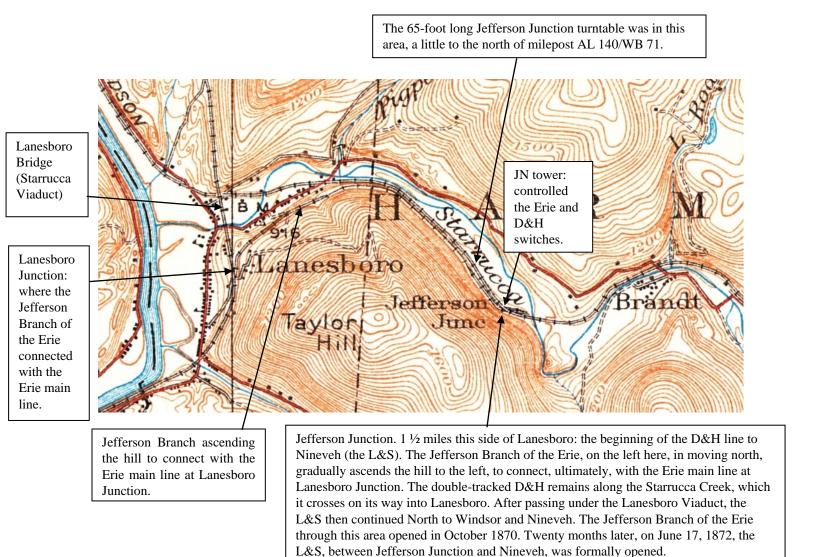
"Bernard Carpenter, locomotive fireman on engine No. 24, of the D. & H., was seriously injured by a collision between engines 24 and 37 on the Nineveh branch early on Tuesday morning. Both engines were badly wrecked and Carpenter was seriously scalded. The collision is said to have been caused by a mistake in train orders sent by dispatcher Gardner." (*The Journal*, April 5, 1888, p. 3)

The L&S on Maps

The L&S began at Jefferson Junction and went North, side by side, with the Jefferson Branch to the left of the Starrucca Creek to the point where the Jefferson Branch curved to the left and went up the hill to Lanesboro Junction. At that point the L&S continued North, crossing the Starrucca Creek before passing under the Lanesboro Viaduct ("Starrucca Bridge").

Let's take a look at the L&S on the appropriate U. S. Geological Survey topographic maps.

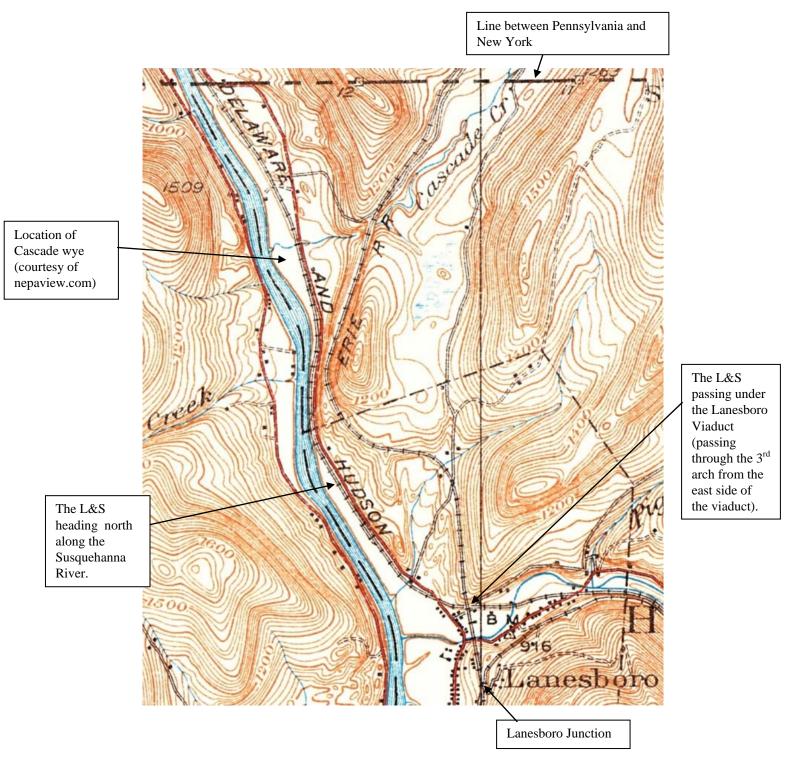
Detail from U. S. Geological Survey, *Susquehanna Quadrangle*, Edition of 1935. Topography by J. K. Bailey, C. S. Maltby, and G. A. Mock. Surveyed in 1931-1932. In this detail, we see Brandt to Jefferson Junction, to Lanesboro Junction, and to Lanesboro. The L&S began at Jefferson Junction.





View looking North on the L&S, south of Lanesboro. The Jefferson Branch of the Erie went up the hill at the center/left of the photo. Straight ahead in this photo is the D&H line (the L&S) to Nineveh (passing under the Starrucca Viaduct). Photo by the author on November 15, 2008 during a visit to this area with John V. Buberniak.

Detail from 1935 U. S. Geological Survey topographical map, Susquehanna quadrangle, showing Lanesboro to New York state line:



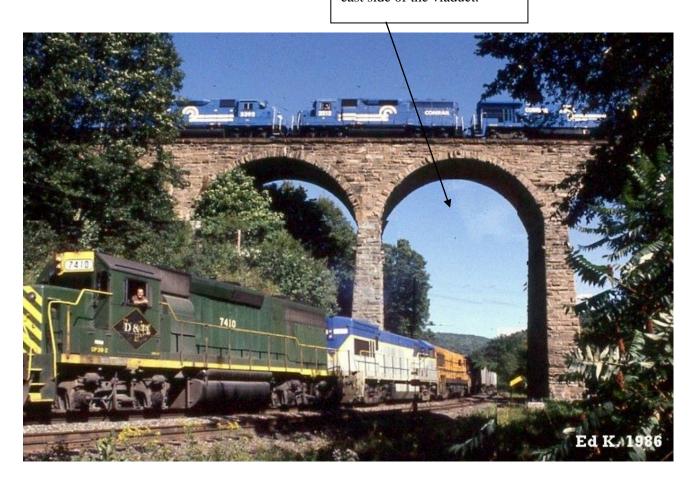
The photograph of the D&H combination passenger and freight station at Lanesboro given below is from *The Delaware and Hudson Company Board of Managers Inspection of Lines*: *June* 7th *to June* 10th, 1928, p. 57:

Lanesboro was settled in 1789 by refugees from the Wyoming Massacre. It was first called Harmony. Grant Lane settled there in 1820 and the community was named Lanesville. It became a borough in 1829, and Lane's son renamed it Lanesboro.



Photographs of the Lanesboro Viaduct ("Starrucca Bridge"), such as this great photo that Edward Kaspriske took in 1986, are world famous. The bridge always looks great, but the best part of the photograph is the D&H train, heading north towards Nineveh, on the former Lackawanna & Susquehanna roadbed. Thanks to Edward Kaspriske for sharing this photo with the entire world, via *Facebook*, on February 3, 2015.

This is the third arch from the east side of the viaduct.



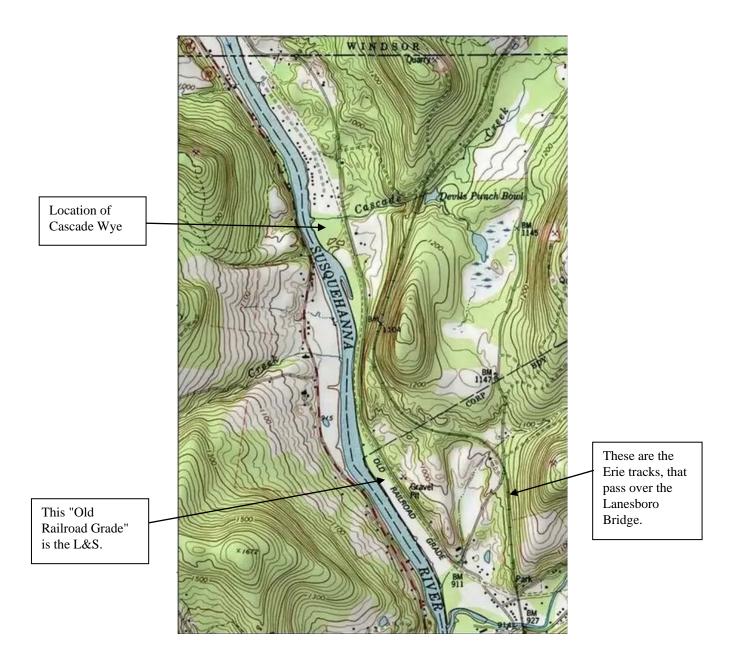
In the January 2015 issue of the *Bridge Line Historical Society Bulletin* on page 21, there are two Starrucca Viaduct photographs. The caption on the first reads as follows: "A D&H northbound freight at Lanesboro, PA. Three relatively new GE U30C's (less than one year old) are on the point. February 25, 1968 photo by Robert K. LaPorte; BLHS Archives." Here is that photo:



The caption on the second photo reads as follows: "A D&H Penn Division freight, southbound under Starrucca Viaduct at Lanesboro, PA. May 6, 1972 photo by Hugh Strobel." Here is that photograph:

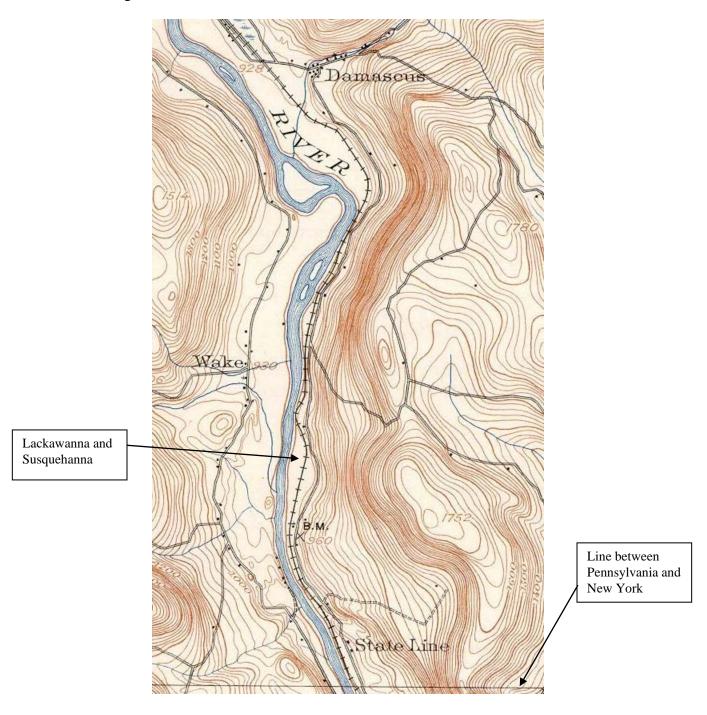


Location of Cascade Wye: Courtesy of Mike Guzzi, nepaview.com



U. S. Geological Survey, *Nineveh Quadrangle*, Surveyed in 1902-1903. H. M. Wilson, Geographer in charge; Topography by C. C. Bassett; Control by N. Y. State Survey, J. H. Jennings and E. G. Hamilton.

Detail showing L&S from State Line to Damascus:

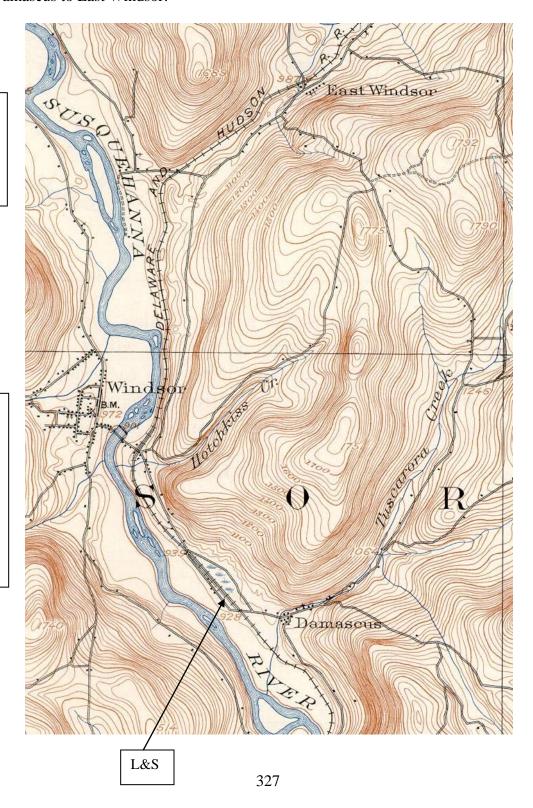


U. S. Geological Survey, *Nineveh Quadrangle*, Surveyed in 1902-1903. Detail showing L&S from Damascus to East Windsor:

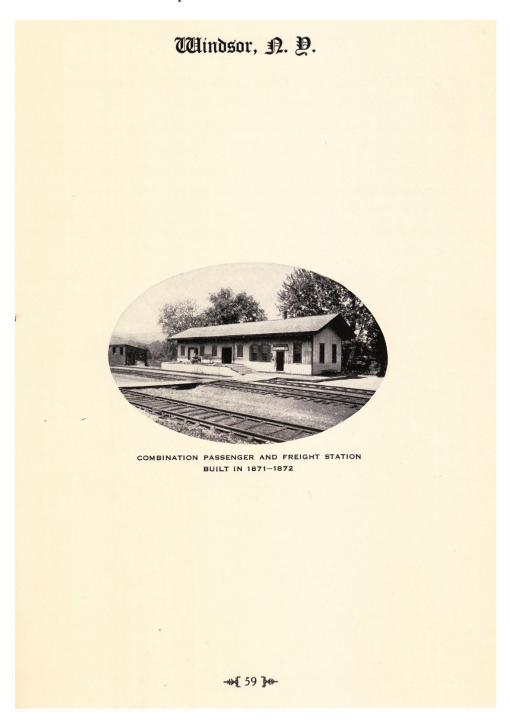
East Windsor was first settled by Jacob Springsteen and Levi Vosbury in 1797.

Windsor was settled by white men from Waterbury and Watertown, CT, in 1788.

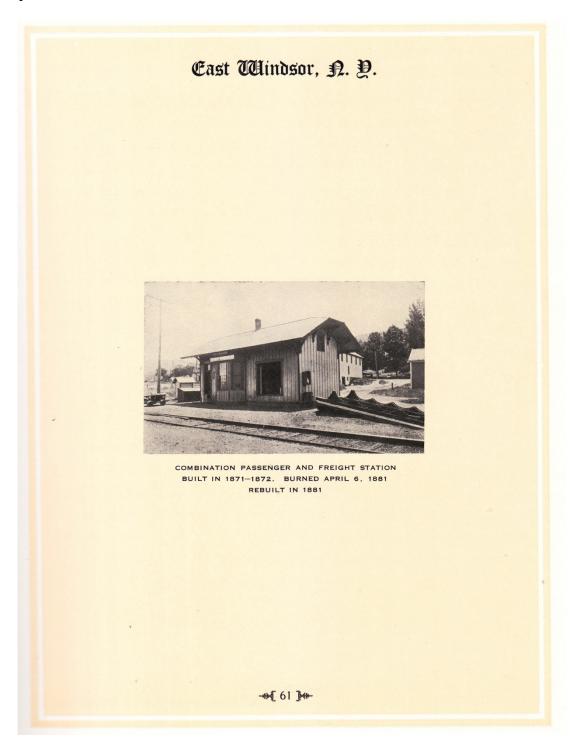
Whips were manufactured in Windsor for many years.



The photograph of the combination passenger and freight station in Windsor, New York that is shown below is from *The Delaware and Hudson Company Board of Managers Inspection of Lines : June 7th to June 10th, 1928, p. 59:*



The photograph of the East Windsor passenger and freight station that is given below is from *The Delaware and Hudson Company Board of Managers Inspection of Lines : June 7th to June 10th, 1928, p. 61:*



In 1863, one of the thirteen Directors of the A&S was from Colesville: Lewis Northrup. The Town of Colesville was established in 1821 from part of the town of Windsor.

". . . . when the Windsor grade is removed. . . " Where were the tracks initially through Windsor?

"BIG CHANGS ARE COMING. / The End Not Yet in Sight But a Revolution Is Under Way on the D. & H. / Railroad men in this city while they do not know the exact plans of the Delaware & Hudson for the future all agree in saying that some great changes are under way and when they are fully developed will mean much for this city. The belief is gaining ground daily that it will not be long before Erie trains are run through this city to Honesdale and on to New York. Considerable work is being done on the Albany & Susquehanna division and Nineveh branch. When the Windsor grade is removed the running of trains to Nineveh will be much different from the present hard pull and will work some train changes [emphasis added]. The company have now a quantity of their construction apparatus in this vicinity—to be used probably on the Honesdale branch. In part it consists of two stationary engines placed upon flat cars and which can thus be readily moved to any point desired. They are worked by steam supplied from the locomotive accompanying the cars. These will operate derricks or steam shovels. Flat cars will be used for carrying earth and they will be unloaded expeditiously by means of a plow which is passed over the top of the car and throws the earth to one side. Sheet iron plates extending from one car to another make the tops of the cars one continuous surface. The apparatus is now being used in the work of drilling Anthracite park for the storage of coal there. / MEN LAID OFF. / Eighteen of the sixty freight engineers, formerly running on the Albany & Susquehanna division of the Delaware & Hudson railroad have been laid off recently owing, the railroad men say, to the fact that the division is replacing the light engines heretofore used on the division, with the massive locomotive known as the 'culm burner.' These ponderous engines are used only for hauling freight traffic and that their adoption has been the cause of the laying off of the engineers is attested by the fact that only the freight engineers have been thinned out. / Six of the new style engines are already running on the division. Their capacity for hauling is about sixty freight cars loaded to their full capacity, which is so much in excess of the old style light weight engine that the services of the engineers who have been layed off will no longer be required, because the new engines necessitate fewer trains. No firemen, brakemen or conductors have as yet been told that they could take vacations for an indefinite time and developments in this direction are awaited with much interest. That the road is interested in the men that have virtually been dismissed, and that the action would not have been taken unless the welfare of the road demanded it is sown by the fact that superintendent Hammond is endeavoring

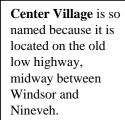
to find positions for the men on other roads Already he has secured places for eight of the men, and it is said, on what is apparently good authority, that he has assurances from the Baltimore & Ohio railroad that by next September that road will take ten of the men. Some of the men may also go back to work with the old line when the new engines arrive. / NO MORE LIGHTWEIGHTS. / The 'culm burners' that will be put on in September will come from the Schenectady Locomotive works and will be quite similar to the ones already at work. In addition to them there will also be installed three 'pushers' for mountain work, thus enabling the Albany & Susquehanna division to do away entirely with the three remaining light weight engines on their tracks. / The saving effected by the heavy engines is threefold. Fewer train crews are required, a grade of coal about one-half cheaper than that heretofore used is available and there is saving in time required to haul the freight from point to point. On the old style engine the coal used is known as 'lump' coal, while the culm burners* will satisfactorily do their work with pea or buckwheat, which is known by the dealers as 'wash' coal [emphasis added]. / The putting of heavier engines on the tracks of the Albany & Susquehanna division is in line with the general policy of this as well as other progressive roads, to acquire heavier rolling stock. On account of the unusually high prices which wrought scrap iron brings at the present time there could be no more favorable opportunity to get rid of the light engines and the other ironwork incidental to light rolling stock. It is said that the light engines that have been retired are being cut into old scrap at the shops of the company at Oneonta. Heavy five-inch rails have been laid along the main tracks and the old steel rails that were torn up are now being laid in the yards along the line. To make room for them the old iron rails that have done service in the yards are being torn up and sold as old iron. This changing of rails is undoubtedly due to the price at present commanded by old iron. / Another movement which shows the tendency toward heavier rolling stock is the replacing of the fifteen-ton or 30,000 pounds capacity freight cars with others that will carry as high as 60,000 pounds." (Carbondale Leader, July 20, 1899, p. 2)

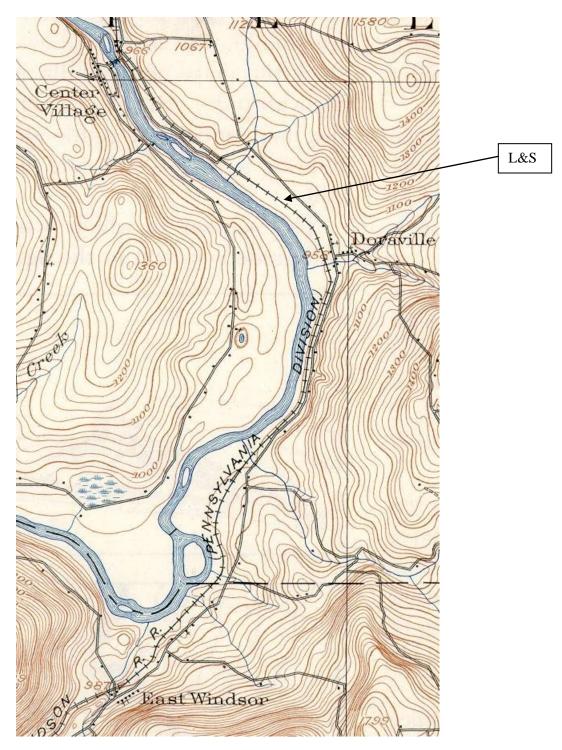
*Culm Burner

References to "culm burners" are frequent when reading about steam engines in the nineteenth century. The definition given here of "culm burner" is very clear:

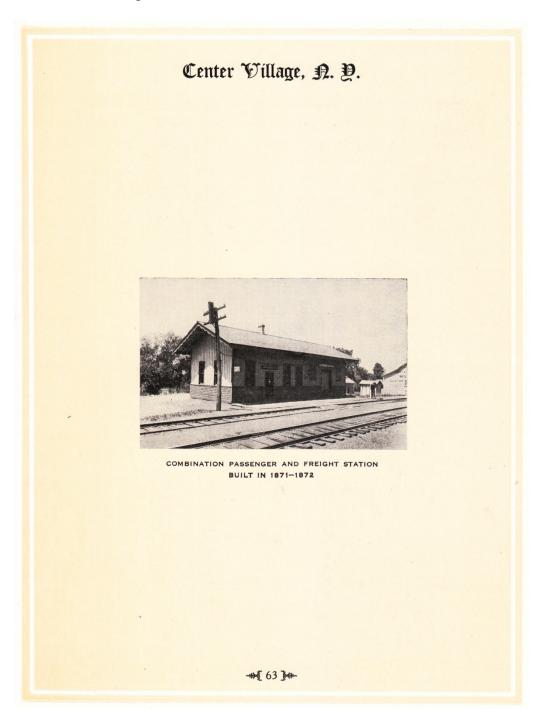
"On the old style engine the coal used is known as 'lump' coal, while the culm burners will satisfactorily do their work with pea or buckwheat, which is known by the dealers as 'wash' coal."

U. S. Geological Survey, *Nineveh Quadrangle*, Surveyed in 1902-1903. Detail showing L&S from East Windsor to Center Village:

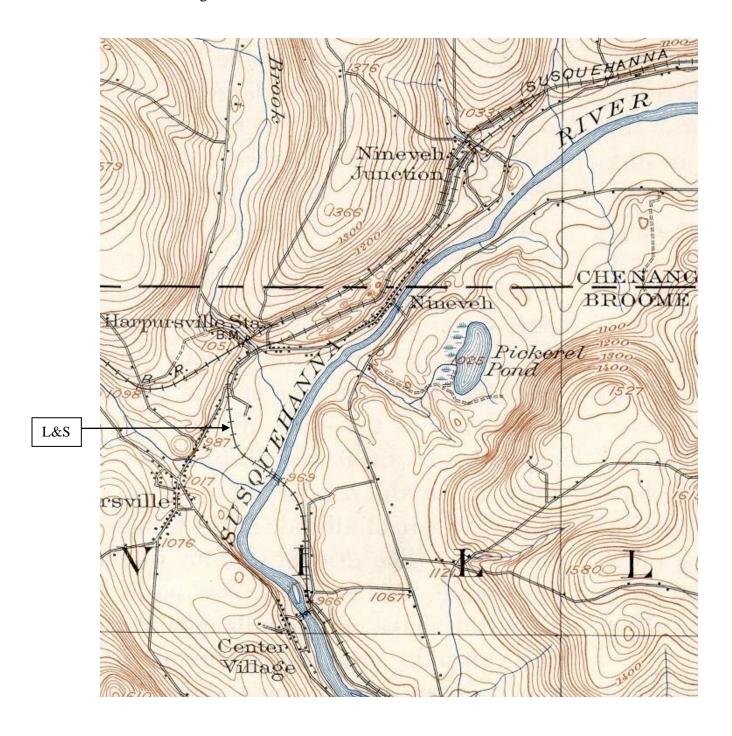




The photograph of the combination passenger and freight station in Center Village that is given below is from *The Delaware and Hudson Company Board of Managers Inspection of Lines*: *June* 7^{th} *to June* 10^{th} , 1928, p. 63:



U. S. Geological Survey, *Nineveh Quadrangle*, Surveyed in 1902-1903. Detail showing L&S from Center Village to Nineveh Junction:



Gary R. Schermerhorn ("The D&H to Albany—Part One," *BLHS Bulletin*, August 2015, p. 17) notes:

"At Nineveh, a passing siding (CPF587 to CPF590) ends at the former Penn Division Junction (formerly SW Cabin) where the line to Lanesboro and Scranton angled southward in the valley while the Susquehanna Division line began its ascent of Belden Hill. The Penn Division was abandoned after Belden Tunnel was refurbished in 1978 and the D&H bought Conrail's ex-DL&W line from Binghamton to Scranton in 1979. Both the junction and track have been removed, leaving only a scarred path where the Penn Division so recently tread. [A]fter Harpursville trestle over Wylie Brook, the A&S begins its 1.3% ascent of the division's second largest obstacle, Belden Hill. Beyond the rebuilt tunnel, the line passes through the small hamlet known as Tunnel (scene of the famous confrontation). From Tunnel the line curves along the hillsides as it descends the 1.06% grade through the communities of Dyes, Sanitaria Springs, Port Crane and finally past CPF611 into Binghamton. Bevier Street Yard in Binghamton was once the end of the Susquehanna Division."

From the biographical portrait of Cyrenius J. Ball ("First Worked on Canal") that is published in the January 15, 1926 issue (pp. 3-4, 14) of *The Delaware and Hudson Company Bulletin*, we learn that in the fall of 1883 Cyrenius (born on November 23, 1856 in Honesdale) was transferred from the D&H Gravity road, where, at the time, he was working at the head of Plane No. 13, to the steam road as a brakeman and was sent to Nineveh with Frank Silvernail, a well known D&H conductor. About this portion of his fifty-three years and eight months of service with the D&H, we read the following in his biographical portrait: "For ten or eleven months he worked out of Nineveh in way freight, accommodation and work train service, and then returned to Carbondale where he held various runs up to the time he was promoted to a conductor in the fall of 1890. / Coal and freight trains were always his preference, and, after the widening of the road from Carbondale to Honesdale, he was transferred back to the latter place because of his knowledge of single-track running, to take charge of a train operating out of there, and then continued in that work until retired on pension." (p. 14) A the time of the retirement of Cyrenius J. Ball in 1925, his son, Irving H. Ball, was also working on the Honesdale branch, as a brakeman.

Cyrenius Ball:

On 09-02-2007, Irving Ball, Jr. (lives at Elk Lake) played golf at the Homestead Golf Course, Carbondale. The author waited on him and his companions. Mr. Ball had apparently seen the stories and photographs in the area newspapers following the opening of the restored D&H Gravity Depot at Waymart on August 25, 2007. He asked: "Are you the Dr. that's involved with the Gravity?" I said that I was and he then said that his grandfather, Cernius [sic] Ball was a conductor on the Gravity Railroad; also worked for a time on the steam line from Carbondale to Honesdale. Cernius's son, Irving Ball, also worked on the Honesdale Branch, as did Irving Ball, Jr. (for three years). Irving Ball, Jr. said that he owns "a Gravity lantern" and that he is not yet sure whether it's going to go—following his death, presumably—to Honesdale (Wayne County Historical Society) or to Waymart.

December 11, 2014: Alex Lilje (1708 West Street, Honesdale, PA 18431) phoned to say that he saw the Mike Stevens segment on Channel 16 yesterday, and that he would like to purchase the DVDs on the Gravity Railroad. Since he lives in Honesdale, I said that he could go to the Wayne County Historical Society and buy the set there. He said he would. The Lilje family had a shoe store in Carbondale.

Alex Lilje's grandfather, Irving Ball, worked for the D&H steam line. When he retired, he was No. 1 on the seniority list.

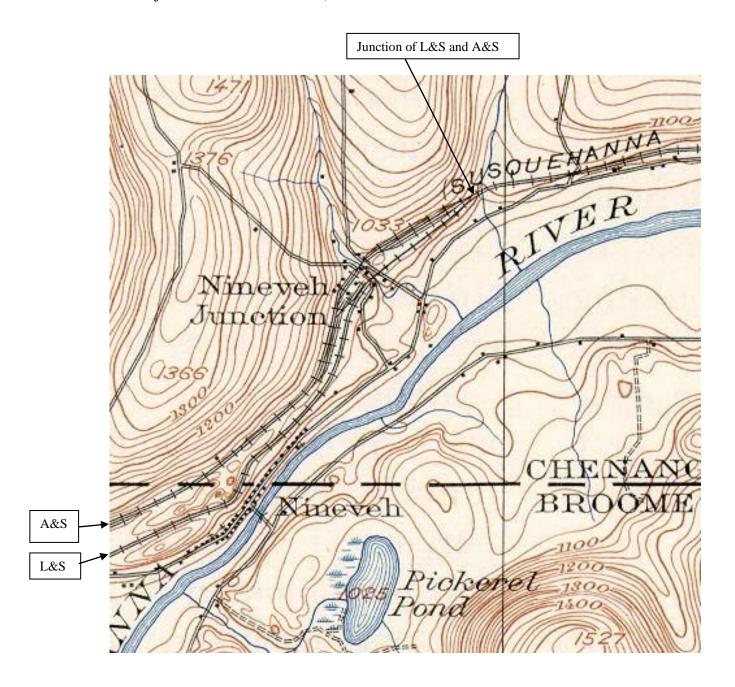
Alex Lilje's great grandfather, Cyrenius Ball, worked for both the D&H Gravity line and then the D&H steam line. Alex has a chair that belonged to Cyrenius Ball.

George Livingston (biographical portrait, "Rafted Logs Down The Delaware," *The Delaware and Hudson Railroad Bulletin*, June 1, 1930, pp. 163-164, 172) started working for the D&H on August 1, 1882 as Agent and Telegrapher at Center Village. "Then," we read in his biography, "every northbound Pennsylvania Division train turned at Nineveh, the next station north. For that reason most of the train crews received orders at Center Village. Inasmuch as one locomotive could only handle 25 cars of coal, the principal commodity shipped over the Division, coal trains were run in sections. For instance 'Coal 5' or 'Coal 7,' as the trains were known, would frequently run in three or four sections, each carrying green signals until the last section, without markers, arrived." (p. 164) In 1930, Livingston's son, G. F. Livingston, was the D&H agent at Center Village, in the same station where his father entered D&H employ almost 50 years earlier.

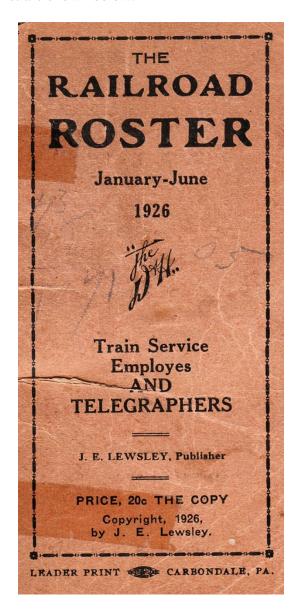
On September 4, 1886, James Shannon, fireman on Engine 61, D. & H. C. Co., who lived on Canaan-Street in Carbondale, lost a leg when he fell under the cars at Nineveh, the wheels taking his limb off close to the trunk. He died shortly thereafter. Here is the report on the accident that was published in the *Carbondale Leader* of September 7, 1886:

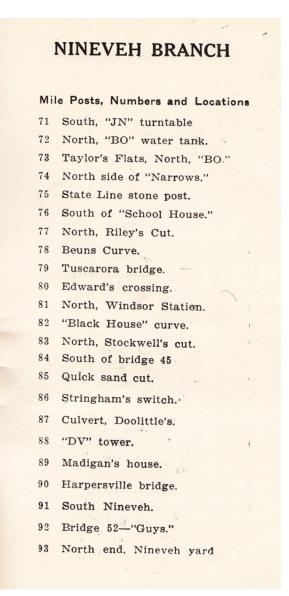
1886: "Death from Loss of Blood. / One of the saddest fatalities of many months occurred on Saturday night at Nineveh, N. Y., when James Shannon, fireman on Engine 61, D. & H. C. Co., who lives on Canaan-st., this city, lost his leg by the cars and died shortly afterward. Shannon had gone back from his engine on the cars for some purpose and in returning to his post he slipped and fell under the cars, the wheels taking his limb off close to the trunk. The train ran nearly a mile before he was missed and when they returned in search they found him lying unconscious at the side of the track. He had sustained no other injury beside the severing of his leg and what remained of that had been tied as tightly as his waning strength could do it before he became senseless. Doctors were hurriedly called but the young man had lost so much blood that they could do nothing for him and he died at Jefferson Junction on the way here. He was twenty-one years of age last Tuesday." (Carbondale Leader, September 7, 1886, p.4)

U. S. Geological Survey, *Nineveh Quadrangle*, Surveyed in 1902-1903, Detail showing Nineveh Junction (junction of A&S and L&S):



On May 11, 2016, Janet Isger (7900 Quigley Road, Union Dale, PA 18470) donated a copy of "The Railroad Roster /January-June 1926 / The D&H Train Service Employees and TLEGRAPHERS," J. E. Lewsley, Publisher) to the Carbondale D&H Transportation Museum. Her grandfather, Ernest Enslin, and both of her husband Robert's grandfathers (Joseph Isger and Leonard Besecker) were all D&H engineers. She also donated to the museum a D&H VETERAN lapel pin that belonged to Leonard Besecker (lived at 26 Sand Street, Carbondale, phone 1236J). Included in that January-June 1926 Roster is a list of the mile posts on the Nineveh Branch (between Lanesboro and Nineveh). The title page of that 1926 roster and that list are shown below:





1207

"Connections of the

Albany & Susq. R. R."

Del. & Hud. and

Connect at Green

Ridge with the L&S

Connect at Carbondale, via

the Jefferson Branch of the Erie, with the Erie R. R., East and West. Connect at Albany with the R&S R. R.

for Saratoga Springs and

Whitehall, "connecting

principal cities East."

Timetables, 1872-1874

With the opening of the Lackawanna & Susquehanna (Jefferson Junction to Nineveh) on January 1, 1872, the door for D&H passengers to upstate New York was now wide open.

Here is the section of D&H timetable that went into effect on July 10, 1872 (published in the Carbondale Advance, August 31, 1872, p. 3) that refers to D&H passenger service beyond the Lackawanna Valley:

ELAWARE AND HUDSON R. R. Takes Effect July 10th, 1872.

DAILY TRAIN BETWEEN SCRANTON AND COOPERSTOWN, SHARON SPRINGS, ALBANY AND TROY, N. Y.

No Change of Cars between Scranton and Albany.

Connections of the Del. & Hud. and

→ GOING SOUTH.—At Green Ridge, with the L. & S. Division of the Central R. R. of New Jersey, for Wilkes-Barre, Penn Haven, Mauch Chunk, Allentown and Philadelphia.

At Scranton, with the Del., Lack, & West, R. R., East, for Manunka Chunk and Philadelphia, Dover, Paterson, Newark and New York; and West, for Binghamton, Syracuse and Oswego: also, with the Lack. & Blooms. R.R., for Northumberland, Williamsport, Lock Haven, Harrisburg and Baltimore.

GOING NORTH.—At Carbondale, via the Jefferson

Branch, with the Eric R.R., East and West.

At Albany, with the Rensselaer & Saratoga R. R., for Saratoga Springs and Whitehall, connecting with the Lake Champlain Steamers, for Plattsburg, &c., and at Rutland with the Rutland & Burlington R.R., for St. Albans, Montreal and Quebec; also, with the Boston & Albany R.R., for Boston and the principal cities East.

The 1010 A. M. Train from Wilkes-Barre. L. & S. R. R., connects at Green Ridge with 11.05 A. M. Through Train, No. 2. to Albany, arriving at 8.80 P. M. Returning, leave Albany at 7.00 A. M., arriving at Carbondale at 5.08 P. M., Scranton 6.15, and Wilkes-R. MANVILLE, Supt. Barre 7.15.

"No Change of Cars between Scranton and Albany."

Connect at the DL&W station for DL&W trains east and west: connect with the Lackawanna & Bloomsburg Railroad

10 A.M. train from Wilkes-Barre arrives at Albany at 8:30 P.M.

7 A.M. train from Albany arrives at Wilkes-Barre at 7:15 P.M.

with the Lake Champlain Steamers, for Plattsburg, &c., and at Rutland for St. Albans, Montreal and Quebec; also with the Boston and Albany R. R., for Boston and the

> These connections to the south were all made possible for travelers from Carbondale by the five daily passenger trains from Carbondale to Scranton.

At Scranton, as we see in the timetable given below (timetable effective August 12, 1872; published in the *Carbondale Advance* of August 31, 1872, p. 3), one could also connect with the trains on the Lackawanna & Bloomsburg Railroad:

TAKES EF	FECT	AUC	UST	12, 1	872.	
Trains	Dio	vina	. No	rth.	(,)	
STATIONS.	2.	1 1	6.		10.	12.
to very service	A.M.	P.M.				A.M.
Northumberland,	8 20	4.50		•		2.80
Danville.	8.55	5.80				8.85
Catawissa.	9.17	5.55				4.20
Danville, Catawissa, Rupert,	9.22	6.00				4 90
Rupert, Bleomsburg,	9.82	6.06				4.40
Espy.	9.88	6.12				4.55
Bloomsburg, Espy, Berwick, Beach Haven. Hicks's Ferry, Shickshinny, Hunlock's Creek, Nanticoke, Plymouth,	10.06	6.40				5.50
Beach Haven.	10.15	6.50				
Hicks's Ferry.	10.22	6.56	11-13	1 1		6.25
Shickshinny.	10.39	7.12	· . T.	. 1	114	7.00
Hunlock's Creek.	10.56	7 27				7 40
Nanticoke,	11.05	7 35		P.M.	P.M.	8.05
Plymouth.	11.15	7.45	cran to	1.15	5.05	8.20
Nanticoke, Plymouth, Kingston, { Arrive, Wyoming,	11.25	7.55	A.M.	1.30	5.20	8.35
Kingston, Leave.	11.30	8 00	8.45	1.30	5.20	8.55
Wyoming.	11.43	8.13	9.00	1.45	5,37	
West Pittston,	11.50	8 19	9.07	1.52	5.45	
Pittston,	11.55	8.24	9.12	2.00	5.50	
Lackawanna,	12.05	8,32	9.20	2.10	6.00	
Taylorville,	12.15	8.40	9.30	2.22	6.12	
Bellevne,	12.22	8.45	9.87	2.80		
Wyoming, West Pittston, Pittston, Lackawanna, Taylorville, Bellevue, Scranton,	12.30	8.50	9.45	2.40		10 85
.1	P.M.	P.M.	A.M.	P.M.	P.M.	A.M.
Train						
1		1 1	7.1			
STATIONS.	1.	8.	5.	7.	9.	11.
Scranton, Bellevue, Taylorville, Lackawanna, Pittston, West Pittston, Wyoming,	P.M.	A.M.	A.M.	P.M.	P.M.	P.M.
Bellama	0.10	C 50	10.10	11.00	8.80	0.40
Tayloryilla	9 18	6.57	10.22	11.00	0.01	0.00
Lackawanna	9 28	7.09	10.20	11 18	Q 88	7.03
Pitteton	2.35	7.18	10.49	11 95	4.05	7 10
West Pittston.	2.40	7.23	10.54	11.:0	4.10	7 91
Wyoming, Kingston, { Arrive, Leave, Plymouth, Nanticoke,	2.47	7.29	11.00	11.85	4.17	7.80
(Arrive.	8.00	7.40	11.15	11.47	4.30	7.45
Kingston. Leave.	8.05	7.45	11.80	P.M.	4.40	8.00
Plymouth.	8.15	7.55	11.45		4.55	8.15
Nanticoke.	8.25	8 05	A.M.		P.M.	8.80
Hunlock's Creek.	8.85	8.15				8.4
Shickehinny,	8.51	8.30				9 10
Nanticoke, Hunlock's Creek, Shicksbinny, Hicks's Ferry,	4.07	8 45				9.40
Beach Haven,	4.14	8 51				9.50
Berwick,	4,23	9.00				10.05
Espy,	4.50	9.25				10 55
Bloomsburg,	4.57	9.32				11.00
Beach Haven, Berwick, Espy, Bloomsburg, Rupert,	5.04	9.38				11.15
Catawisea,	0.00	0.40				11.20
Danville,		10.05				12.00
Northumberland,		10.40				12.55
	P.M.	A.M.				A.M.
CONNECTIONS	A t	Sara	nton	mills.	Dolo	
Lackawanna & We	storn	Ruilr	distrib	nul D	Mon	ware,
Hudson Railroad.	Stern	16aiii	inn' s	illu D	Claw	are or
At Pittston, with	Lehie	h Val	lev D	Ilron	1 10 7	Cur l-
hannock, Towanda,		11 . 4 dt	163 169	mode	, 10 1	unk.
At Kingston, with		t.oare	to W	ilkon	Razza	
At Rupert, with C					Darre	•
At Northumberlan					Fair	Dell
toad, to Williamsp	ort o	11/1	ith N	ortho	en C	avilli-
		11		OLUHU	111 (1)	· m · rn l

"CONNECTIONS. – At Scranton, with . . . Delaware & Hudson Railroad." In the D&H timetable that took effect on June 30, 1873, given below (published in the June 28, 1873 issue of the *Carbondale Advance*, p. 3), the D&H announced "Improved Traveling Facilities" available to D&H passengers:

An important new addition to the D&H passenger service repertory at this time was the **Philadelphia** and Saratoga Express, a first-class passenger train, "making its connection [with D&H rails] at Green Ridge, and making no stops between Carbondale and Green Ridge Depots." We will have a lot more to say about the Saratoga Express in Volume XVI in this series.

"Very few of our people ever anticipated an improvement like this. Carbondale on the great through route between Philadelphia and Albany and Saratoga—and making the distance to and from Scranton in less than 30 minutes."

The New Time Table.

A new time table goes into effect on the Del. & Hud. R. R. on Monday next. Passenger Trains leave Carbondale for Scranton as follows: First Class Trains, at 8.30 A. M., 12.40, 3.20 and 5.35 P. M.—Second Class Trains, at 6.10 and 11.15 A. M.

Leave Scranton for Carbondale—First Class Trains at 9 50 A. M. and 1.22, 2.10 7.20 P. M.—Second Class Trains, at 8.10 A. M. and 2.30 P. M.

The 3.20 P. M. Train South, and 1.22 P. M. Train North is the Philadelphia and Saratoga Express, making its connection at Green Ridge, and making no stops between Carbondale and Green Ridge Depots.

Improved Traveling Facilities.

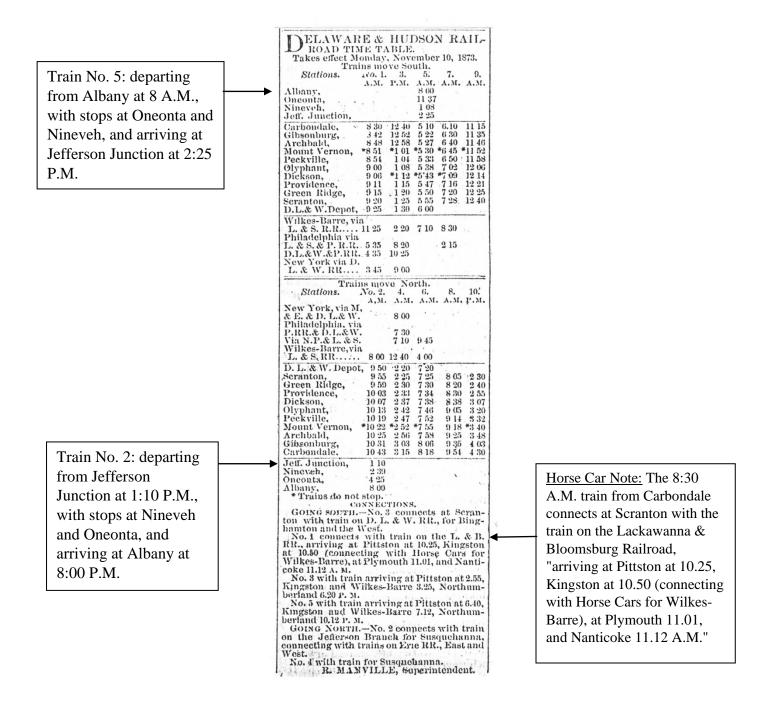
The energetic management of the Del. Hud. C. Co. is giving us still improved traveling facilities. By the new time table which goes into effect on Monday next we are to have six Passenger Trains, daily, both ways—to and from Scranton.

One of these—The Philadelphia and Saratoga Express—makes the time between our depot and Green Ridge in 28 minutes.

Very few of our people ever anticipat ed an improvement like this. Carbondale on the great through route between Philadelphia and Albany and Saratoga—and making the distance to and from Scranton in less than 30 minutes.

Six passenger trains, both ways, to and from Carbondale. "One of these—The Philadelphia and Saratoga Express—makes the time between our depot [Carbondale] and Green Ridge in 28 minutes."

In the D&H time table that took effect on November 10, 1873 given below (published in the December 6, 1973 issue of the *Carbondale Leader*, p. 3), a daily passenger train to Albany, to and from Jefferson Junction, is announced: leaving Albany at 8 A.M., leaving Jefferson Junction at 1:10 P.M.



From the D&H timetable that took effect on Monday, November 2, 1874 (published in the *Carbondale Advance* of November 21, 1874, p. 3), given below, we learn (1) that the Saratoga Express has been withdrawn, and (2) that there is a daily train, both ways, between Nineveh and Carbondale.

"The Saratoga express has been withdrawn. . ."

More on the Saratoga Express in Volume XVI in this series.

> "Leave Carbondale for Nineveh at 10:55 a.m.; arriving at 1:50 p.m.

Leave Nineveh at 2:18 p.m., arriving here [Carbondale] at 5 p.m., and Scranton at 5:50 p.m."

New Time Table.

A new time-table went into effect on the Delaware & Hudson RR. Monday, Nov. 2.

The Saratoga express has been withdrawn; and the other trains leave here as follows:—

and the other trains leave here as follows:— First class passenger trains at 8:30 a. m.; 12:30, and 5:02 p. m.; accommodation and freight, at 6:10 a. m.; 12:40 aud 5:30 p. m.

Leave Scranton, first class at 9:50 a. m; 2:10 and 7:20 p. m.; accommodation at 7:30 a. m., and 3:00 p. m.

→ Leave Carbondale for Nineveh at 10:55 a. m.; arriving at 1:50 p. m.

Leave Nineveh at 2:18 p. m., arriving here at 5:00 p. m., and Scranton at 5:50 p. m.

Carbondale Post-Office.

Mails Close.—For Scranton, New York, Philadelphia, and intermediate places, via D. & H. and D. L. & W. railroads, at 8 A. M. For the same points, and South, West and North via Lack. & Bloomsburg and D L. & W., at 12:10 P. M. North via Jefferson Branch of Erie, at 10:10 A. M. Honesdale at 12:30 P. M., and Dundaff at 3:45 P. M.

Mails Arrive.—From Scranton, New York, Philadelphia and North at 11 a. m. and from Scranton, New York, Philadelphia and South at 3:25 a. m. Honesdale and Dundaff at 11 a. m., and North via Jeff. Branch of Erie at 5:35 h. m.

N. B.—Let all persons bear in mind that the time of closing the mails that are carried by rail, is half an hour before the published time of the departure of the train.

The office will be open, after the first of November, from 7:30 A. M. to 7:30 P. M. P. S. Joslin, P. M.

1208

D&H Railroad to Canada (Whitehall to Canada)

Whitehall to Rouse's Point

Three important sources of information about the extension of the D&H Railroad from Whitehall to Canada are:

- 1. Century of Progress, Chapter X: Further Expansion—New York and Canada Railroad, 1874-1881, pp. 243-280
- 2. Shaughnessy, Chapter 7: NORTH TO CANADA (The New York and Canada Railroad, pp. 135-168)
- 3. The Delaware and Hudson Railroad, Board of Director, Inspection of Lines::, June 2nd June 5th, 1932. The Steamboats of Lake George 1817 to 1932. 174 pages

The extension of D&H rail lines to Canada was one of the many extraordinary accomplishments of Thomas Dickson, a great promoter of railroad expansion (acquisition, construction, improvement). On May 13, 1869, he was elected president of the Delaware and Hudson Canal Company, succeeding George Talbot Olyphant, who resigned on May 10 for health reasons.

Elected on that same date were the following officers and directors of the Delaware and Hudson Canal Company:

"DELAWARE & HUDSON--ELECTION--At New York, on the 13th inst, the following Officers and Managers of the D. & H. Canal Co. were elected: / Thomas Dickson, President; C. P. Hartt, Treasurer; R. H. Nodyne, Secretary; James C. Hartt, Sales Agent; Coe F. Young, General Superintendent; Charles N. Talbot, Edward Woolsey, G. T. Olyphant, A. A. Low, R. N. Kennedy, James M. Halsted, L. G. B. Canon, James R. Taylor, Thomas Dickson, O. D. F. Grant, John Jacob Astor, Thomas Cornell, W. J. Hoppin, Managers." (*Carbondale Advance*, Saturday, May 15, 1869, p.2)

Clark summarizes as follows the extraordinary accomplishments of Thomas Dickson during the first three years his presidency of the D&H:

"The first year after being President, the company obtained by lease the perpetuity of the Albany & Susquehanna Railroad [February 24, 1870], stretching from Binghamton to Albany, with all of its branches, and in the subsequent year the Rensselaer & Saratoga Railroad [May 1, 1871] and

its branches, together with the line of steamers on Lake Champlain, and for the purpose of connecting the entire railroad system, obtained by purchase, the Union Railroad between Wilkes-Barre and Scranton, constructed the road between Scranton and Carbondale [July 4, 1871], and obtained a control over the Jefferson Branch to Susquehanna [October 10, 1870], and from thence to Nineveh [January 1, 1872] by construction again, connecting the coal fields with the railroad system north and east into New England." (p. 156)

The next step in the expansion plans of the D&H under President Thomas Dickson: the railroad to Canada.

On the east side of Lake Champlain, there was a railroad route in place: Rutland and Burlington Railroad to Burlington, 1849; connection there to the Canadian border via the Vermont Central.

The Whitehall and Plattsburg Rail Road Company was incorporated on February 16, 1866, for the purpose of joining Whitehall and Plattsburg by a railroad along the western border of Lake Champlain to Port Henry, thence to the Ausable River near Ausable Forks and from that point to Plattsburgh.

Two portions of the line were completed by 1873: (1) 1868: from Rogers (then called Point of Rocks) to Plattsburgh; (2) 1869-70: a section along the shore of the lake between Addison Junction, now Fort Ticonderoga, and Port Henry. The line remained incomplete for a few more years until with the help of The New York and Canada Railroad Company, "and President Dickson's individual efforts in its behalf, the line between Whitehall and Plattsburgh that The Delaware and Hudson Company owns at the present time was built." (Hollister, 212).

During 1873 and the following two years, The Delaware and Hudson Canal Company was able to pursue in continuous and orderly course the project for which The Whitehall and Plattsburgh Rail Road Company had been incorporated, that is, to build the line.

On March 16, 1872, The New York & Canada Railroad Company was incorporated.

"The New York and Canada Railroad Company of 1872 was essentially a Delaware and Hudson enterprise." (*COP*, p. 243) Its purpose was to provide a through line on the west side of the lake between Whitehall and Plattsburgh, utilizing any road or parts of any road already constructed if found possible and desirable. On March 26, 1872, Isaac V. Baker was elected president.

Isaac V. Baker (formerly a superintendent of the R&S) and James Forsythe, as representatives of the R&S had been elected members of the Board of Directors of the Whitehall and Plattsburgh on March 18, 1868 (two years after the Whitehall and Plattsburgh was incorporated).

Building a line on the west shore met with many obstacles, notably those interests in Vermont who did not want to see a rail line on the west short of Lake Champlain.

Early in 1872, the Honorable Smith M. Weed of Plattsburgh (prominent citizen and member of the state legislature) "journeyed to New York in an effort to interest the Delaware and Hudson Canal Company. At a meeting with some of its officers and Managers, at which I. V. Baker who shared his aspirations was present, Mr. Weed readily convinced George Talbot Olyphant, acting as president in the absence of Mr. Dickson, LeGrand B. Cannon and others that such a line would be of great advantage to the company. Thereupon Mr. Weed drew from his pocket articles of association of The New York & Canada Railroad Company, already signed by several residents of Plattsburg and Clinton county. The remaining signatures necessary to effect its incorporation were quickly supplied." (COP, 246)

The New York and Canada Railroad Company was thereby brought into existence.

"Some construction was effected by this company [the New York & Canada] in 1872, and in January, 1873, attention was given to negotiations with the Whitehall and Plattsburg, that company then offering to ease the operated portions of its line, an offer evidently impelled by realization that the new company would build parallel to the track already constructed along the lake. Accordingly, a lease was taken on January 14, 1873, which was immediately followed by a proposal for consolidation. On January 15, 1873, the Delaware and Hudson authorized the purchase, for the New York and Canada, from the Vermont interests, of their control and leaseholds of the Whitehall and Plattsburg, the Montreal and Plattsburgh and the steamer *Oakes Ames*, thereby paving the way for a consolidation of the rights, franchises and properties of all the roads on the west side of Lake Champlain. . . . Under an agreement dated February 25, 1873, the Whitehall and Plattsburgh, the Montreal and Plattsburgh and the New York and Canada of 1872 were merged and consolidated into The New York and Canada Railroad Company." (*COP*, pp. 246-47)

The D&H goal: to reach Montreal with the company's anthracite, to stimulate and profit from the iron industry then being developed in the Champlain region.

Funding support for the line into Canada was made possible by bonds provided by Messrs. Baring Brothers of London:

"The D. & H. C. Co. is extending its coal trade into Canada, and to complete its connections with Montreal, Messrs. Baring Brothers, of London, have negotiated \$4,000,000 of six per cent. sterling bonds of the Company at the rate of 91 1/2 per cent. The bonds run for thirty years." (*Carbondale Leader*, May 16, 1874, p. 3)

The opening of the several sections of the line is summarized as follows in *Century of Progress*:

"The road was opened from Whitehall to Port Henry, a distance of thirty-nine and three-fourths miles, on November 30, 1874, the twenty-four miles from Whitehall to Addison Junction, now Fort Ticonderoga, being entirely new construction. From Addison Junction to Port Henry the track of the Whitehall and Plattsburg was utilized but with certain relocations and modifications in grade. . . The line to Plattsburg was opened a year later, on November 29, 1875, and by use of the track of the former Montreal and Plattsburgh through service to Mooers Junction was opened on the same day. By the opening of this through route and the use of twelve miles of the Ogdensburgh and Lake Champlain railroad to Rouses Point a connection was made with the Grand Trunk railway for Montreal." (*COP*, pp. 249-250)

By the fall of 1875 the line was nearly completed. In the *Carbondale Advance* of November 6, 1875, it was announced:

- 1. The New York and Canada would be finished in a few days
- 2. The directors will make an excursion over the line, from Albany to Montreal, on November 19, 1875.
- 3. The road will be open for business on November 26, 1875.

Here is that announcement:

"The Delaware & Hudson Canal Co.'s new road, the New York and Canada, is nearly completed, and will be entirely finished in a few days. The directors will make an excursion over it, from Albany to Montreal, on the 19th of this month. A large party of friends will accompany them, from this valley and elsewhere. The road will be opened for business on the 26th. It gives the company access to all Canada for a coal market." (*Carbondale Advance*, November 6, 1875, p. 3)

To commemorate the completion of the D&H line to Plattsburg (with rail connections to Canada), the D&H hosted a commemorative excursion from Albany to Montreal, on November 16-19, 1875. About that excursion, we read the following in *Century of Progress:*

"On November 8, 1875, President Dickson sent cards of invitation to the most prominent railroad officers and to other prominent individuals in the northeastern section of the United States requesting them to join the Managers in a commemorative excursion to precede the opening of the route for public use. Arrangements were made to run from Albany to Montreal, starting on Tuesday, November 16. Among those invited were John Jacob Astor, J. Pierpont Morgan, Samuel Sloan, Cornelius Vanderbilt, Alexander T. Stewart, William H. Vanderbilt, James Blair, William Orton, Thomas T. Eckert, John Taylor Johnston, Chester A. Arthur, F. S. Winston, Richard H. McCurdy, Samuel J. Tilden, Abram S. Hewitt, Hamilton Fish, Galusha A. Grow, Thomas A. Scott, F. B. Gowen, Lucius Robinson, Whitelaw Reid, Charles A. Dana, James Gordon Bennett and Samuel D. Babcock, the last named then the president of the Chamber of Commerce of New York City. President Grant received an invitation, but was unable to accept. / The party left Albany at nine o'clock on the morning of November 16, as planned, stopping at points along the line to pick up some who had not been able to join at the starting point. The train consisted of a baggage car, hotel car, seven Wagner palace coaches, a directors' coach and at the rear an open 'Baldwin' coach. The locomotive used was 'the first class coal-burning engine Saratoga.' The outstanding features of the trip were banquets, the first at Plattsburgh on the evening of November 16, and the second at Montreal on the afternoon of the following day. The Plattsburg banquet was tendered by the citizens and the Montreal 'collation' was provided by the municipality. The speeches delivered on each of these occasions have been preserved in pamphlet form, and are among the company's records. The pamphlet includes comments on the excursion by the Plattsburgh Republican, the Albany Evening Journal, the Albany Argus, the New York Evening Post, the Troy Whig, and the Troy Daily Times. . . / The party reached Plattsburgh at half past four on the afternoon of Tuesday, November 16, 'having made the distance from Albany in seven and one-half hours—the shortest trip on record.' At the Fouquet House, where the banquet was held, the Plattsburgh cornet band was already out in force, playing its best for the entertainment of the excursionists as they left the train. Ranging from the humorous to the seriously grave, the speeches at the banquet were similar to those that on a like occasion would be listened to today. Honorable Smith M. Weed acted as chairman, and President Dickson responded to the first toast in a short address which sketched briefly the early history of the Delaware and Hudson Canal Company. Referring to the construction of the canal, commenced in July, 1825, he declared that it then 'was the largest undertaking that had ever been entered into upon the Continent by any corporation.' He continued: 'Since 1840, however, the company has been prosperous, and for thirty-four consecutive years have never passed a dividend. And as they waxed strong they became indifferent, and for a time depended upon tradition for dividends, and it was not until the young giant, of which my friend Sloan is the head [the DL&W] began to take front rank that they were awakened to the danger of their position, and to the fact that they were likely to be distanced, the result being a comprehensive plan of expansion, the culminating point being the construction of this road.' / This reference to Samuel

Sloan, president of the Delaware, Lackawanna and Western Railroad Company, evoked from Mr. Sloan, when his turn came to speak: 'I know him (Dickson) like a book, and I know Pennsylvania and when Dickson says he will bring down the ore of this valley and bring up the coal of Pennsylvania, I am inclined to think he will do it and make it pay,' a more notable declaration in view of the fact that, as Honorable Henry R. Pierson, the next speaker, remarked, the dividends paid during the present year on the railroads of New York State averaged 'not quite two per cent.' / At half past eight on Wednesday evening, November 17, the party left Plattsburg for Montreal. The train did not halt until Rouse's Point was reached; but at every station crowds of citizens greeted its progress and saluted the visitors as they passed. Along the line, the windows of almost every house, from the most costly mansions to the most humble homes, were filled with observers and waving handkerchiefs. At Rouses Point there was a dense crowd, and 'a grand salute.' There 'the stout old iron horse, I. V. Baker,' which seems somewhere en route to have relieved the locomotive Saratoga, 'turned himself about and went to the rear of the train,' and its place was taken by a Grand Trunk locomotive decorated with the American flag and the Union Jack. Crossing the Canadian border the excursionists looked out upon the narrow Canadian farms, less than fifty rods wide, but extending 'almost endlessly' in depth. Twelve miles more and St. Johns, over two centuries old, was reached, where during a short halt, addresses of welcome were made by the Mayor of the city and the president of the Board of Trade. The latter stated that the region adjacent to the city was almost denuded of its forests, and that 'in five years' the consumption of coal had increased from one thousand to ten thousand tons annually. The train moved on 'along the path' of the old St. Johns and Montreal railroad, completed in 1836, reaching Montreal at one o'clock. / At three o'clock the city authorities entertained their American visitors by a dejeuner reception at the Corn Exchange. At the 'collation' then served covers were laid for three hundred guests. Following the English custom the tables were provided 'with cold meats, game in abundance, and other substantials,' and coffee ginger-pop flowed freely. The Mayor, Doctor Hingston, presided and, rising, first proposed 'The Queen,' a toast that was received with all honors He next proposed 'The President of the United States' and he felt sure that the Americans when they got 'beneath the crust of society' would find that the Canadians were in essential feelings quite like themselves. Here, as at Plattsburgh, President Dickson spoke first for the visiting party, contenting himself, however, with a few words of thanks for the cordial reception and modestly leaving it to Honorable Galusha A. Grow. Speaker of the House of Representatives during the early years of the Civil War, to respond at greater length. As was to be expected, the proceedings were largely international in the character of many of the references which the successive speakers made as the afternoon wore on but quite enough strictly germane to the company's business was said to apprise everyone that it was prepared to furnish anthracite to Montreal. Late in the afternoon Alderman David, evidently having in view the French element in the city, referred to George Washington and Lafayette 'amid applause,' and then gained courage to congratulate all present upon the fact that they had partaken of a cold water banquet. Consul-General Dart, who followed Alderman David, relying on the security of the occasion, was even more audacious, for he intimated that annexations had always proceeded from the north and that his country was then, in a measure, engaged in

annexing the United States. / On the following day the excursionists, many of whom had been forced by the Canadian weather to patronize Montreal merchants to the extent of purchasing warmer overcoats than those with which they had left their homes, started on their return journey. The party arrived at Albany at twenty minutes past five in the afternoon, and the Pennsylvania delegation rested at the Delavan House but the New Yorkers pushed on, going from Montreal to New York 'without change of cars.' On the following morning the Pennsylvanians left Albany at ten o'clock, over the Albany and Susquehanna, and before reaching Oneonta gathered in the car occupied by President Dickson, felicitating him on the unalloyed pleasure and 'princely entertainment' that they had enjoyed. In a brief response, Mr. Dickson complimented the men in the employ of the company, from the highest in authority 'down to those who handle the pick and the shovel,' and expressed the opinion that there was no corporation in the country employing, as his company did, as many as twenty thousand men, that had so little trouble with its employees. He added that his staff always worked in harmony and its members 'were always ready and willing to carry out the orders promulgated.' As the party proceeded over what is now the Pennsylvania division, the wives of several of its members joined their husbands, and at seven o'clock in the evening [passing through Carbondale] the party arrived at Scranton. These concluding particulars have been taken from an old number of the Scranton Republican which gives the number participating as between two and three hundred and adds that 'not an incident occurred that was unpleasant.' " (COP, pp. 253-59)

The *Carbondale Advance* of December 4, 1875 reported additional facts, valuable statistics, and distances about the new D&H line to Canada, the 'New York & Canada railroad. Here is that report:

"The New York and Canada Railroad. / We gave last week extended and interesting extracts from the *Montreal Gazette*, giving an account of the brilliant reception accorded to our railway magnates and their guests in that city the preceding week. / To follow this, we give below, to-day, the following from the *Railroad World*, giving additional facts, valuable statistics, and distances upon this new line: / The new line thus inaugurated has the corporate title of the 'New York & Canada railroad.' It was originally chartered with a capital stock of \$3,000,000, and was designed for two distinct purposes: 1st. To enable the Delaware & Hudson Canal company (which controlled a line of railroad from its coal fields to Whitehall) to ship its anthracite beyond that point to Canada over its own rails; and 2d. To reach and develop; the magnificent iron ore interests of Essex, Clinton, and adjoining counties on the western shore of Lake Champlain. The length of the road from Whitehall to the Canada line is 114 miles. Of this distance the old 'Montreal & Plattsburg' railroad (23 miles long) formed a part, and the old 'Whitehall & Plattsburg' railroad, which was only completed from Port Henry to Ticonderoga (17 miles), formed another part. Both of these incomplete lines were absorbed by, and became portions of,

the 'New York and Canada' line, which was thus obliged, after using the old links, to actually build 74 miles. It will simplify this matter to give an exact statement of the route from the coal fields of Pennsylvania to Montreal by the new railroad:--/

	Miles
Scranton to Carbondale	15
Carbondale to Jefferson Junction	35
Jefferson Junction to Nineveh	21
Nineveh to Albany	119
Albany to Whitehall	60
Whitehall to Ticonderoga	23
Ticonderoga to Port Henry	17
Port Henry to Plattsburg	51
Plattsburg to Canada line	23
Total (coal fields to Canada line)	364

Upon reaching the Canada line, the company has the choice of two routes to Montreal. It can either ship its coal by the Grand Trunk rails to Caughnawaga (on the St. Lawrence river, 10 miles above Montreal) and thence by boats into the city, or it can build a short line to reach Rouse's Point, and thence over the Grand Trunk and Victoria bridge into the heart of the real metropolis of the dominion. It proposes to adopt the latter plan. / Cost of the road to September 31st was \$5,962,000, but additional expenses since that time will make the total cost about \$7,000,000. Of this \$4,000,000 was borrowed in England, \$234,000 guaranteed by towns along the line, as follows:

Plattsburg	\$100,000
Westport	40,000
Essex	40,000
Putnam	12,000
Dresden	5,000
Chesterfield	25,000
Chazy	12,000
Total	\$234,000

The remainder was subscribed by the Delaware & Hudson Canal company. / By the completion of this line, the Delaware & Hudson Canal company obtains an advantage over all its coalmining rivals in being able to ship its anthracite to a foreign market without using water communication [emphasis added]. The Lehigh Valley railroad, which has its northern shipping depot at Fair Haven, on Lake Ontario, the Northern Central, which ships at Sodus Bay, the Delaware, Lackawanna & Western, which ships at Oswego, are all liable to have their lines of water traffic cut off in winter. To a certain extent the Delaware & Hudson will enjoy a winter monopoly [emphasis added]. The consumption of fuel in Canada is on the increase—in the last five years it having risen in the eastern townships alone from 1,000 to 10,000 tons per annum, while the city of Montreal last year consumed 160,000 from the Delaware & Hudson Canal company." (Carbondale Advance, December 4, 1875, p. 2)

In June 1893, dining car service was inaugurated between Whitehall and Montreal.

Maps of the D&H Line North to Rouse's Point

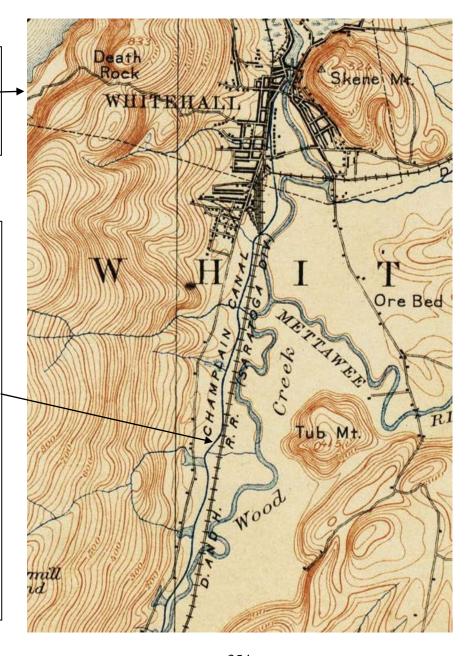
Starting at Whitehall

Detail from U. S. Geological Survey, *Whitehall Quadrangle*, Surveyed in 1893. Henry Gannett, Chief Topographer; H. M. Wilson, Geographer in charge, Triangulation by U. S.

Coast and Geodetic Survey; Topography by G. E. Hyde and Jas. McCormick.

Willis G. Fisher (see boxed text at the right) lived at 22 Saunders Street, Whitehall, NY.

The Champlain Canal officially opened in 1823 and connected the south end of Lake Champlain with the Hudson River at Troy. This 60-mile canal soon helped turn Lake Champlain into a busy thoroughfare with products such as timber, produce, iron, stone, and more moving to the Hudson River. Leisure travel soon became affordable and the tourists came North to see such sites as the ruins of Fort Ticonderoga.



Ulysses Simpson Grant, former president of the United States, died on July 23, 1885 at Mount McGregor, a few miles north of Saratoga. The engineer at the controls of the Grant funeral train (D&H locomotive No. 210, 4-4-0) on its journey from Saratoga Springs to Albany on Tuesday, August 4 (departure from Saratoga at 2:20 P.M. arrival at Albany at 3:40), was Willis G. Fisher of Whitehall, NY. In the biographical portrait of Fisher that is published in the March 1, 1925 issue of *The Delaware* and Hudson Company Bulletin (pp. 3-4, 9) we read: "Other members of the crew were John King, fireman; Leonard Thornton, conductor; John J. Conway, Hiram A. Mitchell and Frank Terriault, trainmen. Theodore Voorhees, superintendent of the road, and other officials accompanied the train." On March 1, 1919, Willis G. Fisher was retired on pension by the D&H after 45 years and 8 months of continuous service.

President Ulysses S. Grant died on July 23, 1885. The flags along the Gravity Railroad were flown at half-mast:

"The flags along the gravity are at half-mast / The gravity boys have a base-ball club. James Bryden is captain and Frank Shannon secretary. They want to play the Carbondale club some time." (Carbondale Leader, July 24, 1885, p. 1)

D&H engine No. 210 hauled the General Grant Funeral Train, August 4, 1885:

Of the passenger power, the "210" is distinctive, having been used to haul the "General Grant Funeral Train," August 4, 1885. This locomotive was built by the Dickson Manufacturing Co. in 1884 and had:—nineteen by twenty-four inch cylinders; sixtyeight inch drivers; boiler, fifty-two inches in diameter, with steam pressure of one hundred thirty-five pounds; firebox, ten feet four inches by forty-two inches wide; one hundred ninety, two inch tubes; weight, ninety-three thousand pounds in working order; tractive power, nineteen thousand one hundred seventy pounds.



(*Railroadians.* . . , p. 46):

Another Grant Funeral Train Experience:

Arthur O. Lee, a D&H police sergeant, was on duty in the Saratoga Springs D&H passenger station when General Grant arrived there on his way to Mount McGregor in June, 1885. Sergeant Lee was also on duty in the Saratoga Springs station on August 4, when General Grant's body was transferred from a Mount McGregor train to a D&H train for the trip to Albany.

Here is the opening paragraph of Arthur O. Lee's biographical portrait ("Guarded Grant Funeral Train") that was published in the August 1, 1937 issue (pp. 115-116, 126) of *The Delaware and Hudson Railroad Bulletin:*

"When ex-President Ulysses S. Grant, suffering from what was shortly to prove his fatal illness, passed through Saratoga Springs en route to Mt. McGregor, in June 1885, one of the few men to actually see him was Arthur O. Lee, retired Delaware and Hudson police sergeant, who was one of those assigned to control the crowd milling about the passenger station. Grant, who arrived over the Delaware and Hudson, did not then appear to be seriously ill, although he was transferred on a cot from the Pullman in which he arrived to the narrow-gauge Mt. McGregor Railroad car in which he was to complete his trip. When he died, five weeks later, on July 23, 1885, his body returning over the same route, Officer Lee was again assigned to duty at the station while the coffin was being transferred to a funeral car for the run to New York via Albany." (p. 115)

Additional interesting details are presented in Arthur Lee's biographical portrait:

"In 1885 Mr. Lee was appointed Police Officer at the Saratoga station and as such wore a blue uniform, a felt police helmet, and carried a night stick and a pocket model .32 calibre revolver. He and Sergeant Joseph F. Ernst, now deceased, are therefore believed to have been the first uniformed police officers of the Delaware and Hudson. . . / When the Delaware and Hudson Police Department was organized, February 1, 1908, he became a patrolman at Saratoga where he was stationed until 1923. In that year Mr. Lee was promoted to the rank of Sergeant, in charge of the Lake George Branch, and he continued in that capacity until his retirement July 1, 1932, after 49 years of continuous service with the company." (p. 116)

From Arthur Lee's biographical portrait, we learn that in the summer of 1884, there were seven passenger train arrivals from the north at Saratoga and fourteen from the south and west, daily; also a like number of passenger trains departing from Saratoga Springs. In addition, two- and three-section special and excursion trains brought thousands of one-day visitors and summer vacationists to Saratoga Springs.

The Champlain Canal:

Roger Shattuck's career with the D&H began in 1876 when he worked with conductor Truman Austin on the local freight between Whitehall and Rouses Point. Shattuck's career in the transportation business, however, began years before that, however, when, at the age of 16, he began working on a boat of the Northern Transportation Line. For some time, he worked as a helmsman on a boat operating between local points on Lake Champlain and nearby points on the Champlain Canal.

The Champlain Canal follows the traditional Native American route that connects the Hudson River to Lake Champlain and eventually the Saint Lawrence Seaway. Officially beginning at Waterford where the Erie Canal departs from the Hudson River, the 60 mile long Champlain Canal continues upstream along the Hudson River to Fort Edward. At Fort Edward, the canal branches away from the Hudson River towards Whitehall.

In Shattuck's biographical portrait ("From Helmsman to Conductor") in the January 1, 1929 issue (pp. 3-4, 12) of *The Delaware and Hudson Company Bulletin*, we read the following of his experience on the Champlain Canal from Whitehall to Montreal to New York City:

"During one season, from approximately the first of May until the ice began to form on the lakes and canals in the late fall, he was on a boat running between Montreal and New York. The boats made three and one-half round trips during the season. They left Whitehall for Montreal in the Spring. Three round trips were then made between Montreal and New York and they returned to Whitehall for the winter. / The general route followed by these early canal men was, roughly, as follows: Leaving Montreal, they went down the St. Lawrence to Sorel, Province of Quebec; from Sorel, up the Richelieu River through Chambly, St. Johns, Rouses Point, thence over Lake Champlain and the canal to Whitehall. Continuing southward through the Champlain Canal, they proceeded to Troy, Albany and down the Hudson River to New York City." (pp. 3-4)

Most interestingly, from the perspective of the recorded history of the D&H, these Whitehall/Montreal/New York City canal boats, in passing Rondout on the Hudson River, stopped to pick up coal, both on their way to New York City and on their way to Montreal. In Shattuck's biographical portrait we read:

"Stops were made at the various cities of importance to take on and unload merchandise and other cargoes. At Rondout coal was loaded for New York. Here he first came into contact with the Delaware and Hudson Canal Company. Returning from New York, another load of coal was taken on for the towns and cities between Rondout and Montreal." (p. 4)

Anthracite coal was unloaded from D&H canal boats at Rondout and then re-loaded into canal boats for shipment to Montreal and New York City. That fact is not generally known, and we are indebted to the author of Shattuck's biographical portrait for recording it in Shattuck's biographical portrait. Similarly, for the record, it is important to remember that anthracite coal at Rondout was also loaded into canal boats for shipment up the Hudson River and West through the Erie Canal.

Corlice Gordon, who was born in 1867 in the village of Whitehall at the southern end of Lake Champlain and who worked for the D&H for 43 years, began his work in the transportation industry at the age of eleven, in 1878, driving lock horses on the Champlain Canal. "It was his duty," we read in the biographical portrait of the man ("Worked on Champlain Canal") that was published in the August 15, 1930 issue (pp. 143-244, 254) of *The Delaware and Hudson Railroad Bulletin*, "to draw the canal boats into the lock and secure them while the lock was being flooded, and then pull them out into the lake with his team of horses. . . Traffic on the canal was very heavy at that time; boat after boat loaded with wood, coal, iron ore, or merchandise passed through the lock every day." (p. 243) In 1883, at the age of 16, Corlice began working for the D&H as a brakeman on the "Northern Freight" between Whitehall and Port Henry. That he did for 14 years. He then worked for a long period "on the rounds," wherever he happened to be sent either as conductor, yard conductor, or trainman.

On January 1, 1875, James Whelan, Sr. was hired by the D&H agent at Whitehall to work in the local freight house. Nine months later, on September 1, 1875, he was hired by Conductor Pat Comfort as trainman on the first freight train to make the complete trip from Whitehall to Plattsburg over the New York and Canada Railroad, the line which became the southern part of the D&H Champlain Division. The following summer, 1876, Superintendent Theodore Voorhees offered him the position of Conductor on a passenger train. He worked in that position until February 14, 1898, when he was injured in an accident at Mohawk. From then, until April 1, 1925 when he was pensioned, he worked in the Maintenance Department. All of the above information about James Whalen, Sr. we have learned from his biographical portrait ("Owned Boat on Champlain Canal") that was published in the April 1, 1931 issue (pp. 99-100, 109) of *The Delaware and Hudson Company Bulletin*.

As interesting as James Whalen's work career is from 1875 to 1925, it pales when compared to his work experience in the years before 1875, when he worked on, and ultimately owned and operated a boat on, the Champlain Canal. About Whitehall, the Champlain Canal, the day-to-day operations of the Champlain Canal, and James Whalen's work experience on the Champlain Canal, there is a great deal of very important historical data in Whalen's biographical portrait. Therein, we read:

"Even before the opening of rail service [on December 10, 1848, the Saratoga and Whitehall Rail Road reached Whitehall], however, Whitehall had assumed an important role as a shipping center by reason of its location at the southern end of Lake Champlain. Located as it was at the northern terminus of the Champlain Canal, it was the logical point at which to interchange the cargoes of lake and canal boats. This also gave it an advantageous position as a manufacturing town. In fact, there were already three foundries, two planning mills, three planning and saw mills, a tannery, and two sash and door factories in operation when the railroad reached this community of 600 people.

"As a 'hired hand' and, later, as owner and operator of a canal-boat, Mr. Whalen spent his earlier years en route between New York and Montreal. There were 'stations' on the canal at Fort Ann, Fort Edward, Schuylerville, Mechanicville, Troy, and Albany. The boats left Whitehall with two-man crews, the driver, whip in hand, urging the team of two or three horses or mules, along the narrow tow-path at the side of the waterway. The beasts, harnessed side by side or in tandem, plodded steadily along. The steersman stood at the rudder in the stern, guiding the craft through the narrow channel, and avoiding other boats and various projections from either bank. When the driver became tired of walking he exchanged positions with the steersman, alternating in this way until the first station was reached.

"Here another crew, with fresh horses, took the boat over, affording the crew and animals relieved an opportunity to rest. The horses were first fed and bedded in the barn which adjoined the station. Then the crew ate and retired in the boarding house maintained at the canal company's expense.

"At each station a list of crews was maintained, similar to the 'extra list' on the railroad, each crew taking its own turn going out on the line when needed, no matter which direction the waiting boat was going. At times, when business was slow, the rest periods between 'runs' were long; when traffic was heavier, they went out shortly after their arrival. Under this system canal men never knew when they would be home or even where they would be from one day to the next. At the end of the season each man received his pay for all the time he had worked, at the rate of \$10. per month, no matter how many trips he had made or how far he had traveled.

"After a number of years spent in the employ of the canal companies, Mr. Whalen purchased a boat of his own which he operated between Ottawa, Three Rivers, Montreal, and Sorel, and Troy, Albany, and New York. The principal commodity then transported on the southward trips was lumber. On the return voyage various items were carried, including an occasional load of coal from Whitehall to Canada.

"As a private boat operator Mr. Whalen had his own horses and other necessary equipment, and hired a driver and steersman. In order to 'make time' he took his turn driving and piloting, enabling the other two members of the crew to rest.

"On a trip from Ottawa to New York, with a cargo of lumber, at the rate of \$8. per 1,000 feet, Mr. Whalen could realize a profit of approximately \$200. Considering that such a trip consumed but eight days it is readily understandable that canalling was a prosperous venture for a private boat owner. The rate from Canada to Albany, too, was \$7. per 1,000 feet, which made such a 'run' likewise profitable.

"From Ottawa, the canal boat was towed, by tug, down the St. Lawrence to Sorel, thence up the Sorel River and Chambly Canal to St. Johns. From St. Johns to Whitehall they were taken in tow of another tug for the run down Lake Champlain to Whitehall. Here horses were substituted for the balance of the trip down to Troy.

"Leaving Whitehall, the two-and-one-half-day trip to Troy was made with Mr. Whalen's team and a hired driver and steersman. It was the practice to drive for about six hours and then stop to feed and rest both horses and men. At night the teams were bedded in privately owned barns along the canal, while the men slept in berths aboard the boat. On account of the short season, however, it was customary to drive from 2 a. m. until 10 p. m., allowing only sufficient time for resting during the intervening 20 hours. At Troy a tug was again hired for the balance of the trip to Albany or New York.

"It cost only about \$20. to hire a tug from a Canadian city to St. Johns: \$20 from St. Johns to Whitehall; \$30. toll through the Champlain Canal; and \$20. from Albany to New York. After paying the driver and steersman, as well as all other items or expense, the owner still had a substantial sum left for himself.

"When the present barge canal was built, however, larger boats were put in operation. This, in addition to the decreased price of lumber, caused the failure of many individual boat owners."

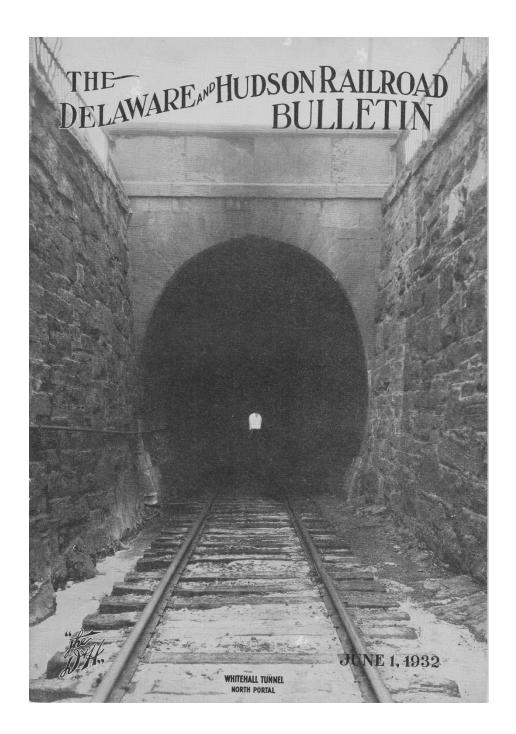
I shouldn't wonder if Whelan's biographical portrait contains more details about the operations of the Champlain Canal than any other document. As we have noted above, D&H anthracite at Rondout was loaded into Champlain Canal boats (such as the one owned by James Whalen) at Rondout and (1) taken down the Hudson River to New York City and other ports on the way to New York, or (2) taken up the Hudson River and into the Champlain Canal and carried north to Montreal.

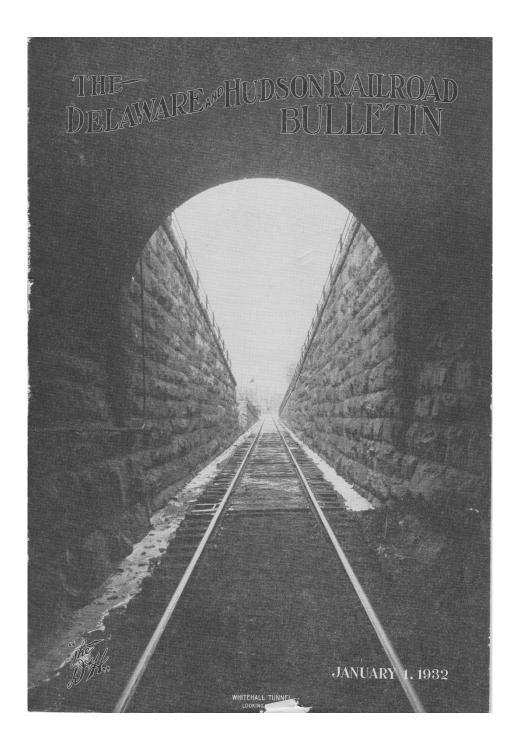
Adolphus Case:

At the age of nine, Adolphus Case, who was born May 2, 1851 at Montreal, began working on the Champlain Canal as the driver of a team of horses on the canal between Montreal and New York. In Case's biographical portrait ("Doughnuts and Pea Soup Famous") published in the September 1, 1932 issue of *The Delaware and Hudson Railroad Bulletin* (pp. 243-244), we read:

"Life on a canal boat was quite interesting, even though there was always more than enough work for the lad to do. The four horses needed to pull the boat through the canal, were kept in a stable on board, a bridge being provided to walk them on and off the craft when the tow-horse teams were changed every six hours. Each driver was on duty six hours, then rested six, while passing through the canal, thus working twelve hours in every 24, for which he was paid, if Adophus' rate may be considered typical, 25 cents per day, plus board. / Leaving Montreal with a cargo of grain, ice, pulp wood, or lumber on board, the canal boat was towed by a steam tug down Lake Champlain; horsepower was used from Whitehall to Troy; then another tug was hired to pull the craft to New York, approximately two weeks being consumed by the one-way trip. / Twice during his twenty years on the canal Mr. Case's boat made the trip from New York to Buffalo and back, via the Erie Canal, before returning to Montreal. / After his marriage to Paulina Aiken, of Whitehall, in 1881, he made only one more trip to New York; the following spring he was hired by Bridge and Building Department Foreman Combers of the Delaware and Hudson as a carpenter." (p. 244)

One of the projects undertaken by the D&H Bridge and Building Department during the time that Adolphus Case worked in that department as a carpenter was the complete relining of the Whitehall Tunnel, which was eliminated in 1932 (see herein the material on the Whitehall Tunnel).





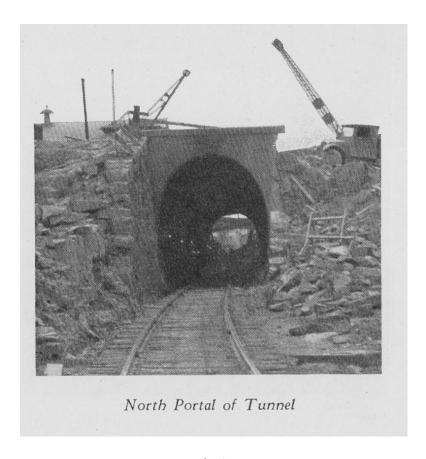
From November 16, 1875 to 1932, the Whitehall Tunnel was single-track. In 1932, the roof was removed from the tunnel, a double-track line installed, and the streets of Whitehall carried over the D&H tracks on bridges.

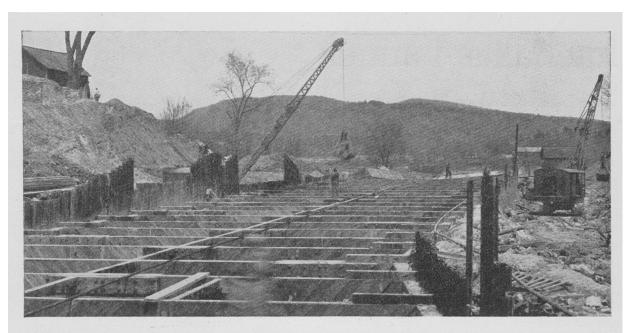
Here are five photographs from "Taking the Roof Off Whitehall's Tunnel" (*The Delaware and Hudson Company Bulletin*, June 1, 1932, pp.168-70).





In the foreground, below the track, is a cross-brace supporting the sheet-piling at the right. Above the clearance limit appears more bracing. Widening the cut has progressed somewhat, while in the distance men may be seen at work removing another section of the tunnel.



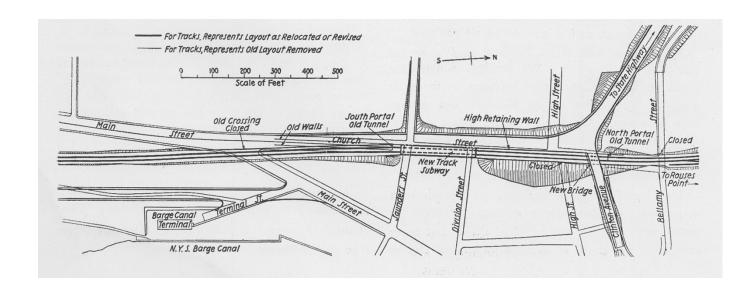


Cross-bracing at North End of New Cut

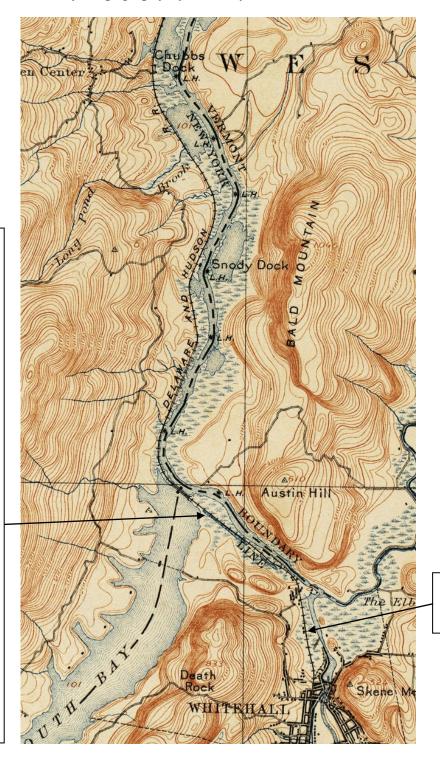


View from intersection of Church and Broad Streets, looking north, showing progress which had been made in the removal of the tunnel on May 8th

"Whitehall Crossings Eliminated by Project Involving Sweeping Changes to Station and Track Layout," pp. 44-45 of *The Delaware and Hudson Railroad Bulletin*, March 1, 1934.



Detail from U. S. Geological Survey, *Whitehall Quadrangle*, Surveyed in 1893 Henry Gannett, Chief Topographer; H. M. Wilson, Geographer in charge, Triangulation by U. S. Coast and Geodetic Survey; Topography by G. E. Hyde and Jas. McCormick.



The D&H heads north from Whitehall

and earth were dumped to make a

"In much of the new

between Whitehall and Addison
Junction and between Port Henry and Plattsburgh, almost insuperable difficulties had been overcome, and this accomplishment stands as a monument to the engineers of the company. Just beyond Whitehall

the road was carried across South Bay by means of a long trestle to the marsh on the north shore which in many

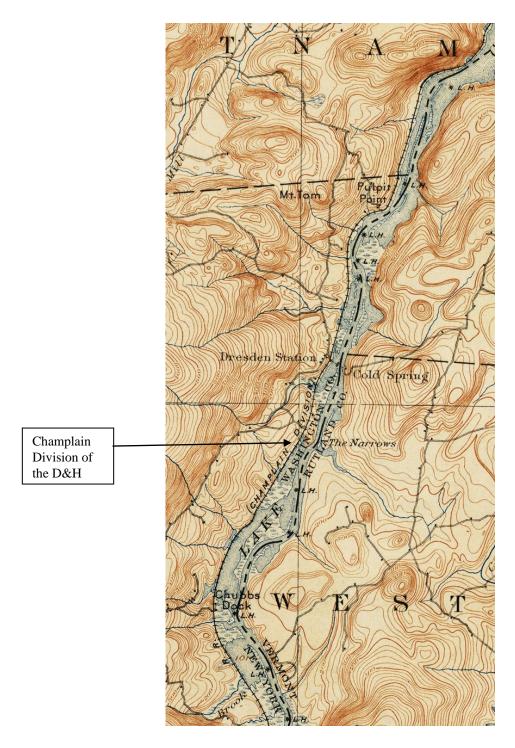
places seemed to be

bottomless. Here old

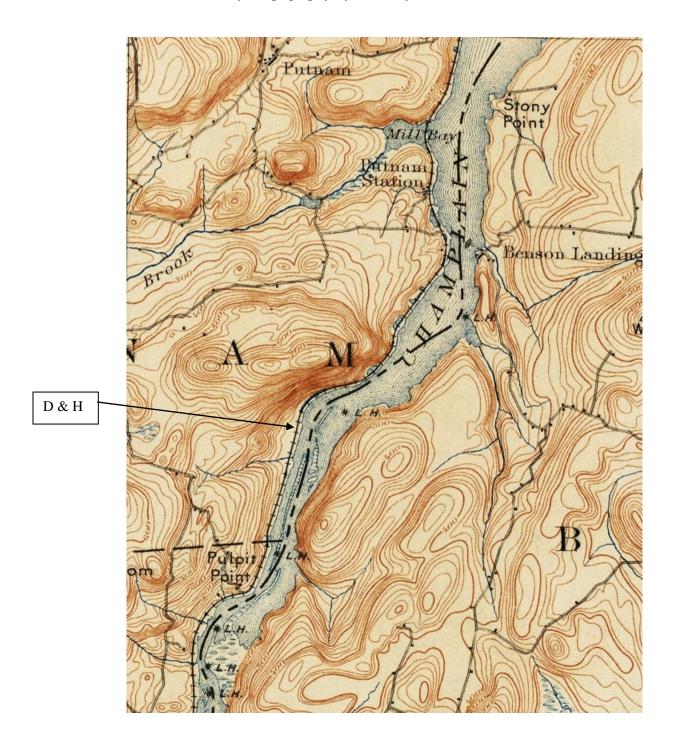
canal boats, trees, timbers and great quantities of rock

construction

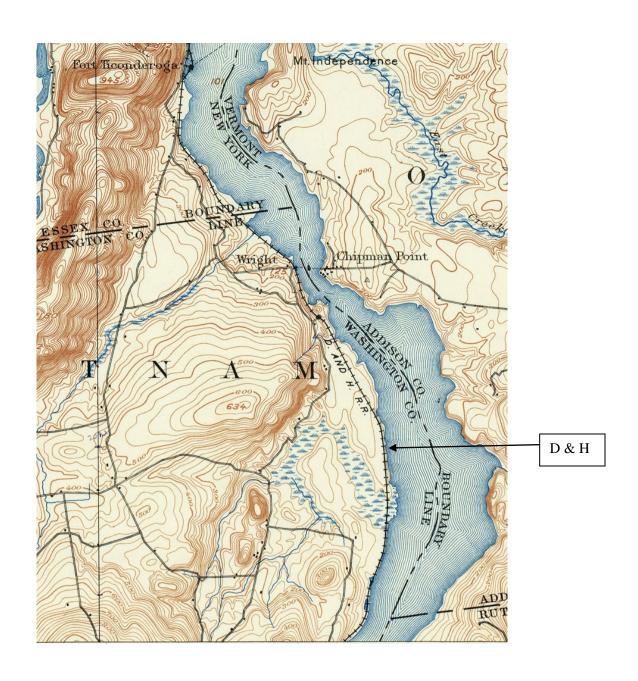
Detail from U. S. Geological Survey, *Whitehall Quadrangle*, Surveyed in 1893 Henry Gannett, Chief Topographer; H. M. Wilson, Geographer in charge, Triangulation by U. S. Coast and Geodetic Survey; Topography by G. E. Hyde and Jas. McCormick.



Detail from U. S. Geological Survey, *Whitehall Quadrangle*, Surveyed in 1893 Henry Gannett, Chief Topographer; H. M. Wilson, Geographer in charge, Triangulation by U. S. Coast and Geodetic Survey; Topography by G. E. Hyde and Jas. McCormick.



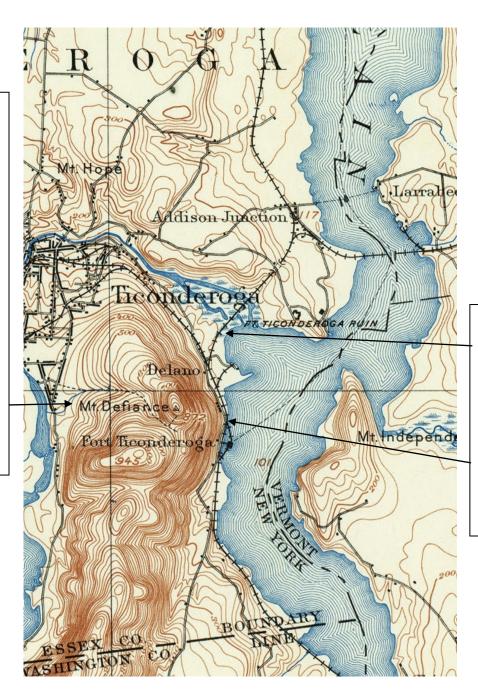
Detail from U. S. Geological Survey, *Ticonderoga Quadrangle*, Surveyed in 1894 Henry Gannett, Chief Topographer; H. M. Wilson, Geographer in charge; Triangulation by U. S. Coast and Geodetic Survey; Topography by R. H. Chapman.



Detail from U. S. Geological Survey, *Ticonderoga Quadrangle*, Surveyed in 1894 Henry Gannett, Chief Topographer; H. M. Wilson, Geographer in charge; Triangulation by U. S. Coast and Geodetic Survey; Topography by R. H. Chapman.

"Further north, five ranges of the Adirondacks were encountered, adding greatly to the construction difficulties. These ranges terminate abruptly, at the shore of Lake Champlain, in rocky headlands or mountains presenting serious obstacles." (COP, p 251)

At the end of the first of these five Adirondack ranges, the Black Mountain, stands Mount Defiance.

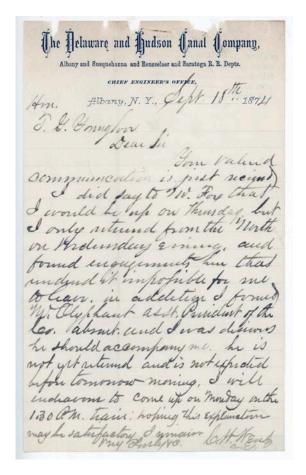


"Skirting the almost perpendicular face of Mount Defiance, at the end of . . . the Black Mountain, the [D&H] line crosses the outlet from Lake George and immediately plunges into a tunnel driven through the headland upon which is located old Fort Ticonderoga." (COP, p. 251)

More about the tunnel at Fort Ticonderoga:

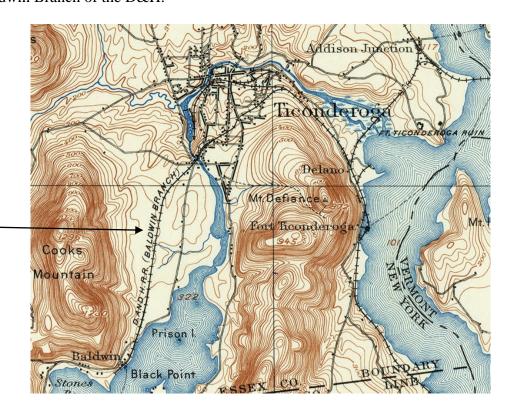
The Fort Ticonderoga tunnel is 440 feet long and is 40 feet under the Fort Ticonderoga battleground. Work on the tunnel began on March 26, 1873. On February 14, 1874, workmen pierced an opening in the rock at the north end of the tunnel. In April 1874, Thomas Dickson and Chief Engineer Wentz* of the New York and Canada Rail Road visited the tunnel site. On April 18, 1874, Mr. Seymour, the engineer in charge of sections 20, 21, and 22, stated that the grading at the north end of the tunnel section was completed and ready for track. The finished work included a long pile trestle across the mouth of "Ti Creek" and the tunnel under the hill near Fort Ticonderoga. A connecting "Y" was formed with the Whitehall & Plattsburgh Railroad by running a curve from a point just north of the tunnel and connecting with the old road at a short distance north of the depot. A railroad engine passed through the tunnel at Fort Ticonderoga for the first time during the week of June 20, 1874.

* Letter from **C. W. Wentz, Chief Engineer,** dated September 18, 1874:. Posted by Steven Brown on *Facebook* on 09-15-2015:



The Baldwin Branch of the D&H:

The Baldwin Branch of the D&H: a rail link between Lake George and Lake Champlain. Work on the branch began on June 11, 1874, and the line was opened on May 24, 1875. The first train, consisting of a coach and baggage car that were drawn by the steam locomotive "Plattsburgh. passed over the new line on May 31, 1875.



Before the New York and Canada was opened on November 8, 1875, we read in *Century of Progress* (p. 261) the Baldwin Branch of the D&H between Montcalm Landing (Port Marshall, Lake Champlain) and Baldwin (Coate's Landing, Lake George) was built: "A short branch line, known as the Baldwin branch, between For Ticonderoga, now Montcalm Landing, and Baldwin, at the lower end of Lake George, was also built and its use began on May 24, 1875." The first run over the new line by the steam locomotive *Plattsburgh* took place a week later, on May 31, 1875.

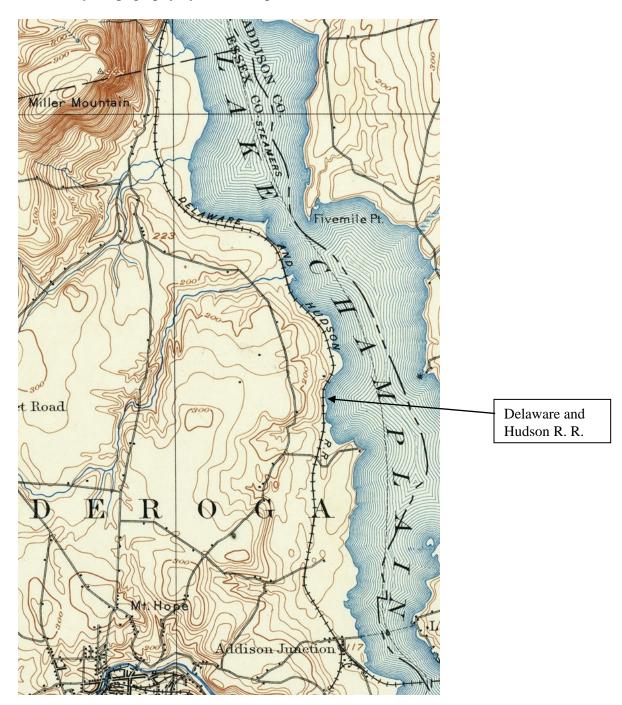
In a self-contradictory statement on page 259 in *COP*, we read that the Baldwin Branch opened in 1876: "During the year [1876] there was some additional construction by the New York and Canada. . . A short branch line, known as the Baldwin branch, between Fort Ticonderoga, now Montcalm Landing, and Baldwin, at the lower end [in elevation, i. e. the North end] of Lake George, was also built and its use began on May 24, 1875. This minor construction, affording railroad communication between the lakes, has been particularly convenient for summer tourists."

Here are the facts: the Baldwin Branch opened on May 24, 1875; the New York and Canada was completed on November 8, 1875; the revised route from Mooers Junction to Rouses Point was placed under contract on February 10, 1876 and brought into use on November 27, 1876.

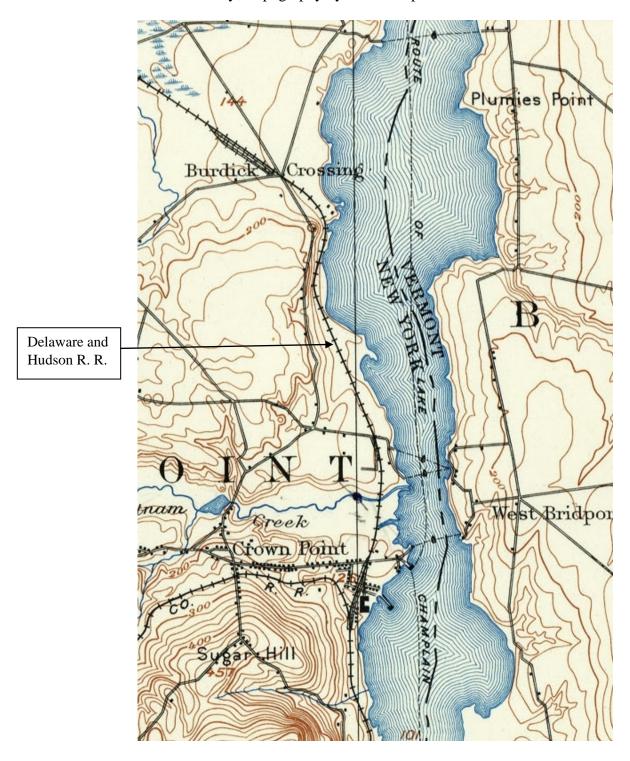
About the Baldwin Branch, we read the following in the article "Old Passes / From Collection of W. G. Baldwin" (*The Delaware and Hudson Company Bulletin*, January 15, 1932, p. 20):

"Prior to the opening of the railroad linking Montcalm Landing on Lake Champlain with Baldwin Landing on Lake George, a stage line of four and six horse coaches was operated between the two lakes by W. G. Baldwin. This stage handled the passenger traffic between the steamer line terminals at old Fort Ticonderoga and what was then Cook's Landing, now called Baldwin Landing, Lake George. Its coaches carried many notables from time to time including President Ulysses S. Grant and General Philip H. Sheridan."

Detail from U. S. Geological Survey, *Ticonderoga Quadrangle*, Surveyed in 1894 Henry Gannett, Chief Topographer; H. M. Wilson, Geographer in charge; Triangulation by U. S. Coast and Geodetic Survey; Topography by R. H. Chapman.



Detail from U. S. Geological Survey, *Ticonderoga Quadrangle*, Surveyed in 1894 Henry Gannett, Chief Topographer; H. M. Wilson, Geographer in charge; Triangulation by U. S. Coast and Geodetic Survey; Topography by R. H. Chapman.



Detail from U.S. Geological Survey, *Port Henry Sheet*, Surveyed in 1892 Henry Gannett, Chief Topographer; H. M. Wilson, Geographer in charge; Triangulation by U. S. Coast and Geodetic Survey; Topography by E. C. Barnard and G. E. Hyde.

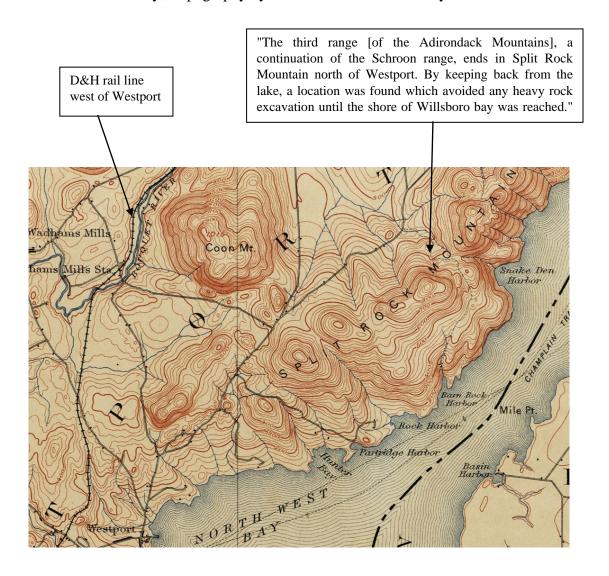
Port Henry: "For "The second range [of the a considerable Adirondack Mountains] portion of the road encountered, the north of Port Kayderosseras, Henry ties, rails terminating in Bulwagga and other track mountain, overlooking the material were bay of the same name transported down north of Crown Point, is the lake in boats passed by means of a deep and unloaded at cut, and skirting the shore convenient points. of the lake through Port Orchard For construction Henry the line encounters work it was a continuous rocky bluff necessary to upon which it was transport a necessary to excavate a locomotive in the ledge to support the same way and railroad. The irregularities after unloading it of the face of the bluff was hauled up the required the construction sloping shore to of one tunnel through a the roadbed over a rocky promontory and the temporary track line is a series of curves laid for that following the purpose." (COP, p. irregularities of the 252) shore." (COP, p. 251)

The 39 3/4-mile line from Whitehall to Port Henry opened on November 30, 1874 and included a new 24-mile section from Whitehall to Addison Junction.

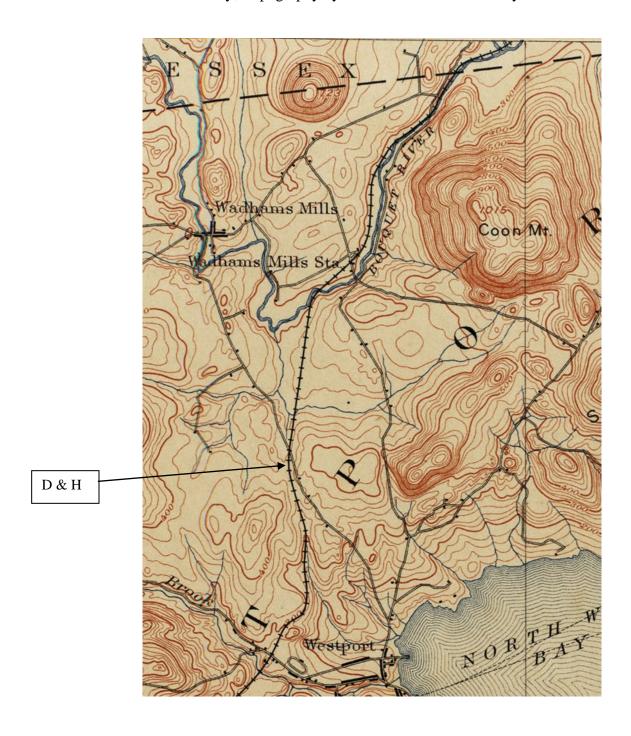


"Along the Lake–Delaware & Hudson's 'Adirondack' South of Port Henry, N. Y. on August 19, 1975." Photo by Tom Nemeth, Railpace Company. Post card in the collection of the Carbondale D&H Transportation Museum.

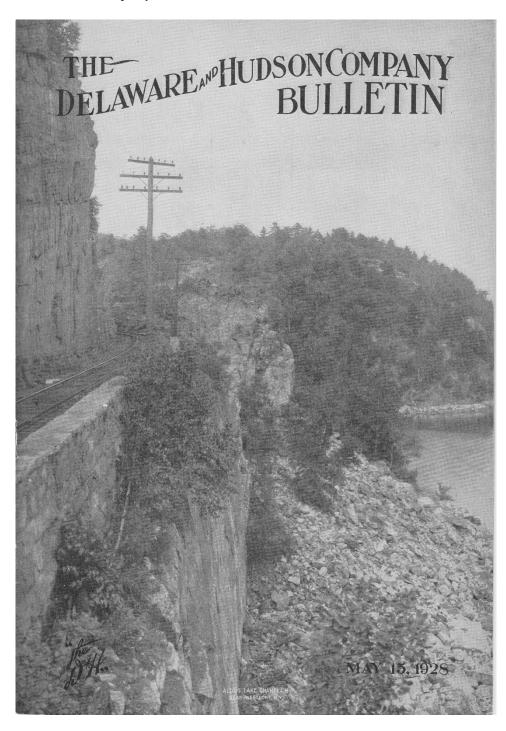
Detail from U.S. Geological Survey, *Port Henry Sheet*, Surveyed in 1892 Henry Gannett, Chief Topographer; H. M. Wilson, Geographer in charge; Triangulation by U. S. Coast and Geodetic Survey; Topography by E. C. Barnard and G. E. Hyde.



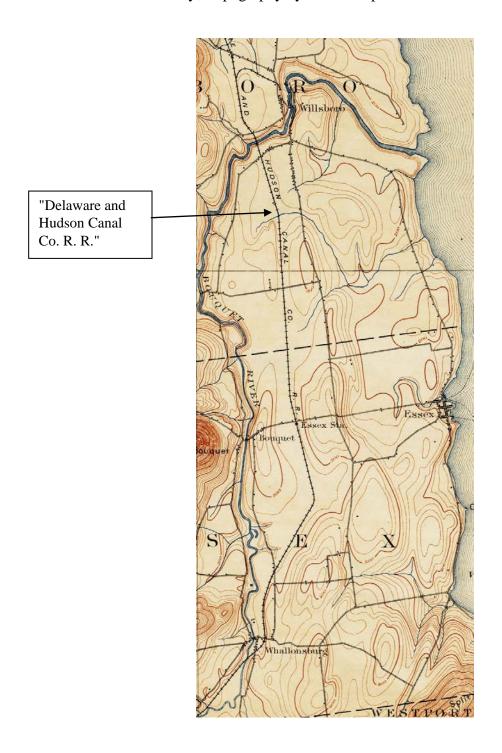
Detail from U.S. Geological Survey, *Port Henry Sheet*, Surveyed in 1892 Henry Gannett, Chief Topographer; H. M. Wilson, Geographer in charge; Triangulation by U. S. Coast and Geodetic Survey; Topography by E. C. Barnard and G. E. Hyde.



Along Lake Champlain / Near Westport, N.Y. Cover photo on the May 15, 1928 issue of The Delaware and Hudson Company Bulletin:

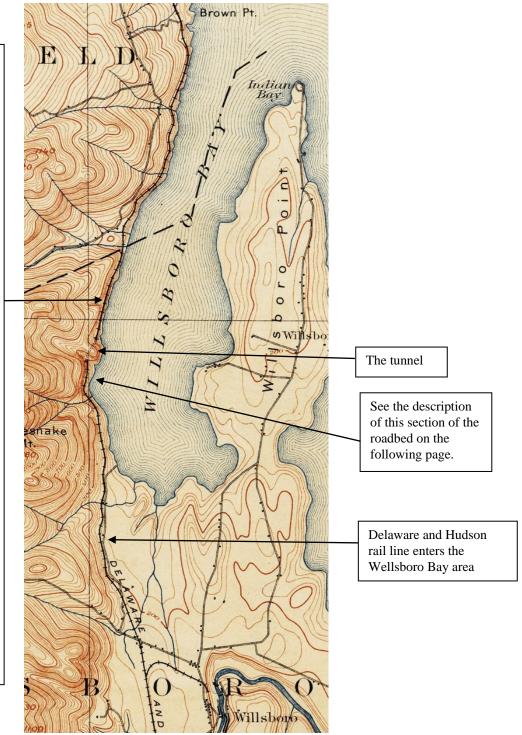


Detail from U. S. Geological Survey, *Willsboro Sheet*, Surveyed in 1893 Henry Gannett, Chief Topographer; H. M. Wilson, Geographer in charge; Triangulation by U. S. Coast and Geodetic Survey; Topography by R. H. Chapman and Robt. Muldrow.



Detail from U. S. Geological Survey, *Willsboro Sheet*, Surveyed in 1893 Henry Gannett, Chief Topographer; H. M. Wilson, Geographer in charge; Triangulation by U. S. Coast and Geodetic Survey; Topography by R. H. Chapman and Robt. Muldrow.

"At this point [Willsboro bay], the Bouquet, the fourth range [of the Adirondack Mountains], ends in the high bluffs extending along the shore of the bay for seven miles. Here the heaviest difficulties were encountered. In places drillers had to be let down by ropes from the top of the ledge, one hundred feet, to commence blasting. The line lies high up on the face of the bluffs and the work involved the construction of one tunnel and many rock cuts. Some of the chasms being filled with the rock from the cuts and others crossed by bridges, the whole work involved the most difficult labor because of the crude facilities available at that time. Here is found some of the most beautiful scenery on the road, comparing favorable with similar locations in the mountains of the western part of the United States." (COP, pp. 251-52)



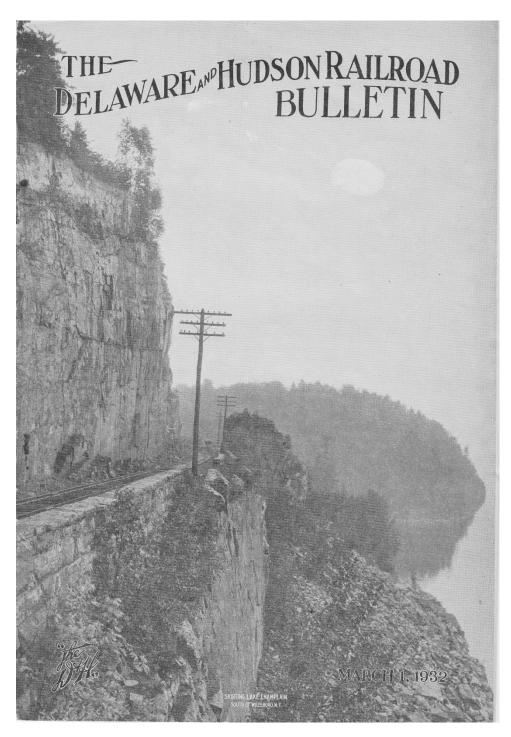
The journalist from the Plattsburgh *Republican* who was on board for the excursion on November 16-18, 1875, when the line opened, wrote the following about the seven-mile section of the line that begins at Willsboro bay:

"On this seven miles the rock cuttings are almost continuous. The track runs about ninety feet above the surface of the lake, and for a long distance there is an almost continuous sheer precipice below, while above on the left the perpendicular wall grows, as we move northward, to a greater and greater height until it finally culminates in the great Red Rock cut*, a smooth perpendicular precipice one hundred and five feet above the track at the highest point, while below it also extends ninety feet down to the water's edge. One hundred feet from the edge of the water the bay is two hundred and ten feet deep, and the tracks runs on a shelf half way up the perpendicular precipice nearly two hundred feet high. A halt (was) made here to give all a chance to view this wonderful piece of work, and then the train (crept) along on the side of the cliff through the tunnel."

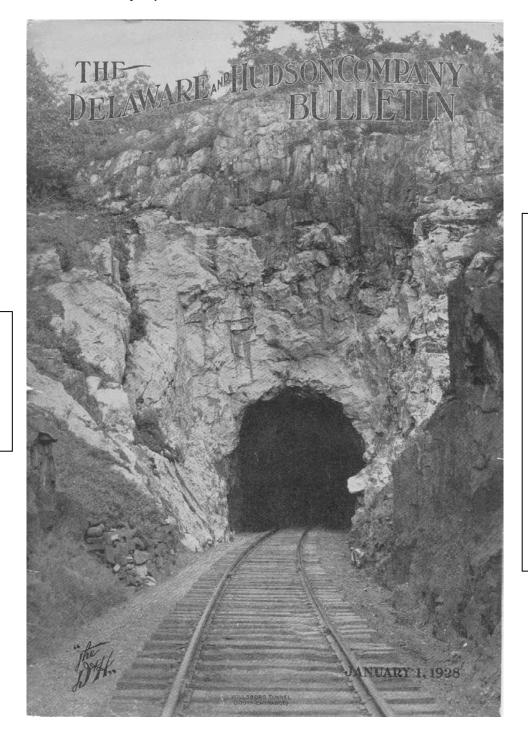
In *COP*, there is a photograph of "The red rock cut" (p. 254); also a photograph of "Lake Champlain, south from red rock cut" (p. 250). There is also a photograph of "the red rock cut" on the following page.

About the tunnel, we read the following in *COP* (p. 255): "This tunnel was described as one of the handsomest ever constructed, its course having a symmetrical curve in the shape of a perfect arch and it was added that 'the south entrance of this tunnel is through a rock which sparkles in the sunshine as if it were composed of crystals.'"

Skirting Lake Champlain / South of Willsboro, N. Y. Cover photograph on the March 1, 1932 issue of *The Delaware and Hudson Railroad Bulletin*.



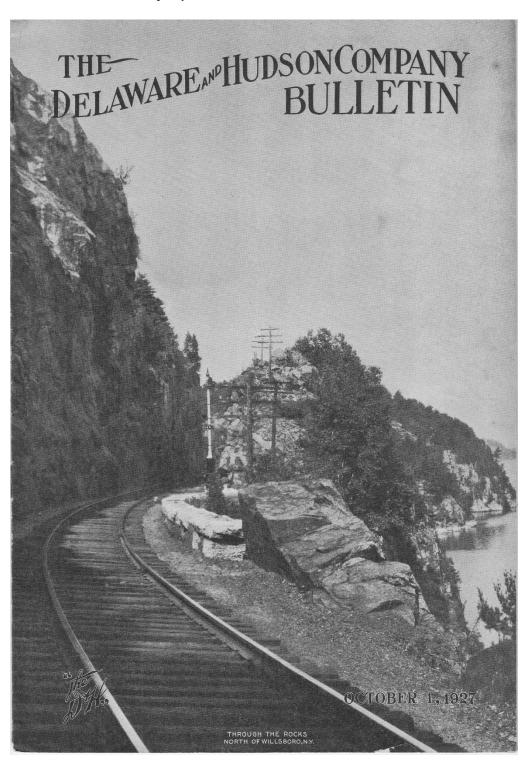
Willsboro Tunnel (South Entrance). Cover photograph on the January 1, 1928 issue of The Delaware and Hudson Company Bulletin.



This tunnel, above Willsboro Bay, was cut through sparkling quartz. (Shaughnessy, p. 134)

"In the vicinity of Willsboro, the track literally winds along the edge of a perpendicular cliff along the lake shore, at one point piercing the solid rock through a tunnel 600 feet long, 90 feet above the lake which at that point is from 50 to 192 feet deep." Biographical portrait of Frederick J. Gibbs, The Delaware and Hudson Company Bulletin, May 1, 1931, pp. 131-132.

Through the Rocks / North of Willsboro, N.Y. Cover photograph on the October 1, 1927 issue of The Delaware and Hudson Company Bulletin.

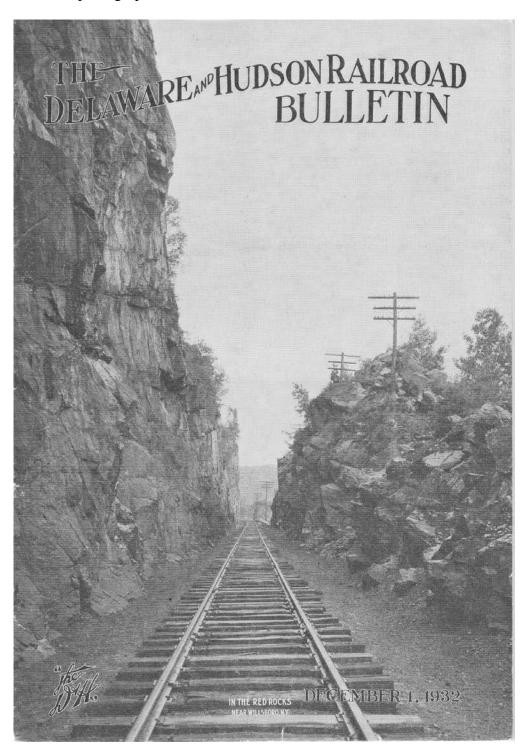


Shown below is a photograph titled "Year 1935 – Train No. 34 at Red Rocks on Lake Champlain" that is in the A. G. Dustin Collection in the D&H Transportation Museum, Carbondale. In the lower right corner of this photograph, in white ink, is the following number: 18263.

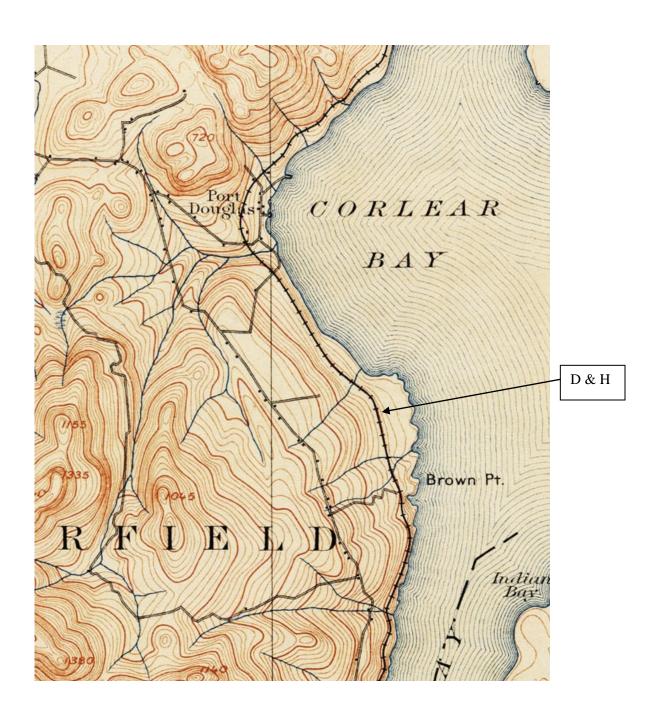


Year 1935 - Train No. 34 at Red Rocks on Lake Champlain

Shown below is the cover of the December 1, 1932 issue of *The Delaware and Hudson Railroad Bulletin*. The cover photograph is titled "In the Red Rocks near Willsboro, NY"

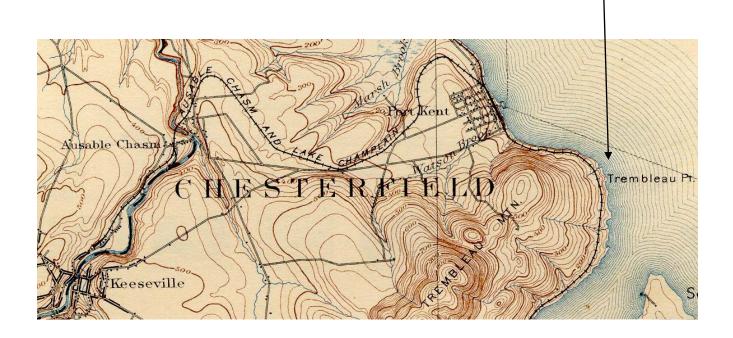


Detail from U. S. Geological Survey, *Willsboro Sheet*, Surveyed in 1893 Henry Gannett, Chief Topographer; H. M. Wilson, Geographer in charge; Triangulation by U. S. Coast and Geodetic Survey; Topography by R. H. Chapman and Robt. Muldrow.



Detail from U. S. Geological Survey, *Plattsburgh Quadrangle*, Surveyed in 1893 Henry Gannett, Chief Topographer; H. M. Wilson, Geographer in charge; Triangulation by U. S. Coast and Geodetic Survey; Topography by E. C. Barnard and James McCormick.

"Just south of Port Kent lies the fifth range [of the Adirondack Mountains], the Clinton, ending in the high promontory of Trembleau Point around which the road passes by means of rock cuts and fills." (*COP*, p. 252)

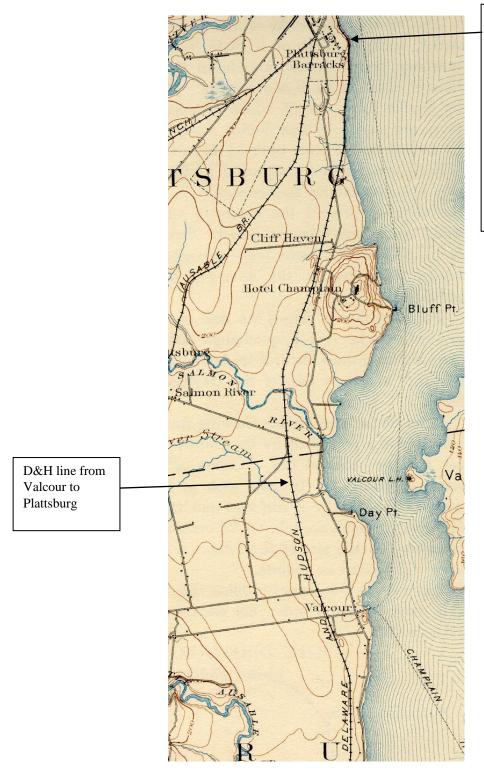


"During dry summer months in the Adirondacks it was necessary to have a fire watchman follow the steam-powered trains in the desolate section of the line between Port Kent and Willsboro." (*Shaughnessy*, p. 418)

Detail from U. S. Geological Survey, *Plattsburgh Quadrangle*, Surveyed in 1893 Henry Gannett, Chief Topographer; H. M. Wilson, Geographer in charge; Triangulation by U. S. Coast and Geodetic Survey; Topography by E. C. Barnard and James McCormick.



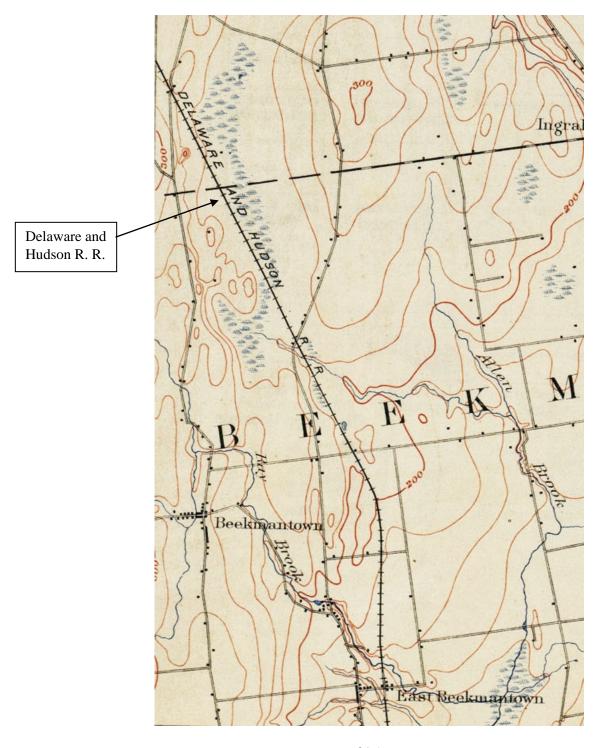
Detail from U. S. Geological Survey, *Plattsburgh Quadrangle*, Surveyed in 1893 Henry Gannett, Chief Topographer; H. M. Wilson, Geographer in charge; Triangulation by U. S. Coast and Geodetic Survey; Topography by E. C. Barnard and James McCormick.



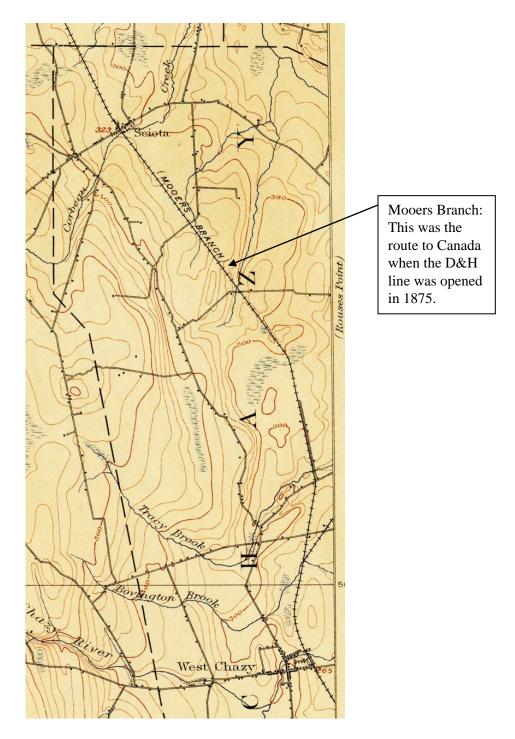
"The line to Plattsburg was opened on November 29, 1875, and by use of the track of the former Montreal and Plattsburgh through service to Mooers Junction was opened on the same day. By the opening of this through route and the use of twelve miles of the Ogdensburgh and Lake Champlain railroad to Rouses Point a connection was made with the Grand Trunk railway for Montreal." (COP, 249-250)

Heading north from Plattsburg:

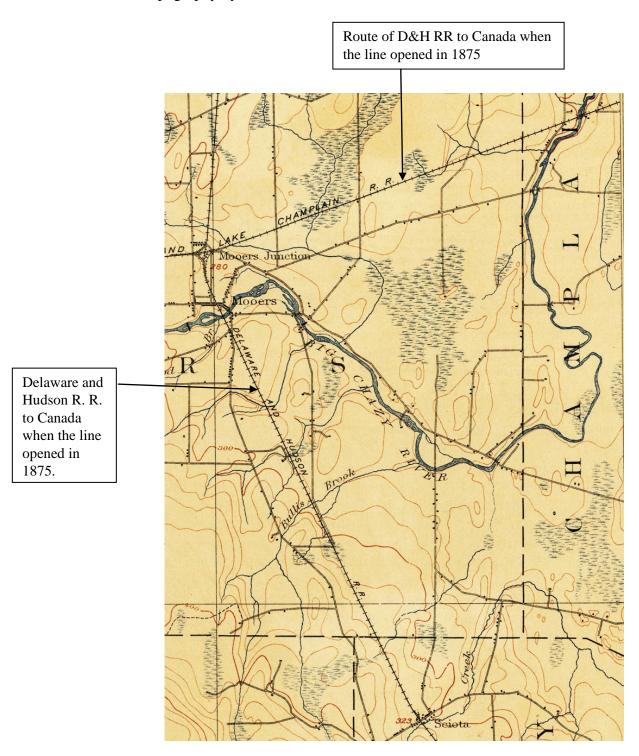
Detail from U. S. Geological Survey, *Rouses Point Quadrangle*, Surveyed in 1893 Henry Gannett, Chief Topographer; H. M. Wilson, Geographer in charge; Triangulation by U. S. Coast and Geodetic Survey; Topography by E. C. Barnard.



Detail from U. S. Geological Survey, *Mooers Quadrangle*, Surveyed 1893 Henry Gannett, Chief Topographer; H. M. Wilson, Geographer in charge; Triangulation by S. S. Gannett; Topography by E. C. Barnard and James McCormick.

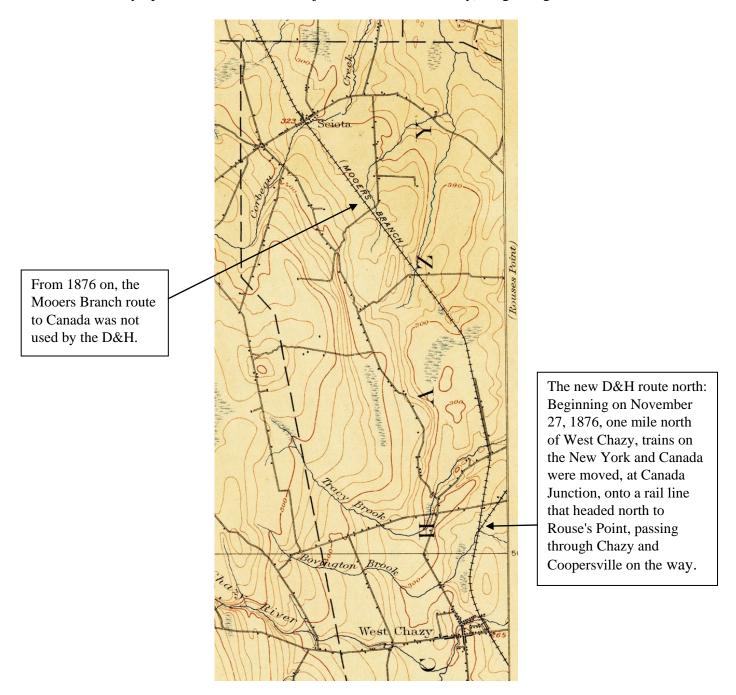


Detail from U. S. Geological Survey, *Mooers Quadrangle*, Surveyed 1893 Henry Gannett, Chief Topographer; H. M. Wilson, Geographer in charge; Triangulation by S. S. Gannett; Topography by E. C. Barnard and James McCormick.



Detail from U. S. Geological Survey, *Mooers Quadrangle*, Surveyed 1893 Henry Gannett, Chief Topographer; H. M. Wilson, Geographer in charge; Triangulation by S. S. Gannett; Topography by E. C. Barnard and James McCormick.

The new way up from Canada Junction (just north of West Chazy), beginning in 1876:

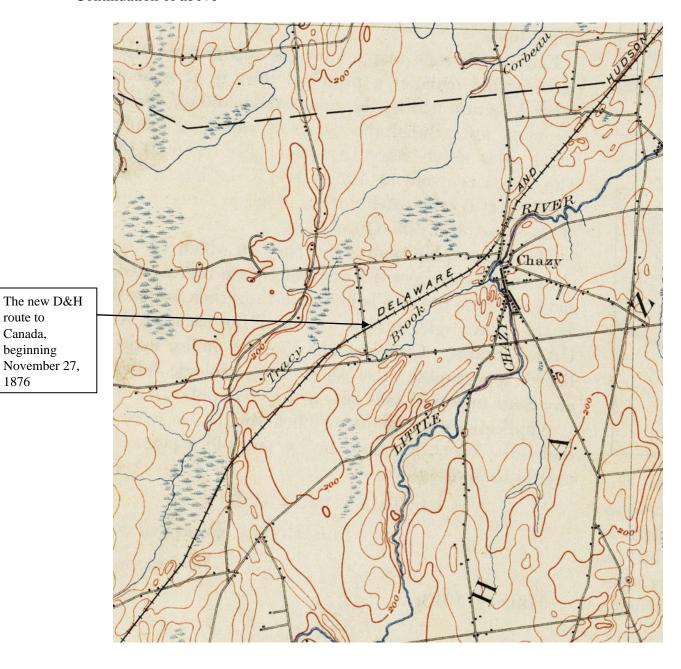


Detail from U. S. Geological Survey, Rouses Point Quadrangle, Surveyed in 1893 Henry Gannett, Chief Topographer; H. M. Wilson, Geographer in charge; Triangulation by U. S. Coast and Geodetic Survey; Topography by E. C. Barnard

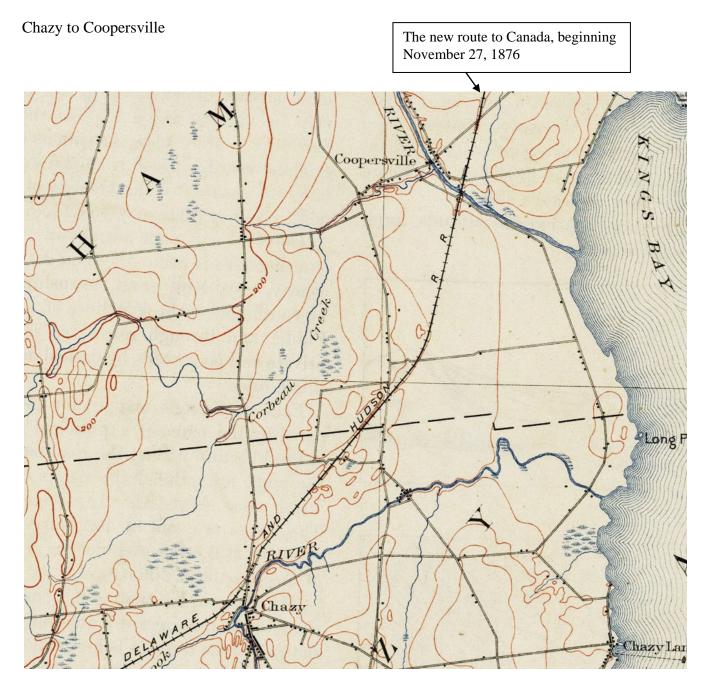
Continuation of above

route to Canada, beginning November 27,

1876

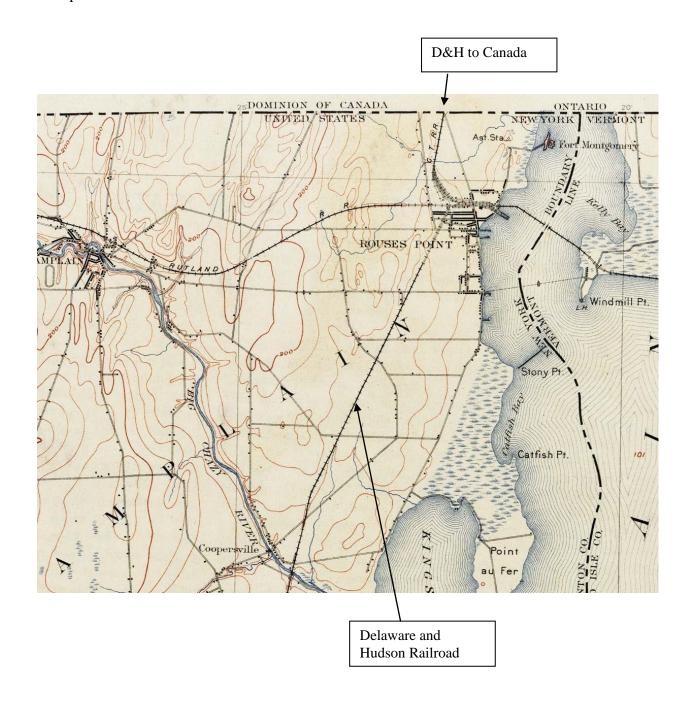


Detail from U. S. Geological Survey, *Rouses Point Quadrangle*, Surveyed in 1893 Henry Gannett, Chief Topographer; H. M. Wilson, Geographer in charge; Triangulation by U. S. Coast and Geodetic Survey; Topography by E. C. Barnard



Detail from U. S. Geological Survey, *Rouses Point Quadrangle*, Surveyed in 1893 Henry Gannett, Chief Topographer; H. M. Wilson, Geographer in charge; Triangulation by U. S. Coast and Geodetic Survey; Topography by E. C. Barnard

Cooopersville to Rouse's Point



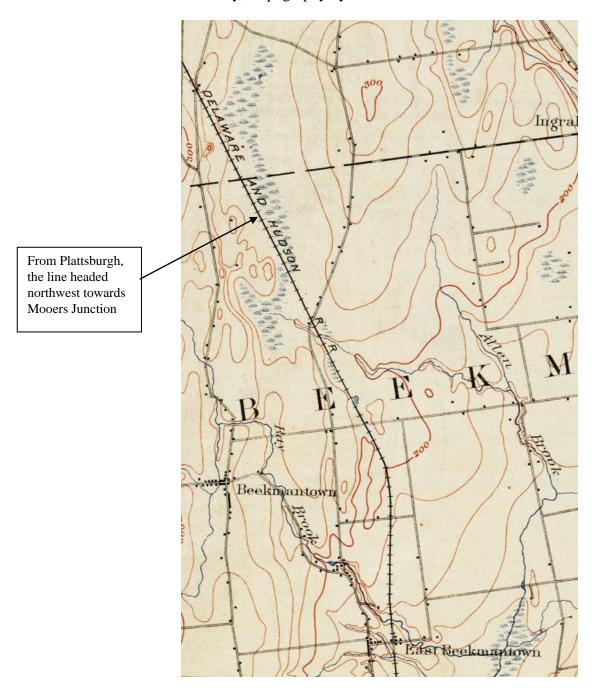
In the year following the opening of the line from Whitehall to Rouses Point, additional construction on the New York and Canada took place just north of Plattsburg (the Mooers Junction revision).

This additional construction on the New York and Canada in 1876 is described in *COP* (pp. 259, 261) as follows:

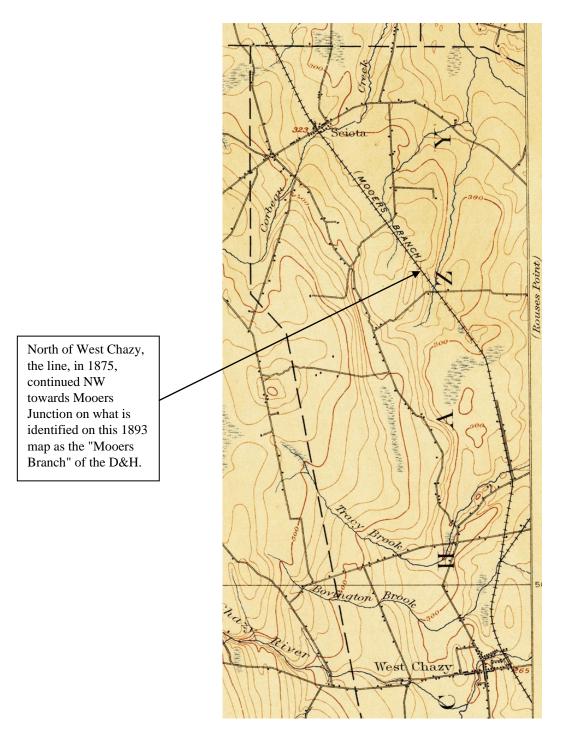
"During the year [1876] there was some additional construction by the New York and Canada. On reaching Mooers Junction, turning sharply to the east, trains had been forced to utilize for twelve miles, on American soil, the track of the Ogdensburg and Lake Champlain railroad in order to reach Rouses Point where connection was made with the Canadian railroad that led to Montreal. To do away with this necessity and to shorten the route, the construction of the present line to Rouses Point, leaving the earlier route at Canada Junction, one mile north of West Chazy, and running substantially northward to Rouses Point, was placed under contract on February 10 and it was brought into use on November 27, 1876."

As presented above, here is the route that the New York and Canada followed from Plattsburg to Rouses Point when the line opened in 1875:

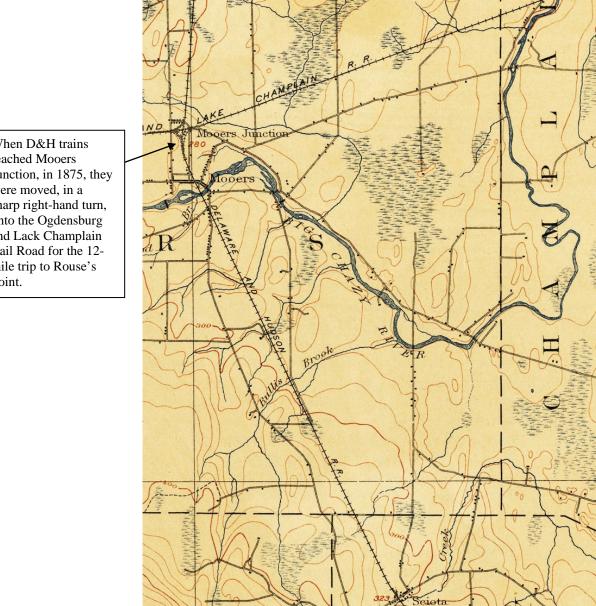
Detail from U. S. Geological Survey, *Rouses Point Quadrangle*, Surveyed in 1893 Henry Gannett, Chief Topographer; H. M. Wilson, Geographer in charge; Triangulation by U. S. Coast and Geodetic Survey; Topography by E. C. Barnard.



Detail from U. S. Geological Survey, *Mooers Quadrangle*, Surveyed 1893 Henry Gannett, Chief Topographer; H. M. Wilson, Geographer in charge; Triangulation by S. S. Gannett; Topography by E. C. Barnard and James McCormick.



Detail from U. S. Geological Survey, Mooers Quadrangle, Surveyed 1893 Henry Gannett, Chief Topographer; H. M. Wilson, Geographer in charge; Triangulation by S. S. Gannett; Topography by E. C. Barnard and James McCormick.

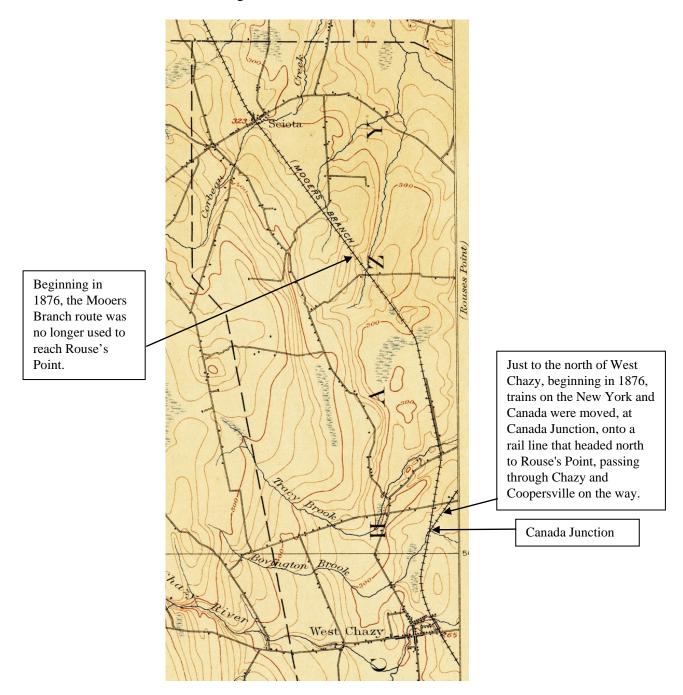


When D&H trains reached Mooers Junction, in 1875, they were moved, in a sharp right-hand turn, onto the Ogdensburg and Lack Champlain Rail Road for the 12mile trip to Rouse's Point.

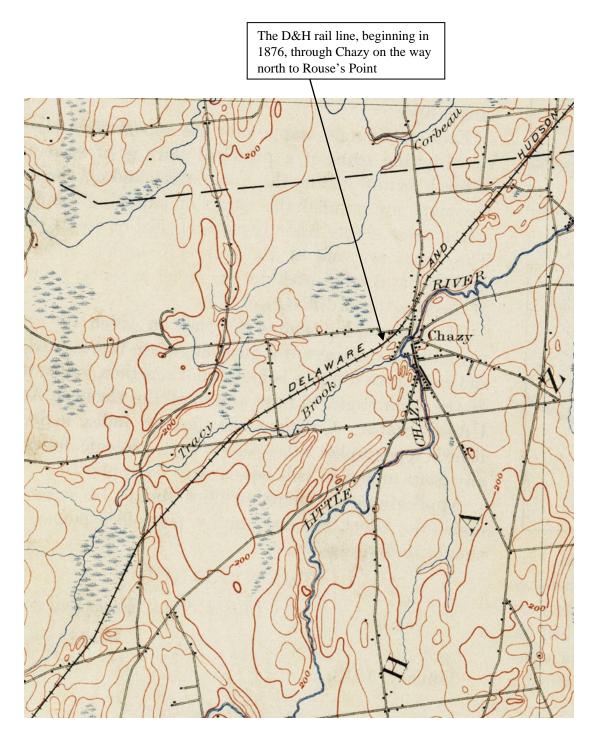
In the year 1876, the line from Plattsburgh to Rouse's Point was revised as follows:

Detail from U. S. Geological Survey, *Mooers Quadrangle*, Surveyed 1893 Henry Gannett, Chief Topographer; H. M. Wilson, Geographer in charge; Triangulation by S. S. Gannett; Topography by E. C. Barnard and James McCormick.

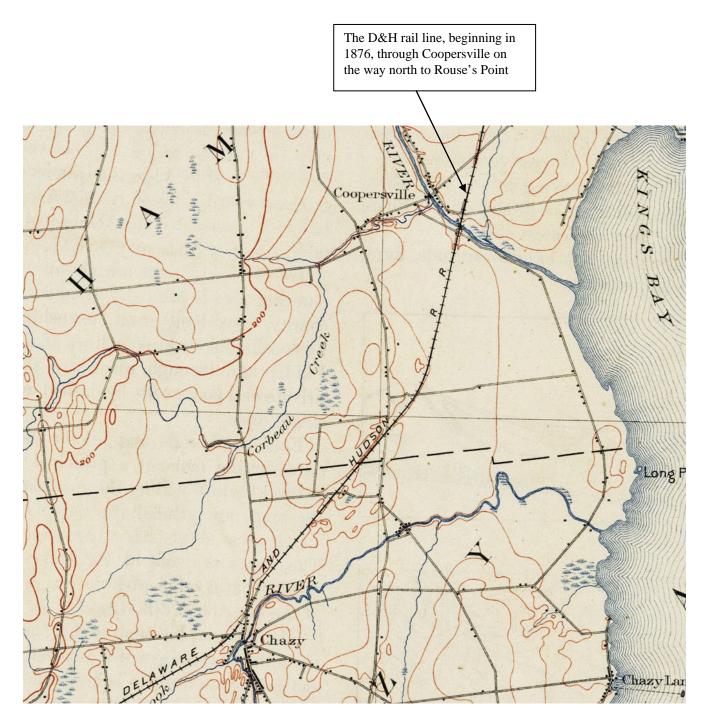
Here is the new route from Plattsburgh to Rouse's Point:



Detail from U. S. Geological Survey, *Rouses Point Quadrangle*, Surveyed in 1893 Henry Gannett, Chief Topographer; H. M. Wilson, Geographer in charge; Triangulation by U. S. Coast and Geodetic Survey; Topography by E. C. Barnard

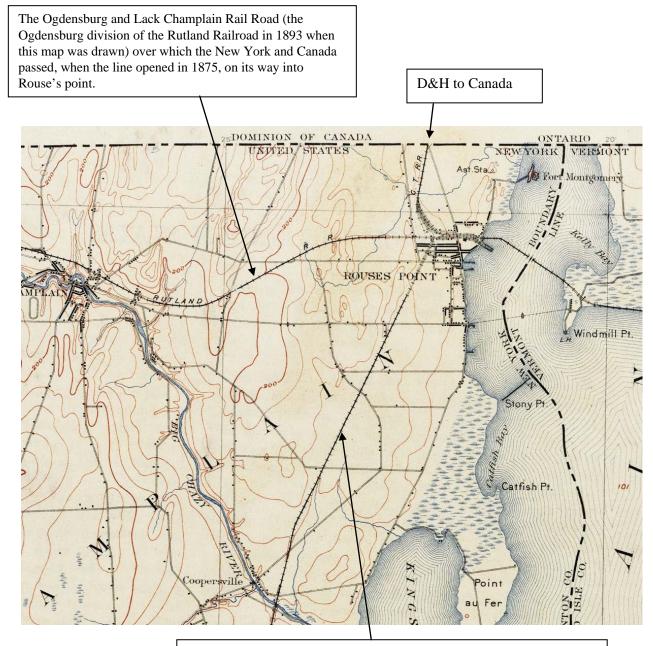


Detail from U. S. Geological Survey, *Rouses Point Quadrangle*, Surveyed in 1893 Henry Gannett, Chief Topographer; H. M. Wilson, Geographer in charge; Triangulation by U. S. Coast and Geodetic Survey; Topography by E. C. Barnard



Detail from U. S. Geological Survey, *Rouses Point Quadrangle*, Surveyed in 1893 Henry Gannett, Chief Topographer; H. M. Wilson, Geographer in charge; Triangulation by U. S. Coast and Geodetic Survey; Topography by E. C. Barnard

Coopersville to Rouse's Point:



Construction of the revised route from Plattsburg to Rouse's Point was begun on February 10, 1876, and brought into use on November 27, 1876.

And so, with these moves North on the part of the D&H, here is what the D&H owned, operated, or leased in 1877-78 (from *Manual of the Railroads of the United States for 1877-78* by Henry V. Poor, pp.263-266):

In 1877, the D&H owned, operated, or leased the following lines of railroad:

Delaware and Hudson R. R.	31.55 m.
Gravity and Coal R. Rs (owned)	56.00 m.
Lackawanna and Susquehanna R. R. (owned)	21.71 m.
Albany and Susq. R. R. (leased)	142.20 m.
Cherry Valley Branch	20.91 m.
Schenectady	13.84 m.
New York and Canada R. R.	112.76 m.
Ausable Branch	19.90 m.
Lake George Branch	4.38 m.
West Chazy	12.75 m.
Rens. and Saratoga R. R. (leased)	79.10 m.
Castleton Branch	13.14 m.
Glen's Falls Branch	6.05 m.
Green Island Branch	0.79 m.
Rutland Branch	61.98 m.
Schenectady Branch	14.97 m.
Waterford Branch	5.57 m.

Total of all lines.

611.60 m.

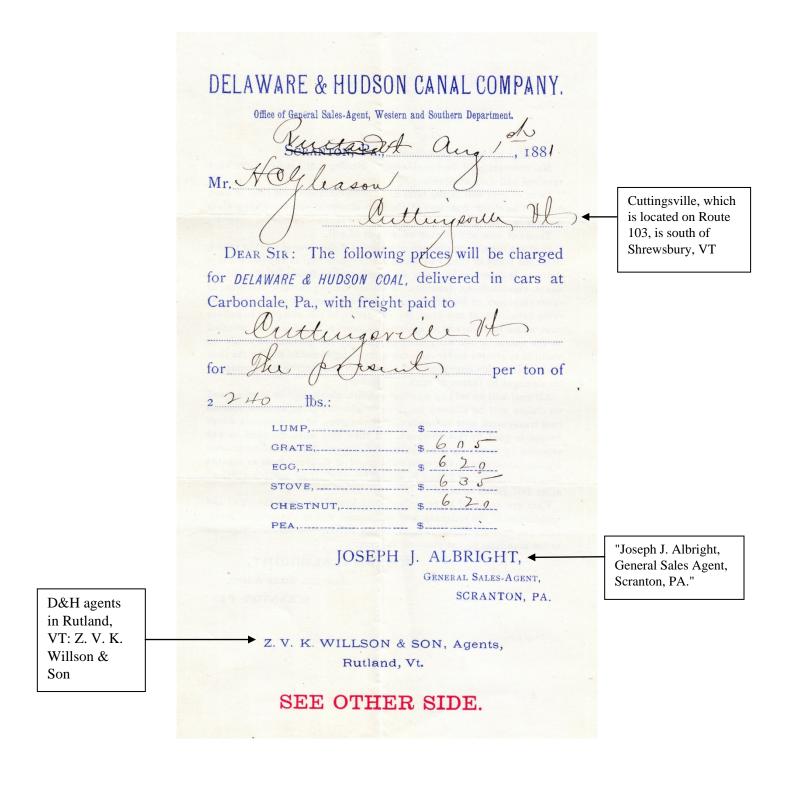
In addition to the length of line here given, the company have, under lease, the right to run freight trains over the Jefferson Branch of the Erie R. R. from Carbondale to Susquehanna, 36.5 miles; on the Lehigh and Susquehanna R. R., 7 miles; and on the Lackawanna and Bloomsburg R. R., 2 miles. Miles of railroad in Pennsylvania, 65.90.

For the year ending December 31, 1876:

Trains run, passenger, 77,688; freight, 102,939; coal, 530,100—total, 710,727 miles.

Passengers carried: 162,151; freight moved (including coal, 1,630,832) 1,760,272 tons"

With that network in place, it was possible for the D&H to sell "Delaware and Hudson coal delivered in cars at Carbondale, Pa., with freight paid to Cuttingsville, Vermont" in 1881, as we learn from the invoice shown on the following page. Our thanks to John V. Buberniak for finding this invoice for sale on E-Bay on February 8, 2016; invoice now in the collection of the Carbondale D&H Transportation Museum.



Here is the reverse of the invoice shown on the preceding page:

All sales subject to the following conditions:

No contract will be considered as binding until orders have been received and duly accepted.

Price of coal will be the rates as fixed by circulars from time to time, and will be charged at rates of circulars at time of shipment, regardless of date of orders.

Terms of sale will be cash on receipt of bill.

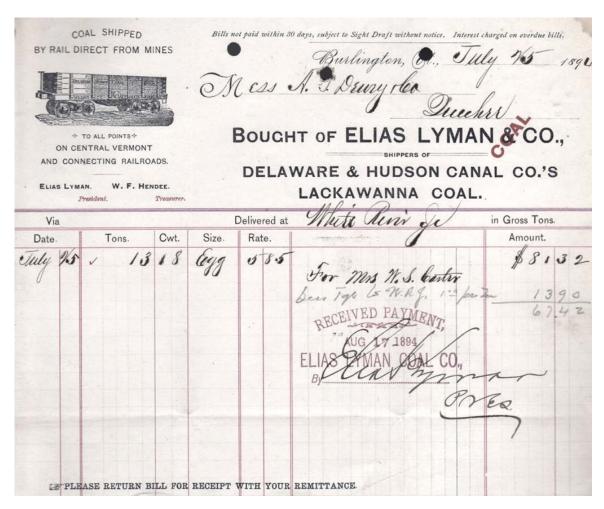
If, at any time, the operations or business of the company, at the mines, or on the roads or canals by which it transports coal to any place where delivery is agreed to be made, are interrupted by floods, breaks, or accidents, combinations, turnouts, or strikes among miners, boatmen, or other employes, or from their refusal to work from any cause whatever; or by casualties of any kind, the obligation of the company to deliver coal, under its contracts or agreements, may, at its option, be canceled to the extent of the daily average quantity the purchasers would be entitled to receive under their contracts or agreements, during the time such interruption continues, and no liability shall accrue to the company for damages by reason of such non-delivery.

All coal will be sold by manifest weights, which shall be binding, and no claims will be allowed for short weight, excepting unusual loss on coal transported over railroads of this company, or on railroads where freight is paid by this company, and then only when certified to and reported by station-agents at points of delivery. Bills of lading or other regular testimony of shipment to be proof of delivery, both as regards time and quantity, and shall be settled for, whether the same has arrived at its destination or not.

Cars are required to be unloaded in accordance with the rules and regulations of the railroads, and demurrage will be charged for detention, and cars are not allowed to be sent beyond the station designated in the manifests as the place of delivery.

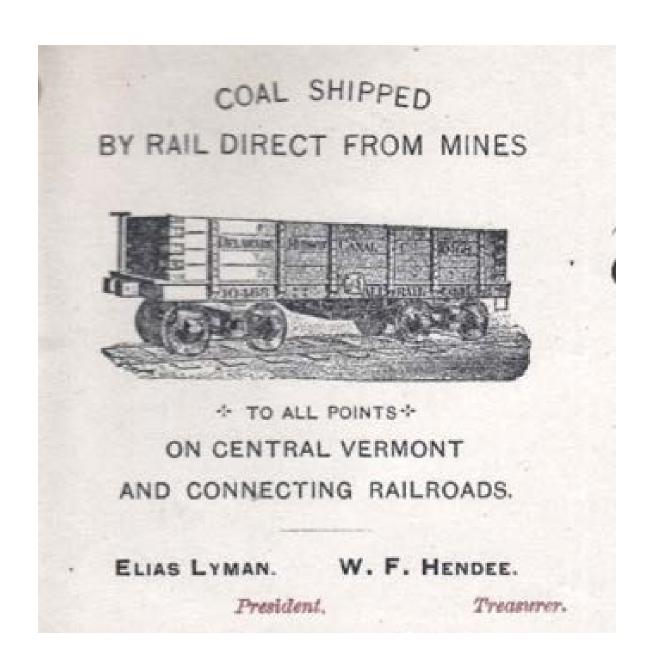
JOSEPH J. ALBRIGHT,

GENERAL SALES AGENT, SCRANTON, PA. Similarly, it was possible for Elias Lyman & Co. to sell, in the 1890s in Burlington, Vermont, Lackawanna "coal shipped by rail direct from mines" in northeastern Pennsylvania:

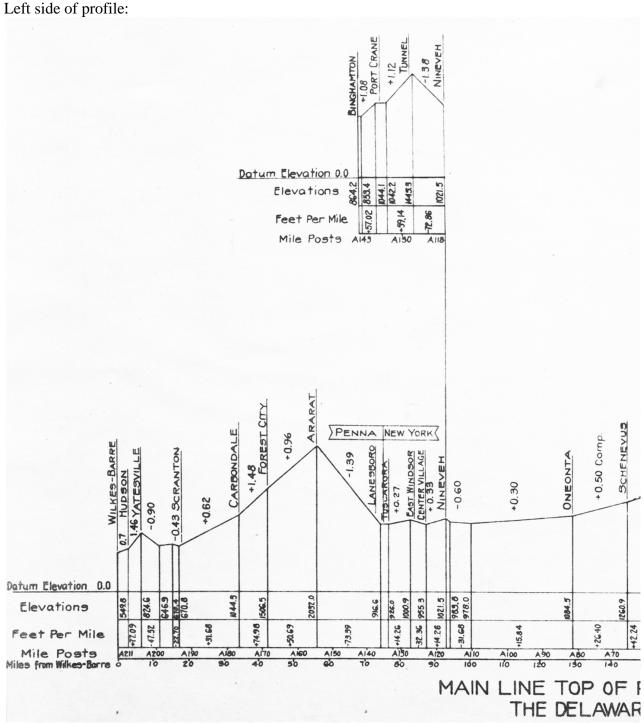


Invoice sold on E-Bay on January 7, 2016; thanks to John V. Buberniak for bringing this item to our attention

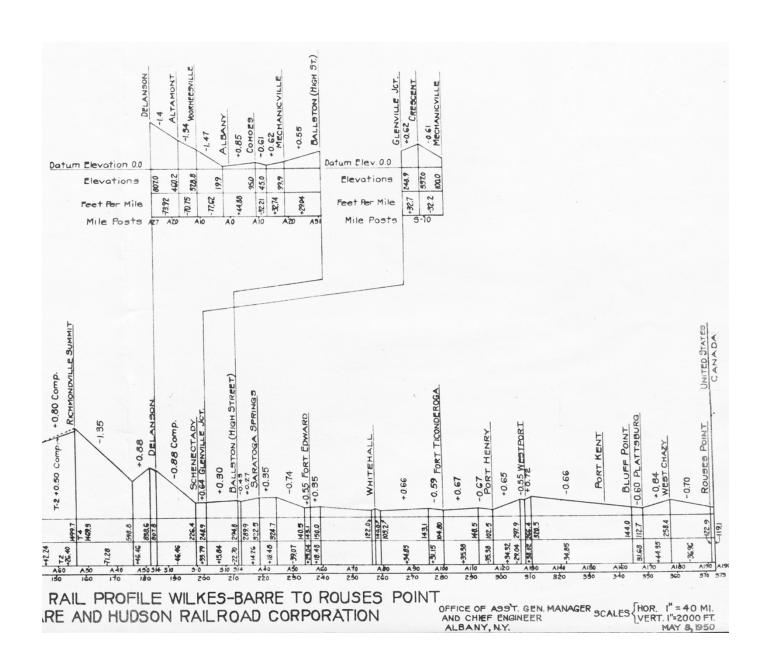
Shown on the following page is an enlarged detail of the upper-left corner of this invoice.



Main Line Top of Rail Profile Wilkes-Barre to Rouse's Point. This profile, which is shown here in two parts, is in the A. G. Dustin Collection in the holdings of the Carbondale D&H Transportation Museum. This profile was produced by the "Office of Ass't Gen. Manager and Chief Engineer Albany, NY" and is dated May 8, 1950.



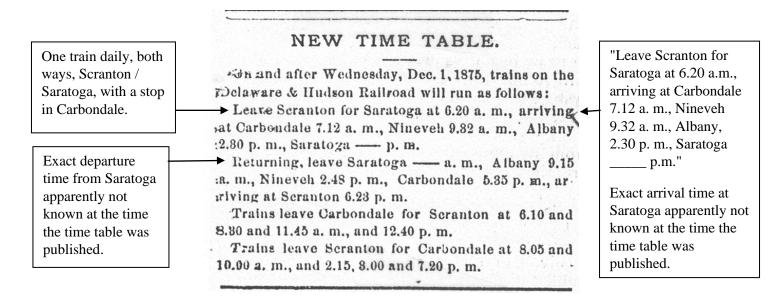
Right side of profile:



Timetables: November 29, 1875 to 1967 for Whitehall to Rouse's Point

D&H timetables that went into effect following the opening of the D&H line to Canada generally contain passenger schedules for D&H trains north of Carbondale.

Here is the "New [D&H] Time Table" that went into effect on and after Wednesday, December 1, 1875, two days after the opening of the D&H to Canada:



(Published in the *Carbondale Advance*, April 1, 1876, p. 3)

A year later, in the D&H time table that went into effect on December 4, 1876, there were announced only "some slight changes in the running of trains on the D. & H. C. Co.'s road," and none of those changes involved trains running north of Carbondale. Here is the notice about those changes that was published in the December 9, 1876 issue of the *Carbondale Leader*, p. 3"

-- Some slight changes were made in the time of running trains on the D. & H. C. Co.'s road, on Monday. Trains now leave Carbondale for Scranton at 6:20 and 8:30 A. M., and at 12:40, 4:00, and 5:10 P. M. Trains reach this city from Scranton at 6:58 and 10:58 A. M., and at 2:55 and 8:00 P. M. The regular morning and noon passenger trains leave Carbondale as formerly, and the last train in the afternoon leaves twenty five minutes earlier. Trains arrive here from the south a little earlier than formerly, the last train at night arriving at 8:00 instead of 8:18.

Time Table, took effect Wednesday, November 14, 1877. Published in *Carbondale Advance* of December 1, 1877, p. 3:

Saratoga Express:

Scranton – Saratoga, one train, both ways, daily.

Saratoga Express stops at stations on the Jefferson Branch

NEW TIME TABLE.

On and after Wednesday, Nov. 14, 1877, trains on the Delaware & Hudson Railroad will run as follows:

SARATOGA EXPRESS.

Leave Scranton for Saratoga at 6.20 a. m., arriving at Carbondale 7.00 a. m., Nineveh 9.30 a. m., Onconta 11.00 a. m., Albany 2.20 p. m., Troy 3.25 p. m., Schenectady 2.25 p. m.; Saratoga 6.25.

Returning—Leave Schencetady 7.50, Troy 7.00, Albany 8.00, Oneonta 11.50 a.m.; Nineveh 1.30 p.m., Carbondale 4.12 p.m., arriving at Scranton 5.04 p.m.

The above train now stops at stations on the Jefferson Branch.

CARBONDALE & SCRANTON PASSENGER TRAINS.

Trains leave Carbondale for Scranton at 6.20 and 8.30 a. m., and 12.30, 4.12 and 5.10 p. m.

Trains leave Scranton for Carbondale at 9.30 and 10.00 a. m., and 2.15 and 7.05 p. m.

9.30 a.m. train leaves Scranton from the old depot.

GRAVITY RR. TO HONESDALE.

Leave Carbondale for Honesdale at 8:10 A. M. and 8:30 P. M.

Leave Honesdale for Carbondale at S:00 A. M. and 2:35 P. M.

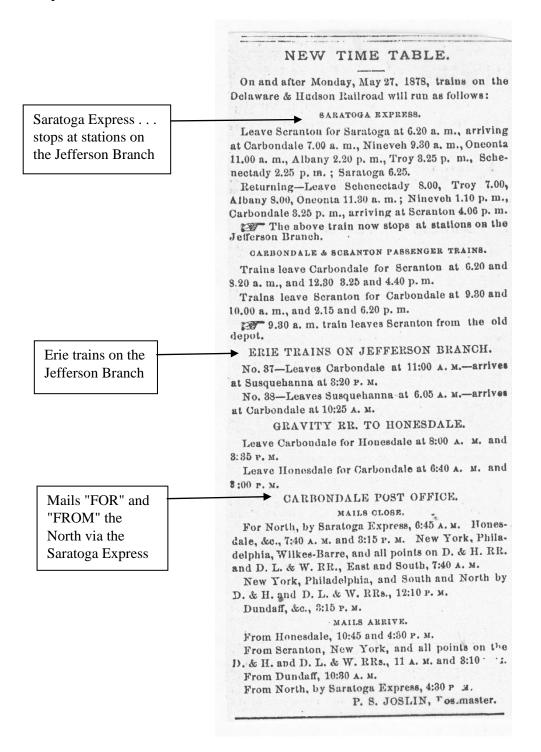
"The Saratoga going south leaves here [Carbondale] as before at 3:24 P.M., but reaches the D., L. & W. Depot, at Scranton, at 4:06, six minutes earlier."

New Time-Table.

A new time-table went into effect on the Del. & Hud. RR. here on Wednesday of this week. Sixteen new coal trains are added between Green Ridge and the foot of G plane, at Olyphant, where they are transferred to the Gravity track. The changes in the time of the passenger trains here are very slight. The Saratoga going south leaves here as before at 3:24 p. m, but reaches the D., L. & W. Depot, at Scranton, at 4:06, six minutes earlier.

The last train coming north from Scranton now leaves the D., L. & W. Depot there at 7:05 P. M., and reaches here at 8:05.

D&H time table, effective May 27, 1878. Published in *Carbondale Advance*, October 11, 1879, p. 3



D&H time table, effective April 1, 1880. Published in the *Carbondale Advance*, May 15, 1880, p. 3

Train *for* Saratoga leaves Scranton at 6:20 A.M. Train *from* Saratoga arrives at Scranton at 4 P.M.

The Mails *for* and *from* the North were carried on the Saratoga Express.

NEW TIME TABLE.

On and after Thursday, April 1st, 1880, trains on the Delaware & Hudson Railroad will run as follows.

SARATOGA EXPRESS.

Leave Scranton for Saratoga at 6.20 a. m., arriving at Carbondale 7.00 a. m., Nineveh 9.25 a. m., Oneonta 10.55 a. m., Albany 2.00 p. m., Troy 2.25 p. m., Schenectady 1.50 p. m.; Saratoga 2.40.

Returning—Saratoga Express, leaves Troy 7.00 Albany 8.00, Oneonta 11.35 a.m.; Nineveh 1.15 p.m., Carbondale 3.25 p.m., arriving at Scranton 4.00 p.m.

The above train now stops at stations on the Jefferson Branch.

CARBONDALE & SCRANTON PASSENGER TRAINS.

Trains leave Carbondale for Scranton at 6.20 and 8.20 a, m., and 12.30 3.25 and 4.50 p.m.

Trains leave Scranton for Carbondale at 9.80 and 10.00 a. m., and 2.8 and 6.20 p. m.

9.30 a. m. train leaves Scranton from the old depot.

ERIE TRAINS ON JEFFERSON BRANCH.

No. 87—Leaves Carbondale at 11:00 A. M.—arrives at Susquehanna at 3:20 P. M.

No. 89—Leaves Susquehanna at 6.05 A. M.—arrives at Carbondale at 10:25 A. M.

GRAVITY RR. TO HONESDALE.

Leave Carbondale for Honesdale at 8:00 A. M. and 8:45 P. M.

Leave Honesdale for Carbondale at 6:40 A. M. and 3:00 P. M.

CARBONDALE POST OFFICE.

MAILS CLOSE.

For North, by Saratoga Express, 6:45 A.M. Honesdale, &c., 7:40 A.M. and 3:15 P.M. New York, Philadelphia, Wilkes-Barre, and all points on D. & H. RR. and D. L. & W. RR., East and South, 7:40 A.M.

New York, Philadelphia, and South and North by D. & H. and D. L. & W. RRs., 12.10 p. M.

Dundaff, &c., 3:15 P. M.

MAILS ARRIVE.

From Honesdale, 10:45 and 4:30 P. M.

From Scranton, New York, and all points on the D. & H. and D. L. & W. RRs., 11 A. M. and 8:10

1. From Dundaff, 10:30 A. M.

From North, by Saratoga Express, 4:80 P 4.

P. S. JOSLIN, Pos.master.

Philander S. Joslin, Postmaster D&H time table, effective July 4, 1881. Published in the *Carbondale Leader* of September 9, 1881, p. 4.

Saratoga Express leaves Carbondale daily at 7:00 A.M.; arrives at Saratoga at 2:25 P. M.

RAILROAD TIME-TABLES.

On and after Monday, July 4, 1881, trains on the Delaware & Hudson Railroad will run as follows:

SARATOGA EXPRESS.

Leave Scranton for Saratoga at 6:20 A. M., arriving at Carbondale at 7:00 A. M., Nineveh, 9:25 A. M., Oneonta, 10:48 A. M., Albany, 2:10 P. M., Troy, 3:24 P. M., Schenectady, 1:40 P. M., and Saratoga, 2.25.

Returning, leave Schenectady at 7:35 A. m., Troy, 8:18 A. M., Albany, 8:30 A. M., Oneonta, 12:00 A. M., Nineveh, 1:27 P. M., Carbondale, 3:45 P. M., arriving at Scranton at 4:28 P. M.

The above train now stops at stations on the Jefferson Branch.

CARBONDALE AND SCRANTON PASSENGER TRAINS.
Trains leave Carbondale for Scranton at 6:20 and
8:20 A. M., and 12:30, 3:45, and 4:55 P. M.

Trains leave Scranton for Carbondale at 6:20 and 10:00 A. M., and 2:00 and 7:05 P. M.

GRAVITY RAILROAD TO HONESDALE.

Leave Carbondale for Honesdale at 8:00 A. M., and 3:45 P. M.

Leave Honesdale for Carbondale at 6:40 A. M. and 3:00 P. M.

D. L. & W. Main Line — On and after Monday, November 17, 1879, passenger trains will leave as follows:

Leave Scranton for New York and Philadelphia at 1:15 and 9:50 A. M., and 3:55 P. M. Arrive at New York at 7:10 A. M., 3:30, and 9:25 P. M.; at Philadelphia 9:40 A. M., 3:45, and 9:55 P. M. Leave Scranton for Binghamton at 7:10 A. M., 2:05, 6:30 and 12:45 A. M. Arrive from Binghamton at 1:10 and 9:40 A. M., 3:50 and 8:25 P. M. An accommodation train leaves Scranton for Gouldsboro at 5:30 P. M., arriving at Gouldsboro at 7:20 P. M. Leave Gouldsboro for Scranton at 7:20 A. M.; arriving at Scranton at 8:45 A. M.

Saratoga Express

"JEFFERSON BRANCH

accommodation trains leave CARBONDALE going North 11 a.m. and 3 p.m. Returning, leave Susquehanna 6.05 a.m., arriving at Carbondale 10.25 a. m. and leave Nineveh at 9.35 a.m., arriving at Carbondale 2.40 p. m."

RAILROAD DIRECTORY.

DELAWARE & HUDSON CANAL CO. R. R.

Trains leave CARBONDALE for SCRANTON Trains leave CARBONDALL for SCHANTON at 6.20 a. m., 8.20 a. m., 12.30 p. m., 3.45 p. m. and 5 p. m., arriving at Scranton 7.45 a. m., 9.10 a. m., 1.24 p. m., 4.28 p. m. and 5.55 p. m. Trains leave SCRANTON for CARBONDALE at 6.20 a. m., 10 a. m., 2 p. m. and 7.05 p. m., arriving at Carbondale 7 a. m., 10.58 a. m., 2.25 p. m. and 8.05 p. m.

SARATOGA EXPRESS leaves SCRANTON 6.20 a. m., Carbondale 7 a. m., arriving at Albany 2.10 p. m., and Saratoga 2.25 p. m. Returning, leaves Saratoga 7 a. m., Albany 8.30 a. m., arriving at Carbondale 3.45 p. m. and Scranton 4.28 p. m.

and Scranton 4.28 p. m.

Trains *via GRAVITY ROAD leave CAR-BONDALE 8 a. m. and 3.50 p. m. arriving at Honesdale 9.30 a. m. and 5.05 p. m. Returning, leave Honesdale 6.40 a. m. and 3 p. m., arriving at Carbondale 8.20 a. m. and 5 p. m. JEFFERSON BRANCH accommodation trains leave CARBONDALE going North 11 a. m. and 3 p. m. Returning, leave Susquehanna 6.05 a. m., arriving at Carbondale 10.25 a. m. and leave Nineveh 9.35 a. m., arriving at Carbondale 2.40 p. m.

DEL. LACK & WESTERN R. R.

MAIN LINE.

MAIN LINE.

Leave SCRANTON for NEW YORK and PHILADELPHIA at 1.15 a. m., 9.50 a. m. and 4 p. m., arriving at New York 7.10 a. m., 3.30 p. m. and 0 p. m., and at Philadelphia 850. p. m., 3.40 p. m. and 9.45 p. m.

For WASHINGTON, N. J. (accommodation) 1.30 p. m., stopping at all stations. Returning, arrive 11.10 p. m.

For BINGHAMTON leave 8 a. m 2.09 p. m., 6.30 p. m., and 12 45 a. m. Returning, arrive from Binghamton 1.10 a. m., 9.23 a. m., 3.55 p. m., and 7 p. m.

For GOULDSBORO, (accommodation) leave. Srcanton 5 p. m., arriving at Gouldsboro 7.20,

Broanton 5 p. m., arriving at Gouldsboro 7.20, p. m. Returning, leave Gold sboro 7.20 a. m., arriving in Scranton 8.45 a. m. Arrive in Scranton from New York and Philadelphia at 12.40 a. m., 1.49 p. m., and 6 p. m.

BLOOMSBURG DIVISION.

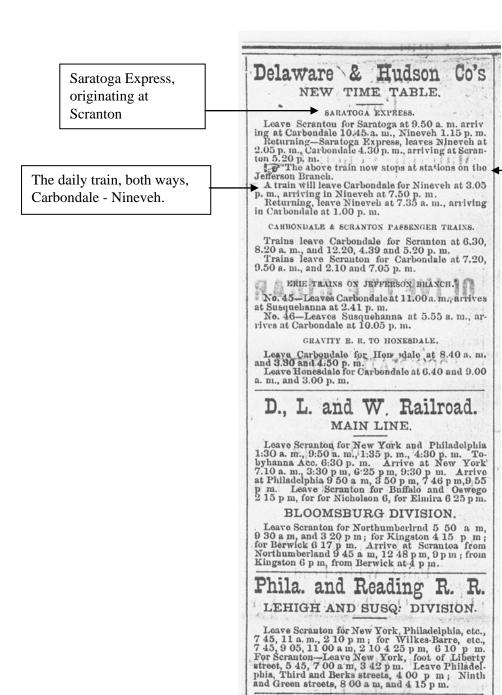
Leave SCRANTON for Northumberland 9.30 a. m., and 2.15 p. m. arriving at Northumberland 12.45 p. m., and 5.45 p. m. Returning, arrive in Scranton 9.45 a. m., 3.42 p. m., and 9.50 p. m.

5.00 p. m., arriving 4.58 p. m. Returning, leave Kingston 5.10 p. m., arriving in Scranton 5.48 p. m. For NANTICOKE, leave Scranton 7.40 a. m., arriving 8.55 a. m. Returning, arrive at Scranton 1.40 p. m.

for BERWICK, leave Scranton et 6.13 p. m.

arriving 9:55 p. m.

Saratoga Express: NEW TIME TABLE. now leaves BARATOGA EXPRESS. Scranton at 9.50 Leave Scranton for Saratoga at 9.50 a. m., arriv-A. M. and not 6.20 ing at Carbondale 10.45 a. m., Nineveh 1.15 p. m. Returning-Saratoga Express, leaves Nineveh at A.M. 2.05 p.m., Carbondale 4.80 p. m., arriving at Scranton 5.20 p. m. The above train now stops at stations on the "A train will leave Jefferson Branch. Carbondale for Nineveh A train will leave Carbondale for Nineveh at 8.05 p.m., arriving in Nineveh at 7.50 p.m. at 6.05 p.m., arriving in Returning, leave Nineveh at 7 85 a.m., arriving Nineveh at 7.50 p. m. / in Carbondale at 1.00 p. m. Returning, leave Nineveh CARBONDALE & SCRANTON PASSENGER TRAINS. at 7.35 a.m., arriving in Trains leave Carbondale for Seranton at 6.80, 8:20 a. m., and 12 20, 4.80 and 5.20 p. m. Carbondale at 1.00 p. m." Trains leave Scranton for Carbondale at 7.20, 9.50, a. m., and 2.10 and 7.05 p. m. ERIE TRAINS ERIE TRAINS ON JEFFERSON BRANCH. ON THE No 45-Leaves Carbondale at 11.00 a. m. -arrives **JEFFERSON** at Susquehanna at 2.41 p. m. No 46-Leaves Susquehanna at 5.55 a. m .- ar **BRANCH** rives at Carbondale at 10.05 p. m. GRAVITY RR. TO HONESDALE. Leave Carbondale for Honescale at 8.4) a. m. and 3.30 and 4.50 p. m. Leave Honesdale for Carbondale at 6.40 and 9.00 a.m., 8.00 p. m. CARBONDALE POST OFFICE. Mails to and from MAILS CLOSE. the North via the For North, by Saratoga Express, 10.80 a. m. Honesdale, &c., 8.20 a. m. and 8.15 p.m. New York, Saratoga Express Philadelphia, Walkes-Barre and all points on D. & H. RR. and D. L. & W. RR., East and South, 8.00 4. M. Scranton, New York, Philadelphia and South, and North by D. & H. and D. L. & W. RR., (and all soints on D. & H. RR.) 12.05 and 5.05 p. m. Dundaff and Clifford, 8:25 P. M. From Scranton and New York (via closed pouch) 8:15 A. M. and 8:15 P. M. From Scranton, New York, Philadelphia, and all points on the D. & H. and D. L. & W. RR., 10.50 A. M. and 8:10 P. 1. (Western Mail at 10,50 A. M.) From Honesdale, 8:10 A. M. and 4.20 P. M. From Dundaff and Clifford, 10.80 A. M. From North, by Saratoga Express, 4.80 P.M. Post Office Hours, from 7 A. M. to 7.80 P. M. Also open upon arrival of 8.15 P. M mail. E. Y. DAVIES. E. Y. DAVIES. l'ostmaster. Postmaster.



The Saratoga
Express stopped at
the stations on the
Jefferson Branch of
the Erie from
Carbondale to
Jefferson Junction.

The Saratoga Express now leaves Scranton at 6 A.M., earlier than ever. Point of origin of the train: Scranton.

The evening train from Carbondale to Nineveh, leaving Carbondale at 6:08 P.M., and arriving at Nineveh at 7:50 P. M.

One train, daily, both ways, Carbondale / Susquehanna, on the Erie Railroad on the Jefferson Branch.

NEW TIME TABLE.

SARATOGA EXPRESS.

→ Leave Scranton for Saratoga at 6,00 a. m., arrivng at Carbondale 6.48 a. m., Nineveh 9.12 a. m. Returning—Saratoga Express, leaves Nineveh at 1 30 p. m., Carbondale 3.55 p. m., arriving at Scranton 4.40 p. m.

The above train now stops at stations on the Jefferson Brauch.

A train will leave Carbondale for Nineveh at :: 08 p.m., arriving in Nineveh at 7.50 p.m.

Returning, leave Nineveh at 7.85 a.m., arriving in Carbondale at 1.00 p. m.

CARBONDALE & SCRANTON PASSENGER TRAINS.

Trains leave Carbondale for Seranton at 6.10, 8:20 a. m., and 12 20, 4.80 and 5.20 p. m.

Trains leave Scranton for Carbondale at 7.20, 9.55, a. m., and 2.10 and 7.10 p. m.

→ ERIE TRAINS ON JEFFERSON BRANCH.

No 45—Leaves Carbondale at 11.00 a. m —arrives at Susquehanna at 2.41 p. m

No 46—Leaves Susquehanna at 5.55 a. in.—arrives at Carbondale at 10.05 a. m.

GRAVITY RR. TO HONESDALE.

Leave Carbondale for Honesdale at 8.40 a. m. and 3.80 p. m.

Leave Honesdale for Carbondale at 6.40 a. m., and 8.00 p. m.

CARBONDALE POST OFFICE. MAILS CLOSE.

For North, by Saratoga Express, 6.30 a.m. Honesdaie, &c., 6.20 a.m. and 8.15 p.m. New York. Philadelphia, Wsikes-Barre and all points on D. & H. RR. and D. L. & W. RR., East and South, 8.00

Scranton, New York, Philadelphia and South, and North by D. & H. and D. L. & W. RR., (and all points on D. & H. RR.) 12.05 and 5.05 p. m.

Dundaff and Clifford, 8:25 P. M.

MAILS ABRIVE.

From Scranton and New York (via closed pouch) 8:15 a. m. and 8:15 p. m.

From Scranton, New York, Philadelphia, and all points on the D. & H. and D. L. & W. RR., 10.50 A. M. and 8:10 P. M. (Western Mail at 10.50 A. M.)

From Honesdale, 8:10 A. M. and 4.20 P. M.

From Dundaff and Clifford, 10.80 A. M.

From North, by Saratega Express, 4.80 P.M.

Post Office Hours, from 7 A. M. to 7.80 P. M. Also open upon arrival of 8.15 P. M. mail.

E. Y. DAVIES. Postmaster.

"Saratoga express leaves Carbondale for Saratoga, Albany and the north at 11 a.m.; arrive at Carbondale from Albany and Saratoga at 3:46 p. m., stopping at all stations on the Jefferson branch." Point of origin of the train: Carbondale.

"ERIE—Trains on the Carbondale Branch leave Carbondale for Susquehanna at 6.45 a. m., 1.15 p. m. (connecting with fast trains east and west) and 5.55 p. m. Leave Susquehanna at 7.45 a. m. (express), 8 a. m. and 3.40 p. m., arriving at Carbondale at 9.35 a.m., 12.05 and 7.25 p.m."

Railroad Time Table.

D. & H. C. Co.-Trains leave Carbondale for Scranton at 7.00, 8.20, 9.50, 11.20 a. m., and 1.00, 3.46, 5.20 and 8.15 p. m.

Trains leave Scranton for Carbondale at 7.00, 8.40, 10.00, 11.45 a. m., and 2.10, 5.00, 7.00 and 11.00 p. m.

 Saratoga express leaves Carbondale for Saratoga, Albany and the north at 11.00 a. m.; arrive at Carbondale from Albany and Saratoga at 3:46p. m., stopping at all stations on the Jefferson branch.

A train will leave Carbondale for Nineveh at 3.05

p. m., arriving in Ninevel at 7.50 p. m.
Returning, leave Ninevel at 7.35 a. m., arriving in Carbondale at 1.00 p. m.

Trains on the Gravity road leave Carbondale for Honesdale at 7.55, 9.40 and 11.10 a. m., and 3.10 and 4.00 p. m.

Leave Honesdale for Carbondale at 6.40, 9.30 and 11.20 a. m., and 3.30 p. m.

ERIE .- Trains on the Carbondale Branch leave Carbondale for Susquehanna at 6.45 a. m., 1.15 p.m. (connecting with fast trains east and west) and 5.55 p.m. Leave Susquehanna at 7.45 a. m. (express), 8.00 a. m. and 3.40 p. m., arrivng at Carbondale at 9.35 a.m., 12,05 and 7.25 p. m.

For New York via. Honesdale, leave Carbondale on Gravity trains at 9.40 a. m. and 3.10 p. m.

arriving in New York at 5 and 8.55 p. m.

D L. & W.—Trains leave Scranton for New York and Philadelphia at 1.45, 2.05, 5.00 and 8.30 a.m., 12.40 and 4.28 p.m.; arrive at New York 7.10 7.25, 11.20 a. m., and 1.40, 5.30 and 9.30 p. m.; arrive at Philadelphia at 9.55 a. m., and 2.25, 6.05 and 10.30 p. m. Leave Scranton for Buffalo at 2.15 a. m., 1.52 11.45 p. m.; for Elmira at 6.15 a. m.; for Binghamton at 1 a. m., and 1 p. m.

The daily Nineveh train, to and from Carbondale

THE JOURNAL

CARBONDALE, PA, DECEMBER 22, 1887

D. & H. C. Co.

Trains leave Carbondale for Scranton at 7,03, 8,20, 9,55 and 11.20 a.m., 1.00, 8.46, 5.20 and 8,15 p.m. arriving at Scranton at 7,43 9.10 and 10,40 a m., 12,07, 1,49, 4.30, 6.10 and 9,00 p.m.

Trains leave Scranton for Carbordale at 7,00, 8,40, 0,00 and 11,47 a. m., 2.10, 5,00, 7.00 and 11,05 p. m., arriving in this city at 7,50, 9,30 and 10.50 a. m., 12,37, 3.03, 5.50, 7,50 and 11,50 p. m.

D. AND H. GRAVITY RAILROAD.

Trains leave this city for Honesdale at 7.55, 9 40 and 11,10 a. m., 3.10 and 4.00 p. m., arriving in Honesdale at 9,10 and 10.55 a. m., 12.25, 4.25 and 5.15 p. m.

Trains leave Honesdale at 6.40, 9.30 and 11.20 a. m., and 3.30 p. m., arriving in this city at 8.05, and 10.55 a. m., 12.45, and 4.55 p. m.

→ Erie R. R.—Jefferson Branch.

Trains leave Carbondale on the Jefferson Branch for Susquehanna at 6.45 10.30 a. m., and 5.55 p. m., for Jefferson Junction at 11.00 a. m. and 3 00 p. m. Trains arrive in Carbondale from Susquehanna at 9.35 a m. 12.15 and 7.25 p. m., and from Jefferson Junction at 1.00 and 8.46 p. m.

By the Erie's short route from Carbondale to New York via Honesdale, passengers leaving here on the Gravity R. R. at 9.40 a. m. will reach New York at 4.55 p. m. and leaving here at 3.10 p. m. will reach New York at 9.40 p. m.

"Erie R. R. – Jefferson Branch. / Trains leave Carbondale on the Jefferson Branch for Susquehanna at 6.45 10.30 a.m., and 5.55 p.m., for Jefferson Junction at 11.00 a. m. and 3.00 p. m. Trains arrive in Carbondale from Susquehanna at 9.35 a. m. 12.15 and 7.25 p. m., and from Jefferson Junction at 1.00 and 3.46 p. m."

Railroad Time Tables.

Saratoga Express leaves Carbondale at 9.19 a. m.

Nineveh train

"A Sunday special train has been put on the [Jefferson] Branch, leaving Carbondale 8.30 a.m.; returning, leave Susquehanna at 4.15 p.m., arriving at Carbondale at 5.45."

D. & H. C, Co.—Trains leave Carbondale for Scranton at 7.00, 8.20, 9.50, 11.10 a.m. and 1.00 3.30, 5.20, 6.20 and 8.30 p. m.

Trains leave Scranton for Carbondale at 7.00,

3.15, 10.00, 12.00 a. m., and 2.10, 3.30, 5.10, 7.25 and 11.13 p. m.

▶ Saratoga express leaves Carbondale for Saratoga, Albany and the north at 9.19 a. m.; arrive at Carbondale from Albany and Saratoga at 3.25 p. m., stopping at all stations on the Jefferson branch.

A train will leave Carbondale for Nineveh at 1.00 p. m., arriving in Nineveh at 7.32 p. m., Acturning, leave Nineveh at 7.35 a. m., arriving

in Carbondale at 12.20 p. m

frains on the Gravity road leave Carbondale for fonesdale at 7.55, 9.40 and 11.10 a. m., 1.25, 3.10 and 6.00 p. m. Leave Honesdale for Carbondale at 6.50 9.30 and

11.20 a. m., and 1.20, 3.30 and 5.30 p. m. ERIE. —Trains on the Carbondale Branch leave Carbondale for Susquehanna at 6.00 a. m., 11.13 a. m. and 6.25 p. m (connecting with fast trains cast, and west). Leave Susquehanna at 6.30 a. m. (express) 8.05 a. m. and 4.15 p. m., arriving at Carbondale at 9.35 a. m., 12.05 and 5.45 p. m. Trains on main line leave Susquehanna Eastward 1:00, 2:33, 8:09 and 12:01 a. m., 12.30 and 4.90 p. m. arriving at New York 7:07, 7.52, 10.38 a. 4:30 p. m. arriving at New York 7:07, 7.52, 10.38 a. m., 12:57, 8:37, 6:00 and 10:45 p. m. Westward *3:05, *6:05 a. m., 1:30, 3:31, 6:05, 9:20 and 11:55 p. m. for Hornellsville, Buffalo, Salamanca and the West

→ A Sunday special train has been put on the Branch, leaving Carbondale 8.30 a.m.; returning, leave Susquehanna at 4.15 p. m., arriving at Car-

bendale at 5.45.

For New York via Henesdale, leave Carbondale on Gravity trains at 9.40 a. m. and 3.10 p. m. arriving in New York at 5.07 and 11.15 p. m.

D. L. & W. trains leave Scranton for New York and Philadelphia at 1.50, 2.30, 4.55, 8.00 and 9.50 and Philadelphia at 1.50, 2.50, 4.55, 8.00 and 9.50 a. m., 1.25 and 4.31 p. m.; arrive at New York 7.10, 7.30, 9.40 a. m. and 12.30, 3.00, 6.00 and 9.00 p. m. arrive at Philadelphia at 9.55 a. m., 1.20, 8.50, 7.30 and 9.30 p. m. Leave Scranton for Buffaio at 2.15 a. m., 1.52, 11.50 p. m., for Elmira at 6.15 p. m.; for Elmira at mira at 6.15 p. m.; for Binghamton at 1 p.m.

"Railroad Time Tables," published in the Carbondale Leader, August 21, 1890, p. 3.

"A train known as the Boston Express will leave Scranton at 2.17 p. m., arriving at Carbondale at 3:05; leave at 3:13 arriving at Nineveh 5:18, stopping at all stations on the Jefferson branch.

Railroad Time Tables.

Delaware & Hudson C. Co.

Delaware & Hudson C. Co.
Trains leave Carbondale for Scranton at 6.55,
8.20, 9.50, 11.10 a. m. and 1.00, 3.30, 5.20, 7.00 8.30
and 10.50 p. m. Leave JERMYN 10 minutes later.
Trains leave Scranton for Carbondale at 7.00,
8.30, 10.10, 12.00 a. m., and 2.17, 3.30, 5.10, 7.25 and
11.18 p. m. Arrive at JERMYN 25 minutes later.
Saratoga expressiences Carbondale for Saratoga,
Albany and the north at 11.00 a. m.; arrive at
Carbondale from Albany and Saratoga at 3.25 p.
m., stopping at all stations on the Jefferson
branch.

m., stopping at an stations on the occasion branch.

train known as the Boston Express will leave Scranton at 2:17 p. m., arriving at Carbondale at 2:05; leave at 3:13 arriving at Nineveh 5:18, stopping at all stations on the Jefferson branch.

D. & H. Gravity R. R.

Trains on the Gravity road leave Carbondale for Henesdale at 7.55, 9.40 and 11.10 a.m., 1.25, 3.10

and 6.00 p. m. Leave Honesdale for Carbondale at 6.50 9.30 and 11.20 a. m., and 1.20, 3.30 and 5.25 p. m.

N. Y., Ontario & Western. Ontario & Western trains leave Carbondale for Scranton at 6.55, 8.35, 10.20 a. m., and 2.00, 3.30 5.05 and 9.30 p. m.
Trains arrive in Carbondale from Scranton at 7.30, 9.10, 11.57 a. m., and 1.40, 4.30, 7.00 and 11.30

p. m. For New York via Hancock—leave Carbondale at 11.57 a. m., arrive in New York at 7.15 p. m. From New York—train leaves New York at 7.50 a. m., arriving in Carbondale at 3.30 p. m. Trains between Carbondale and Hancock leave Carbondale at 7.30 and 11.57 a. m. (New York ex-

Trains for Forest City leave Carbondale at 7.30 and 11.57 a. m.

Trains leave Forest City for Carbondale at 8.18 a. m., and 3.15 p. m.

Ontario & Western trains leave Jermyn for Carbondale 7;18, 8;59, 11.46 a.m. and 1;28, 4;19, 6.42 and 11.12 p. m.

Leave JERMYN for Scranton at 7;06, 8;46, 10;31 a m. and 2;11, 3;40, 5;15, 9,40 p. m.

N. Y., L. E. & W. R. R.

N. Y., L. E. & W. R. R.

Trains on the Carbondale Branch leave
Carbondale for Susquehanna at 8.05 and
10.00 a. m. and 7.15 p. m. (connecting with fast
trains east, and west). Leave Susquehanna at 6.30
a. m. (express) 8.05 a. m. and 5.25 p. m., arriving
at Carbondale at 9.35 a. m., 12.05 and 6.50
p. m. Trains on main line leave Susquehanna
Eastward *1.00, 5:20, 8:00, 11:20 a. m., 5.05 p. m.
arriving at New York 7:30, 11.07 a. m. 3 37, 5:22,
10:37 p. m. Westward *12.01, 3:05, 5:20 a. m. 3:29,
9:00 p. m. for Hornelisville, Buffalo, Salamanca
and the West Daily.

A Sunday special train has been put on the
Branch, leaving Carbondale 9.35 a. m.; returning,
leave Susquehanna at 5.25 p. m., arriving at Carbendale at 6.45.

For New York via Hønesdale, leave Carbondale
on Gravity trains at 7.55 a. m. and 3.10 p. m.
arriving in New York at 5.07 and 11.15 p. m.

Del. Lack. & Western.

Del. Lack. & Western.

D. L. & W. trains leave Scranton for New York and Philadelphia at 1.50, 2.55, 5.19, 8.00 and 9.50 a. m., 1 03 and 4.31 p. m.; arrive at New York 7.10, 7.30, 10.04 a. m., and 12.30, 8.00, 5.30 and 9.00 p. m. arrive at Philadelphia at 9.55 a. m., 1.20, 5.30, 7.30 and 9.30 p. m. Leave Scranton for Buffalo at 2.15 a. m., 1.42, 11.50 p. m., for Elmira at 6.15 p. m.; for Binghamton at 8:10 a. m. and 12.44 p. m.

Saratoga Express leaves Carbondale at 11 a. m.

N. Y., Ontario & Western

Saratoga Express leaves Carbondale

for Saratoga, Albany and he north at 6.25 a.m.

..." Point of

Carbondale.

origin of the train:

Railroad Time Tables.

NEW YORK ONTARIO & WESTERN.

Ontario & Western trains leave Carbondale for Scranton at 7.10, 8.25, 9.45 a. m., and 2.35, 3.55, 5.10, 7.10 p. m. Trains arrive in Carbondale from Scranton at 7.45, 9.15, 11.50 a. m., and 2.15, 5.45, 6.50 and 11.45 p. m.

P. m. Trains between Carbondale and Hancock leave Carbondale at 11.50 a. m. (New York express), and

Trains between Carbondale and Hancock leave Carbondale at 11.50 a. m. (New York express), and 5.45 p. m.

I Trains for Forest City leave Carbondale at 11.50 a. m. and 5.45 p. m.

Trains leave Forest City for Carbondale at 8.10 a. m. and 3.38 p. m.

Ontario & Western trains leave Jermyn for Carbondale 7.38, 9.03, and 11.40 a. m. and 2.03, 5.33, 6.40 and 11.33 p. m.

Leave JERMYN for Scranton at 7.22, 8.37, 7.59 a. m. and 2.47, 4.04, 5.22, and 7.19 p. m.

For New York.

BEAUTIFUL SCENIC ROUTE.

BEAUTIFUL SCENIC ROUTE.

Only 7 Hours from Carbondale at 11.50 a. m., daily arriving in New York at 7.20 p. m. Leave New York at 8.00 a. m. arriving in Carbondale at 3.55 p. m.

Fare to New York,

84.35.

Fare to New York,
New York and Return
Good for 30 days. Baggage checked through to
destination. Try the new route.
J. C. Anderson,
G. P. A.
Gen. M'g'r.

Delaware & Hudson Railroad.

Trains leave Carbondale for Scranton at 7.00, 8.00, 9.00,19.00, 11.10, a. m. and 12.46, 2.00, 3.00, 4.13, 5.00, 7.00, 8.30 and 10.50 p. m. Leave JERMYN 10 minutes later.

Trains leave Scranton for Carbondale at 5.40, 7.00, 8.30, 10.10 a. m. 12.00 and 2.17, 3.25, 5.10, 6.20, and 9.35 p. m. From Bridge street depot 2.08 a.m. 7.16 and 11.13 p. m. Arrive at JERMYN 35 minutes later.

7.16 and 11.13 p. m. Afrive at Jermin of Minutes later.

Saratoga express leaves Carbondale for Saratoga, Albany and the north at 6.25 a. m.; arrive at Carbondale from Albany and Saratoga at 1.0 p. m. and 10.48 p. m., stopping at all stations on the Jefferson branch.

A train known as the Boston Express will leave Scranton at 2.17 p. m., arriving at Carbondale at 3.00; leave at 3.05 arriving at Nineveh 5.00, stopping at all stations on the Jefferson branch.

D. & H. Gravity R. R.

D. & H. Gravity R. R.

Trains on the Gravity road leave Carbondale for Honesdale at 7.55, 9.30 and 11.05 a. m., 1.25, 3.09, and 6.00 p. m.
Leave Honesdale for Carbondale at 7.25 9.25 and 11.15 a. m., and 1.25. 3.30 and 5.30 p. m.

ERIE RAILWAY.

Trains on the Carbondale Branch leave Carbondale for Susquehanna at 9.20 a, m., and 6.35 p. m. (connecting with fast trains east and west). Leave Susquehanna at 7.28 a, m. and 4.22 p. m., arriving at Carbondale at 8.59 a. m., and 4.22 p. m., arriving at Carbondale at 8.59 a. m., and 4.22 p. m., arriving at Carbondale at 9.20 a, m. Binghamton 11.42 a, m. Elmira 1.45 p. m. arriving at Kochester local leave Carbondale at 9.20 a, m. Binghamton 11.42 a, m. Elmira 1.45 p. m. arriving at Kochester 5.55 p. m. Trains for Forest City and all points on the Branch leave Carbondale at 6.25 a, m. and 3.05 and 6.35 p. m.

A Sunday special train has been put on the Branch, leaving Carbondale 9.10 a, m., returning, leave Susduehanna at 4.10 p. m., arriving at Carbondale on Gravity trains at 7.55 a, m. and 1.25 p. m arriving in New York at 5.07 and 8.37 p. m.

For full information see Pocket Time Table.

H. G. Baker, Agent, Carbondale, Pa.
D. I. Roberts,
S. T. Seely,
Gen'l Pass. Agent
Division Pass. Agent,
Rimira, N. Y.

Del. Lack. & Western.

D. L. & W. trains leave Scranton as follows:
Express for Syracuse, Oswego, Utica, Buffalo and
the West, 12.15 and 2.15 a. m., 1.32 p. m., making
close connections at Buffalo to all points in the
West, Northwest and Southwest. Express for
New York, 150, 2.50, 5.05, 8.00, 9.50 a. m., 12.55 and
3.50 p. m. Express for Philadelphia, Easton,
Trenton, 5.05, 8.00 and 9.50 a. m., 12.55 and 3.50
p. m. For Binghamton at 6.05 p. m. Pullman
parlor and sleeping coaches are attached to all
through trains.

Central R. R. of New Jersey

Lehigh & Susq. Division.

Time Table in Effect Oct. 11th, 1892.

Shortest Line! Quickest Time!

Wilkes-Barre, Mauch Chunk, Tainaqua, Harrisburg, Reading, Allentown, Bethlehem, Philadelphia, Ocean Grove, Long Branch, &c.

Trains leave Carbondale
For Mauch Ohunk, Allentown and Bethlehem
from N. Y. O. & W. R.R. depot at 2.35 p.m.
From D. & H. R. R. depot 3.00 p. m.
For Tamaqua from N. Y. O. & W. R. R. depot
2.35 p. m. from D. & H. R. R. depot 3.00 p. m.
For Reading & Harrisburg via Allentown from
N. Y. O. & W. R. R. depot at 2.35 p. m. from
D. & H. R. R. depot at 2.35 p. m. from
D. & H. R. R. depot 3.00 p. m.
For Philadelphia from N. Y. O. & W. R. R. depot
2.35 p. m. from D. & H. R. R. depot 3.00 p. m.
J. H. Olhausen.
H. P. Baldwin H. P. Baldwin, J. H. Olhausen, Gen'l Supt. Genl. Pass. Agt.

"Rochester local [on the Erie Railway] leaves Carbondale at 9.20 a. m., Binghamton 11.42 a.m. Elmira 1.45 p.m. arriving at Rochester 5.55 p.m..."

"New D. & H. Time Table," effective January 16, 1898, published in the January 11, 1898 issue of the Carbondale Leader, p. 8.

NEW D. & H. TIME TABLE.

Will Go Into Effect Next Monday With Im. portant Changes.

Sunday. After that trains will leave

Carbondale at 6:05, 7:05, 8:00, 9:00, 10:

00, 11:20 a. m. and 12:40 1:45, 2:50, 4:03,

A new timetable will go into effect on the Delaware and Hudson railroad next

Fourteen trains, daily, both ways, Carbondale / Scranton.

5:05, 7:05, 9:45 and 10:50 p. m. Trains will arrive at Carbondale at 6.57, 8.35, 9.35, 10 55 a. m. and 12.41. 2.00, 3.00, 4.30, 6.05, 7.05, 8.35 9.52, 11.38 p. m., and 1.54 a. m.

Trains to and from Honesdale will run same as heretofore except the train now arriving at 12:25 noon which is changed to 12:35.

Trains to and from Albany, Boston. Saratoga and the north will leave at at 7:00 a.m. and 3:05 p.m. and arrive at 4:00 and 9:45 p. m.

The principal changes are in the Boston express which will reach Nineveh at 7:55 p. m. instead of 8:25 as at present and will arrive in this city at 9:45. It will leave this city at 12:40 a. m., will arrive at 1:54 and leave Wilkes-Barre at 12:30 and Scranton at 1:16. Train twenty which leaves this city at 1:50 will leave at 1:45 or five minutes earlier than at present.

Some time changes on the Boston Express from Wilkes-Barre to the north.

Two trains, daily, from Carbondale to and from Albany, Boston, Saratoga, and the north.

D&H Time Table, effective July 3, 1898, published in the *Carbondale Leader*, March 2, 1899, p. 7

Trains from Carbondale to just about any place you might want to go to.

DELAWARE AND HUDSON RAILROAD.

July 3rd, 1898.

Trains will leave Carbondale as follows.

For Scranton and Wilkes-Barre: 6.05, 7.05, 8.8, 9.00, 10.00, 11.20 a. m., 12.45, 1.42, 2.50, 3.49, 5.03 7.05, 10.00, 10.50 p m.

Sunday trains leave 8.55, 11.00 a. m, 1.15, 2.43

5.00, 7.05 p. m.

For Albany, Saratoga, Montreal, Boston, New England points, &c., 7.00 a. m., 3.05 p. m.

For Waymart and Honesdale: 7.13, 11.01 a.m.

3.09, 6.08 p. m.

For New York, Philadelphia, &c., via Lehigi Valley R. R., 6.05, 11.20 a. m., 12.45, 1.42, 3.49 'with Black Diamond Express) 10.50 p. m.

For Western points via Lehigh Valley R. R. 7.05, 11.20 a. m., 2.50, (with Black Diamond Mx press) 10 00, 10.50 p. m.

For Pennsylvania Railroad points: 6.05, 9.8.

a. m., 1.42, 3.49 p. m.

For New York, Philadelphia, &c., via Dels ware, Lackawanna & Western: 7.05, 9.00, 10.05 11.20 a.m., 10.50 p.m.

For Elmira, Buffalo and Western points, via Delaware, Lackawanna & Western: 8.00 a. m.

12.45, 5.05, 10.50 p. m.

Trains will arrive at Carbondale from Wilkes Barre and Scranton as follows: 6.57, 8.31, 9.31 10.51 a. m., 12.38, 2.00, 3.00, 4.30, 6.05, 7.05, 8.35, 9.52 11.38 p. m., 1.54 a. m.

Sunday trains arrive 9.43 a. m, 12.11, 2.31, 4.30

6.25, 10.31 p. m.

J. W. BURDICK, G. P. A., Albany, N. Y. H. W. Cross, D. P. A., Scranton, Pa.

For complete information relative to ticker rates and routes to all points in the United States and Canada, address

H. W CROSS, D. P. A., Scranton, Pa

J. W. Burdick was the D&H General Passenger Agent in Albany; H. W. Cross was the D&H Division Passenger Agent in Scranton. D&H Timetable, effective May 14, 1899, published in the July 12, 1899 issue (p. 8) of the *Carbondale Leader:*

DELAWARE AND HUDSON RAILROAD.

May 14, 1899.

Trains will leave Carbondale as follows; For Scranton and Wilkes-Barre: 6:05, 7:05. 8.00, 9,00, 10.00, 11.20 a. m., 12.45, 1.42, 2.50, 3 49, 5.05, 7.05, 10 00 10.50 p. m.

Sunday trains leave 8.56, 11.21 a. m., 1.16, 2.46

5.01, 7.06 p. m.

For Albany, Saratoga, Montreal, Boston New England points, &c., 7-00 a m., 3.50 p: m. (daily) For Waymart and Honesdale: 7-13, 11 of a. m., 3 09, 6,08 p. m.

For New York, Philadelphia, &c, via Lehigh Valley R. R., 6.05, 11.20, a. m., 12.45, 1.42, 8.45, (with Black Diamond Express) 10.50 p. m.

For Western points via Lehigh Valley R. R. 7.05, 11.20 a. m., 2 50, (with Black Diamond Express) 10.00 10.50 p. m.

For Pennsylvania Railroad points: 6.05, 9.00

a. m, 1.45, 3.49 p. m.

For New York, Philadelphia, &c., via Delaware, Lackawanna & Western: 7:05, 9:00, 10:00 11:20 a. m , 10.50 p. m.

For Elmira, Buffalo and Western points, via Delaware Lackawanna & Western: 8:00 a. m,

12:45, 5:05, 10:50 p, m.

Nrains will arrive at Carbondale from Wilkes-Barre and Scranton as follows: 6:57, 8:3t, 9:31, 10:51 a. m., 12:38, 2:00, 3:00, 4:30, 6:05, 7:05, 8:35, 9:55, 11:38 p. m., 1:54 a. m.

Sunday trains arrive 9:42 a. m, 12:10, 2:59, 4:59,

6:24, 10:30 p. m.

J. W. BURDICK, G. P. A., Albany, N. Y. H. W. Cross, D. P. A., Scranton, Pa.

For complete information relative to ticket rates and routes to all points in the United States and Canada, address,

H. W. CROSS, D. P. A., Scranton, Pa.

Timetable, Pennsylvania Division, took effect on October 1, 1911:

Here are the (1) Wilkesbarre and Scranton to Boston and Montreal, and (2) Boston and Montreal to Scranton and Wilkesbarre passenger schedules from the D&H Pennsylvania Division timetable that went into effect on October 1, 1911:

(1)

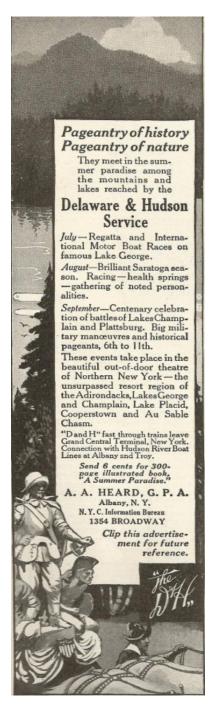
Boston and Mor		n to
Wilkesbarre . Lv. Pittston Scranton Olyphant Carbondale Nineveh Albany Due Troy Saratoga Springs' Whitehall Rutland Plattsburg Rouses Point Montreal Boston, B. & M Boston, B. & A	A. M. †7 10 7 27 7 55 8 10 8 40 10 40 2 00 2 26 5 55 7 15 8 10 10 20 8 40 10 20 P. M.	P. M *3 15 3 34 4 05 4 20 4 50 7 03 10 50 11 15 12 45 1 5 4 4 8 15 8 7 00 5 15 A. M

Boston and Montreal to Scranton and Wilkesbarre

Boston, B.&M. Lv		P. M.	
Boston, B.& A. " *1	0 00	*11.15	11.15
Montreal "	8 45	* 7 00	7:00
Rouses Point " 1	0 25	* 8 35	8 35
Plattsburg "	1.15	* 9 25	9.25
Rutland "	1415	t 6400	77.
Whitehall	100	1.000	
Saratoga Springs "	3.15	1 8 00	1.35
Troy *	4.00	* 9.30	6.55
Albany		†10.00	TO SERVICE STATE OF THE SERVICE OF
Nineveh	No bear	+ 2005	15 PROPERTY N. P. (1985). 1
Carbondale Due +1	0.25	+ 4.00	2.30
Olyphant "	0.53	† 4.25	3.01
Scranton "	1.07	+ 4.37	3.15
Pittston "			
Wilkesbarre "	5 T C 10 - 1	Complete Com	P. G. 2008; S. 70-85; Vol. 11. Co. 1965 (gr.)
P.	M.	P. M.	P. M.

Connects with D. & H. at Albany daily except Monday.

Upstate New York was not only a market for D&H coal, it was also a destination for recreational and historic tourism, and the D&H promoted/marketed the region as such to encourage passenger travel there. Given below is an ad that the D&H placed in *Literary Digest* magazine in 1914 in which the summertime activities in upstate New York are promoted. This ad was posted on *Facebook* on June 1, 2015 by Greg Flynn of the Delaware and Hudson Railroad group.



In January 2015, we borrowed from G. E. Ford (Post Office Box 1482, Westfield, MA 01086) a copy of *The Summer Paradise in History A Compilation of Fact and Tradition Covering Lake George, Lake Champlain, the Adirondack Mountains, and Other Sections Reached by the Rail and Steamer Lines of the Delaware and Hudson Company by Warwick Stevens Carpenter (Copyright 1914 by A. A. Heard. Published by the General Passenger Department of The Delaware and Hudson Company, Albany).*

The frontispiece of the book is a likeness of Samuel de Champlain"



The book is dedicated to:

To the Members of

THE NEW YORK STATE HISTORICAL ASSOCIATION and

THE LAKE CHAMPLAIN ASSOCIATION

To whose efforts are so largely due the cherishing of old landmarks and the recording of history and tradition in the territory here covered

THIS BOOK IS DEDICATED

FOREWORD

THIS volume is the direct outgrowth of the "Literary and Historic Note Book," covering the same territory, written for the Passenger Department of the Delaware and Hudson Company by Mr. Henry P. Phelps, and published in 1907. It appeared as a booklet of eighty pages, and at once met with an appreciation which has in no degree abated during the seven years of its circulation. Since that date the interest which had already developed in the historic country reached by the Delaware and Hudson lines has been tremendously augmented, a fact well evidenced by the attention that historical societies and other organizations are giving to the subject. Among the latter may be mentioned the Glens Falls Insurance Company, which for many years has commissioned some of the leading American artists to make paintings of the more striking events. These pictures have been reproduced in original colors and widely distributed. The Champlain Tercentenary Celebration brought popular attention to a clearer focus, and it has been further sustained by the subsequent completion and dedication of the memorials at Crown Point and at Plattsburg, and by the restoration and preservation of the two old forts at Ticonderoga and Crown Point. This interest resulted in continued requests for the "Literary and Historic Note Book," which was out of print, and indicated the need for this larger and more complete and permanent volume.

Even briefly to describe every event of romantic or historic moment in this territory would have required many times the space here available. Much has been necessarily eliminated, but an attempt has been made to include every really important incident and those minor ones which are of particular interest to that large class of visitors who wish to know the history of their Summer Paradise. The style of the text, that of separate paragraphs for each event, after the manner of Lossing's well-known "Cyclopædia of United States History," was determined by the need of such vaca-

tionists for much data that could not well be put into connected form in a small volume. A synoptical narrative introduction, covering the great campaigns for the control of the Champlain Valley, together with the Chronology which follows, are designed as a background against which each paragraph may be thrown into relief to show its proper relation to the times and to other events.

In the preparation of the "Summer Paradise in History" part of the text in the "Literary and Historic Note Book" has been used, though largely revised to meet the requirements of the present volume. While no original research has been made, many authorities have been consulted, and material drawn from them. A list of these appears in the bibliography, and acknowledgment of indebtedness to them is hereby made.

W. S. C.

January 1, 1914

ILLUSTRATIONS Cover Design from Detail of Champlain Memorial at Plattsburg, by Carl Augustus Heber Restoration of Fort Ticonderoga..... 12 Ruins of Fort Ticonderoga......Facing 16 Embarkation of Abercrombie's Expedition..... 17 Plan of Abercrombie's Attack on Carillon..... 18 32 Battle of Lake Champlain......Facing Grave of Captain Downie..... 33 Monument on Crab Island..... 33 Bloody Morning Scout..... 37 Statue on Site of Cooper's Residence......Facing 48 Tablet to Commemorate Dam at Otsego Lake...... 48 49 Scene of the Battle of Lake Champlain..... The Stourbridge Lion..... 50 58 Fort St. Frederic..... Lake George Battle Monument......Facing 64 Ruins at Crown Point..... 65 Plan of Investment of Fort William Henry..... 66 Jogues's Island......Facing 80 81 High Rock Spring in 1845..... 81 Saratoga as It Is Today..... 90 Two Early Steamers on Lake Champlain..... 96 Champlain Memorial at Crown Point......Facing The Black Watch at Storming of Carillon..... 97 112 Champlain Memorial at Plattsburg..... 113 The Deep Cleft of Split Rock.....

The primary content of the book is an alphabetical list of the attractions, historical personages, and points of interest in 'The Summer Paradise." In the pages that follow, we present some of those primary features of "The Summer Paradise". At the head of the list is Benedict Arnold, "whose name is written in the history of America with letters of infamy. . ."

THE SUMMER PARADISE IN HISTORY

ARNOLD, BENEDICT, whose name is written in the history of America with letters of infamy, might well have been remembered as one of the nation's greatest patriots and benefactors. It seems evident, however, that the honorable part of his career in the Revolution is traceable more to personal bravery, to ambition, and to spontaneous reaction to the conditions in which he found himself, than to deep-rooted attachment to the cause of independence. His services to the country, nevertheless, were no less valuable on this account. He claimed to have conceived the idea of capturing Ticonderoga, and was commissioned a colonel by the Massachusetts Committee of Safety for the accomplishment of this object. Finding Ethan Allen and others already embarked upon a similar mission, he deferred to Allen and joined the expedition as a volunteer. Later he commanded an expedition against Quebec, which marched northward through the entire extent of the Maine wilderness, after which he went up Lake Champlain to Ticonderoga, where he was placed in command of a fleet on the lake. His engagement with the British under Carleton (see Battle of Valcour) was the first naval conflict with the mother country. He was largely responsible for the defeat of Burgoyne in the battle of Saratoga (q. v.), where he was severely wounded. After a reprimand by Washington, ordered by the Continental Congress because of fraudulent transactions while he was military governor of Philadelphia, he plotted to betray the country, his plans being all but consummated at West Point in September, 1780.

AUSABLE RIVER rises in Indian Pass but a short distance from the source of the Hudson, and takes its tumultuous course northward and eastward, passing near its mouth through a tremendous rocky chasm which has become world-famous as one of the natural wonders of this continent. It takes its name from its sandy bed near its mouth the French word for sand being sable.

DAKER, CAPT. REMEMBER, one of the most prominent and daring leaders of the Green Mountain Boys, was killed by Indians near the mouth of the Lacolle River, a tributary of the Richelieu, while on scout service in connection with Montgomery's Expedition (q. v.) to Canada in August, 1775. He is said to have been the first American killed on Canadian soil during the Revolutionary War. A tablet to his memory has been erected on Isle La Motte. (See Commemorative Boulder on Isle La Motte.)

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BAKER'S FALLS is the local name for the falls on the Hudson River at Hudson Falls, so named from Albert Baker, the first settler, who built a sawmill there in 1768. The fall from the crest of Richard's Dam to the foot of Baker's Falls is about eighty-five feet, making the water-power at Hudson Falls second only to Niagara Falls in the State of New York.

Ballston Spa was an important destination for many travelers on the D&H.

→ BALLSTON SPA, although less widely known than Saratoga Springs, is really, of the two, the original resort. Hither (although some authorities claim it was the High Rock spring) in 1767 was brought by his Indian friends Sir William Johnson, when very ill, and here he quickly recovered his health, and returned to his home and Indian mistress in Johnstown. Ballston suffered a "northern invasion" in the fall of 1780, when Captain Munroe, formerly a trader in Schenectady, headed a detachment of Major Carleton's band of Tories and Mohawk Indians, devastating, plundering, and taking prisoners. (See Carleton's Raid.)

BATTLE OF DIAMOND ISLAND. Following the capture of Ticonderoga by Burgoyne, in 1777 (see Burgoyne's Campaign), large quantities of supplies were placed on Diamond Island in Lake George, under guard of two companies of the British. Here they were attacked by Colonels Brown and Warner of the American army on July 24th, but without success. Brown and Warner thereupon retired to the east side of the lake, burned their boats and retreated through the woods to Paulet, Vermont.

BATTLE OF HUBBARDTON. Upon the evacuation of Fort Ticonderoga by the American troops in July, 1777 (see Burgoyne's Campaign), a portion of the American army, acting as a rear guard to St. Clair's retreating orces, took up a position at Hubbardton, where they were attacked by the British on the morning of the 8th. The British were held in check for some time, but receiving a reinforcement of Hessian troops under Baron Riedesel, the Americans were obliged to give way. It is here that tradition credits Col. Seth Warner, who was in command, with shouting to his men, "Take to the woods, boys, and meet me at Manchester." They vanished from the sight of the astonished British and Hessians like mist before the morning sun.

BATTLE OF LAKE CHAMPLAIN. One of the most important naval engagements of the War of 1812 was fought off the town of Plattsburg, September 11, 1814, between a British fleet under

COOPERSTOWN, which stands at the foot of Otsego Lake on the site of an old Indian village, was founded by William Cooper, father of James Fenimore Cooper, the novelist, who was brought here in 1790 when an infant. Here he lived the greater part of his life, and here he is buried. His father's log-house in a few years gave way to Otsego Hall, for many years the most stately and spacious private residence in central New York. It was burned a few years after the novelist's death, a stone Indian hunter now marking the site. A statue of Leather-Stocking, with rifle and dog, surmounts the Cooper Memorial in the cemetery. Many natural features in the vicinity are named after characters in the Cooper novels.

COUNCIL ROCK is a large boulder in Otsego Lake, referred to by Cooper, and generally believed to have been a favorite haunt of the Indians. In time of extreme low water the rock now appears as an oval cone about nine feet in diameter one way and six feet the other. From the bed on which it rests it rises about four and a half feet. When the water is extremely high the rock is covered.

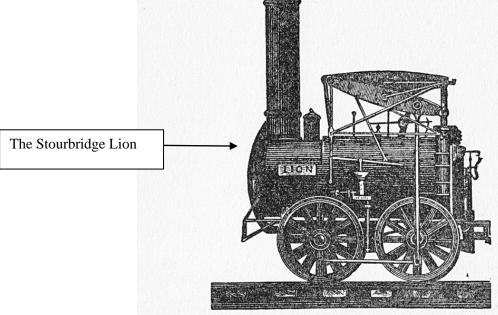
CUMBERLAND HEAD is one of the best-known landmarks of Lake Champlain. It closes Plattsburg Bay from the main lake on the northeast and is clearly visible from the trains entering Plattsburg. It was on a line between this long peninsula and Crab Island to the south that MacDonough's fleet was anchored during the Battle of Lake Champlain (q. v.).

PARK AND BLOODY GROUND. The Saratoga county of the present time was like Kentucky, "the dark and bloody ground," the hunting and fishing country of the Five Nations on the south, and their enemies, the Algonquins, on the north. Through here their war trails led, and here they often planned their ambuscades. Under more civilized strife it was scarcely less bloody, until the culmination of all its conflicts in the Battle of Saratoga (q. v.).

DELAWARE AND HUDSON COMPANY. The history of the Delaware and Hudson Company is inseparably bound up with the finding of coal in Pennsylvania and the tremendous industrial development of the country which followed as one of the immediate results of its distribution. The coal of Virginia, discovered in 1701, had been mined since 1750. It is said in "The World's Progress" that the anthracite coal of Pennsylvania was first used by a blacksmith in the Wyoming Valley in 1775. But in 1792, when Charles Cist, a Philadelphia printer and publisher, brought to that city several

Delaware and Hudson Company.

wagon-loads of anthracite which, in order to introduce what he called a new fuel, he offered to give away, he was very nearly mobbed for trying to impose on the people with a lot of "black stones." In 1806 some mining was done at Mauch Chunk, and in 1812 William Wurts, a Philadelphia merchant, and his brother Maurice, after months of prospecting up and down the valley of the Lackawaxen



THE STOURBRIDGE LION

and Lackawanna, managed to raft a few tons to that city, where it was still thought to be of little or no value. But the brothers went on buying coal lands at from fifty cents to three dollars an acre, which subsequently formed the first holdings of the Delaware and Hudson Company.

The original charter was granted to the Delaware and Hudson Canal Company by the legislature of the State of New York in 1823. Two years later ground was broken for a canal, which, reaching from Rondout, on the Hudson, to Honesdale, Pennsylvania, one hundred and eight miles, was completed in 1828, at a cost of \$6,300,000. This was within the estimates, and less than had been calculated by the engineers. The canal was intended almost solely for carrying

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coal, which was first mined within the present limits of Carbondale. It was carried over a gravity railroad, begun in 1827 and completed in 1829, to the canal at Honesdale. It was on this railroad that the "Stourbridge Lion," the first locomotive engine that ever turned a wheel on any railroad on this continent, was used. It was imported from England by the Delaware and Hudson Canal Company, taken by canal-boat from New York to Carbondale, Pa., and the first trip made August 8, 1829, from Honesdale to Seeleyville and return. The first boats carried twenty-five tons each, but, by enlargements of the canal in 1844 and in 1862, boats carrying from one hundred and twenty-five to one hundred and fifty tons were used. The final capacity of the canal, with its equipments, in ordinary boating seasons, was 2,500,000 tons annually. The canal was abandoned January 1, 1899, since which time the entire coal and freight carrying business of the company has been done by rail.

DE COURCELLES, EXPEDITIONS OF. The first armed French expedition from the forts at the foot of Lake Champlain started southward in January, 1666, to punish the Iroquois for their depredations against the French settlements. It consisted of three hundred of the Carignan regiment and two hundred habitants. They lost their way through the incompetence of guides, and on February 9th reached the vicinity of what is now Schenectady. Here they were led into an ambush by the Indians and many were killed. But for the intercession of Arendt Van Corlear, an influential settler of Albany, they would doubtless all have been massacred. On October 1st of the same year De Courcelles commanded the vanguard of another expedition into the Mohawk country, under De Tracy, which was entirely successful. The villages were ravaged and large stores of corn and other provisions were burned, as the result of which the French settlements enjoyed several years of comparative peace.

DOWNIE, CAPTAIN GEORGE, commanded the British squadron in the battle of Lake Champlain (q.v.). He was killed in the action and buried, with the other British and American officers who fell in the same engagement, in the Plattsburg cemetery.

DUTCHMAN'S POINT, on the island of North Hero in Lake Champlain, was the location of a British post, which was maintained there for thirteen years after the close of the Revolution. It made no demonstration against the inhabitants and was finally abandoned. Another post was held at Point au Fer at the same time.

RURAL CEMETERY, THE ALBANY, for which there is a special station on the Delaware and Hudson, is the resting place of many men eminent in their country's history. Among them are Gen. Philip Schuyler, Gen. Solomon Van Rensselaer, Gen. Peter Gansevoort, Col. John Mills, Pres. Chester A. Arthur, William L. Marcy, Daniel Manning, Thurlow Weed, and many heroes of the Civil War.

SANDY HILL, probably originally named Kingsbury, was at first so nicknamed from the long sandy hill on the main highway leading north from the village. It was said to have been fastened to the beautiful village by Burgoyne's teamsters. It was incorporated in 1810 and the name changed to Hudson Falls in 1910. Sandy Hill in the first half of the last century was the most prominent village north of Troy and was noted for its distinguished men. Among many could be named Gov. Silas Wright, Gov. Nathaniel Pitcher, William L. Lee, Chief Justice and Lord High Chancellor of the Sandwich Islands, Gen. Orville Clark, Atty.-Gen. John H. Martindale, and Hon. Charles Rogers.

Saratoga was a primary destination for a great many D&H travelers.

SARATOGA BATTLE MONUMENT, erected to commemorate the surrender of Burgovne (see Burgovne's Campaign), is in the village of Schuylerville, which was formerly known as Old Saratoga. It stands within the lines of Burgoyne's intrenchments, on a bluff three hundred and fifty feet above the Hudson, and from a height of one hundred and fifty-five feet overlooks the grounds of the surrender. A staircase of bronze leads from the base to the top, where can be seen the entire region between Lake George, the Green Mountains and the Catskills. On each of three sides of the monument is a niche containing heroic statues of Generals Gates, Schuyler and Morgan, while the fourth is left vacant, with the name of Arnold inscribed underneath. With the monument, and lining its two stories, are decorations in bronze representing historical and allegorical scenes connected with the campaign of Burgoyne. The corner-stone was laid on October 17, 1877, when poems and addresses were delivered by Horatio Seymour, George William Curtis, James Grant Wilson, Alfred B. Street and William L. Stone. (See Battle of Saratoga.) It was formally dedicated by the State of New York in October, 1912, with impressive civil and military ceremonies.

SARATOGA LAKE is three and a half miles east of the village of Saratoga Springs. It is about five miles in length, with an average width of one mile. Here have taken place some of the most brilliant

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and exciting college regattas ever held in America. It was a favorite resort of the Indians, as it is now of summer vacationists and excursion parties.

Saratoga Massacre SARATOGA MASSACRE occurred on the morning of the 17th of November, 1745, at old Fort Saratoga, near the mouth of Fish Creek, where Schuylerville now stands. A horde of French and Indians, under the leadership of Marin, had come down from Montreal to raid the settlements in the Connecticut Valley. The approach of winter, however, and the lack of suitable provisions, led the Indians to refuse to go eastward of Crown Point. Accordingly, at the instance of Father Piquet, the French Prefect, Apostolique of Canada, the band turned southward towards Fort Orange.

"The scowling portholes of the old Schuyler mansion seemed to laugh between the tendrils of the creeping vines. Suddenly, in the early morning, the scene of peace and prosperity was changed to slaughter, pillage and destruction. Philip Schuyler, the elder, uncle of Gen. Philip Schuyler, was offered immunity in the midst of the fray; but he scorned safety at the expense of his neighbors, and was shot to death in his own doorway. The houses and fort were burned to the ground, the cattle killed or burned in their stalls, and only one or two inhabitants escaped to tell the tale."—Ellen Hardin Walworth, in Historic Towns.

Saratoga Springs

→SARATOGA SPRINGS takes its name from Fort Saratoga, which stood beside the Hudson on the present site of Schuylerville. The derivation of the word Saratoga, however, is shrouded in obscurity. Many attempts have been made to establish its meaning, but all have been conjectures, most of which are without sound foundation in the Iroquois language. It has been asserted, for instance, that it comes from two Indian words meaning "Place of Salt," whence the Salt Springs, and also that it means "Place of Sparkling Waters." These interpretations are erroneous, a fact especially evident since the original application of the word was to a point on the Hudson and not to the Springs at all. The changes of time, however, have caused Old Saratoga to be entirely forgotten, except by those who find interest in history and tradition, while to the modern world the name has become a synonym for Salt Springs and Sparkling Waters, and a designation for one of the best known health and pleasure resorts on the American continent. The development of this celebrated watering place began at the High Rock Springs, which was known to the Indians. As long ago as 1783,

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George Washington, Alexander Hamilton and Gov. George Clinton visited the Springs together. The list of distinguished personages who have since enjoyed its benefits and pleasures would more than fill this book of notes to overflowing.

Next to the original Indian occupants, the ground on which the village of Saratoga stands, and through which the mineral waters of Saratoga percolate, belonged to Rip Van Dam, who received it by allotment in 1770, but is otherwise unknown to fame. The first hotel was built by Dirck Scoughten in 1771, near the High Rock Spring, and was occupied three years later by John Arnold of Rhode Island. The surroundings at that time included sixteen Indian cabins in plain sight. Wolves howled and panthers screamed by night, black bears were out for berries in the daytime, wild deer and moose drank from brook and lake, and overhead eagles soared and built their nests in the lofty pines.

The first cottage owner was Gen. Philip Schuyler, who, in 1783, built a summer-house near the Springs. At the time Sir William Johnson visited the High Rock Spring, it is said that the waters had ceased to flow over the top, and there is a legend to the effect that it was because some squaws had washed themselves there that the offended waters shrank from their polluting touch into the bosom of the rock. It was not till 1866 that a little scientific tubing induced them to resume their original channel. Knowledge of the other springs, of which there are many, has come in some cases by careful searching, in others by chance. Congress Spring was discovered in 1792 by Gov. John Taylor Gilman, of New Hampshire, a Revolutionary soldier and member of the Continental Congress, in honor of which it was named. Columbian Spring was first tubed in 1805 by Gideon Putnam, who two years before had opened the Union Hotel, which was much larger than any that had preceded it, but small, indeed, compared with the magnificent structures of the present day. Saratoga has had many fires, each conflagration resulting in more commodious and palatial accommodations, till, like the health and pleasure attractions which surround them, they are without an equal in the United States. The erection of an adequate convention hall, in 1893, perfected arrangements by which the largest assemblages-political, religious, or fraternal-can be admirably housed, and many of the most important and interesting national gatherings are now regularly held at Saratoga.

All of the important springs in Saratoga have been taken over by the State, and many of those that had failed have been brought

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back to their former liberal flow by scientific treatment. They are now controlled by the State for the benefit of all the people, with a conservatism which will forever maintain the supremacy of Saratoga among American watering places.

"Scalping was practiced by both French and English throughout the Colonial wars...

SCALPING was practiced by both French and English throughout the Colonial wars. They took not only the scalps of Indians but those of white men as well, and at some of the more atrocious massacres it also appears that women and children were scalped in true Some of the early writers justified the taking of Indian fashion. scalps from Indians on the ground that it increased Indian respect for white men as fighters. The custom might better be attributed. however, to the thoroughness with which the first white invaders of the wilderness copied all of the methods of warfare of their savage opponents and allies. Rogers in his journal repeatedly tells of the scalps that he or his rangers took in their skirmishes with the French. With a fine sense of propriety, however, he adopted a different tone when explaining in England how the Indians waged war in America. "They always scalped their victims," he said, "for such is their barbarous custom." During Amherst's Campaign of 1759 against Ticonderoga the scalping of women and children was expressly forbidden in General Orders of June 12th. "It is the General's orders that no scouting parties or others in the army under his command shall, whatsoever opportunity they have, scalp any women or children belonging to the enemy. They may bring them away if they can; but, if not, they are to leave them unhurted; and he is determined that, if they (the French) should murther or scalp any women or children who are subjects of the king of England, he will revenge it by the death of two men of the enemy, whenever he has occasion, for every man, woman, or child murthered by the enemy."

Schenectady Massacre

SCHENECTADY MASSACRE. Schenectady stands on the site of the great Mohawk "castle" and capital of the Five Nations. It was settled by Arendt Van Curler, or Corlear, from whom Lake Champlain received one of its early names, Corlear's Lake. In February, 1690, a party of one hundred French and as many Indians, the latter under the leadership of Kryn, "The Great Mohawk," all being sent southward from Quebec by Frontenac, approached the town at midnight, on snowshoes, in the midst of a driving snowstorm, entered without being discovered, awoke the two hundred and fifty inhabitants with the war-whoop, killed sixty on the spot, captured ninety, and of the sixty-six houses burned all but six.

f 109 1

five Indians (the same one who had been spared at the Schenectady Massacre), who had proceeded as far as 'Ticonderoga,' where he erected some stone breastworks, and had been since the fifth of August waiting for the expedition to come up.

"It was now found that the time was so far spent, the bark would not peel, so no more canoes could be made.

"The provisions were also giving out, and it was ascertained from the commissaries at Albany that no further considerable supply could be forwarded. It was, therefore, on the 15th, resolved in a council of war to return with the army."

Though Winthrop's Expedition was a failure, a portion of his forces, under Captain John Schuyler, of that family which was always at the forefront in the Colonial wars (see Schuyler Family), proceeded on down Lake Champlain, as the army turned back, and delivered the first attack upon Fort La Prairie (q. v.).

WOOD CREEK, which flows into South Bay, at the head of Lake Champlain, was an important portion of the water highway between the St. Lawrence and the Hudson. It was navigable for canoes to a point within eleven miles of the Hudson at Fort Edward. The portage between these two places was known as the Great Carrying Place, and the route was often used by both French and English. Today it lies on the highway between Albany and Montreal, the tracks of the Delaware and Hudson Railroad following it mile for mile after leaving Fort Edward. The sluggish waters of the creek flow silently beside the car windows, giving never a hint to travelers and vacationists of the savage war parties and scarcely less relentless military expeditions that once plied its waters.

Wyoming Massacre

→ WYOMING MASSACRE. The beautiful Wyoming Valley, about twenty-one miles long by three wide, through which runs the north branch of the Susquehanna River, was early claimed, under charter rights, by both Connecticut and Pennsylvania, although no attempt was made at settlement till 1763, when the Susquehanna Company, of Connecticut, which had purchased the lands from the Indians about ten years previous, sent out colonists. But in less than twelve months they were all massacred or driven away. In 1768 Pennsylvania also bought the land from the Indians, and established a settlement the year following. About the same time another party arrived from Connecticut, and there was continual strife between the two, till, in 1771, the king confirmed the claim of Connecticut. On the breaking out of the Revolution the eastern settlers, after expelling what few Tories there were in the neighborhood, resolved

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that they would "unanimously join our brethren of Connecticut in the common cause of defending our country." But in 1778 the expelled Tories and an additional white force, with seven hundred Indians, eleven hundred in all, led by John Butler, marched against the settlement. At first the settlers took refuge in the "Forty Fort," near the present Wilkes-Barre, but, on July 3, about all the males (400) sallied forth to attack the invaders, and were disastrously defeated, two-thirds of their number being killed, captured or massacred. The remainder took refuge in the fort, which the next day surrendered. Many prisoners were killed and tortured by Indian squaws, and the sufferings of those who sought to escape were terrible. "Shades of Death" is the name by which a swamp near Wilkes-Barre is known, and where a hundred women perished of fatigue and starvation following the massacre.

Yankee Doodle

ANKEE DOODLE. The tune itself is very old, and may have originated either in Holland, France or Spain. It was sung in England in the reign of Charles I, and words were set to it in ridicule of Cromwell:

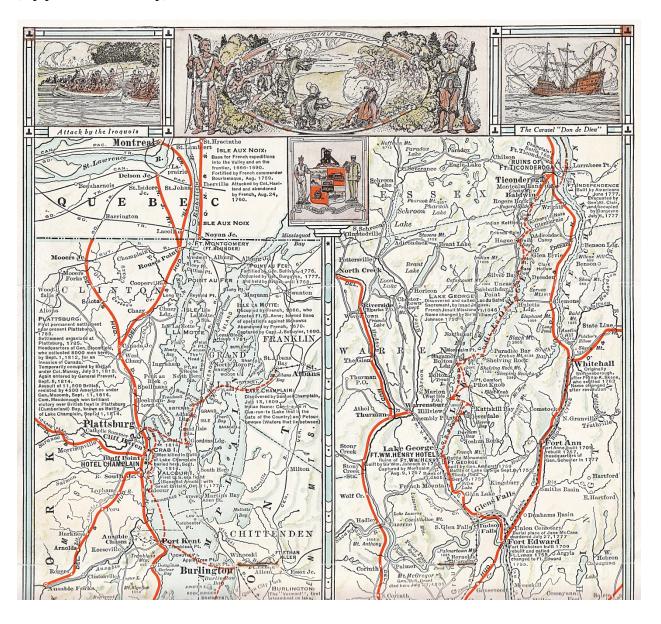
"Yankee Doodle came to town
Upon a Kentish pony,
He stuck a feather in his cap
Upon a macaroni."

In the summer of 1758, while the British Army, under the unfortunate General Abercrombie, lay encamped in Greenbush, now Rensselaer, on the grounds belonging to Jeremiah Van Rennselaer, in anticipation of the march to Crown Point, which ended so disastrously at FortTiconderoga, reinforcements, consisting of Continental Militia, arrived from the east. Their uniforms, and the lack thereof, their accoutrements and general appearance afforded much food for mirth among the regulars. Attached to the staff of the commanding general was a musical wit named Dr. Richard Shuckburg, afterwards appointed Secretary of Indian Affairs by Sir William Johnson, and he, with an idea of teasing rather than pleasing, wrote down the notes of the old tune, changing the words slightly, and gave the composition to the chief musician of the Eastern troops as the latest martial music of England. Greatly to his surprise and amusement, it was taken seriously, and the camp rang morning, noon and night with the strains of Yankee Doodle, which, then and there, was unanimously adopted as the favorite air of the Continental Militia, and served as such throughout the Revolution.

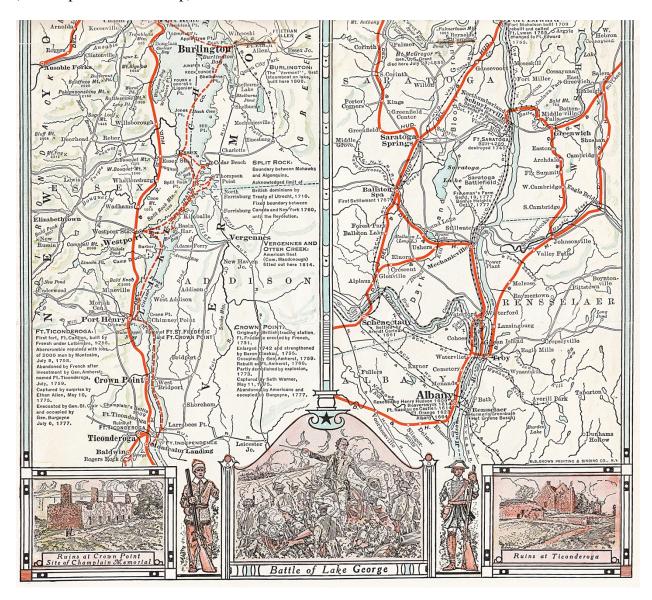
[124]

A Pictorial Map of the Summer Paradise:

(top portion of the map)



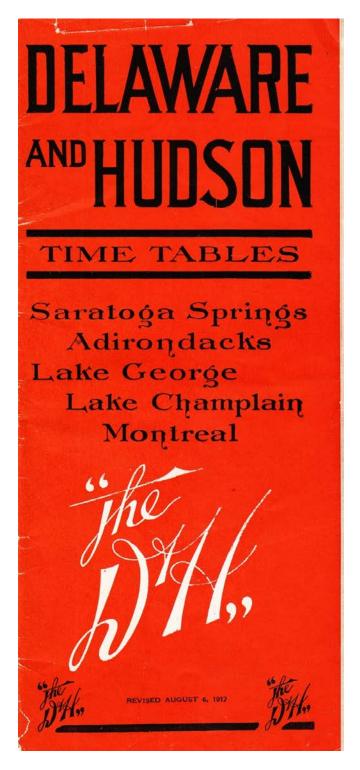
(bottom portion of the map)

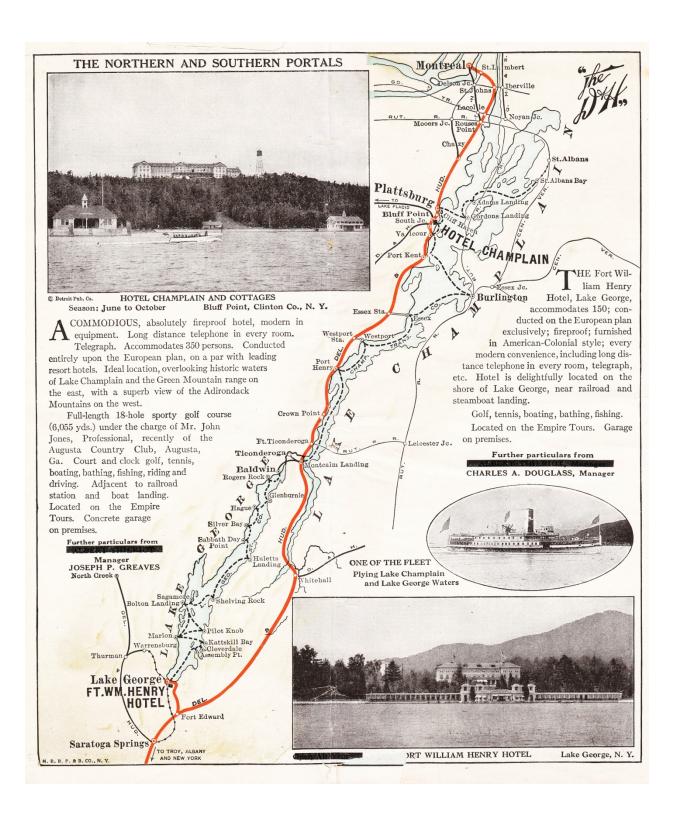


See also:

"A Summer Paradise A directory of places interest, hotels, and boarding houses in America's most popular vacation land: Lake George, Lake Champlain, and the Adirondacks, on the rail and steamer lines of the Delaware & Hudson Co. Published by the Passenger Department of the D&H in 1927 (308 pages).

1917 D&H Timetable (revised August 6, 1917) for travel (1) to Saratoga Springs, Adirondacks, Lake George, Lake Champlain, and Montreal, and (2) to all D&H destinations:





INDEX

INFORMATION FOR TRAVELERS

TIME TABLES.—Time tables show the time trains should arrive at and depart from stations and connect with other trains, but their departure, arrival or connection at time stated is not guaranteed. Time of trains on connecting lines is published for the information of passengers, and every care is taken to keep it correct, but this Company does not hold itself responsible for any errors or omissions.

LOCAL TICKETS .- Local tickets of this Company's issues require their use on the date of sale or the day following.

MILEAGE TICKETS.—Mileage tickets are accepted for passage of bearer for one or more persons. Mileage tickets that have been borrowed or hired from ticket speculators are not good for passage. D. & H. mileage tickets are accepted on steamers on Lake Champlain (minimum distance 10 miles), but not on Lake George.

TICKETS reading via D. & H. Co.'s Rail Line are honored for transportation without extra charge on steamers of Champlain Transp. Co. on Lake Champlain, but not on Lake George.

REDEMPTION OF TICKETS.—Unused tickets or portions of tickets of all classes that are valid for passage over the Delaware & Hudson Lines, will be promptly redeemed in accordance with Tariff Regulations. They should be deposited with nearby D. & H. Station Agents or mailed direct to the General Passenger Agent, Albany, N. Y.

STOP-OVERS will be granted at any point within time limit on all first-class tickets, except on commutation tickets.

CHILDREN.—Children under 5 years of age in charge of parent or guardian are carried free; 5 years of age and under 12 are required to pay half fare; 12 years of age and over, full fare. Mileage tickets are not accepted for child's half fares. Local child's half fare ticket only should be tendered.

FARES CANNOT BE ADJUSTED BY CONDUCTORS. - In the event of any disagreement with a conductor relative to tickets required, privileges allowed, etc., passengers should pay the conductor, take his receipt, and refer the case to the General Passenger Agent for adjustment. The conductor is governed by rules he cannot change.

TRANSPORTATION OF DOGS.—Dogs will be transported in accordance with tariff regulatio

BAGGACE REGULATIONS.—One hundred and fifty (150) pounds of baggage, not exceeding one hundred (\$100) dollars in value, will be checked free on each whole ticket and seventy-five (75) pounds, not exceeding fifty (\$50) dollars in value on each half-ticket; except when baggage is checked between two points in the State of New York, not requiring transit through another State, the value is limited to one hundred and fifty (\$150) dollars for each adult passenger and seventy-five (\$75) dollars for each child traveling on half ticket. Passengers paying charges for excess weight baggage will be entitled to an additional value allowance of 66% cents for each pound of excess weight on which charges are paid. This Company will not accept for transportation in regular baggage service from any one passenger, property that is declared to exceed twenty-five hundred dollars (\$2,500) in value. Baggage in excess of weight or value stated will be charged for in accordance with Tariff Regulation, but no

piece weighing more than 250 pounds will be received as baggage, but must be forwarded by express or freight. Baggage exceeding forty-five (45) inches in length will be charged for in accordance with Tariff.

Baby carriages are accepted for transportation locally and to points on con-

necting lines. The weight of each carriage being estimated at 50 pounds, and collection made at regular excess baggage rates.

Bicycles and baby carriages will not be checked over Hudson River boat

Passengers should be at station at least fifteen minutes before arrival of train and present tickets to have baggage checked, thus affording ample time for Station Baggagemen to check and load their baggage for that train.

Passengers are requested to claim their baggage upon its arrival. The Company does not hold itself liable for loss or damage to baggage after a sufficient time has been allowed for its removal.

CHARGE FOR BAGGAGE STORAGE.—First 24 hours, free, Second 24 hours or fraction thereof, 25 cts., and 10 cts. for each succeeding 24 hours or fraction thereof. Maximum charge \$2.00 on any single piece.

Baggage received any hour Saturday will be held without charge until same hour Monday, and baggage received any hour Sunday will be held without charge until midnight Monday. If not claimed within the time specified, storage will commence twenty-four hours after receipt of baggage. Legal Holidays will be treated the same as Sundays, but no deduction will be made for Sundays or Legal Holidays after storage has begun.

SPECIAL DELIVERY BAGGAGE CHECKS may be purchased of the Baggage Agents at all stations covering delivery through to hotel, residence, steamship pier, or ferry, in New York City, Hoboken or Jersey City. Also to hotel or residence in Albany, Troy, Saratoga Springs, Schenectady, Saranac Lake, Lake Placid, Utica, Syracuse, Rochester, Buffalo, Boston, Montreal and Quebec.

BONDING OF CANADIAN BAGGAGE TO AND FROM THE PORT OF NEW YORK.—Bagsage intended for immediate exportation to Canada, arriving at seaboard of Port of New York, need not be examined by U. S. Customs, but may be checked through from any Steamship Dock in New York City to any point in the Dominion of Canada. It is forwarded in bond, and no examination is necessary by U. S. Customs Officers.

In the opposite direction, baggage may be checked through from Montreal to any Steamship Dock in New York City, avoiding Customs examination at United States frontier point.

WESTERN UNION TELEGRAMS.—Telegrams, cablegrams and night letters accepted at all telegraph offices in railway stations along the Delaware & Hudson Lines for transmission via Western Union. Fifty-word letters, for delivery the following morning to any part of the United States, at the rate of a 10-word message.

NATIONAL EXPRESS COMPANY.—Operating the express business upon the Delaware & Hudson Lines, has unexcelled facilities for transporting parcels, baggage, merchandise, valuables, etc., between all points, with speed and safety.

The following representatives of the D. & H. Co., located at the offices named below, will cheerfully furnish information with respect to tours, fares, time-tables, maps, guides, etc. J. T. HAYDEN, Southern Passenger Agent, | 1354 Broadway (near 36th Street), F. E. McGraff, Traveling Passenger Agent, New York City. O. W. Jordan, New England Passenger Agent, 210-211 Old South Bldg., Boston, Mass. J. J. COYLE, Division Passenger and Freight Agent, T. J. McNamara, Traveling Passenger Agent, Scranton, Pa. James Fitzsimons, General Canadian Passenger Agent, F. B. Moffitt, Canadian Passenger Agent, 286 St. James Street, Montreal, P. Q. Scranton, Pa.

W. F. SHEEHAN. Traveling Passenger Agent,
Albany, N. Y.

MAX V. BECKSTEPT, Division Passenger and Freight Agent,
Binghamton, N. Y. F. J. FORSTER, Division Passenger and Freight Agent, Troy, N. Y.

F. P. GUTELIUS, Vice-President,

W. J. MULLIN, General Traffic Manager, Albany, N. Y.

(Schedule of July 1, 1917.) No. A57-8-3-1917-34M-92.

(Revised August 6, 1917.)

ALBANY AND TROY TO MONTREAL																								
Schodule of Tule 1 1017	Except	Daily	Except									Except Sun'y					I I	Run	Sunday	s Only	1	1 1	1	
Schedule of July 1, 1917	Sun'y 179	21	Sun'y	1	33	23 23	39	3	173	Sun y	sun y	Sun y	Sun y	Sun y	9	* 7	31	1 167	3		171			
Mls. NEW YORK, Leave "GRAND CEN. TER	A. M.	A. M.		A. M. 12 25	A. M. 12 25	P. M.	P. M.	A. M	P. M.	P. M.	P. M.	P. M.	P. M.	P. M.	P. M.	P. M.	A. M.	A. M.					_	
" 125TH STREET				12 36	12 36			8 57		12 52		2 00 2 11	4 00		7 57	9 46			8 57	12 52			::::	
" H. Nav. Co. Peoples Line . " H. Nav. Co. Citizens Line .			7500	6 00	6 00				1															
" DAY LINE STEAMERS									1				8040											
MO ALBANY, D. & H			6 20									P. M.	P. M.	P. M.	P. M.	A. M. 1 30	A. M.	A. M.	P. M.	P. M.	P. M			
7 WATERVLIET				7r11				12r27		4 58		6 15	7 31		11†21				12r27	4 56	P. M.			
8 TROY			6 53				12 15				5 03	6 15		8 30	12*01		7 00		12e00 12e05	4e03				
12 WATERFORD			7 03								0 14						7 11							
9 COHOES				7 17			12 25	12 34 12 i38		5 05		6 20	7 36	8 39	12 09	1 54			12 34 12 j 38	5 02				
11 WEST WATERFORD			7 17																					
26 ROUND LAKE			7 28	7 33			12 42	12 55		5 36	5 28 5 40		7 55 8 03	8 56 9 08	12h25 12h36 12h48	2 12	7 25		12 53 1 j06	5 19 5 30				
32 BALLSTON SPA			7 39 7 50	7h53	8 15	12 59	1 07	1 18		5 23 5 36 5 48 6 00	5 53 6 05		8 14	9 19	12h48 1 00	2 38 2 50	7 47		1 18	b 43			::::	
39 SARATOGA SPRINGS Lv.		Except	7 55	8 10	8 20	Except	Except	1 35		6 10	6 15		0 20	9 30	1 10	2 55	8 04		1 90			· · · · · ·	-	• • •
50 GANSEVOORT		Sun'y	8f12			Suny	Sun'y	1f52		6 25	6 30		8f45			3k11	8 20		1f52	6 20		:		
56 FORT EDWARDAr. 56 FORT EDWARDLv.		161 8A00	8 25	8 37	8 45		165 12830	2 06	5.40		6 40					3 22	8 29		2 06	6 33				
58 HUDSON FALLS	6 11	8 07			8 54	9 07	12 37	2 37	5 47		6 51		9 04				8 38	9 17	2 37	6 40				
65 GLEN LAKE	6 55	8 15			9 03	9 15	12 45	2 45 2f55	5 55		7 00 7 f 12		9 12				8 45	9 25 9f35	2 45 2f 55	6 55	9 00			
61 GLENS FALLS. 65 GLEN LAKE. 67 FRENCH MOUNTAIN. 70 LAKE GEORGE. Ar.								3100			7f20							9f40	3f 00	7f10		:	::::	• • •
LAKE GEORGELv.	•••••		7 10		9 30			3 10			/ 30					•••••	9 15	CHIMINET			9 25			
FRENCH MOUNTAIN			7f20										ck,		:::::				1 20	5 45 5f 55	V			
GLENS FALLS			7 34					1 45		6 05						:::::			1f33 1 45	5f 58 6 10				
HUDSON FALLS			7 42		Co.			1 53		6 15			Line m.						1 53	6 18	* Runs o	six mile	s less.	
FORT EDWARDAr. 56 FORT EDWARDLv.	•••••		7 49		Nav.			2 00		6 25			y I		-	3 22			2 00		† Daily, ‡ Conne	except S	unday main	iin
63 SMITH'S BASIN			8 37	8149				2 20		6 49			Day 00 p.						2 20	6 33 6 48	a Via Be	except Sets with	Poin	t.
67 FORT ANN	• • • • • •		8 45 8 53	8x57 9x04	Hud.			2 28		6 56			1, 7			3f41			2 28 2 36 2 50	6 48 6 55 7 02	Albo	nu 10 20	ays ie	eav
78 WHITEHALLAr.			9 05	9 15				2 00		7 15			will leave Albany, 7.0		2 10	4 00			2 50	7 02 7 15	c Stops	o leave i	p. m.	ger
78 WHITEHALLLv.				9 20 9 45	from 6.50					7 25		::	WII							7 25				
91 CASTLETON				10 05	ts f			3 50		8 10			41						:::::	7 50 8 10	e Via B	elt Line ervliet.	Local	l to
RUTLANDLv.					starts f Dock			12 45					ain						12 45		f Stop o	n signal.		
CASTLETONAr.					33			1 30					E						1 06		g Stop o	n signal	to rece	eiv
78 WHITEHALLLv.				9 25						7 25						4 05			3 00		for o	scharge I	Plattsb	our
85 CLEMONS				9f40 9f47	Ë			3 16		7 42						4f 17 4f 22			3 16		for	or from	n T	roy
93 PUTNAM				9f59				3 35		7 56						4f32			3 35					
98 WRIGHTS		:::::		10f 10 10 24						8103						4645			3f 44 3 54		h Stop t	ge passe	e or ngers	fo
TICONDEROGAAr.				10†52				4 04						_							Ding	points so	th th	an
TICONDEROGALv.	• • • • •			10†05																	j Stops t	o dischar	ze nas	sen
10 CROWN POINT				10 33 10 48				4 00 4 15		8 15				• • • • • •	• • • • •	5f02			4 00			from Al	bany	an
17 PORT HENRY								4 32		8 40					3h19	5 17			4 32		k Stops	o dischar	op fou	ir o
31 WADHAMS				11f49			:::::	5 11		9f06					3h40	5 42 5f49			5 03		n Stop o	passeng n signal	to woo.	eiv
								5 19		9f12						5f55			5 19		burg	engers 10	r Pla	itts
43 WILLSBOROUGH				12 14						9 28						6f 12			5 41		and	to disch	arge j	pas
										9f42						6c27			5f56 6 04		Alba	ny or po	ints so	out
AUSABLE CHASMAr.		Sun'v	Except	12 52		Sun'v				10 38									6 20		r Stonto	noooire o	asseng	ers
AUSABLE CHASMLv.		71	Sun'v	12 00		73	Daily	b 35		9 25						• • • • •					x Stops to char daily	eceive	and	dis
163 SOUTH JUNCTION		A. M. 8 37	A. M.	12148		P. M. 3 44	83			9f59		:::::				6f44			6115		dail	ge passer except	sund	an
165 BLUFF POINT, HOTEL CHAMPLAIN		1	9 55				P. M.					1									from	Albany	nd no	
65 CLIFF HAVEN.							4 20			10 10						6 53			6 26		Trains 1			
CATHOLIC SUMMER SCHOOL 68 PLATTSBURGAr.			9 57				4 22 4 35			10 13 10 25					1 50	6 57			6 28		Club	and Cu	ake a	Sid
97 PLATTSBURGLv.	-	0 40		1 10	-	3 00		6‡30		10 25					4 50 8 40				6 35		Train 3	vill stop	on sig	gna
16 LOON LAKE								8 32							10 42	10 42					Train N	0. 45 Wil	I stop	0
44 LAKE PLACID Ar.		1	:::::					9 30							11 20	11 20					Brid Train 1	ge.	nsingb	ur
LAKE PLACIDLv.				7†00				1 35											1 35		Edw	ge. 79 leaviard at 6	ng I	IO.
LOON LAKE				7†22 8†06															1 55		Stree	et. Fort	Edwa	arc
PLATTSBURGAr.				10†05				4 30									:::::	:::::	4 30		Johr	and Riv	or Stro	ante
68 PLATTSBURGLv. 72 BEEKMANTOWN		8 50		1 20				6 45							5 00	7 10			6 45			son Fa		
74 SPELLMANS		9f04		1f33				6f 58											6154			ed figure	es der	ot
77 WEST CHAZY		9 12		1 40				7 05								7f30			7 05		Dark fa	ced fig	ires	de
87 COOPERVILLE				1 53 2f00				7 f25								7 43			7f25		пои	T. IVA.	illie.	
191 ROUSE'S POINTAr.				2 10				7 35							5 45	8 00			1 90		FOR	RRANC	EME	N
214 ST. JOHNS, G. T 241 MONTREAL, G. T Ar.								8 35							6 35 7 29	9 45			8 35		OF PAR	LOR, SL	EEPI RS S	NO
	A 34	A M.	A. M.	P. M.	A. M.	P. M.	P. M.	P. M.	P. M.	P. M.	P. M.	P. M.	P. M.	P. M.	A. M.	A. M.	A. M.	A. M.	P. M.	P. M.	PAGE 5			-

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Schedule of July 1, 1917 fis. Leave	Sun'y	Sun's	y Sun	y Sun'y	Sun'	Sun'y	Daily	Sun'y	Sun'y	Excep Sun'y	Sun'y	Except Sun'y	Daily 26	Daily	Sun	Daily	Daily	14	un Sur	ndays	0nly				
MONTREAL, G. T	A.M	. A. M	. A. M	. A. M	. A. M	. A. M.	A. M	A. M.	A, M.	P. M	P. M.	P. M.	Р. М.	P. M	P. M	P. M.	P. M	A. M	A. M	1. P. M	I. P. M				
MONTREAL, G. T St. Johns, G. T O ROUSE'S POINT, D. & H.LV								8 12											8 1	4					
4 COOPERVILLE		.1													2	9117	7					IT EX	cept S	Sundays	3.
8 CHAZY								9 30	11 12					3 39		9 25	7		9 3	0		. + 0	nnect	s with	main
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0 BEEKMANTOWNAr								9 58	11 25					4 10		9f48 9 54						c St	op or	s. n sign Sunda	al da
PLATTSBURG. LV														6†30)								Alban	y, and	on sig
LOON LAKESARANAC LAKE														9110)			1					Sunda	ys to pas	receive
LAKE PLACIDAr								7 00						9†30)							e Vi	a Belt	Line fr	om Gr
SARANAC LAKE								7 22						1 38								f Ste	ops on	signal signal gers f y and	to rece
LOON LAKEAr								8 06						2 41									passen Alban	gers f	or Ti
4 PLATTSBURGLv												1 30		4 20					10 0	-					ne fr
7 CLIFF HAVEN, CATHOLIC SUMMER SCHOOL	6 58	3				8 29	8 43	10 07				1 34		4 26	6 35	10 04			10 0	7		k Ste	Water ops to	vliet.	arge 4
7 BLUFF POINT, HOTEL CHAMPLAIN	7 00			1								1 35			6 40		1	1				n Ste	ons on	signal	to rece
8 SOUTH JUNCTION	7 03					8 37	SERVICE SERVICE	1000				1 40			CHAPTE.	10 06			1000				south	of Plat	or po
7 PORT KENT						8f41 8 53		10 28						4f40		10 f 13			10 1	8		V Ste	ops to senger	disch:	arge 1 Pla
AUSABLE CHASMAr						9 45		10 43						5 † 09		10138							and t	nd poi to rece s for	nts no
AUSABLE CHASMLv														5£03									bany a	and poi	Proy,
WILLSBOROUGH				1		9 18		10 52						5 19		10*45			110 5	2		x Su	hereo	f. arrive	New Y
ESSEX						0 22		11 00						5 39		11 f 00			11 0	9		W 9	3. C.	St. 10 T. 11.0	5 p.m.
WADHAMS						9 37		11 20						5f 47		11 14			11 20	0		Trois	op on nornin	signal	Mon
PORT HENRY						10 06		11 52						6 19		11 33			11 5	2		Trail	t Oak	Point.	T
2 CROWN POINT						110 40		11Z UC						0143		11148			12 2	5		Tran	t 6.05	p. m.	will s
2 MONTCALM LANDINGAr. TICONDEROGAAr.						10 42		12 35				<u></u>											Glens	p. m. askell Falls John	; R
TICONDEROGALv						10 05	:::::	12 20	:::::					6144					12 20	0			Hudso	n Fa	lls,
2 MONTCALM LANDINGLv. 4 WRIGHT'S						10 42 10 47								6 59		12f06				5		1	ed war	c	
9 PUTNAM						10 58		1 00						7 13		12y18			1 00)		1 2	it Cun	stops	and L
4 DRESDEN		14.				11 07 11 13		1 16						7 23		12y28 12y33			1 1 10	JI		Trair	Side C	vill stor	at M
4 WHITEHALL Ar.		Albany.				11 30		1 35						7 50		12 50	1 15		1 35			8	treet	Cohe	Cemet
CASTLETON			:::::			11 35 12 00		3 25														Train	s 262	e, Nort and 264 er Stree	wills
RUTLANDAr.		Dock,				12 20		3 50			7.55											Train	264	er Stree will at_Lans	ston
CASTLETON		Ď		6 25		10 30 10 55		1 06			3 55 4 15 4 40	:::::						7 50	1 06	i		Light	Bridge	, 7.58 a	a. m.
WHITEHALLAr.		Line		6 50 7 00		11 20 11 40		1 30			4 40					10 55	1.00	8 15	1 30			1	. M.	time.	
1 COMSTOCK		y I		7 12 7 20		11 52					4 45			8 07		1f 06	1 20	8 32	1 40			Dark	. M.	l figure time.	s den
4 FORT ANN		Day	:::::	7 20 7 28		12 01 12 09					5 05			8 14				8 39 8 46				FO	D AD	DANG	EMEDI
6 FORT EDWARDAr.		5		7 45		12 25		2 18			5 30			8*40		1 32		9 00	2 18			OF P	ARLC	RANG R, SLI	EEPII
FORT EDWARDLv.		run		8 00		12 30 12 37		2 25 27			E 40			8†57				9 10 9 17	2 25			PAG	E 5.	E CA.	no, S
GLENS FALLSGLEN LAKE		will		8 15		12 45		2 45			5 55	Daily		9†12	Except			9 25	2 45			Sun'y			
FRENCH MOUNTAIN		30 w						3f 00			5 47 5 55	44			Sun'y 190			9f35 9f40	3f 00			176		RANG DR, SLI	:::
LAKE GEORGEAr. LAKE GEORGELv.		in 3		7 10				3 10			4 45	5 00		9†35	P. M.			9 50	3 10	5 10	5 45	P. M.			
FRENCH MOUNTAIN		Train		7f20												- 1		8 f 30			E f F F				
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HUDSON FALLS				7 42	12 09			1 53	Excent		5 18	5h32			6 15			8 54	1 53	5 42	6 18 6 25	8 28			
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GANSEVOORT	Sun'y 262			8 05		12 35		2 45	00		0 41	6.05		9 05		1842		9 10 9 25		1 6 00					
SARATOGA SPRINGS. AT. SARATOGA SPRINGS. LV. BALLSTON SPA. ROUND LAKE. MECHANICVILLE. MECHANICVILLE, PARK AVE WEST WATERFORD. COHOES.	A. M.	7 00		8 23	12 55	1 00		2 50	2 55	3 00	6 20	6 15	6 40	9 10		2 08	2 22	9 30	2 50	6 25					
BALLSTON SPA		7 11 7 22		8 35		1 11		3c01	3 07	3 11	6 20 6 32 6 45 6 58	0.	6 52	9 22		2 20 2f32		9 42	3f 01	6 37					
MECHANICVILLE BARK AVE		7 33	7 35	8 57	1 27	1 34		3 22	3 30		6 58	V. C		9_46		2 43		10 04	3 22	7 00					
WEST WATERFORD	6 30	7 47		9 10					3 44 3 48		7 13	Na						10 18		7 14					:::
WATERFORD	6 36	7 52	7 50	9 14	1 43	1 40		3 39	3 48		7 19	nd.	1	0 03		3 00		10 22	3 39	7 18					-
WATERFORD GREEN ISLAND TROYArrive	6 43		8 04	9j25		1 57			3 56		7 26	OCK,						10 28	3 j50	7 26					
I ROYArrive	6 501		8 08	9129		2 00		75.44	4 00		7 13 7 19 7 26 7 30	3 <u>0</u>		01.00		3 10	3 20	10 35	3 j55	7 30					
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NEW YORK, 125th ST GRAND CEN. TER H. Nav. Co. Peoples Line H. Nav. Co. Citigang Line		12 03		1 46	6 15	6 04		8 45				0139 0150		4 53 5 05		7 14 7 26	7 14 7 26	3 19	8 45						
H. Nav. Co. Peoples Line											8 30	7 00		8 20						8 30					
						P. M.														1 0 30					

ALBANY TO TROY—WEST SIDE
Schedule July 1, 1917 ALBANY. LV. 6 00 6 20 7 01 8 00 9 03 10 00 11 00 12 00 1 00 15 52 00 3 00 4 00 5 00 6 6 12 7 09 8 08 8 9 08 11 10 8 11 0 8 11
ALBANY TO TROY—EAST SIDE
Respt Recept Recept Recept Recept Sun Su
TROY TO ALBANY—WEST SIDE
Daily Sex Se
TROY TO ALBANY—EAST SIDE
Surfamor Street Street Street Surfamor Surf
ALBANY, TROY AND RUTLAND VIA EAGLE BRIDGE
Daily Daily Suny
Light faced figures denote A. M. time. Dark faced figures denote P. M. time. a Sundays arrive in New York, 125th St., 10.54 P. M., and at Grand Central Terminal, 11.05 P. M. b Stops to discharge passengers. f Stops on signal. j Sundays leave Albany 6.48 A. M. w Does not stop on Sundays. x Trains 54, 55, 60, 138 and 141 run through between Troy and Rutland.

CHATEAUC	GAY BRANCH	·	MOOER'S BRANCH
Frid'y Except Except Only Sun'y Sun'y Daily Sun'y Daily Daily Daily Schedule See See	lle of July 1, 1917 Daily Su 8	ept Except Daily Daily Except Daily Daily	Except Sun'y Schedule of July 1, 1917 Except Sun'y
P. M. A. M. P. M. P. M. P. M. P. M. A. M. A. M. MIs. Leav 9 35	Arrive A. M. P. YORK, Grand C. Ter. 8 8 NY, D. & H. 3 3 SBURG. 10 HAVEN. 9 POINT. 9 POINT. 9 SONVILLE 19 9	M. A. M. P. M. A. M. P. M. P. M. 45 5 55 10 20	71
7 40 9 50 31 CHAZY 7 53 10 04 37 LYON	LAKE. 9 MOUNTAIN 8	13 3 4 1 5 1 5 1 5 1 5 1 5 1 5 1 5 1 5 1 5 1	AUSABLE BRANCH
8105 10116 43 TWIN 8110 10122 45 MIDDL 45 MIDDL 48 WOLF 8125 10127 48 WOLF 8222 10134 51 PLUMA 52 EVEN 55 LOON 640 10150 59 LAKE 645	20NDS 8 8 8 8 8 9 8 8 1 8 8 9 8 9 8 8 8 8 8	42	Except Except Sur y Schedule of July 1, 1917 Sur y Sur y 70
Trains 80, 81, 83 and 84 will stop at Gap on signorth of Russia.	rnal to receive or discharge	passengers for or from points	2117 7137 21 ROGERS. 8701 3106 2 25 7 45 24 AUSABLE FORKS. 7 55 3 00 P. M. A. M. Arrive Leave A. M. P. M.
BALDWI	N BRANCH	iba Daila Except Except Except	SCHENECTADY AND SARATOGA SPRINGS
P. M. P. M. P. M. P. M. P. M. P. M. A. M. MIs, Leav 1 15 0 BALL 7 58 6 44 3 39 1 12 48 12 90 10 05 4 TIONN	Arrive A. M. P.	45 12 45 4 04 7 09 8 20	Daily Daily Daily Daily Daily Schedule of July 1, 1917 Daily Sun'y 21 23 23 23 23 24 24 24 25 24 24 25 24 25 25
ADIRONDACK BRANCH		WHITEHALL AN	ND RUTLAND
Recept Daily Schedule of July 1, 1917 Steept Daily 181 N. M.	55 129 127 125 123 P. M. P. M. P. M. A. M. A. M. 12 40 8 45 4 45 12 15	141 121 121 121 121 121 121 121 122 123 124 125 124 125 124 125	A 0 00 A 00 T TO 10 0
Light faced figures denote A. M. time. Dark faced figures denote P. M. time. † Except Sunday. f Stop on signal.		b Stops to leave passengers d Sundays leave Troy 7.00 x Stops on signal to receive	s. A. M. passengers for points via N.Y.C. beyond Lake Clear.
Arran Train No. 1—Daily—Cafe Car Albany to Platt Train No. 33—Daily—Parlor Car Albany to Lak Dining Car Albany to Lak Dining Car Albany to Lak Train No. 3—Daily—Parlor Car New York to Cafe Car Albany to Mont Train No. 13—Daily—Cafe Parlor Car Albany t Train No. 9—Daily—Sleeping Cars New York Train No. 7—Daily—Sleeping Car New York t Sleeping Car Albany to N Train No. 16—Daily—Cafe Parlor Car Plattsbur Saratoga to Albany on Tr	Saratoga and Chase sburg. e George. te George. Montreal. real. o Plattsburg. to Montreal. Plattsburg. to Montreal. p Plattsburg. to Saratoga Springs.	Train No. 36—Daily—(Train No. 6—Daily—I Train No. 44—Daily—I Train No. 8—Daily—S	Parlor Car Montreal to New York. Cafe Car Montreal to Fort Edward. Cafe Parlor Car Saratoga to Albany. Dining Car Lake George to Albany. Parlor Cafe Car Plattsburg to Albany. Carlor Car Lake George to Albany. Carlor Car Lake George to Albany. Carlor Car Saratoga to Albany. Carlor Car Lake George to Albany. Carlor Car Lake George to Albany. Carlor Car Lake George to Albany. Carlor Car Plattsburg to New York. Carlor Plattsburg to New York. Carlor C

TO AND THROUGH

LAKE CHAMPLAIN AND LAKE GEORGE

SERVICE ON LAKE GEORGE AND LAKE CHAMPLAIN

SERVICE ON LAKE G	LUKU	GE AND LAKE CHAMPLAIN
NORTHBOUND		VERMONT Ticonderoga
7-1-1917 Daily Except Except Except Run Sundays Only	1	LANDINGS Daily A Daily Breept Breept Sun'y Breept Sun'y Sun'y Sun'y Sun'y
NEW YORK. LV, A, M, A, M, P, M, P, M, A, M, P, M, A, M, P, M, P, M, A, M, M, P, M, A, M, P, M, P		PLATTSBURG. Lv. A. M. A. M. A. M. P. M.
7-1-1917 HORI- SAGAMORE MOHICAN		47 KIMBALLS 6855
LANDINGS Daily Daily Except Except Sun'y Sun'y Sun'y Suny Daily Except Sun'y		52 WESTPORT. 10 15 10 30 7 20
LAKE GEORGE L.V. 9250 4500 . 1530 6540 1125		BALDWIN D.&H.Ar. 1 05 1 05
1 LAKE GEORGE PARK		HORI- SAGAMORE MOHICAN
7 CLEVERDALE. 10 10 4 30 . 2:00 7:15 12:00 7 8 ROCKHURST. 10 15 4 35 . 2:05 7:20 12:05 7 7 TROUT PAVILION. 10 20 4 40 . 2:10 7:25 12:10		LANDINGS Lake George Daily Daily Except Sun'y
10 MARION		Sun y
18 PEARL POINT. 11 05 5 35 2 55 8 10 1 10 20 PARADISE BAY 22 PARADISE BAY 11 35 6 05 1 30 1 30		BALDWIN Leave 1 20 7 30
29 SILVER BAY. 12-00 6 30		9 GLEN EYRIE. 2:00 8 10 11 SILVER BAY. 2:005 8 15 12 UNCAS. 2 16 8 20 13 SABBATH DAY PT. 2:20 8 30
BALDWIN Lv. 1 15		21 PEARL POINT. 3 10 9 10 7 00 3 10 8 00 6 10 24 SAGAMORE 7 s10 8 s15 6 s20 8 s20 6 s25 24 BOLTON. 3 30 9 30 7 s15 8 s20 6 s25
MONTCALM L'D'G Ar 1630 P. M. P. M. P. M. P. M.		30 MARION. 3 55 9 50 7835 8840 6850
VERMONT Classification		33 ROCKHURST.
MONTCALM LANDING. LV. 1 35 1 35		39 LAKE GEORGE Ar. 4840 10550 8840 4850 9845 7850 SOUTHBOUND
3 LARABEES		
34 KIMBALLS 6830 3 4 FINANCIA 6830 4 6 50 4 6 50 4 6 50 6 7 7 7 7 7 7 7 7 7 7 7 7 7 7 7 7 7 7		Lake George Lv. 7 10 11 35 4 45 5 00 8 20 1 20 5 10 7 55
45 ESSEX 4 10 / 20		SARATOGA SPRINGS.Ar. 8 23 12 55 6 20 6 15 9 30 2 50 6 25 9 10
70 BLUFF POINT. 6849 6 25 10 10 2835. 77 CLIFF HAVEN. 6845 6830 10815 2840		ALBANY. 9 33 \$\bar{2}\$ \$\bar{0}0
P. M. A. M. A. M. P. M. Light faced figures denote A. M. time. Dark faced figures denote P. M. time. A Runs Sundays only July 8 to September 2, inclusive.		h Landings on Sundays only. s Denotes signal landing.

A Runs Sundays only July 8 to Sept a Via Belt Line fromWatervliet. b Via Belt Line to Watervliet. c Landings on week days only.

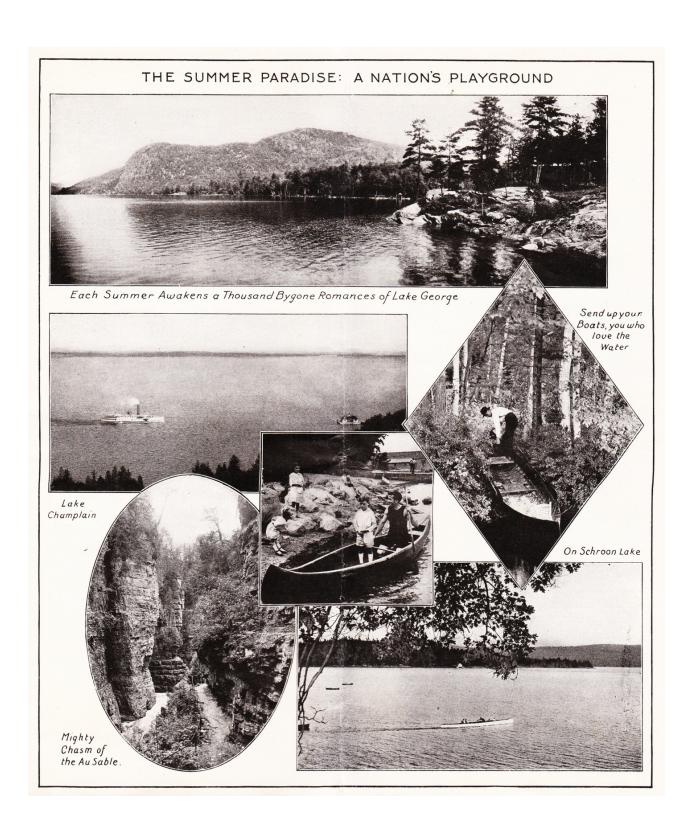
x Transfer at Saratoga Springs.

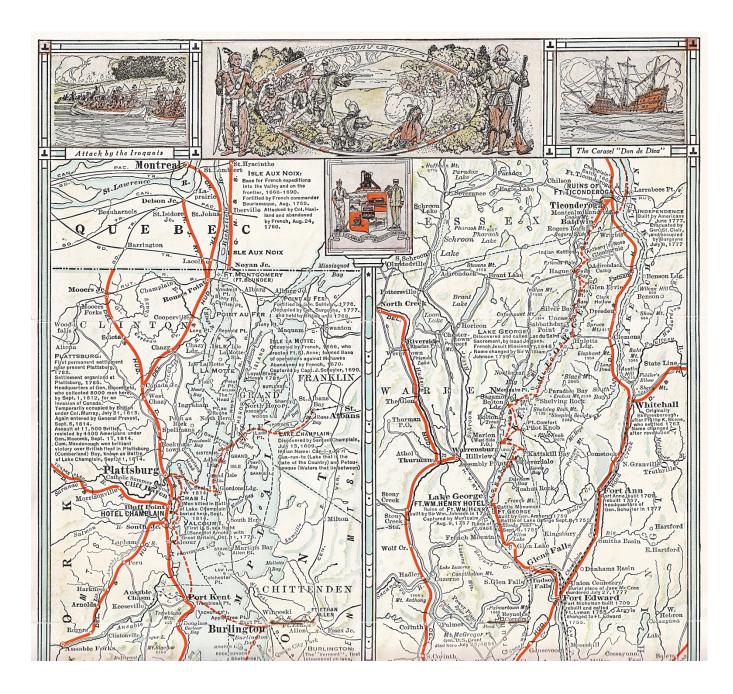
† Except Sundays.

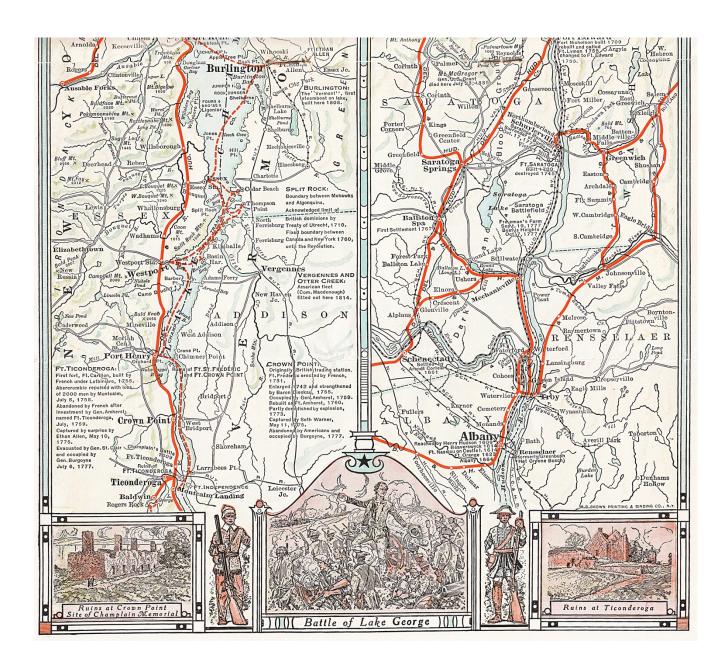
SUBURBAN SERVICE DURING JULY AND AUGUST

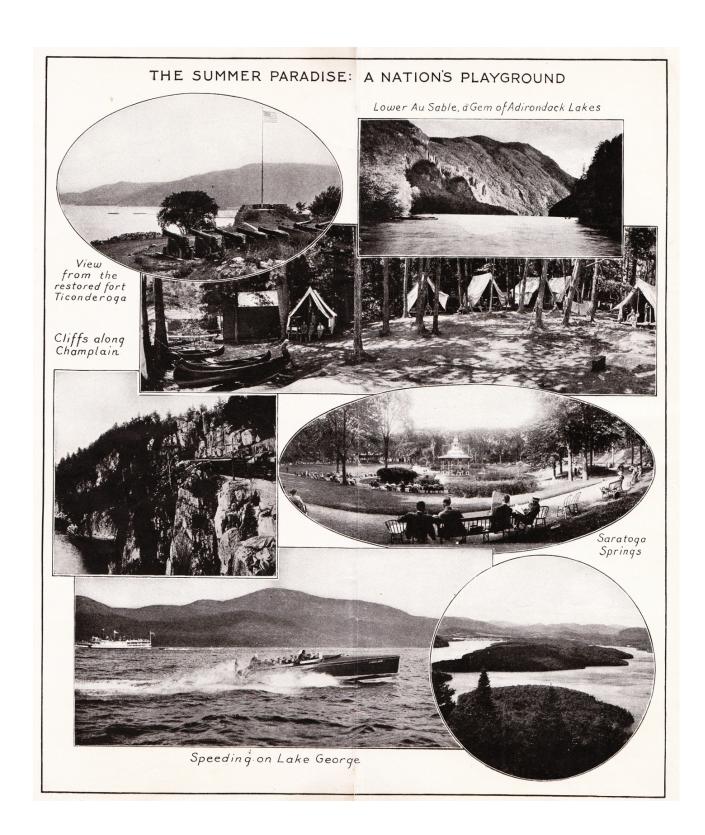
Yacht "The Mountaineer" will operate the following schedule, leaving Lake George Station daily:
10.00 a.m. and 3.00 p.m., making various landings on signal along the west shore as far as the Lake George Country Club and the Marion House, thence across to Pilot
Knob and Katskilli Bay, returning by the east shore.
5.00 p.m. and 8.00 p.m., making various signal landings along the east shore to Kattskill Bay and Pilot Knob, thence across to Marion House and the Lake George Country
Club, returning by the west shore.

The entire trip occupies about an hour and thirty minutes. Signal landings are made for the accommondation of patrons at all docks along the route, both public and private.
This arrangement furnishes a local service between suburban landings at which the larger boats do not make stops, as well as a delightful short excursion trip for parties visiting Lake George with only limited time at their command. No baggage is carried on this yacht.









THE QUEBEC, MONTREAL & SOUTHERN RAILWAY CO.

						*		
Sun'y Except Except Only Sun'y 8 4 2			Except Sun'y Sun'y Only 3	Except Sun'y 152 Except Sun'y 60	Daily 164	6-24-1917 Dominion Express Co. Except as noted(x)	Daily Sun'y	Except Sun'y Daily 61 165
8 00 4 40 7 3 8 25 5 10 8 00 9 20 20 20 20 20 20 20 20 20 20 20 20 20	7 ST LAMBERT (2) 10 LONGUEUL. 15 BOUCHERVILLE 20 VARENNES. 28 VERCHERES. 28 ST ANTOINE* 38 CONTRECŒUR 41 ST. ROCH. 51 SOREL. 62 VAMASKA EAST. 68 ST. FRANCOIS DU LAC. 68 PIERREVILLE. 75 LABAIE 83 NICOLET (3). 89 ST. GREGGIRE (4). 95 BECANCOUR. 100 RIVIERE GENTILLY* 112 BECQUETS.	Runs Tuesdays, Thurs- days and Saturdays. 6 9 2 2 2 2 2 8 8 8 6 2 2 2 2 2 2 2 2 2 2 2	6 20 8 29 56 6 6 6 6 6 6 6 6 6 6 6 6 6 6 6 6 6 6	P. M. P. M. A. M. 3 45 6 10 3 57 6 25 4 105 6 15 4 17 6 15 3 4 27 7 07 4 36 7 115 4 41 7 26 6 15 9 7 150 5 109	9 50 10f03 10f13 10f125 10 30	MIS. Arrive 0 SOREL	10 18 10 10 10 10 10 10 10 10 10 10 10 10 10	7 20 7 07 6 54 6 27 6 26 6 12 6 12 6 12 6 12 6 13 8 13 8 13 8 23 8 21 8 21 8 21

TABLE OF CONNECTIONS

(1) With G. T. Ry. for all points west. Joint station. (2) With C. V., G. T. & I. C. Rys. (also with D. & H. Trains). Joint station. (3) With I. C. Ry. Separate stations. (4) With G. T. Ry. Separate stations. (5) With L. & M. Ry. Joint station. (6) With G. T. & I. C. Rys. for certain trains. Joint station. (7) With G. T. and I. C. Rys. Separate station. (8) With C. V. Ry. Separate stations. (9) With C. V. Ry. Joint station. (10) With C. P. Ry. (joint station) and C. V. Ry. (separate stations). (11) With Rutland R. R. & G. T. Ry. Joint station.

(x) American Express Offices.

f Stop on signal.

*No agent. Light faced figures denote A. M. time.

Dark faced figures denote P. M. time.

GREENWIC	H AND JOHNSON	NVILLE RAILWAY		NAPIE	RVILLE JUNCTION RAILW	AY CO.
Sun'y Sun'y Sun'y Sun'y National Express	Except Except Except Sun'y Sun'y Sun'y 4 6	SALEM BRANCH		Dail;	National Express Co.	Daily 17
P. M. P. M. A. M. Mls. 4 55 12 15 7 40 Lv. 0 GREENWICH 5 604 12 724 7 749 EASTON. 5 108 12 78 7 753 . 5 ARCHDALE. 5 12 12 732 7 757 . VLY SUMMIT.	9606 3636 6611 9602 3632 6607 8658 3628 6603	Except Except Sun'y 14 12	Except Except Sun'y 11 13	7 1 7 2 7 3	MONTREAL. 5 (Bonaventure Sta.G.T.) 0 St. HENRI. 5 St. LAMBERT (4)	9 40
5716 12736 8701 WEST CAMBRID 5720 12750 8705 10 SOUTH CAMBRID 6 30 1 00 8 15 Ar. 15 JOHNSONVII 6 54 2 00 9 12 Ar. TROV (B.&M.R 7 27 2 30 9 55 Ar. ALBANY(BELT P. M. P. M. P. M. A. M.	GE 8f54 3f24 5f59 3 GGE 8f50 3f20 5f55 2 LLELv. 8 40 3 10 5 45 .R.) . Lv. 7 45 2 30 5 00	P. M. A. M. MIS. 4 10 8 50 0 GREENWICH 4 17 8 857 3 CENTER FALLS 4 17 9 14 8 EAST GREENWICH 4 40 9 20 11 GREENWICH JC	9f46 6f51	8 1 8 2 8 3 8 5	5 *LA TORTUE. 5 10 ST. EDOUARD. 15 NAPIERVILLE. 23 LACOLLE (2). 29 ROUSES POINT (3).	8f 49 8 38 8 30

TABLE OF CONNECTIONS

(1) With C. P. Ry. Joint station. (2) With G. T. Ry. Separate station. Rutland R. R. Separate station. (4) With Q. M. & S. and G. T. Rys. Joint station.

(3) D. & H. Co. and C. V. and G. T. Rys. (Joint station), also with *No Agent.

PUBLICATIONS

Issued by the Passenger Department

These publications are for gratuitous distribution, and will be mailed on receipt of postage.

"A Summer Paradise"
A handsome, illustrated descriptive guide to the famous northern resorts,
Saratoga Springs, Lake George, Lake Champlain, Hotel Champlain, Ausable Chasm, the Adirondacks, Cooperstown, Sharon Springs, Lake St.
Catherine, Lake Bomoseen, and many other charming, cool country places
along the picturesque D. & H. Includes information relative to hotels
and boarding houses and list of camps and cottages to rent or for sale.
Postage 6 cents.

"The Summer Paradise in Picture"
Covering the Adirondacks; Lake George and Lake Champlain in lovely photographic scenes. Postage 2 cents.

"Saratoga Springs"
"Saratoga Springs, the Gift of the Great Spirit," is the name of a handsome booklet describing the wonderful vogue of this famous resort from the earliest times to the present in words and pictures. Postage 1 cent.

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Directory of Golf Courses in the Summer Paradise. Describing the many excellent golf courses reached by D. & H. lines. Postage 1 cent.

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A Compilation of Facts and Traditions. In this book of 144 beautifully
printed and well illustrated pages is described every really important incident,
whether historical or traditional, enacted in the territory of the Delaware and
Hudson Company. Copies will be mailed, boxed and prepaid, on receipt of
25 cents, currency or stamps.

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Contains fine birds-eye view map of Adirondacks, Lake George and
Lake Champlain, with complete time tables. Postage 1 cent.

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Pocket size, containing accurate and revised map. Postage 1 cent.

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List of camps and cottages located in the Adirondack region for sale and to rent. Postage 2 cents.

Folders containing complete time tables, maps, etc., will be mailed on application. To any one sending a permanent address copies of each new issue as published will be sent without charge for a period of one year.

Albany, N. Y.

W. J. MULLIN, General Traffic Manager

			Т	RO	Y	AN	D	AL	BA	NY	T	O I	BING	GH	AM	TO	N							
Schedule of July 22, 1917	Except Sun'y	Excep'									t Excep					days 0				I	1	1	ī	I
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		12 25				10 30	12 40			2 0	4 50	7 10		12 25	8 45	12 40	4 50							
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GRAND CENT. TER HUDSON NAV. CO BOSTON (B. & M.) BOSTON (B. & A.) MONTREAL (G. T. RY.) RUTLAND TROY (BELT LINE)		8 30				8 30	7 20				2 00	4 48				7 20								
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TROY (BELT LINE) 0 ALBANY		7 30		12 30		2 30	4 30	5 10		0 40	0 41	44 45		8 20										
5 ELSMERE		7 48	3	12 46		2 48		5 2	6	6 2	8 3	12 03		8 f38	31 1 21		9 31							
8 SLINGERLANDS		7 54		12 45		2 51	ny	5 23	3	6 2	8 34	12 07		8 42	1 24		9 38							
9 FONT GROVE		8 03	3	12150	· · · · ·	3 02	lba n.	5 3	6	6 3	8 f41	12 18		8 52	1 1 151		9 141							
14 MEADOWDALE		8f07		1f11		3 08	ur A	5 4		6 5	8 53	12 j23		8 57	1 45									
8 SLINGERLANDS. 9 FONT GROVE. 11 VOORHEESVILLE. 14 MEADOWDALE. 18 ALTAMONT. 25 DUANE. MECHANICVILLE. Ly				THE SECOND		3 32	Ca	0 00		DESCRIPTION OF	a tensormen	14 40		9f16	1 00	5 05	10 00							
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27 DELANSONLv.		8 34												9 24		5f22								
31 ESPERANCE		8 41				5 41	5 30					1h00		9f31		Dar ng-								
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40 HOWE'S CAVE		9 03				4 09						1h19		9148		Jafe Viba ami								
45 COBLESKILL		9 23				4 29	5 50					1 32		10 02		0 00								
SHARON SPRINGSAr. SHARON SPRINGSLv.		10 07				3 50	6 35																	
50 RICHMONDVILLE		0 35				4 41						1j46												•••••
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67 WORCESTER		9 58				5 05						2j10		10 37 10 45		6 25								
71 MARYLAND		10 12				5 20						2h23		10 21										
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32 ONEONTAAr.		10 40				5 45	6 50					2 45		11 10		5 10 7 00								
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95 WELL'S BRIDGE	8 44	11 14				6 24	7200							11 f38		7 28								
4 SIDNEYAr.	9 05	11 35				6 42	7 31					:::::		11 54	:::::	7 43								
9 BAINBRIDGE	9 17	11 44		:::::	3 15 3 35	6 51	7 31 7h37			:::::				11 54 12 03	:::::	7 43	:::::							
19 NINEVEH	9 30 9 45	11 55 12 05			4 00	7 01	7h45 7 55							12 14		8 01					• • • • •			
21 HARPURSVILLE	9 49 9 455	12 10			4 23	7 20								12 29		8 16								
20 ONEONTA . Ar. 22 ONEONTA . Ar. 23 ONEONTA . Lv. 20 OTEGO . 25 WELL'S BRIDGE . 26 WELL'S BRIDGE . 26 WELL'S BRIDGE . 27 WASHIDA. Ar. 28 SIDNEY . Ar. 29 BAINBRIDGE . Lv. 29 BAINBRIDGE . Lv. 20 BAINBRIDGE . Lv. 21 HAFPURSVILLE . 25 BELDEN . 28 TUNNEL . 33 SANITARIA SPRINGS . 36 PORT CRANE . 38 INGHAMTON . Ar.	10 05	12 29			4 39	7 39								12 f47		8 31 8 39								
36 PORT CRANE	10 23	12 44	:::::		4 59	7 52										8 39 8 44 9 00								
Ar.	10 40 A. M.	P. M.	A. M.	P. M.	5 15 P. M.	8 10 P. M.	8 40 P. M.	P. M.	P. M.	P. M.	P. M.	A. M.		1 30 P. M.	Р.М.	9 00 P. M.	P. M.							
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Schedule of July 22, 1917	Except Sun'y 342	344	Except Sun'y 346												S	345	347 S	ccept un'y 349						
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f Stop on signal. h Stop to discharge passe					Schen	ectad	v and	Sidn	ev		Trai	ns 30 ns 38) will st 2 and 3 6 and	314 wi 390 w	ll stor ill sto	at H	udson ignal :	Nav	. Co. l	Pier to			sseng	

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Schedule of July 22, 1917	Except Sun'y			Daily 21	Except Sun'y	Except Sun'y	Daily Da	aily S	un'y	Sun'y	Except Sun'y	Except Sun'y	Daily 311			Sunda 333	ys Only		1	l					
Ils. Leave	A. M.	A. M.	A. M.	A. M.	A. M.	P. M.	A. M. A.	. M. I	P. M.	P. M.	P. M.	P. M.	P. M.		A. M	P. M		-	-	- -					
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Schedule of June 24, 1917	Sun'y	Sun'y	Daily	Sun'y	Sun'y		Daily	Sun'y		Sun	y Sun'y				,	s Only									
	502	506	660			516	518	504	520 P. M.	522	-		524		-	528 P. M.	530								_
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	P. M.	P. M.	•]	# L 141		-																			

Honesdale Branch: Carbondale to Honesdale

Schedule of June 24, 1917	Sun'y		Sun'y	Sun'y	Daily 669	Except Sun'y	Daily 511	Sun'y	Except Sun'y 515	Sun'y	Sun'y	521		unday 525		529				
lls. Leave		501 A. M.	503 P. M. 8 50		P. M.	509 P. M. 11‡20		513 A. M. 3 20	P. M.	517 P. M. 9 40	P. M.	A. M.	A. M.	P. M.	A. M.		 	 	 	
PHILADELPHIA NEW YORK (D., L. & W.)		·····	3 00				9 10 10s00		1r24		4 10	 2 00	·····	10 00		6 30			 	
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PHILADELPHIA (P. & R. & L. V.)			2 00				9 15				5 00	 2 15		9 15	12 30		 	 	 	
WILKES-BARRE,D.&H. WILKES-BARRE,D.&H. ARSONS. MINERS MILLS. HUDSON. COLUMNIC. COLUMNIC. WILLE. WILLE.	5 54 5 56 5 56 6 02 6 05 6 09 6 15 6 29 6 35 6 45 6 49 6 52 6 7 01 7 05 7 12 7 17 7 21	7 39 7 41 7 43 7 f47 7 f50 7 54 7 59 8 03 8 09 8 13 8 20 8 25 8 29 8 32	9 15 9 19 9 21 9 23 9 27 9 f30 9 34 9 43 9 49 9 f52 10 00 10 05 10 12 10 12 10 25 10 32	11 45 11 49 11 52 11 56		12 334 12 342 12 542 12 542 12 503 1 1 1 1 1 1 1 1 1 1 1 1 1 1 1 1 1 1 1	3 10 3 14 3 16 3 18 3 525 3 34 3 35 4 10 4 13 4 13 4 23 4 28 4 44 4 44	4 44 44 44 44 44 44 44 44 44 44 44 44 4	6 20 6 24 6 30 6 35 6 40 6 51 6 54	8f 45 8 49 8 53 8 57 9 03 9 15 9 25 9 29 9 34 9 38 9 42 9 45 9 45 9 55	10 25 10 29 10 31 10 33 10 37 10 44 10 49 10 53 11 62 11 9 11 24 11 29 11 35 11 40	7 49 7 51 7 53 7 f57 8 f00 8 04 8 09 8 13 8 f22 8 30 8 35 8 42 8 46 8 45 8 45 8 45 8 55 8 f58 9 02 9 07 9 10	11 49 11 52 11 56 12 01 12 05 12 f08	1 49 1 51 1 53 1 57 2 08 2 12 2 18 2 2 18 2 2 18 2 2 2 3 2 2 3 2 2 4 2 2 2 5 3 3 0 3 0 3 0	7 04 7 06 7 08 7 712 7 712 8 7 24 8 7 34 7 73 7 745 7 750 7 750 8 03 8 03 8 13	11 05 11 09 11 12 11 14 11 18 11 f21 11 29 11 33 11 39 11 f43 11 50 12 00 12 04 12 15 12 20 12 20 12 23				
5 CARBONDALE. Lv. 1 FOREST CITY 7 UNIONDALE. 9 HERRICK CENTRE 2 BURNWOOD. 5 ARARAT 0 THOMPSON. 14 STARRUCCA. 9 STEVENS POINT. 0 BRANDT.	7 35 7 49 8 00 8 05 8f11 8 18 8 27 8 36 8 46 8 51				12 30 12 44 12 55 12 59 1 f 05 1 12 1 21 1 23 1 f 38 1 42		4 55 5 09 5 20 5 25 5 131 5 38 5 47 6 6 10													
1 JEFFERSON JUNCTION LANESBORO JUNCTION 2 LANESBORO. COLUMBIA GROVE 31 WINDSOR 4 EAST WINDSOR 9 CENTRE VILLAGE	8 57 9f 05 9 14 9 21 9 31				1 50		6 16 6f24 6 32 6 39 6 49 7 00											 		
3 NINEVEH. Ar 7 BINGHAMTON Ar 80 ONEONTA. COOPERSTOWN. 12 ALBANY 12 TROY	10 40						8 25 11 00 11†28					 						 	 	
SCHENECTADY SARATOGA SPRINGS RUTLAND MONTREAL (G. T. RY.) BOSTON (B. & M.) BOSTON (B. & A.) AFRIV	6 00			P. M.	Р. М	P. M.	7 29					 				A. M	 	 	 	

PREVENT FOREST FIRES

FOREST FIRES cause a loss not only to the owner of the timber destroyed, but also to every member of the community.

YOU SHARE THIS LOSS.

The State Conservation Commission is doing its best to prevent forest fires: The Delaware and Hudson Company is taking steps to prevent the escape of sparks and coals from its locomotives, but IT TAKES THE HELP OF EVERY PERSON WHO GOES INTO THE FOREST FOR BUSINESS OR PLEASURE TO MAKE THESE MEASURES EFFECTIVE.

EXERCISE THE SAME CARE WITH FIRE IN THE FOREST THAT YOU WOULD TAKE WITHOUT QUESTION IN YOUR OWN HOME OR IN THE CITY. THE LAW REQUIRES IT.

Write to the Conservation Commission, Albany, N. Y., for the bulletin on "Forest Fires," which will tell you about the danger of fires and how to prevent them. Send for a copy of the fire laws.

1,000,000 Lines of Wire

TELEGRAMS

For transmission over the wires of

Western Union Telegraph Company

Accepted at public telegraph stations of this road

NIGHT LETTERS

Of Fifty Words or less handled at the regular rate of a 10-word message

DAY LETTERS

Of Fifty Words or less at one and one-half times night letter rate

RAILROAD CONNECTIONS

ALBANY, with New York Central Railroad, West Shore R. R., Boston & Albany R. R., Hudson Nav. Co. Steamers, Day Line Steamers, Schenectady Rwy. and Albany Southern R. R.
BALDWIN, with Lake George Steamers.
BALLSTON SPA, with Schenectady Rwy., Hudson Valley Rwy., Eastern New York R. R.
BINGHAMTON, with Erie R. R., Delaware, Lackawanna & Western R. R.
CARRONDALE, with Boston & Maine R. R.
FORT EDWARD, with Budson Valley Rwy.
FORT TICONDEROGA, with Rutland R. R.
GLENS FALLS, with Hudson Valley Rwy.
GREENWICH JUNCTION, with Greenwich & Johnsonville Rwy.
HONESDALE, with Fire R. R.
HUDSON FALLS, with Hudson Valley Rwy.
LAKE GEORGE, with Hudson Valley Rwy.
LAKE GEORGE, with Hudson Valley Rwy.
LAKE, with New York Central Railroad.
MECHANICVILLE, with Boston & Maine R. R., Hudson Valley Rwy.
MONTCALM LANDING, with Lake Champlain Steamers.
MOOERS JUNCTION, with Grand Trunk Rwy., Rutland R. R.
ONEONTA, with Ulster & Delaware R. R., C. & C. V. R. R.

PORT KENT, with Keeseville, Ausable Chasm & Lake Champlain R. R. PLATTSBURG, with Lake Champlain Steamers.

PLATTSBURG, with Lake Champlain Steamers.
PITTSTON, with Central R. R. of New Jersey.
RUTLAND, with Rutland R. R.
ROUSES POINT, with Grand Trunk Rwy., Rutland R. R., Central Vermont R. R.,
Napierville Junction Rwy.

SARATOGA SPRINGS, with Boston & Maine R. R., Schenectady Rwy., Hudson
Valley Rwy.

SARATOGA SPRINGS, with Boston & Maine R. R., Schenectady Rwy., Hudson
Valley Rwy.

SARAMAC LAKE, with New York Central Railroad,
SCHENECTADY, with New York Central Railroad,
SCHENECTADY, with New York Central Railroad,
SCHENECTADY, with New York, Ontario & Western R. R.
SIDNEY, with New York, Ontario & Western R. R., Central R. R. of New
Jersey, Erie R. R., New York, Ontario & Western R.

TROY, with Boston & Maine R. R., New York Central Railroad, Hudson Nav.
Co. Steamers, Hudson Valley Rwy., Schenectady Rwy.

VOORBEESVILE, with West Shore R. R.
WATERVLIET, with Schenectady Rwy.
WILKES-BARKE, with Lehigh Valley R. R., Pennsylvania R. R., Central R. R.
of New Jersey, Wilkes-Barre & Hazleton Rwy.

LAKE CHAMPLAIN—WHITE MOUNTAIN LINE

From July 9th to September 22d closer relations between White Mountains and Adirondack resorts are assured by the operation of through Pullman parlor car service between Bretton Woods and Burlington (weekdays), where connections are made with steamers of the Champlain Transportation Co. for Hotel Champlain and the Summer Paradise resorts reached by the D. & H.

Lv. Bretton Woods8.45 a. m.	Lv. Westport6.15 a. m.
Ar. Montpelier12.04 p. m.	Lv. Plattsburg6.45 a. m.
Ar. Burlington3.37 p. m.	Lv. Hotel Champlain7.00 a. m.
Lv. Burlington5.30 p. m.	Ar. Burlington8.15 a. m.
Ar. Hotel Champlain6.40 p. m.	Lv. Burlington11.05 a. m.
Ar. Plattsburg7.00 p. m.	Ar. Montpelier12.40 p. m.
Ar. Westport7.20 p. m.	Ar. Bretton Woods4.55 p. m.

From Hotel Champlain connections are made with both day and night trains for the Adirondacks, Ticonderoga, Lake George, Saratoga Springs, Albany and the Hudson River, and a similar service obtains in the opposite

STAGE CONNECTIONS

Hadley—Stage for Conklingville, Day, West Day, Edinburgh and Batchellerville leaves Hadley 3.00 p.m. weekdays.

THURMAN—Stage for Ywarrensburg, daily, on arrival of trains from the South. Stage for Athol, weekdays, on arrival of trains from the South.

THE GLEN—Carriages are in waiting for points on Friends Lake on arrival of all trains from the South.

RIVERSIDE—Stage for Pottersville and Schroon Lake 10.40 a. m. daily, and 4.00 p. m. weekdays. Stage for Chestertown, Horicon and Brant Lake 10.40 a. m. daily, and 4.00 p. m. weekdays. For Johnsburg and Weavertown 10.40 a. m. weekdays.

NORTH CREEK—Stage for North River, Indian Lake and Blue Mountain Lake leaves North Creek 11.00 a. m. weekdays. For Olmstedville, Minerva, Aiden Lair and Newcomb 11.00 a. m. weekdays. New autostage line leaves North Creek 11.00 a. m. daily, arriving Newcomb 12.45 p. m., Long Lake 3.15 p. m.

TICONDEROGA—Stage leaves for Schroon Lake and Hague 1.00 p. m. weekdays.

PORT HENRY—Stage leaves for Mineville 7.00 a.m. and 2.30 p.m. weekdays.

Westport—Stages leave for Elizabethtown 5.45 a. m. and 11.40 a. m., and 5.30 p. m. weekdays. Stage leaves Elizabethtown for Westport at 9.30 a. m., 3.30 p. m. and 8.00 p. m. weekdays. Stage for Lake Placid leaves Elizabethtown 8.00 a. m. weekdays. Returning, leaves Lake Placid 8.00 a. m. weekdays.

HUDSON NAVIGATION COMPANY HUDSON RIVER NIGHT LINES PEOPLES LINE

Steamers BERKSHIRE and C. W. MORSE

Arrive Albany	
D. & H. train leaves Dock at 6.50 a.m. daily with café car Leave Union Station at 7.15 a.m. daily.	for Lake George.
Passengers for points north of Fort Edward should take the North Albany Union Station 7.00 a. m. daily. Cafe car Albany to Plattsburg daily.	ern New Yorker at
Leave Albany	

NIGHT EXPRESS LINE

Steamers TROJAN and RENSSELAER

Leave New York, Pier 32, N. R		ONLY 9.00 p. m.
Arrive Troy	6.00 a.m.	
Leave Troy		
Leave Albany		
Arrive New York, Pier 32, N. R	8.30 a. m.	8.30 a. m.

HUDSON RIVER DAY LINE

Steamers WASHINGTON IRVING and HENDRICK HUDSON

Leave New York Daily, except Sunday, as follows:

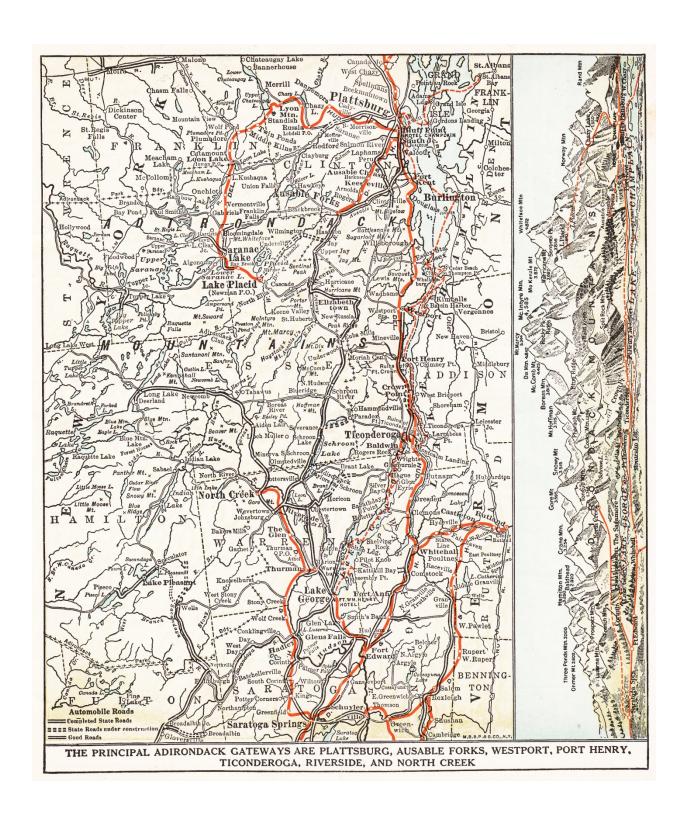
From Pier foot of Desbrosses Street, North River 8.	.40 a. m.
From Pier foot of 42d Street, North River	
From Pier foot of 129th Street, North River	
Arrive Albany	.10 p. m.

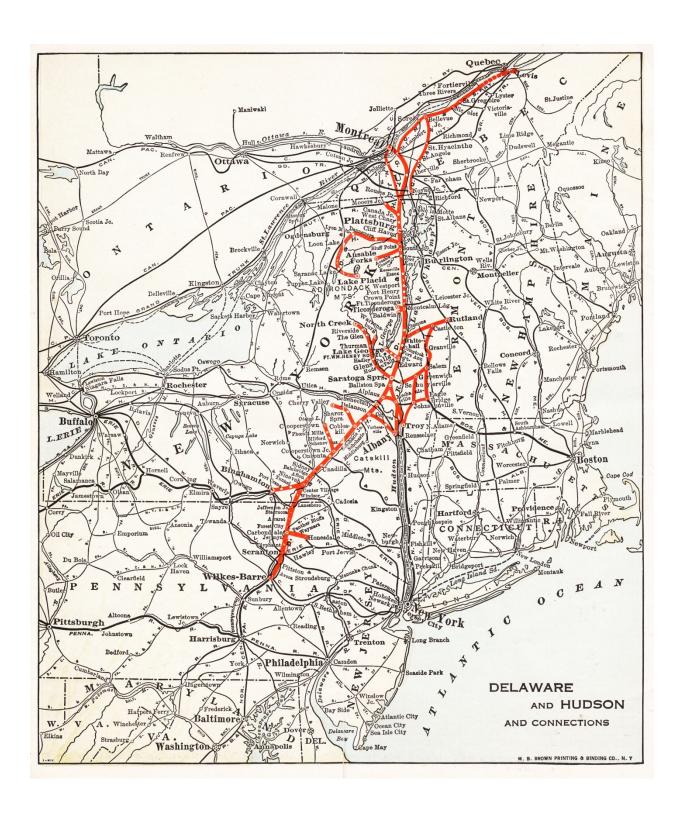
DAY BOAT TRAIN TO SARATOGA SPRINGS

Direct from boat landing at 7.00 p. m.

Leave Albany, Union Station (daily except Sunday)	
Arrive Saratoga Springs	8.25 p. m.
Arrive Lake George	9.35 p.m.
Southbound boat leaves Albany	8.30 a. m.
Arrive New York	6.00 p. m.

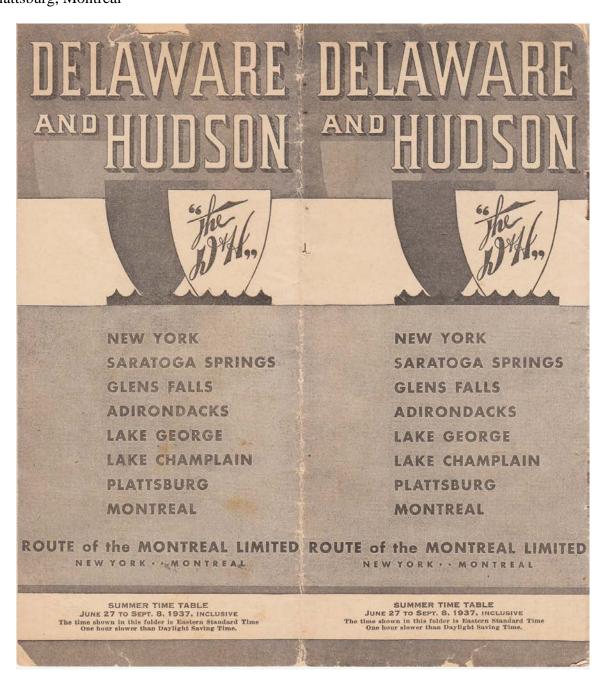
D. & H. Train 30 (see page 3) runs direct to steamer's dock, making immediate





D&H Time Tables and Laurentian booklet, all in the Alan G. Dustin Collection in the holdings of the Carbondale D&H Transportation Museum.

Delaware and Hudson Summer Time Table, June 27 to September 8, 1937, inclusive. New York, Saratoga Springs, Glens Falls, Adirondacks, Lake George, Lake Champlain, Plattsburg, Montreal





6:42 A.M.

AIR-CONDITIONED COMFORT

(Grand Central Terminal)

Arrive NEW YORK

On the fast, smart, all-Pullman MONTREAL LIMITED you enjoy de luxe transportation at its best. Modern airconditioning scientifically controls the temperature, humidity and purity of the air at all seasons. No dust, no grime, no noise . . . just restful, luxurious comfort that brings you to the end of your trip relaxed, fresh and alert.

Leav	NEW YORK 9:00 A.M.
	(Grand Central Terminal)
Arriv	e MONTREAL 6:30 P.M.
	(Windsor Station)
Leav	MONTREAL 10:00 A.M.
	(Windsor Station)
Arriv	e NEW YORK 7:40 P.M.

SCENIC TRIP DE LUXE

Delightful, comfortable ride along the Hudson, Lake Champlain, through the charming Adirondack region. Parlor Cars — Coaches — Dining Car



Parlor, Sleeping, Dining and Cafe Car Service-Saratoga and Champlain Division

Coaches on all trains, unless otherwise specified

+ Indicates air-conditioned cars.

NORTH BOUND SOUTH BOUND No. 1 Lv. New York (N.Y.C.). (Weekdays) Lv. New York (N.Y.C.). (11 55 pm.) Lv. Albany (D.&H.). 6 30 a.m. Lv. Albany (D.&H.). 6 30 a.m. Ar. Saratoga Springs. 7 50 Ar. Fort Edward. 8 23 Whitehall. 9 15 Whitehall. 9 15 Lv. Whitehall. 9 35 am. Parlor Cafe Car Whitehall to Albany. North Creek (180) 8 40 Riverside. 9 00 Lake George (162) 9 30 Saratoga Springs. 10 50 Ar. Albany. 12 05 p.m. New York (N.Y.C.) 3 30 No. 3 (Daily New York (N.Y.C.) 8 30 a.m. Albany (D.&.H.) 11 35 Saratoga Springs. 12 45 p.m. Wittehall 1 55 Lv. Lake George (No. 164), 3 30 p.... Parlor Car Lake George to New York. Glens Falls (164) 3 55 Saratoga Springs 4 45 Ar Albany. 6 00 New York (N.Y.C.) 9 10 No. 7 Lv. New York (N.Y.C.) 7 45 p.m. Albany (D.&H.) 12 25 a.m. Ar. Westport. 4 04 Cliff Haven (Bluff Point) 5 17 Platteburg. 5 22 Montreal. 8 00 Sleeping Cars New York to Platteburg, dally except that Fridays from New York, June 25 to Sept. 3, Inc., will operate on N.Y.C. Train No. 145 and D.&H. No. 13. Sleeping Car New York to Platteburg, dally except that Fridays from New York, June 25 to Sept. 3, Inc., will operate on N.Y.C. Train No. 145 and D.&H. No. 13. Sleeping Car Montreal to Albany. Sleeping Car Plattsburg to New York. Sleeping Car Westport to New York Sunday only, July 11 to August 29, Inclusive. Also Mondays July 5 and Sept. 6. Ready for occupancy 9 00 p.m. Sleeping Car Westponday night) July 12 to August Monday only (Sunday night) July 12 to August Monday only (Sunday night) July 12 to August Monday night) July 6 and Sept. 7. Ready for occupancy 9 30 p.m. Sleeping Car Fort Edward to New York. Car ready for occupancy at Fort Edward or ready for occupancy at Fort Edward I leeve Fort Edward 1 35 a.m., daily except Sunday. Sleeping Car Saratoga Springs to New York. Sunday only (Saturday night) August 1 to 29, inclusive. Ready for occupancy 9 30 P.M. No. 9—Montreal Limited (Daily) Lv. New York (N.Y.C.)...10 30 p.m.: *Lounge Car (Buffet) New York to Troy. *Lounge Car (Buffet) Whitehall to Montreal. *Lounge Car (Buffet) New York to Troy. *Lounge Car (Buffet) New York to Montreal. *Lounge Car (B No. 15 (Fridays only). Lv. New York (N.Y.C.). 2 35 p.m., Albany (D.&H.). 5 35 Saratoga Springs. 6 35 Westport. 8 51 Cliff Haven. 10 00 Plattsburg 10 10 No. 34—The Laurentian. (Daily) Lv. Montreal (D.&H.). 10 00 a.m. Plattsburg 11 46 Westport. 12 57 p.m. Saratoga Springs 3 20 Ar. Troy 4 16 New York (N.Y.C.) 7 40 No. 34-The Laurentian. (Daily) Springs. Dining Car Saratoga Springs to New York. Through coaches Montreal to New York. Lv. Lake George. 4 00 p.m. AParlor Cafe Car Lake George to Albany. Glens Falls. 4 23 North Creek (184) 2 40 Riverside. 3 00 Saratoga Springs. 5 07 Ar. Albany. 6 05 New York (N.Y.C.) 9 30 No. 48-186 (Sunday only) July 11 to August 29, inc. Also Mondays, July 5 and Sept. 6. Lv. Lake George. 10 30 p.m. Glens Falls. 10 56 Fort Edward. 11 13 North Creek (186). 9 30 Riverside. 9 52 Ar. New York (N.Y.C.) 6 20 a.m. Sunday Sund No. 41 Lv. New York (N.Y.C.) . 5 30 p.m. Albany (D.&H.) . 8 45 Ar. Saratoga Springs . 9 59 Glens Falls . . . 10 43 Lake George . 11 05 | Parlor Car New York to Lake George . Parlor Car New York to Saratoga Springs . First trip Friday , July 23; thence Wednesday . Thursday and Friday only . Last trip Sept. 3, Dining Car New York to Albany . CITY TICKET OFFICES TICKETS AND PULLMAN ACCOMMODATIONS NEW YORK, N. Y.: 33 West 42nd Street, Room 610. May be secured at D. & H. CITY TICKET OFFICE, 106 Drummond Bldg., 1117 St. Catherine Street, West NEW YORK, N. Y .: 17 John Street. NEW YORK, N. Y.: 4 West 33rd Street near Fifth Avenue. Montreal - Phone Marquette 8400 NEW YORK, N. Y.: 3 West 47th Street near Fifth Avenue. BROOKLYN, N. Y.: 155 Pierrepont Street. Also CANADIAN PACIFIC RY. CITY TICKET OFFICE, 201 St. James Street and Windsor Station



GRAND CENTRAL TERMINAL (N.Y.C.R.R.) NEW YORK

WINDSOR STATION (C. P. RY.) MONTREAL



SERVICE BETWEEN TWO OF THE FINEST TERMINALS IN AMERICA NEW YORK AND MONTREAL

THE POPULAR SHORT WAY



Lv. New York City from Grand Central Terminal

Lv. Montreal from Windsor Station

		1	READ DOWN	V				READ U	P		
Daily	Montreal Limited Daily	Except Sunday	The Laurentian Daily	Except Sunday		Except Sunday	The Laurentian Daily	Except Sunday	Daily	Montreal Limited Daily	Sunday Only
7	9	5	35	1	D. & H. TRAIN NOS.	2	34	18-4	8	10	18-38
7 45 PM r7 55 PM r8 12 PM 8 39 PM 8 55 PM 9 43 PM 10 44 PM	10 30 PM r10 40 PM r11 18 PM	12 30 PM r12 40 PM r12 57 PM 1 23 PM 1 37 PM 2 16 PM 3 09 PM 3 42 PM	9 00 AM r9 11 AM 9 52 AM n10 39 AM	11 55 PM 112r05 AM 1 13 AM 1 156 AM 2 39 AM	NEW YORK Lv. Grand Central Term (N.Y.C.)Ar 125th Street Yorkers Yorkers Harmon Harmon Peekskill Poughkeepsie Hudson Hudson	3 30 PM t3 19 PM 2 33 PM 1 44 PM	7 40 PM t7 28 PM 6 44 PM 5 55 PM	9 10 PM t8 58 PM t8 15 PM	6 57 AM t6 01 AM 5 13 AM	6 42 AM t6 31 AM t6 13 AM t5 45 AM	9 30 PM t9 19 PM t8 35 PM
11 15 PM	1 31 AM	3 42 PM	12 05 PM	3 10 AM	ArALBANY "Lv.	12 30 PM	4 25 PM	6 22 PM	3 46 AM	3 31 AM	6 35 PM
12 25 AM 1 33 AM 2 00 AM 2 05 AM 2 15 AM 3 15 AM 3 15 AM 4 04 AM 4 55 AM 5 17 AM 6 25 AM 7 45 AM 7 45 AM	Air Conditioned Pullman Cars only between New York and Montreal No coach passengers carried 7 15 AM 7 22 AM 7 30 AM	4 15 PM 5 30 PM 5 35 PM 5 59 PM 6 05 PM 6 05 PM 6 40 PM ×7 26 PM ×7 53 PM ×8 14 PM ×9 106 PM ×9 24 PM ×9 30 PM	12 10 PM 1 01 PM 1 13 PM 1 35 PM 2 01 PM 2 01 PM 3 05 PM 3 05 PM 4 25 PM 4 34 PM 6 02 PM 6 15 PM 6 15 PM	6 30 AM a6 35 AM 7 50 AM 7 58 AM 8 23 AM 8 33 AM 10 07 AM 11 08 AM 12 12 PM 12 38 PM 12 45 PM 1 40 PM	Lv. ALBANY (D.&H.)Ar. Ar. Saratoga Springs Lv. Lv. Saratoga Springs Ar. Ar. Fort Edward Lv. Lv. Fort Edward Ar. Whitehall. Lv. "Fort Ticonderoga" "Port Henry. Westport. "Port Kent." "Cliff Haven (Bluff Point) "Flattsburg. "Rouses Point. Montreal West Westmoutt. "Montreal West Westmoutt. "Montreal Windsor Sta. Lv. Vyia Can. Pac. Ry.	12 05 PM 10 50 AM 10 43 AM 10 15 AM 10 10 AM 9 35 AM 8 42 AM 8 05 AM 7 42 AM 6 50 AM 6 25 AM	4 10 PM 3 20 PM 3 12 PM 2 48 PM 2 46 PM 2 40 PM 1 40 PM 1 57 PM 12 12 PM 1 14 AM 10 12 AM 10 00 AM	6 00 PM 4 45 PM 4 38 PM 4 13 PM 4 07 PM 10 07 PM 11 22 AM 10 00 AM 10 00 AM 9 38 AM 9 33 AM 8 35 AM	3 55 AM 2 08 AM 2 02 AM 1 35 AM 1 30 AM 1 30 AM 1 1 1 30 AM 1 1 1 1 AM 1 1 1 1 AM 1 1 1 1 AM 1 1 1 1 AM 1 AM	3 26 AM Air Conditioned Pullman Cars only between Montreal and New York No coach passengers carried 9 42 PM 9 36 PM 9 30 PM	6 20 PM 5 10 AM 4 56 PM 4 28 PM 4 28 PM 3 50 PM 1 15 PM 12 00N'N 11 22 AM 10 00 AM 9 38 AM 8 35 AM
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For Explanation of Reference Marks, see Page 8.

Sundays one hour later.

PRIVATE BEDROOM CARS ON THE "MONTREAL LIMITED" TRAINS 9 AND 10.

Sleep in a real bed in a private room while traveling to or from New York and Montreal. The charge for single occupancy is only one adult rail ticket at fare of \$12.05 plus \$4.50 Pullman charge—the charge for double occupancy is two regular fares plus \$5.00 Pullman charge. Canadian revenue tax charged on all Pullman tickets sold in Canada.

Trains 9 and 10, the Montreal Limited (see above timetable):

For the five years immediately preceding his retirement, effective February 1, 1927, Wilbur C. Parker was conductor on Trains 9 and 10. In the biographical portrait of the man ("Liked 'Montrealer' Best"), published in the June 1, 1928 issue (pp. 163-164) in *The Delaware and Hudson Company Bulletin*, we read: "During the five years immediately preceding his retirement he was conductor of trains 9 and 10, the 'Montrealer' our crack train running between New York and Montreal. That, he begs to state, is not a job—it is a position of the highest rank, and is, beyond a doubt, the nicest train on the road." (p. 163)

(The Delaware and Hudson Company INSPECTION of LINES:: JUNE 7, 8, 9, 1929, pp. 82-83)

A very good account of "The Montreal Limited" is included in the 1929 D&D *Inspection of Lines* book. Here is that account:

New York-Montreal Passenger Trains

Much could be written, if space permitted, about the viçissitudes of travel by land and water in the early years of the Eighteenth Century, when it is recorded sixteen days were occupied making the journey from Montreal to New York.

It was not until the year 1849 that a through line was established by water and rail. The route was as follows: Between New York and Troy by steamboat, between Troy and Whitehall via Castleton by rail, between Whitehall and St. Johns, Quebec, through Lake Champlain and the Richelieu River, thence by rail between St. Johns and Montreal.

By November, 1875, the rail line had been completed between Albany and Montreal. For the purpose of celebrating this important accomplishment, the then President of your company, caused a special train to be operated from Albany to Montreal and return, having as his guests many persons prominent in business affairs of that time. The train which was composed of seven Wagner Palace cars, a director's car and a coach, left Albany at 9:00 A. M., November 16th, remained over night at Plattsburg, then proceeded to Montreal, and the return trip was made the following day, thus opening this route for public service.

At the outset and for many years, night trains in each direction were provided. They were operated between the Grand Central Terminal in New York City and Troy via the New York Central, between Troy and Rouses Point over your line, the tracks of the Grand Trunk Railway (now a part of the Canadian National System), being used between Rouses Point and Montreal, and the Bonaventure Station of the Grand Trunk was the Canadian Terminus. During this period four sleeping cars were regularly provided, in each direction, a fifth being added when necessary.

Commencing October 1, 1917, a radical change was made in the routing of these trains between Rouses Point and Montreal. Long investigation had convinced your officers that the growing traffic could be better served, and that the volume would be increased by utilizing the facilities of the Canadian Pacific in Montreal. Therefore, arrangements were made to route the trains over the tracks of a subsidiary, the Napierville Junction Railway, between Rouses Point and Delson Junction, where a connection was available over the main line of the Canadian Pacific, through Montreal West, into Windsor station. Prompt connections were possible in this way at Montreal West to and from Ottawa and other points west of Montreal, and east to the City of Quebec.

The most optimistic predictions as to the outcome of this change were realized. A fifth regular sleeping car was immediately necessary, and many extras

were required for week-end and holiday travel. The popularity of Montreal and Canada generally had, in the meantime, grown immensely.

By June 15, 1924, the demand for service had become so great that the trains were operated intact between the termini, carrying through sleeping cars to and from Ottawa and Quebec, as well as Montreal. The equipment was improved by the addition of a Club car. New compartment sleeping cars were substituted for the older type, and the train was named for the first time, being called "The Montreal Limited."

An indication of the demand for service is typified by the performance last New Years. On December 29th, the train from New York was run in four sections carrying 38 sleeping cars with 636 passengers. Returning from Montreal to New York on the night of January 1st, six sections were necessary to haul sixty sleeping cars, carrying 939 passengers who had spent the holiday in Montreal, Quebec and Ottawa.

The preponderance of this travel has always been over night, but in recent years the number of persons using the day trains has steadily increased. To meet this demand, commencing September 30, 1928, an all year-round through day service was inaugurated. The day train is called "The Laurentian."

About the engines used on this passenger trips to Canada, there is very good information on p. 86 of the 1929 Inspection of Lines book. Here are those data from *The Delaware and Hudson Company INSPECTION of LINES*:: JUNE 7, 8, 9, 1929, pp. 86:

Passenger Engine Runs

Of all your passenger train service provided, that between the cities of New York and Montreal is the most important, operating over the line between Troy or Albany and Montreal.

The high class type of travel, the keen competition with other lines, and the necessity for making the best time possible, consistent with safety and regularity, suggests that the fewer engine changes there are enroute, the more efficient and satisfactory will be the operation.

Recognizing the importance of this matter, it has been the ambition of the operating officers to lengthen out the engine runs, thereby avoiding intermediate coaling, watering and fire cleaning.

During the years 1915 to 1917, locomotives were run through from Albany or Troy to Rouses Point, a maximum distance of 193 miles. The limited capacity of locomotives available at that time, however, did not permit of entirely satisfactory operation.

Therefore, it became necessary to shorten the runs by changing at Whitehall, one locomotive being used from Albany to Whitehall, 78 miles, and a second from Whitehall to Montreal, 156 miles. While the train handling was improved by this method, difficulties were encountered in having engines coaled at Montreal, and it became necessary to put them in at Rouses Point for that purpose.

It was apparent that radical improvements were necessary in the type and capacity of locomotives and in the facilities for coaling and watering enroute.

In 1914, ten Pacific type passenger locomotives were purchased. They were equipped with 69" driving wheels, and with tanks having a capacity of 8,000 gallons of water and 14 tons of coal. The diameter of the wheels proved to be too small for the speed required and the water capacity inadequate. The diameter of the driving wheels was increased to 73", and the water capacity of the tanks to 11,000 gallons.

In 1924 the main track coaling and watering plant (described elsewhere), was erected at South Junction—71 miles from Montreal and 28 miles from Rouses Point. This did away with the difficulties encountered in coaling engines at Montreal, because with the larger capacity tanks they could readily make the trip from South Junction to Montreal and return to South Junction without any attention at Rouses Point.

As a result of the changes enumerated, it became evident that one locomotive could handle a train the entire distance between Albany or Troy and Montreal, and such method of handling was inaugurated in March 1924. Incidentally, the running time has been shortened, a number of locomotives have been released for use elsewhere, and the idle time of locomotives at terminals has been reduced about 20%.

NEW YORK Grand Central Teri 125th Street	1 "	1	3	35		4 10							-	SUND			
Grand Central Terr	1 "			33	5	15 Friday only	41	9	7	13	Sat. only	33	3-17	35	9	7	
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0 ALBANY, D. & H. 3.3 Menands. 3.9 Cemetery. 4.7 Colonie 5.0 Watervliet. 7.8 TROY. 7.2 Green Island. 5.8 Cohoes. 5.4 Round Lake. 6.8 Ballston. 5.3 Saratoga Springs. 6.4 Fort Edward. 6.5 Fort Edward. 7.8 Hudson Falls. 7.9 Clens Falls. 7.9 Lake George. 7.6 West Waterlord 8.8 Cohoes. 7.7 Whitehall. 7.4 Whitehall. 7.4 Whitehall. 7.4 Whitehall. 7.5 Port Henry. 7.6 Westyort. 7.8 Fort Edward. 8.9 Fort Henry. 8.1 Comstock. 8.1 Constock. 8.2 Constock. 8.3 Constock. 8.4 Whitehall. 9.5 Fort Ticonderoga. 9.5 Fort Ticonderoga. 9.6 Crown Point. 9.7 West Wilsboro. 9.8 Douglass. 9.8 Douglass. 9.8 Collift Haven (Buff Poi. 9.8 Port Kent. 9.9 Chazy. 9.7 Rouses Point. 9.7 Rouses Point. 10.7 Rouses Point. 11 Rouses Point. 12 Rouses Point. 13 Rouses Point. 14 Rouses Point. 15 Rouses Point. 16 Rouses Point. 17 Rouses Point. 18 Rouses Point.		16 38 46 46 46 46 46 47 77 77 77 77 77 77 77 77 77 77 77 77	11 48	The	No baggage service on this train No baggage service on this train 102 555555	Runs Fridays only July 2 to September 3, inclusive.	9 02 9 08 9 1355 9 1355 9 10 20 25 10 27 11 05 10 27 1	r between New York and Montreal. Montreal Limited	AM 12 25 12 37 12 43 12 59 12 13 37 12 59 12 23 25 25 25 25 25 25 25 25 25 25 25 25 25	1	oga Race Train. Runs Saturday July 31, August 7, 14, 21, 28, incl	AM 6 45 5 6 58 5 7 7 10 7 22 7 45 5 6 58 8 27 7 8 92 9 15 6 6 7 7 5 7 5 7 5 7 5 7 5 7 5 7 5 7 5		1 11 1 33 1 40 11 48 1 56 2 20 1 35 2 01 2 01 2 01 2 41 3 05 3 25 3 46 4 10	Pullman Cars only between New York and Montreal . Montreal Limited Pullman No coach passengers carried. Air Conditioned 99	AM 12 25 37 12 43 12 59 1 1 21 59 1 1 21 23 2 1 1 33 1 1 33 1 1 33 1 1 2 50 1 1 21 23 2 1 1 2 2 2 1 1 2 2 2 1 1 2 2 2 1 1 2 2 2 1 1 2 2 2 1 1 1 2 2 2 1 1 1 2 2 2 1 1 1 2 2 2 1	

| 180 | 184 | 2 | 185 | 181 | Mis. | TABLE 3 | 180 | 184 | 2 | 186 | Note A | 180 | 184 | 2 | 186 | Note A | 180 | 184 | 2 | 186 | Note A | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180 | 180

Note A—Train 186 will also run Mondays July 5 and September 6. For Explanation of Reference Marks, see page 8.

New York and Saratoga Springs

SATURDAYS ONLY

July 31, August 7, 14, 21 and 28.

	Grand Central Terminal	8.35 a.m.
	125th Street Station	8.46 a.m.
Ar.	Saratoga Springs	12.35 p.m.
Lv.	Saratoga Springs	6 10 nm
Ar.	New York, 125th Street Station	10.24 p.m.
Ar.	New York, Grand Central Terminal	10.35 p.m.

Lounge Car, Parlor Cars, Dining Car and Coaches

On Fridays July 30, August 6, 13, 20 and 27, a through sleeper to Saratoga Springs will be carried on train No. 145, The Ticonderoga, leaving Grand Central Terminal at 10.45 p.m., Standard Time, arriving at Saratoga Springs 3.22 a.m.

Lv. New York

						WEE	K DA	Y TRA	INS							SUND	AY T	RAINS	5					
files	TABLE 2	160	264		2	18 M	34	4	8	10				18 M	34	38	44	48	8	10				
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Train 264 stops at River St., Troy, 7.09 a. m. FORT EDWARD—GLENS FALLS—LAKE GEORGE READ DOWN READ UP 33 161 167 165 163 M M M M M M M M M										FAST DAY NIGH	TEL LET	gram EGRA TERS	AMS-	-for and and -50 w teles teles -50 w Deli	l at :	Station to bus report for 1 Transfor the next in new	iness of arts of times avel a	JNION Is of this Railroad these or social messages, a farrivals and departure of the progress of the tritimes the cost of a 10-word a little slower than factoring.						

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10 40 6*50 1*45 10+50 7*25 82.8 Ar.LAKE PLACID.Lv 11*30 11+50 8*25 PM PM PM PM PM	v5 44 8 25 39.2 Howe's Cave 10 17 5 06 12 22 25 5 54 8 41 44.7 Cobleskill 10 07 4 56 12 12
NAPIERVILLE JUNCTION RAILWAY	v6 03 8 52 49.9 Richmondville 9 58 4 47 12 04 v6 15 9 07 56.9 East Worcester 9 45 14 35 111 51
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8 35 10 49 Napierville 6 58 5 36 10 59 Lacolle (2) 6 46 5 25	7 39 10 51 108.4Bainbridge 8 04 3 09 10 21
*8 57 *11 09 ROUSES POINT (3). *6 33 *5 14	7 56 11 09 118.8 Nineveh. 7 45 2 52 10 05 h7 59 11 13 120.3 Harpursville 7 41 w2 48 10 01
K Stop on signal Saturdays and Sundays to discharge passengers from Montreal West, Westmount and Montreal.	7 766 11 09 118.8 Nineveh 7 45 2 52 10 05 h 7 59 11 13 120.3 Harpursville 7 41 w2 48 10 01 h 8 11 11 25 127.0 Tunnel 7 29 w2 37 9 49 h 8 20 11 34 132.5 Sanitaria Springs 7 18 w2 27 9 39
N Stop on signal Sundays to receive passengers for Montreal West, Westmount and Montreal.	PM AM Ar. Lv. AM PM AM
	Trains 302, 305, 309, and 313 will stop at site of Esperance Station (located between Delanson and Schoharie Junction), to pick up and discharge passengers.
(1) With C. P. Ry. Joint Stations. (2) With C. N. Ry. Separate Station. (3) D. & H. R. R. (Joint Station), also with C. N. Ry., C. V. Ry. and Rutland R. R.	PARLOR CAFE CARS ON SUSQUEHANNA DIVISION
(3) D. & H. R. R. (Joint Station), also with C. N. Ry., C. V. Ry. and Rutland R. R. Separate Station.	Third die die on sespending Division
SCHOHARIE VALLEY RAILROAD	No. 305 (Weekdays)—No. 313 (Sundays) Parlor Cafe CarBinghamton to Albany.
READ DOWN READ UP	
4 2 1 3 2 2 2 2 2 2 3 2 3 3	No. 308—(Daily) Parlor Cafe Car
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RUTLAND BUS COMPANY, INC. Service between Whitehall, N. Y. and Rutland, Vt.	READ DOWN READ UP
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Fair Haven	V V V V V V V V V V
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Fast Train Friday Afternoons for Lake Champlain Resorts

A week-end time-saver from New York to Saratoga Springs, Westport, Plattsburg, and favorite play-spots on Lake Champlain

With no loss of business time, you add hours to your time for recreation. You arrive at your favorite resort in the evening—then a good night's sleep and you are ready in the morning for a round of golf, or whatever your favorite sport or pastime may be. Through parlor car and dining car service to add to your comfort.

FRIDAYS ONLY

JULY 2 TO SEPTEMBER 3, INCLUSIVE

(Eastern Standard Time)

Lv New York (G.C.T.) 2.35 PM	Ar Fort Ticonderoga	8.05 PM
Lv Harmon	3.26 PM	Ar Port Henry	8.30 PM
Lv Albany	5.35 PM	Ar Westport	8.51 PM
Ar Saratoga Springs	6.35 PM	Ar Port Kent	9.42 PM
Ar Fort Edward	6.57 PM	Ar Cliff Haven	10.00 PM
Ar Whitehall	7.23 PM	Ar Plattsburg	10.10 PM

DELAWARE and HUDSON

AT BLUFF POINT ON LAKE CHAMPLAIN, N. Y.



In one of the most beautiful locations in the Adiron-dack-lake region. Every room a front room. Championship 18-hole golf course and additional sporty 9-hole course. Tennis, bathing, boating. Modern fireproof building-cottages-villas. Garage.

American Plan-Rates on application.

FRANK W. REGAN, Manager

N. Y. Office, Room 610, 33 W. 42nd St.

FORT WILLIAM HENRY HOTEL AT LAKE GEORGE, N. Y.



A modern, fireproof hotel. Beautiful view. Excellent cuisine. Comfortable rooms with splendid ventilation and plenty of sunlight. Bathing, tennis, boating. An ideal place to spend your vacation or a week-end.

American and European Plan-Rates on application.

J. L. MacDONALD, Manager N. Y. Office, Room 610, 33 W. 42nd St.

TRAVELERS

• It is now possible, before you start on your journey, to change the cash you must carry into American Express Travelers Cheques at the larger ticket offices when you secure your ticket. This precaution is recommended as the use of these Cheques protects you against loss should your funds be mislaid or stolen.

ANYWHERE...

You have noted the Railway Express Agency signs on the stations? Almost every station has one, for wherever the stations? Almost every station has one, for wherever the principal railway lines go, Railway Express goes too. Being nation-wide, with 23,000 offices spotting the continent, we can deliver shipments to practically anyone anywhere in America, at passenger train speed. Railway Express rates are low, and include pick-up and delivery by motor truck in all cities and principal towns, without extra charge. Two receipts, one to the shipper, another to Railway Express on delivery make suppages doubly sure. For service and indelivery, make sureness doubly sure. For service and information, phone your nearest Railway Express office.

RAILWAY EXPRESS AGENCY

NATION-WIDE SERVICE

SPECIAL SERVICE — Without extra charge, a Western Union messenger will pick up your Railway Express packages on phone call, or you can deposit them at any Western Union office.

INDEX TO STATIONS

STATIONS ARE INDEXED UNDER TABLE NUMBERS, NOT PAGE NUMBERS

Afton, N. Y	Table No. Harpursville, N. Y	Table No. Montreal, P. Q 1, 2, 6 Montreal West, P. Q 1, 2 Moosic, Pa 9	Table No. Richmondville, N. Y. 8 Riverside, N. Y. 3 Round Lake, N. Y. 1, 2 Rouses Point, N. Y. 1, 2, 6	Table No. Ticonderoga, N. Y 1, 2 R. R. Sta. Fort Ticonderoga Troy, N. Y 1, 2 Tunnel, N. Y 8
#Douglass, N. Y 1, 2 Dresden, N. Y	Jermyn, Pa 9 Jessup-Peckville, Pa 9	Napierville, P. Q. 6 Nineveh, N. Y. 8 North Creek, N. Y. 3 Olyphant, Pa. 9	Springs, N. Y1, 2, 3	Unadilla, N. Y
Carbondale, Pa. 9 Cemetery, N. Y. 1, 2 Central Bridge, N. Y. 8 Chazy, N. Y. 1, 2 Cliff Haven, N. Y. 1, 2 Gliens Falls, N. Y. 1, 2, 4	Lake George, N. Y. 1, 2, 4 Lake Placid, N. Y. 5 Lyon Mountain, N. Y. 5	Oneonta, N. Y. 8 Otego, N. Y. 8 Parsons, Pa. 9 Pittston, Pa. 9 Plattsburg, N. Y. 1, 2, 5	Schoharie Jet., N. Y. 7, 8 Scranton, Pa. 9 Sidney, N. Y. 8 Famith's Basin, N. Y. 1, 2 South Scranton, Pa. 9 Standish, N. Y. 5 St. Edouard, P. Q. 6	Watervliet, N. Y 1, 2 Wells Bridge, N. Y 8 West Chazy, N. Y 1, 2 Westmount, P. Q 1, 2 Westport, N. Y 1, 2 Westwaterford, N. Y 2 Whitehall, N. Y , 2
Cobleskill, N. Y. 8 Green Island, N. Y. 1, 2 Green N. Y. 1, 2 Green Ridge, Pa. 9 Groundlers, N. Y. 8 Groundlers, N. Y. 1, 2 Hadley, N. Y. 8 Grown Point, N. Y. 1, 2 Hadlenbecks, N. Y. 7			The Glen, N. Y 3	Wilkes Barre, Pa. 9 Wilksboro, N. Y

Non-Agency Station. Baggage for stations where there is no agent or a train arriving at destination when station is closed, must be claimed at baggage car door immediately on arrival at station; otherwise baggage will be carried to next station where agent is on duty and held for further orders.

EXPLANATION OF REFERENCE MARKS SHOWN ON TIME TABLE PAGES

- Daily, † Daily except Sunday. ‡ Daily except Saturday. § Sunday only.
 M Indicates mixed train.
 Mixed train Fort Edward to Lake George.
 Mixed train Lake George to Fort Edward.
 a Via connecting train No. 263 Troy to Cohoes.
 Stops weekdays except Saturdays to receive passengers.
 f Stops on signal.
 g Stop on signal to receive passengers for Plattsburg and points north and to discharge passengers from Albany and points south.
 h Stops only to discharge passengers from Albany and Delanson.
 j Stops on signal Monday to Friday; regular stop Saturday.
 n Stops on signal to receive passengers for Troy and beyond.

- p Stops only to discharge passengers from points south of Delanson.
 r Stops only to receive passengers.
 s Stops Saturdays only.
 t Stops only to discharge passengers.
 u Via connecting train Troy to Albany.
 v Stops to discharge passengers from Albany and Delanson and to pick up passengers for Binghamton.
 v Stops to signal to receive or discharge revenue passengers.
 x No baggage service on this train north of Whitchall.
 **Ostops on signal to receive or discharge passengers for or from Albany, Troy or points beyond.

LAKE GEORGE STEAMER SERVICE

EASTERN STANDARD TIME

Time Table in effect June 15 to September 15, inclusive, 1937.

		Daily	LANDINGS on Lake George	Daily		
		9 30 110 00 110 15 110 25 110 30	" Bolton Landing "	PM 3 30 12 40 12 30 12 15 12 10	 	
 	12.20	f10 40 f11 10 f11 15	"Pearl Point" "Hulett's Landing" "Sabbath Day Pt"	12 00 11 25 11 20	 	
 		111 30 111 45 112 00	"Silver Bay" "Hague" "Glenburnie"	ri 10 ri 00		
 		12 20	"Rogers Rock" ArBaldwinLv.	112 35 12 30	 	

No through tickets in connection with rail ticket will be sold. Purchase rail ticket to Lake George Station only. Purchase ticket on Steamer from Lake George to destination on Lake.

For information address Captain G. H. Stafford, Lake George, N. Y.

INFORMATION FOR TRAVELERS

REGARDING TIME TABLES—This Railroad is not responsible for errors in time tables, inconvenience or damage resulting from delayed trains or failure to make connections; schedules herein are subject to change without notice.

BUY TICKETS before boarding trains and avoid payment of extra charge.

CHILDREN under five years of age, when accompanied by parent or guardian, will be carried free, except that if the sole occupant of Pullman space, they will be charged half railroad fare and regular Pullman fare; children between the ages of five and eleven years, inclusive, will be charged half fare, and children twelve years of age and over will be charged full fare.

ADJUSTMENT OF FARES—In case of dispute with Conductors or Agents
pay fare required, take receipt and communicate with General Passenger Agent.

REDEMPTION OF TICKETS—Tickets unused or partly used, will be redeemed under tariff regulations at proper value.

BAGGAGE—This Railroad does not guarantee to forward baggage on same train with passenger, or within a given or specified time limit, but reserves the right to forward it upon a preceding or following train.

BAGGAGE MAXIMUMS—No single piece of baggage exceeding 300 pounds in weight or 72 inches in greatest dimension or single shipment exceeding \$2,500.00 in value will be checked. Free allowance subject to tariff stipulations as to contents, weight, value and size.

BAGGAGE LIABILITY LIMITED-Excess value to be declared and paid for

BAGGAGE FOR STATIONS WHERE NO AGENT IS ON DUTY must be claimed at baggage car door immediately on arrival of train at that station, otherwise it will be carried to the first station where agent is on duty, and held for further orders.

BIGYCLES (not Motorcycles), BABY CARRIAGES, DOGS AND GUNS are transported in baggage cars subject to tariff regulations. Dogs (except lap dogs in closed receptacles) will not be permitted in passenger coaches.

LOST ARTICLES—When articles are lost on trains, or left in waiting rooms at stations, owners should apply at once to Agent at station where they leave the train, or communicate with R. C. Walker, General Baggage Agent, Albany, N. Y. No responsibility is assumed for unchecked articles left in stations or cars.

CUSTOMS REGULATIONS—Residents of United States may take into Canada their wearing apparel and other personal effects and upon return to United States are entitled to have the same admitted free of duty. Residents of United States upon return must declare all articles acquired abroad, in their baggage or on their person, whether by purchase, by gift, or otherwise, and whether dutiable or free of duty.

free of duty.

Special provision is made in the case of sealskin garments. To save yourself embarrassment, inquire about the regulations of U. S. Customs Inspector at port of entry before you leave the United States, or if you are a traveler from Canada, before you enter the United States.

Baggage may be examined at Toronto, London, Ottawa, Montreal and Quebec by American customs officials and checked through to destination in the United States, thus avoiding inconvenience of examination at port of entry on the frontier. Baggage from the United States destined to cities just named will be carried through Canada in bond to destination, avoiding examination at Canadian frontier.

SPECIAL ARRANGEMENTS FOR ORGANIZED PARTIES—Members of organizations or organized parties contemplating trips will find it greatly to their interest to consult any representative of the Delaware and Hudson Railroad who will be pleased to quote fares and make all arrangements for the trip.

The New SAIRATOGA SPA

AT SARATOGA SPRINGS, N. Y.

OPEN THE YEAR 'ROUND



America's foremost continental-type "cure." Magnificent spa buildings in a thousand-acre park. Incomparable waters. Golf, tennis, riding and other summer sports. The Gideon Putnam, the new spa hotel, adjoins.

For descriptive booklet write Saratoga Springs Authority, Saratoga Springs, N. Y., or 80 Center Street, New York City.

OWNED AND OPERATED BY



THE STATE OF NEW YORK

The territory served by The Delaware and Hudson Railroad Corporation offers numerous advantages for the establishment

Natural resources of the territory contribute coal, iron ore, granite, graphite, garnet, feldspar, marble, sandstone, limestone, slate, sand and gravel, clay, and other materials, each of which has been developed to lend itself to a variety of uses.

Forest resources provide huge quantities of Spruce, Hemlock, and White Pine among the soft woods, and Beech, Birch, Maple and White and Red Oak among the hard woods.

Agricultural conditions are exceptionally favorable and dairying, orcharding, the raising of extensive crops of hay, potatoes, corn, and general truck gardening offer profitable pursuits.

The Port of Albany, within a 300 mile radius of which is located one-third of the population of the United States and to which 85 per cent of the ships of the world may navigate, is located midway between the extremities of the system, materially enhancing market possibilities.

Our Development Department is in a position to furnish deof the Development Department is in a position to furnish detailed information concerning the communities served. Phases of economical plant location, such as source of raw material, markets for finished products, and available warehouse space, will be furnished upon request.

GEO. E. BATES,

Assistant to Vice-President for Industrial Development,

The Delaware and Hudson Railroad Corporation, Albany, New York.

THE DELAWARE AND HUDSON RAILROAD CORPORATION	=
Passenger representatives of the D. & H. R. R. located at the offices named below will cheerfully furnish information with respect to tours, fares, time-tables, maps, illustrated literature, etc.	1
ALBANY, N. Y.—Telephone 3-1141 E. T. GillooleyGeneral Passenger AgentD. & H. Building	-
MONTREAL, P. Q.—Telephone Marquette 8400 N. J. Ferguson	-
NEW YORK CITY—Telephone Longacre, 5-7107, 5-7108 W. F. SHEEHAN General Agent, Passenger Dept	t
SCRANTON, PA.—Telephone 2-8751 J. J. COYLE Division Passenger Agent D. & H. Passenger Station T. J. McNamara Traveling Passenger Agent D. & H. Passenger Station	I GOO
AGENCIES OF THE FREIGHT DEPARTMENT ALBANY, N. Y.—Telephone 3-1141 C. F. BECK General Eastern Freight Agent D. & H. Building	I
ATLANTA, GA.—Telephone Walnut 5464 Monte Pickens, Jr Southeastern Freight Agent	CO
BOSTON, MASS.—Telephone Liberty 4106 and 4107 A. W. ACKLEY New England Freight Agent, 429–430 Chamber of Com-	st
BUFFALO, N. Y.—Telephone Washington 0853 J. N. VANDENBERGE. General Agent, Freight Department, 622 Ellicott Square	-
CHICAGO, ILL.—Telephone Wabash 8994 CHAS. H. CLARKGeneral Agent, Freight Department1102 Utilities Building	
CLEVELAND, OHIO—Telephone Main 1405 C. J. CONNOLLYGeneral Agent, Freight Department.1237–1238 Terminal Tower Building	
DETROIT, MICH.—Telephone Madison 5291 F. L. Dunn	
MONTREAL, QUE.—Telephone Marquette 7424 N. J. Ferguson General Canadian Freight Agent, 1117 St. Catherine St., West	DIG
NEW YORK, N. Y.—Telephone Vanderbitt 3-9017 and 3-9018 CHAS. G. HOWLAND. General Agent, Freight Department, 2114 Lincoln Bldg., 60 East 42nd St. PHILADELPHIA, PA.—Telephone Rittenhouse 1271	L
A. A. Gallagher General Southern Freight Agent. 1420-26 South Penn Square	OT A
PITTSBURGH, PA.—Telephone Grant 5516 THOMAS BYERS General Agent, Freight Department1824 Koppers Building	A MILE
SCRANTON, PA.—Telephone 2-8751 J. J. COYLEDivision Freight AgentD. & H. Passenger Station	1
ST. LOUIS, MO.—Telephone Chestnut 9178 W. R. St. John General Agent, Freight Department, 2083–2084 Railway Exchange Building	AT DED
Full information regarding Service, Rates, Routes, etc., can be obtained by applying to any of the named agencies.	N. A. A.
C. S. HALLENBECK, General Freight Agent, Albany, N. Y. J. A. FLANDERS, General Freight Agent, Albany, N. Y. F. NEIL HILLER, General Freight Agent, Albany, N. Y. FRED W. NYLAND, Coal Traffic Manager, Albany, N. Y. W. G. STORY, Freight Traffic Manager, Albany, N. Y.	T TITO
M. V. BECKSTEDT, Asst. General Traffic Manager, Albany, N. Y.	

J. T. LOREE,

Vice President and General Manager,

Albany, N. Y.

			F	ARE WI	LL BE F	OR		
Where		SEC	TION	Сомра	RTMENT	DRAWIN		
Berth is	Upper Berth	1 Person	or More Persons		or More Persons	1 Person	or More Persons	Bed- room #
\$2.00 2.25 2.50 3.00	\$1.60 1.80 2.00 2.40	\$2.80 3.15 3.50 4.20	\$3.20 3.60 4.00 4.80	\$4.00 4.50 5.00 6.00	\$6.00 6.50 7.00 8.50	\$5.00 5.75 6.25 7.50	\$7.00 8.00 9.00 11.00	\$3.60 4.05 4.50 5.40

■ Indicates charge for Section when occupied by one adult and one child under twelve years of age provided lower berth only is used for night occupancy.

■ Lower berth only prepared for night occupancy; when both berths are used, charge will be sum of the lower and upper berth fares.

Indicates charge for single bedroom occupied by only one person. If more than one person, charge will be 2 lower berth fares.

Minimum Number of Railroad Tickets Required for Exclusive Occupancy of Section, Drawing Rooms, Compartments or Bedrooms in Sleeping Cars and Drawing Room in Parlor Car.

Minimum Number of Adult Railroad Tickets Required Section.

	Railroad	Tickets Requ
Section		1
Single or Double Bedroom		î
Compartment, berth service		1 1-10
Drawing Room, berth service		11/4
Drawing Room, seat service		2 4
Drawing Room in Parlor Cars		2
		-

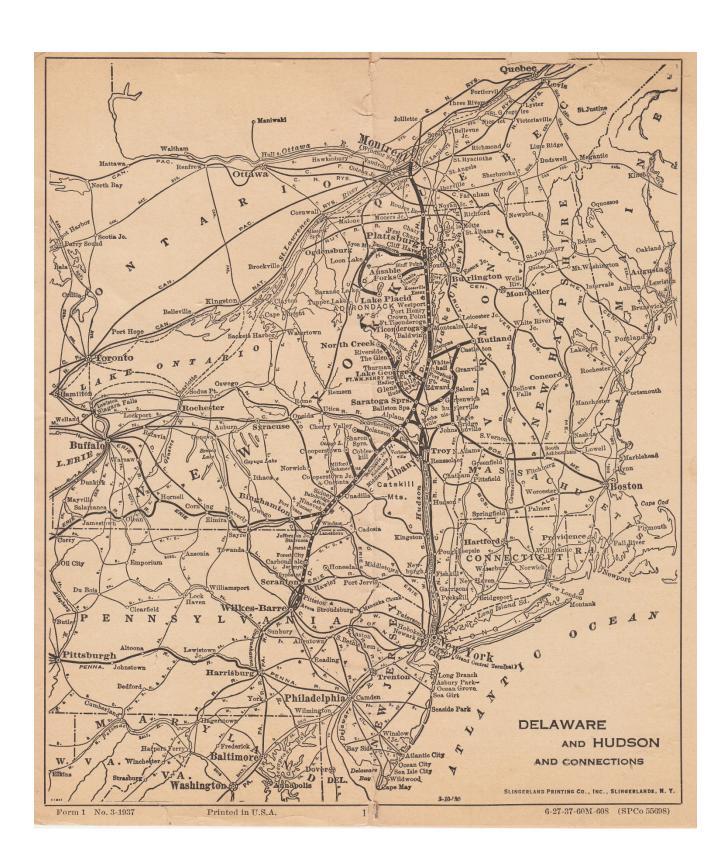
A collection of one-half (½) adult fare in addition to sleeping or parlor car space charge will be made for each berth or seat in sleeping car or seat in parlor car occupied exclusively by one or more children under five (5) years of age, accompanied by parent or guardian, except that such charge will not be made when such berth or seat in sleeping car is in the same section as that occupied by the parent or guardian. Only one-half (½) fare collection will be made when berth or seat is occupied by two or more children of such age.

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	Westport		69	20 20	.50		.35 1	:18	1 1	20	:1	: 12	1	-	20	:1	-	
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J. E. ROBERTS,

General Traffic Manager,

Albany, N. Y.



Delaware and Hudson Summer Time Table, June 25 to September 6, 1939, inclusive.

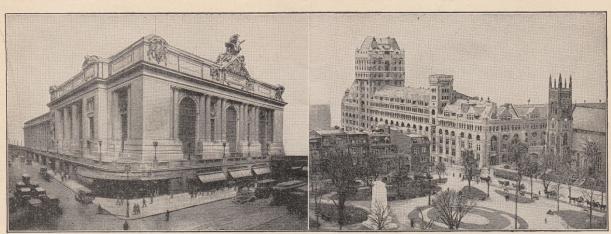
New York, Saratoga Springs, Glens Falls, Adirondacks, Lake George, Lake Champlain, Plattsburg, Montreal





Parlor, Sleeping, Dining and Cafe Car Service—Saratoga and Champlain Division Coaches on all trains, unless otherwise specified

★ Regularly assigned cars are air-conditioned.	
NORTH BOUND	SOUTH BOUND
No. 1 (Weekdays) Lv. New York (N.Y.C.) . 11 05 p.m. Lv. Albany (D.&H.) 6 30 a.m. Ar. Saratoga Springs 7 46 Ar. Fort Edward 8 18 Whitehall. 9 05 Whitehall. 9 05	No. 2 (Weekdays) Lv. North Creek (180) . 9 05 a.m. Farlor Cafe Car Whitehall to Albany. First Lake George (162) . 9 55 Whitehall . 10 05 Saratoga Springs . 11 17 Ar. Albany . 12 30 p.m. New York (N.Y.C.) 4 05
No. 3 (Daily) Lv. New York (N.Y.C.) 7 10 a.m. Albany (D.&.H.)	No. 4 (Weekdays) Lv. Lake George (No. 164). 3 05 p.m. Glens Falls (164). 3 30 Saratoga Springs. 4 30 Ar Albany. 5 45 New York (N.Y.C.). 9 12 Weekdays Parlor Car Lake George to New York. First trip June 26. Last trip September 6. Club Dining Car Whitehall to Albany.
No. 7 (Daily) Ly. New York (N.Y.C.) 7 46 p.m. Albany (D.&H)	No. 8 (Dally) Lv. Montreal (D.&H.) 7 45 p.m. Sleeping Car Montreal to Albany. Sleeping Car Plattsburg to New York. Sleeping Car Plattsburg to New York Sunday only, July 9 to August 27, inclusive. Also Tuesday July 4 and Monday Sept. 4. Ready Whitehall to Saratoga Springs 2 20 Ar. Troy 3 15 Ar. Albany. 3 16 Ar. Albany. 4 00 New York (N.Y.C.) 7 04 Sept. 3 18 p.m. Sleeping Car Port Ticonderoga to New York (Monday only (Sunday night) July 10 to August 28, inclusive. Will also run Wednesday (Monday night) September 5. Sept. 3 2 2 3 2 3 2 3 3 3 3 3 3 3 3 3 3 3 3
No. 9—Montreal Limited (Dally) Liv. New York (N.Y.C.). 10 30 p.m. Troy (D.&H.) 1 36 a.m. Montreal 7 30 **Lounge Car (Buffet) Whitehall to Montreal. **Sleeping Cars New York to Montreal. **Sleeping Cars New York to Montreal. **One 13 Double Bedrooms. **One 3 Compartments-2 D.R. **Two 12 Section-D.R.—2 Compartments. **Two 12 Section-D.R.—2 Compartments. **Pullman Cars only between New York and Montreal. No coach passengers carried.	No. 8 (Dally) Lv. Montreal (D.&H.) 7 45 p.m. Rouses Point 9 22 Plattsburg 10 12 Cliff Haven 10 18 Fort Ticonderoga 12 26 a.m. Whitehall 15 Fort Edward 1 50 Saratoga Springs 2 20 Ar. Troy 3 15 Ar. Albany 400 New York (N.Y.C.) 7 04 Sleeping Car Plattsburg to New York Sunday only (Sunday night) July 10 to August 27, inclusive. Also only Glady night of the Company of the Company only (Sunday night) July 10 to August 28, inclusive. Will also run Wednesday (Tuesday night) July 5 and Tuesday (Monday night) September 5. Ready for occupancy 9 30 p.m. Sleeping Car Fort Edward to New York Monday only (Sunday night) July 10 to August 28, inclusive. Will also run Wednesday (Tuesday night) July 5 and Tuesday (Monday night) September 5. Sleeping Car Fort Edward to New York Monday only (Sunday night) July 10 to August 28, inclusive. Will also run Wednesday (Tuesday night) September 5. Sleeping Car Fort Edward to New York September 5. Sleeping Car Westport to New York Sleeping Car Fort Edward 1 50 a.m., and I wester September 5. Sleeping Car Westport to New York Sleeping Car Fort Edward 1 50 a.m., and I wester September 5. Sleeping Car Westport to New York Sleeping Car Fort Edward 1 50 a.m., and will eave Fort Edward 1 50 a.m., and will eave Fort Edward 1 50 a.m., and ye seep tsunday. Sleeping Car Fort Edward 1 50 a.m., and ye seep tsunday. Sleeping Car Fort Edward 1 50 a.m., and ye seep tsunday and will leave the fort Edward 1 50 a.m., and ye seep tsunday. Sleeping Car Fort Edward 1 50 a.m., and ye seep tsunday and will leave the fort Edward 1 50 a.m., and ye seep tsunday and will leave the fort Edward 1 50 a.m., and ye seep tsunday and will leave the fort Edward 1 50 a.m., and ye seep tsunday and yell leave the fort Edward 1 50 a.m., and ye seep tsunday and yell leave the fort Edward 1 50 a.m., and yell eave the fort Edward 1 50 a.m., and yell eave the fort Edward 1 50 a.m., and yell eave the fort Edward 1 50 a.m., and yell eave the fort Edward 1 50 a.m., and yell eave the fort Edward 1 50 a.m., and yell eave the fort
No. 13 (Saturday only) (Friday night from New York) (Yerlday 25, last trip Stephing Cars New York to Saratoga Springs. From New York (Yerlday 25, last trip Stephing Cars New York to Westport. (Yerlday night from New York) (Yerlday 25, last trip Stephing Cars New York to Westport. (Yerlday New York) (Yerlday 15, last trip Stephing Cars New York to Westport. (Yerlday 16, last 16,	No. 10—Montreal Limited. (Dally) Lv. Montreal (D.&H.) 9 30 p.m. Ar. Troy 320 a.m. New York (N.Y.C.) 6 42 **Indicates air-conditioned cars.* **Lounge Car (Buffet) Montreal to Whitest, **Lounge Car (Buffet) Montreal to Whitest, **Lounge Car (Buffet) Troy to New York. Pullman Cars only between Montreal and New York. No coach passengers carried.
No. 15 (Fridays only). June 30 to Sept. 1, inclusive. Lv. New York (N.Y.C.). 2 45 p.m., Albany (D.&H.). 5 50 Ar. Saratoga Springs. 9 45 Cliff Haven. 10 10 Plattsburg. 10 20	No. 34—The Laurentian. (Dally) Lv. Montreal (D&H.)
No. 33 (Sunday only) (Saturday night from New York) LV. New York (N Y.C.). 11 05 pm. Albany (D.&H.). 12 05 pm. Albany (D.&H.). 7 57 Fort Edward. 8 27 Glens Falls. 8 50 Lake George. 9 15	No. 36—The Champlain. (Daily) Lv. Plattsburg. 11 30 a.m. Port Kent. 11 52 Westport. 12 39 p.m. Port Henry 12 59 Fort Ticonderoga. 1 23 Fort Edward. 2 38 Saratoga Springs. 3 10 Ar. Troy. 4 00 New York. 7 40
No. 35—The Laurentian. (Daily) Lv. New York (N.Y.C.) 9 15 a.m. *Parlor Car New York to Montreal.	No. 44 (Sunday only) Lv. Lake George . 4 35 p.m. *Parlor Cafe Car Lake George to Albany. Clens Falls . 4 55 Parlor Cafe Car Lake George to New York. First Sorth Creek (184) . 3 25 Parlor Car Lake George to New York. First trip June 25, last trip August 27, Riverside . 3 45 Parlor Car North Creek to New York. Sundays only July 9 to August 27, inclusive. Ar Albany . 6 45
No. 39—The Champlain. (Daily) Lv, New York (N.Y.C.). 8 30 a.m. Troy (D.&H.). 1 39 Ar. Saratoga Springs. 12 30 p.m. Lake George. 1 45 Riverside. 2 30 North Creek. 2 55 Fort Tieonderoga 2 10 Westport. 2 54 Port Kent. 3 38 Plattsburg. 4 03 Parlor Car New York to Plattsburg. Parlor Car New York to North Creek. First trip June 28, last trip Sept. 2. Parlor Car New York to Saratogs prings. Adally July 31 to Sept. 2, inclusive. Dining Car New York to North Creek. Through Caceh New York to North Creek. Through Coach New York to Plattsburg.	Ar. Albany 6 45 Ar. Albany 6 45 New York (N.Y.C.) 10 00 No. 46-186
No. 43 (Weekdays) Lv. New York (N.Y.C.) 5 30 p.m. Albany (D.&H.) 8 45 Ar. Saratoga Springs 9 58 Glens Falls. 10 43 Lake George. 11 05 Parlor Car New York to Lake George. Parlor Car New York to Saratoga Springs. First trip Friday, July 21; thence Wednesday, Thursday and Friday only. Last trip Sept. 1.	No. 48–186 Tuesday only, July 4 and Monday Sept. 4. Lv. Lake George (48) 10 45 p.m. Sleeping Cars Lake George to New York. Glens Falls 11 11 Fort Edward 11 23 Sleeping Cars North Creek to New York Sleeping Cars Saratoga Springs to New York Sleeping Car Saratoga Springs to New York Sleeping Cars Saratoga Springs Sleeping Cars ready for occupancy 9 00 p.m. Ar. New York (N.Y.C.) 6 15



GRAND CENTRAL TERMINAL (N.Y.C.R.R.) NEW YORK

WINDSOR STATION (C. P. RY.) MONTREAL



SERVICE BETWEEN TWO OF THE FINEST TERMINALS IN AMERICA NEW YORK AND MONTREAL

THE POPULAR SHORT WAY

Lv. Montreal from Windsor Station

Lv. New York City from Grand Central Terminal

	READ DOWN READ UP												
Daily	Montreal Limited Daily	Except Sunday	The Laurentian Daily	The Champlain Daily	Except Sunday		Except Sunday	The Champlain Daity	The Laurentian Daily	Except Sunday	Daily	Montreal Limited Daily	Sunday Only
7	9	5	35	39	1	D. & H. TRAIN NOS.	2	36	34	18-4	8	10	18-38
7 40 PM 7 500 PM 8 34 PM 8 34 PM 9 39 PM 11 15 PM 12 20 AM 1 1 32 AM 1 1 32 AM 3 10 AM 3 10 AM 3 10 AM 3 10 AM 3 10 AM 3 15 AM 5 13 AM 6 14 AM 7 7 42 AM 7 7 50 AM	10 30 PM 10 40 PM 11 22 PM 11 31 AM Air Con- ditioned. Pullman Cars only between New York and Montreal	1 05 PM 1 15 PM 1 57 PM 2 44 PM 3 26 PM 3 58 PM 4 15 PM 6 05 PM 6 05 PM 6 05 PM 6 05 PM 6 440 PM 7 26 PM 7 7 53 PM 8 9 04 PM 9 24 PM 9 24 PM 9 29 PM	9 15 AM 9 26 AM 10 07 AM 12 19 PM 12 24 PM	8 30 AM 8 40 AM 9 21 AM 10 08 AM 11 34 AM 11 39 AM 12 30 PM 11 25 PM 12 55 PM 12 15 PM 12 234 PM 12 37 PM 13 93 AM 10 12 37 PM 13 95 PM 14 25 PM 15 PM 16 PM 17 PM 17 PM 17 PM 17 PM 17 PM 18 PM	11 05 PM 11 15 PM 11 13 PM 11 203 AM 12 203 AM 1 22 267 AM 3 3 00 AM 6 30 AM 6 35 AM 7 54 AM 8 28 AM 9 59 AM 10 51 AM 11 52 PM 11 52 PM	NEW YORK Ly. Grand Central Term.(N.Y.C.)Ar. 125th Street. "Yonkers." Nyonkers. "Peekskill." Peekskill. "Peekskill." Hudson. "Hudson." Ar. ALBANY. Ly. Troy. "" Ly. ALBANY. (D. & H.)Ar. Troy. "" Ar. Saratoga Springs Ly. Ly. Saratoga Springs Ar. Ar. Fort Edward. Ly. Ly. Fort Edward. Ar. Fort Edward. Ar. Fort Edward. Ar. Whitehall. Ly. Fort Teonderoga "Port Henry." Westport. "Port Kent." Port Kent. "Plattsburg." Rouses Point. "Rouses Point." Montreal West. "Westmount." Westmount. "Westmount." Montreal West. "Westmount." Westmount. "Montreal West. "Montreal Windsor Sta. Ly. Via Can. Pee. Ry.	4 05 PM 3 56 PM 3 08 PM 2 17 PM 12 30 PM 12 30 PM 11 17 AM 11 10 AM 10 05 AM 10 05 AM 9 07 AM 8 06 AM 7 10 AM 6 50 AM 6 45 AM	7 40 PM 7 30 PM 6 46 PM 4 25 PM 4 25 PM 4 20 PM 3 10 PM 3 10 PM 2 36 PM 2 38 PM 1 23 PM 1 23 PM 1 1 30 AM	7 40 PM 7 30 PM 6 46 PM 5 15 PM 4 25 PM 4 15 PM 1 5 30 PM 1 1 30 PM 1 1 10 PM 1 1 20 PM 1 1 20 PM 1 1 22 AM 1 0 27 AM 1 0 15 AM	9 12 PM 9 01 PM 8 17 PM 7 28 PM 6 43 PM 6 10 PM 4 30 PM 4 23 Ph 3 52 PM 3 52 PM 3 552 PM 1 155 PM 11 155 PM 11 155 PM 11 20 AM 10 00 AM 10 00 AM 9 33 AM 9 33 AM 8 35 AM	7 04 AM 6 05 AM 5 14 AM 4 00 AM 3 15 AM 2 15 AM 1 15 A	6 42 AM 6 31 AM 6 12 AM 5 45 AM 3 31 AM 3 26 AM Air Conditioned, Pullman Cars only between Montreal and New York No coach pasengers carried 3 44 PM 9 37 PM 9 30 PM	7 05 PM 6 55 PM 6 55 PM 6 55 PM 6 00 PM 6 00 PM 1 05 P
	9â22 1 1 10 11 1 1 1 1 1 1 1 1 1 1 1 1 1 1 1		8502 11502			Lv. Montreal, Windsor Sta. Ar. "Montreal West." Ar. Quebec, Palais Sta. Lv. "OTTAWA, Union Sta" "Murry Bay (C. S. S. Lines)"			10555			9800 6820 9825 8845 6805 9810 5800 3820 5830	

For Explanation of Reference Marks, see Page 8.

PRIVATE BEDROOM CARS ON THE "MONTREAL LIMITED" TRAINS 9 AND 10.

Sleep in a real bed in a private room while traveling to or from New York and Montreal. The charge for single occupancy is only one adult rail ticket at fare of \$12.05 plus \$4.75 Pullman charge — the charge for double occupancy is two regular fares plus \$5.25 Pullman charge. Canadian revenue tax charged on all Pullman tickets sold in Canada.

							WEEK	DAY TRA	AINS					1	S	UNDA	Y TF	RAINS		
Ailes	TABLE 1	263	1	3	39	35	5	15 Friday only	43	7	9	13	Sat.	33	3-17	39	35	7	9	
	NEW YORK Lv. Grand Central Term 125th Street	AM	PM 11 05 11 15	AM 7 10 7 20	AM 8 30 8 40	AM 9 15 9 26	PM 1 05 1 15	PM 2 45	PM 5 30	PM 7 40 7 50	PM 10 30 10 40	Troy July 1 carried.	AM 8 30 8 40	PM 11 05 11 15	AM 7 10 7 20	AM 8 30 8 40	AM 9 15 9 26	PM 7 40 7 50	PM 10 30 10 40	
67.4	ALBANY, D. & H. Lv. Menands. Cemetery Colonie. Watervliet. TROY Green Island. Cohoes. West Waterford Mechanicyllie. Round Lake. Ballston. Saratoga Springs. Ar. Saratoga Springs. Ar. Fort Edward. Lv. Hudson Falls. Ar. Glens Falls. Ar. Glens Falls. Ar. Comet Edward. Lv. Smith's Basin. Fort Edward. Lv. Smith's Basin. Fort Edward. Lv. Smith's Basin. Fort Henry. Whitehall. Lv. Dresden. Fort Ticonderoga. (Ticonderoga. (Ticonderoga.) (Ticonderoga.) (Tronderoga.) Fort Henry. Westport. Essex. Willsboro. Douglass. Port Kent. Cliff Haven (Bluff Point) Plattsburg. Ar. Rouses Point. Rouses Point. Rouses Point. Chay Chay Chay Chay Chay Chay Chay Chay	AM 6 35 6 39 6 47	AM 6 30 16 37 6 42 6 46 6 53 6 53 6 53 6 53 6 53 8 28 8 50 5 9 15 8 28 8 46 8 9 5 9 15 11 0 28 8 11 10 28 11 11 20 11 11 52 12 15 12 12 12 12 12 12 12 12 12 12 12 12 12	12 21 12 44 1 05 11 13 1 21 1 45 12 49 11 03 11 08 1 20	11 39 	The Laurentian 200 2	No baggage service on this train north of Whitehall and the service on this train north of Whitehall bears and service on this train north of Whitehall bears are service on this train north of Whitehall bears are service on this train north of Whitehall bears are service on this train north of Whitehall bears are service on this train north of Whitehall bears are service on this train north of Whitehall bears are service on this train north of Whitehall bears are serviced by the service of the se	90 Septen 30 to Septen 7 08 7 08	9 02 9 08 9 08 19 12 9 24 9 46 9 58 10 01 10 25 10 27 10 43 11 05 Popularian Para Para Para Para Para Para Para Pa	12 20 12 32 12 32 12 32 12 32 14 15 15 15 15 16 17 16 17 17 17 18 17 17 17 18 17 18 17 18 17 18 18 18 18 18 18 18 18 18 18 18 18 18	ween New York and Montreal Lim passengers carried.	Fig. 2	Runs Saturdays only 5 and Sept. 2, inclusive.	AM 6 45 6 58 6 58 7 7 05 7 10 7 22 7 45 8 27 7 45 8 27 8 8 27 8 5 9 15	11 23 11 23 11 33 11 45 11 54 12 04 12 16 12 21 12 44 1 15	12 30 12 37 12 57 12 59 1 105 1 105 1 45 1 01 1 33 2 10	The Laurentian	2 35 f2 51 3 10	tween New York and Montreal Limited : 1 hassengers carried. Air Conditioned : 1 hassengers carried.	
SARATOGA SPRINGS—NORTH CREEK READ DOWN READ UP SARATOGA RACING SPECIAL New York and Saratoga Springs SATURDAYS ONLY																				

Note A—Train 186 will also run Tuesday July 4 and Monday September 4. For Explanation of Reference Marks, see page 8.

Lv.	New York	
	Grand Central Terminal	8 30 a m
	125th Street Station	8 40 a.m.
Ar.	Saratoga Springs.	12 20 n.m.
	Returning Same Day	12.50 p.m.
Lv.	Saratoga Springs	6 10 n m
Ar.	New York, 125th Street Station	10 24 nm
Ar.	New York, Grand Central Terminal	10.24 p.m.
		10.33 p.m.
	Lounge Con Dealer Cons Dining Con and Control	

Lounge Car, Parlor Cars, Dining Car and Coaches
On Fridays July 28, August 4, 11, 18, 25 and September 1, a through sleeper to
Saratoga Springs will be carried on train No. 145, The Ticonderoga, leaving Grand
Central Terminal at 10.45 p.m., Standard Time, arriving at Saratoga Springs
3.22 a.m.

4

MONTREAL, PLATTSBURG, SARATOGA SPRINGS, TROY AND ALBANY WEEK DAY TRAINS SUNDAY TRAINS																					
Miles	TABLE 2				WEE	K DA	Y TRA	AINS							SUND	AY T	RAINS	5			
		40	264	2	18 M	36	34	4	8	10		18 M	36	34	38	44	46	48	8	10	
7.7 14.0 23.3 26.2 36.4 40.9 53.6 63.1 74.1 81.8 91.2 98.5 103.5 113.3 113.3 119.9 123.9	MONTREAL Windsor Station Lv. Westmount Montreal West Rouses Point Lv. Chasy West Chary Plattsburg Plattsburg Lv. Cliff Haven (Bluff Point) Port Kent Douglass Willsboro Essex Westport Fort Iconderoga (Ticonderoga) Putnam Troin Crown Point Fort Teonderoga Whitehall Lv. Comstock Fort Ann Smith's Basin Fort Edward Fort Edward Lake George Lv. Glens Falls Fort Edward Fort E	AM Paggage Carried No baggage Ca	6 58 7 05 7 10	12 17 12 30 PM	9 100 9 26 9 33 9 38 9 38 9 38 10 00 11 0 25 10 33 11 20 11 20 11 25 11 20 11 25 11 20 11 25 11 20 11 25 11 20 11 25 11 20 11 25 11	11 30 11 52 12 16 12 39 12 59 1 23 2 10 2 36 2 30 2 3 10 2 38 3 02 3 10	Md The	75 157 334 40 75 357 357 377 377 377 377 377 377 377 3	3 15 4 00 AM	Pullman Cars only between Montreal and York. No coach passengers carried	First trip on 25 % Saratoga Race Train. Runs Saturdays only August 5 of 47 August 5, 12, 19, 26 and September 2.	8 35 8 47 9 10 00 110 25 10 13 20 11 15 20 8 1 10 5 1 12 10 8 1 10 5 1 10 10 10 10 10 10 10 10 10 10 10 10 1	11 30 11 52 12 16 12 2 39 12 2 30 2 10 2 36 2 30 2 3 10 2 36 3 02 3 10	AM 10 15 10 217 11 24 11 29 11 58 12 00 11	4 254 4 377 4 44 4 450 5 000 5 6 36 6 36 6 36 6 42	9M 4 35 4 58 4 5 11 5 11 5 13 7 5 42 6 45 9M 6 9M	12 45	11 26 11 25 11 55 12 05	7 457 7 577 9 122 9 227 9 10 05 10 12 10 10 11 10 10 12 11 1547 11 1 547 11 1 26 11 2 36 11 2 39 11 2 36 11 2 30 11 2 30 11 2 36 11 2 30 11 3 3 3 3 5 3 11 3 4 3 3 3 5 3 11 3 4 3 3 3 5 3 3 5 3 3 5 3 3 5 3 3 5 3 3 5 3 3 5 3 3 5 3 3 5 3 3 5 3 3 5 3 3 5 3 3 5 3 3 5 3 3 5 3 5 3 3 5 3 3 5 3 3 5 3 3 5 3 3 5 3 3 5 3 3 5 3 3 5 3 3 5 3 3 5 3 5 3 3 5 3 5 3 3 5 5 3 5 3 5 3 5 3 5 5 3 5 3 5 5 3 5 5 3 5 5 3 5	Montreal Limited Goog Workers only between Montreal and Montreal Limited Goog Work No coach passengers carried.	
	NEW YORK, 125th StAr. Grand Central Term"	10 15 AM	AM	3 56 4 05 PM	PM	7 30 7 40 PM	7 30 7 40 PM	9 01 9 12 PM	7 04 AM	6 31 6 42 AM	10 24 10 35 PM	PM	7 30 7 40 PM	7 30 7 40 PM	9 49 10 00 PM	9 49 10 00 PM	6 03 6 15 AM	6 03 6 15 AM	7 04 AM	6 31 6 42 AM	
FORT EDWARD—GLENS FALLS—LAKE GEORGE READ DOWN READ UP TABLE 4 162 164 164 165 165 TABLE 4 162 164 164 165 165 TABLE 4 165 165 TABLE 4 166 167 TABLE 4 167 TABLE 4 168 TABLE 4 168 TABLE 4 169 TABLE 4 TA																					

PLATTSBURG-SARANAC LAKE-LAKE PLACID READ DOWN 102 98 94 82 M 88 Mls. 81 M 97 TABLE 5 91 PM Fridays only.

NAPIERVILLE JUNCTION RAILWAY

READ DOWN

	8		34	TABLE 6	7		35						
	PM	-	AM	LvMONTREAL.Ar.	AM		PM						
 	*7 45			Windsor Station Delson (1)			*6 10						
	8 28		K10 50	St. Mathieu	7 05		N5 34						
				St. Edouard Napierville									
	9 00		11 14	Lacolle (2)	6 35		5 10						
 	*9 12		*11 24	.ROUSES POINT (3).	*6 22		*5 00						
				Ar T.v									

READ UP

K Stop on signal Saturdays and Sundays to discharge passengers from Montreal West, Westmount and Montreal.

N Stop on signal Sundays to receive passengers for Montreal West, Westmount and Montreal.

TABLE OF CONNECTIONS

(1) With C. P. Ry. Joint Stations. (2) With C. N. Ry. Separate Station. (3) D. & H. R. R. (Joint Station), also with C. N. Ry., and Rutland R. R. Separate Station.

ALBANY, COBLESKILL, ONEONTA AND BINGHAMTON

REA	DD	OWN	1			RE	AD L	IP	
308 Daily		302 Except Sun.	Mls.	TABLE 8	305 Except Sun.		309 Except Sun.		313 Sun. Only
PM 4 20 4 48 5 04 5 21 v5 28 v5 50 03 v6 31 v6 33 v6 3		AM 7 10 17 23 7 33 7 44 8 02 8 20 8 28 8 41 8 52 9 06 9 14 9 23 19 37 9 45	5.5 10.8 17.2 26.8 36.2 39.2 44.7 49.9 61.5 66.5 76.3 81.7	Central Bridge Howe's Cave. Cobleskill Richmondville East Worcester Worcester Schenevus	AM 11 10 10 59 10 51 10 42 10 27 10 13 10 07 9 57 9 48 9 35 9 27 9 19 19 05 8 58		PM 6 25 p6 12 p6 53 p5 54 5 17 5 07 4 54 4 4 38 4 30 4 10 9		PM 1 10 12 57 12 48 12 39 12 21 12 11 11 56 11 48 11 30 11 30 11 22 11 07 11 00
6 45 v6 58 v7 04 7 11 7 21 7 32 v7 40 7 48 h7 51 h8 02 h8 11 8 30 PM		9 55 10 09 10 16 10 24 10 35 10 45 10 54 11 03 11 07 11 18 11 27 11 45 AM	94.0 98.8 103.3 108.4 114.3 118.8 120.3 127.0 132.5	Otego Lv. Wells Bridge Unadilla Sidney Bainbridge Afton Nineveh	8 50 8 37 8 29 8 22 8 13 8 01 7 50 7 42 7 39 7 27 7 17 7 00 AM		4 02 3 49 13 43 3 36 3 27 3 18 3 08 w2 58 w2 47 w2 37 PM		10 55 10 42 f10 34 10 26 10 18 10 09 10 01 9 53 9 50 f9 38 f9 28 9 10 AM

Trains 302, 305, 309 and 313 will stop at former site of Esperance Station (located between Delanson and Central Bridge), to pick up and discharge passengers.

PENNSYLVANIA DIVISION-CARBONDALE. SCRANTON AND WILKES-BARRE

	RE	AD	DOW		REAL	D UI	P		
512 Except Sun.	510 Except Sun.	Except	504 Except Sun.	Mls.	TABLE 9	501 Except Sun.	505 Except Sun.	507 Except Sun.	509 Except Sun.
PM 5 20 5 26 5 30 5 35 5 38 5 41 5 45 5 54 6 00	2 45 12 48 2 51 2 55 2 59 3 04	AM 8 00 8 06 8 10 8 15 8 28 8 29 8 34 8 36 8 40	7 09 7 14 17 17 7 20 7 24 7 28 7 33 7 36	.0 2.6 3.9 6.2 7.3 8.6 10.1 11.6 13.8 14.4 16.0	Lv	AM 7 25 7 19 7 15 7 10 17 07 7 04 7 00 6 56 6 51	PM 4 40 4 34 4 30 4 25 4 19 4 16 4 04 4 00	PM 600 554 55 55 55 55 55 55 55 55 55 55 55 55	PM 7 00 6 54 6 50 6 45 6 39 6 35 6 31 6 26
	PM		7 59 8 04 8 10 18 13 18 16 8 20	17.3 22.0 23.8 26.1 27.5 29.0 31.0 32.4 34.4	Lv. Scranton. Ar. South Scranton Lv. Moosle. " Pittston. " Yatesville. " Laflin. " Hudson. " Parsons. Ar. WILKES-BARRE. Lv.	AM		Does not carry baggage	6 10 16 04 5 55 5 50 5 54 15 36 5 50 6 531 6 520 PM

LAKE GEORGE STEAMER SERVICE

EASTERN STANDARD TIME

Time Table in effect June 25 to September 15, inclusive, 1939.

f10 00 f10 15 f10 25 f10 30 f10 40	" Bolton Landing " " Sagamore Hotel " " Pearl Point "	PM 3 25 12 40 12 30 12 15 12 10 12 90 11 25	Purchase rail tick Station only. Pu Steamer from Lake tion on Lake. For information G. H. Stafford, L&
	" Pearl Point "	12 00	
f11 10	" Hulett's Landing "	f1 25	G. H. Stafford, La
f11 15	" Sabbath Day Pt "	11 20	
f11 30	"Silver Bay "	11 10	
f11 45	"	11 00	
12 20 PM	ArBaldwinLv.		
P.M		PM	

ket to Lake George irchase ticket on George to destina-

address Captain ake George, N. Y.

RUTLAND BUS COMPANY, INC.

Service between Whitehall, N. Y. and Rutland, Vt.

6-25-39	Ex. Sun.		Daily		Sun. Only	
Whitehall Bus TerminalLv. Fair Haven.	10.00 10.30	2.35 3.05	6.00	7.00 7.30	8.25 8.55	
Hydeville. Castleton Corners (Lake Bomoseen). Castleton.	10.40 10.45	3.15 3.20	6.10	7.40 7.45	9.05 9.10	
West Rutland				8.00 8.15		

Rutland, Vt. to Whitehall, N. Y.

Rutland Lv West Rutland Castleton Castleton Corners (Lake Bomoseen) Hydeville Fair Haven Whitehall Bus Terminal Ar	7.45 8.00 8.15 8.20 8.25 8.30	1.00	P.M. 4.15 4.30 4.45 4.50 4.55 5.00	P.M. 5.25 5.40 5.55 6.00 6.05	7.10 7.25 7.40 7.45 7.50 7.55	
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FAST TRAIN FRIDAY AFTERNOONS for

LAKE CHAMPLAIN RESORTS

A week-end time-saver from New York to Saratoga Springs, Westport, Plattsburg, and favorite playspots on Lake Champlain

With no loss of business time, you add hours to your time for recreation. You arrive at your favorite resort in the evening—then a good night's sleep and you are ready in the morning for a round of golf, or whatever your favorite sport or pastime may be. Through parlor car and dining car service to add to your comfort.

FRIDAYS ONLY

JUNE 30 TO SEPTEMBER 1, INCLUSIVE

(Eastern Standard Time)

Lv New York (G.C.T.) 2.45 PM	Ar Fort Ticonderoga	8.15 PM
Lv Harmon	3.34 PM	Ar Port Henry	8.40 PM
Lv Albany	5.50 PM	Ar Westport	9.01 PM
Ar Saratoga Springs	6.45 PM	Ar Port Kent	9.52 PM
Ar Fort Edward	7.07 PM	Ar Cliff Haven	10.10 PM
Ar Whitehall	7.33 PM	Ar Plattsburg	10.20 PM

DELAWARE and HUDSON

FORT WILLIAM HENRY HOTEL



A modern, fireproof hotel. Beautiful view. Excellent cuisine. Comfortable rooms with splendid ventilation and plenty of sunlight. Bathing, tennis, boating. An ideal place to spend your vacation or a week-end.

American and European Plan—Rates on application. F. R. SIMPSON, Manager

AUSABLE CHASM

This great natural wonder ranks among nature's masterpieces. The Ausable River, in its passage from the Adirondacks to Lake Champlain has cut this vast fissure through the granite mountains. The walls of this chasm are an interesting example of river carving and a fascinating study for the tourist. Ausable Chasm is but three miles from Port Kent station on The Delaware and Hudson Railroad and is easily accessible by auto or taxi.



TRAVELERS CHEQUES

At many of our offices where you secure your railroad ticket, you may safeguard the funds you are carrying by changing them into American Express Travelers Cheques. These Cheques, when presented with the necessary signature, are accepted on our dining and buffet cars, at hotels and other places serving travelers. They are issued in denominations of \$10, \$20, \$50 and \$100 at a standard price of 75 cents for each \$100 purchased. A prompt refund is made if any of the cheques are lost or stolen.

VISIT RAILROADS EXHIBIT

at the New York World's Fair

See "Railroads on Parade" portraying transportation from the days of the stagecoach to the modern locomotive and streamlined train of today.

See "Building the Railroad," the story of railroad con-

See "Railroads at Work," the story of railroad operation.

23,000 nation-wide offices of Railway Express at your service, ready to ship your packages at top

25,000 hatton-wide offices of Kallway Express at your service, ready to ship your packages at top speed. One *complete*, door-to-door service. Pickup and delivery at no *extra charge* in all cities and principal towns. Low, economical rates. For service merely phone our nearest office.



1839-A CENTURY OF SERVICE-1939

FARE WILL BE FOR Where SECTION COMPARTMENT DRAWING ROOM Lower Berth Bed-Upper Berth or More Persons or More Persons or More Persons is Person Person Person \$2.10 2.40 2.65 \$1.45 1.65 1.80 \$2.80 3.20 3.55 \$3.25 3.70 4.10 \$4.20 4.75 5.25 \$6.30 6.85 7.35 \$5.25 6.05 6.60 \$7.35 8.40 9.45 \$3.80 4.30 4.75 • Indicates charge for Section when occupied by one adult and one child under twelve years of age provided lower berth only is used for night occupancy. Lower berth only prepared for night occupancy; when both berths are used, charge will be sum of the lower and upper berth fares. # Indicates charge for single bedroom occupied by only one person. If more than one person, charge will be 2 lower berth fares. Minimum Number of Railroad Tickets Required for Exclusive Occupancy of Section, Drawing Room, Compartments or Bedrooms in Sleeping Cars and Drawing Room in Parlor Car. Minimum Number of Adult Railroad Tickets Required Section. Single or Double Bedroom. Compartment, berth service. Drawing Room, berth service. Drawing Room, seat service. Drawing Room in Parlor Cars.

A collection of one-half (14) adult fare in addition to sleeping or parlor car space charge will be made for each berth or seat in sleeping car or seat in parlor car occupied exclusively by one or more children under five (5) years of age, accompanied by parent or guardian, except that such charge will not be made when such berth or seat in sleeping car is in the same section as that occupied by the parent or guardian. Only one-half (14) fare collection will be made when berth or seat is occupied by two or more children of such age.

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EXPLANATION OF REFERENCE MARKS SHOWN ON TIME TABLE PAGES

- * Daily. † Daily except Sunday. ‡ Daily except Saturday. § Sunday only. M Indicates mixed train.

 A Via connecting train No. 263 Troy to Cohoes.

 e Stops weekdays except Saturdays to receive passengers.

 f Stops on signal.

 g Stops on signal to receive passengers for Plattsburg and points north and to discharge passengers from Troy, Albany and points south.

 h Stops only to discharge passengers from Albany and Delanson.

 j Stops on signal to discharge passengers from Montreal and points beyond.

 n Stops on signal to discharge passengers from Montreal and points beyond.

 n Stops on signal to receive passengers from be passenger som Stops only to discharge passengers from points south of Delanson.

 s Stops Saturdays only.

 v Stops to discharge passengers from Albany and Delanson and to pick up passengers for Binghamton.

 w Stops on signal to receive or discharge revenue passengers.

 x No baggage service on this train north of Whitehal!.

 Stops on signal to receive or discharge passengers for or from Albany, Troy, or points beyond.

THE DELAWARE AND HUDSON RAILROAD

CORPORATION

Passenger representatives of the D. & H. R. R. located at the offices named below will cheerfully furnish information with respect to tours, fares, time-tables, maps, illustrated literature, etc.

MONTREAL, P. Q.—Telephone Marquette 8400

N. J. FERGUSON.... General Canadian Passenger Agent 106 Drummond Bldg. W. DEW. STOTT... Canadian Passenger Agent... 1117 St. Catherine T. J. FARRELL... Traveling Passenger Agent... St., West NEW YORK CITY—Telephone Murray Hill 4-0552, 4-0553

W. F. SHEEHAN. General Agent, Passenger Dept. G. T. ALTHISAR. Traveling Passenger Agent. 230 Park Avenue, J. D. QUINN. City Passenger Agent. Room 848

SCRANTON, PA.—Telephone 2-8751

J. T. Walsh Division Passenger Agent . . . D. & H. Passenger Station T. J. McNamara . . . Traveling Passenger Agent . . . D. & H. Passenger Station

E. T. GILLOOLEY, General Passenger Agent Albany, N. Y.

TICKETS AND PULLMAN ACCOMMODATIONS

May be secured at the following city ticket offices: IN MONTREAL

D. & H. CITY TICKET OFFICE, 106 Drummond Bldg., 1117 St. Catherine Street, West

Montreal—Phone Marquette 8400 CANADIAN PACIFIC RY. CITY TICKET OFFICE, 201 St. James Street, West and Windsor Station

IN NEW YORK CITY

230 Park Ave., Room 848.
17 John Street.
4 West 33rd Street near Fifth Avenue.
3 West 47th Street near Fifth Avenue.

IN BROOKLYN, N. Y. 155 Pierrepont Street.

AGENCIES OF THE FREIGHT DEPARTMENT

MONTE PICKENS, JR. Southleastern - 1.080 Aug.

BOSTON, MASS.—Telephone Liberty 4106 and 4107

A. W. Ackley New England Freight Agent, 429-430 Chamber of Commerce Building

BUFFALO, N. Y.—Telephone Washington 0853
J. N. VANDENBERGE. General Agent, Freight Department, 622 Ellicott Square
Building

CHICAGO, ILL.—Telephone Wabash 8994
CHAS. H. CLARK.....General Agent, Freight Department...838-839-840 Utilities

Building 327 South La Salle Street

CLEVELAND, OHIO—Telephone Main 1405
C. J. CONNOLLY.....General Agent, Freight Department 1237-1238 Terminal
Tower Building

DETROIT, MICH.—Telephone Madison 5291

F. L. DUNN..........General Agent, Freight Department, 5-114-115 General
Motors Building

MONTREAL, QUE.—Telephone Marquette 7424
N. J. FERGUSON.....General Canadian Freight Agent, 1117 St. Catherine St.,
West

PHILADELPHIA, PA.—Telephone Rittenhouse 1271

A. A. Gallagher.... General Southern Freight Agent. 1104-05 Finance Bldg., 1420-26 South Penn Square

PITTSBURGH, PA—Telephone Grant 5516
THOMAS BYERS.....General Agent, Freight Department......1824 Koppers
Building

Full information regarding Service, Rates, Routes, etc., can be obtained by applying to any of the named agencies.

C. S. HALLENBECK, General Freight Agent, Albany, N. Y.

FRED W. NYLAND, Coal Traffic Manager, Albany, N. Y. W. G. STORY, Freight Traffic Manager, Albany, N. Y.

M. V. BECKSTEDT, Asst. General Traffic Manager, Albany, N. Y.

J. E. ROBERTS, General Traffic Manager Albany, N. Y



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For descriptive booklet write Saratoga Springs Authority, Saratoga Springs, N. Y., or 80 Center Street, New York City.

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THE STATE OF NEW YORK

INFORMATION FOR TRAVELERS

REGARDING TIME TABLES—This Railroad is not responsible for errors in time tables, inconvenience or damage resulting from delayed trains or failure to make connections; schedules herein are subject to change without notice.

BUY TICKETS before boarding trains and avoid payment of extra charge.

CHILDREN under five years of age, when accompanied by parent or guardian, will be carried free, except that if the sole occupant of Pullman space, they will be charged half railroad fare and regular Pullman fare; children between the ages of five and eleven years, inclusive, will be charged half fare, and children twelve years of age and over will be charged full fare.

ADJUSTMENT OF FARES—In case of dispute with Conductors or Agents
pay fare required, take receipt and communicate with General Passenger Agent.

REDEMPTION OF TICKETS—Tickets unused or partly used, will be redeemed under tariff regulations at proper value.

BAGGAGE—This Railroad does not guarantee to forward baggage on same train with passenger, or within a given or specified time limit, but reserves the right to forward it upon a preceding or following train.

BAGGAGE MAXIMUMS—No single piece of baggage exceeding 300 pounds in weight or 72 inches in greatest dimension or single shipment exceeding \$2,500.00 in value will be checked. Free allowance subject to tariff stipulations as to contents, weight, value and size.

BAGGAGE LIABILITY LIMITED—Excess value to be declared and paid for

BAGGAGE FOR STATIONS WHERE NO AGENT IS ON DUTY must be claimed at baggage car door immediately on arrival of train at that station, otherwise it will be carried to the first station where agent is on duty, and held for further orders.

BICYCLES (not Motorcycles), BABY CARRIAGES, DOGS AND GUNS are transported in baggage cars subject to tariff regulations. Dogs (except lap dogs in closed receptacles) will not be permitted in passenger coaches.

LOST ARTICLES.—When articles are lost on trains, or left in waiting rooms at stations, owners should apply at once to Agent at station where they leave the train, or communicate with E. T. Gillooley, General Passenger Agent, Albany, N. Y. No responsibility is assumed for unchecked articles left in stations or cars.

CUSTOMS INFORMATION FOR RESIDENTS OF THE UNITED STATES RETURNING FROM CANADA

Residents of United States may take into Canada their wearing apparel and other personal effects and upon return to United States are entitled to have the same admitted free of duty. Residents of United States upon return must declare all articles acquired abroad, in their baggage or on their person, whether by purchase, by gift, or otherwise, and whether dutiable or free of duty.

Each returning resident of the United States is entitled to an exemption from duty on articles up to a total of One Hunderd Dollars (\$100.00) in value acquired outside the United States under the following conditions, if declared when the resident arrives at the United States port of entry:

- 1. Provided they have been outside the United States over forty-eight hours.
- 2. Provided they have had no previous exemption for the past thirty days
- Provided the articles are not intended for sale, or as an order for others, or prohibited by law.
- 4. Provided they are acquired as an incident of the foreign journey.
- Foodstuffs, cigarettes, tobacco, 100 cigars, and not over one wine gallon of distilled spirits, wines or malt liquors (provided the quantity is not prohibited by State law) may be included in this exemption.
- 6. When a husband and wife, or a parent or parents and minor or financially dependent children, travel together, the articles included within the exemption may be grouped and allowance made without regard to which member is the owner; PROVIDED, that the group exemption shall not include servants.

 Articles intended for future delivery must be declared in writing at the port of entry at the time of the resident's return to the United States.

Special provision is made in the case of sealskin garments. To save yourself embarrassment, inquire about the regulations of U. S. Customs Inspector at port of entry before you leave the United States, or if you are a traveler from Canada, before you enter the United States.

CUSTOMS INFORMATION FOR RESIDENTS OF CANADA RETURNING FROM THE UNITED STATES

Goods valued at not more than one hundred dollars included in the baggage companying residents of Canada returning from abroad after an absence from Canada of not less than 48 hours and acquired by them for personal or household use or as souvenirs or gifts, but not bought on commission or as an accommodation for other persons or for sale, under regulations prescribed by the Minister will be admitted free.

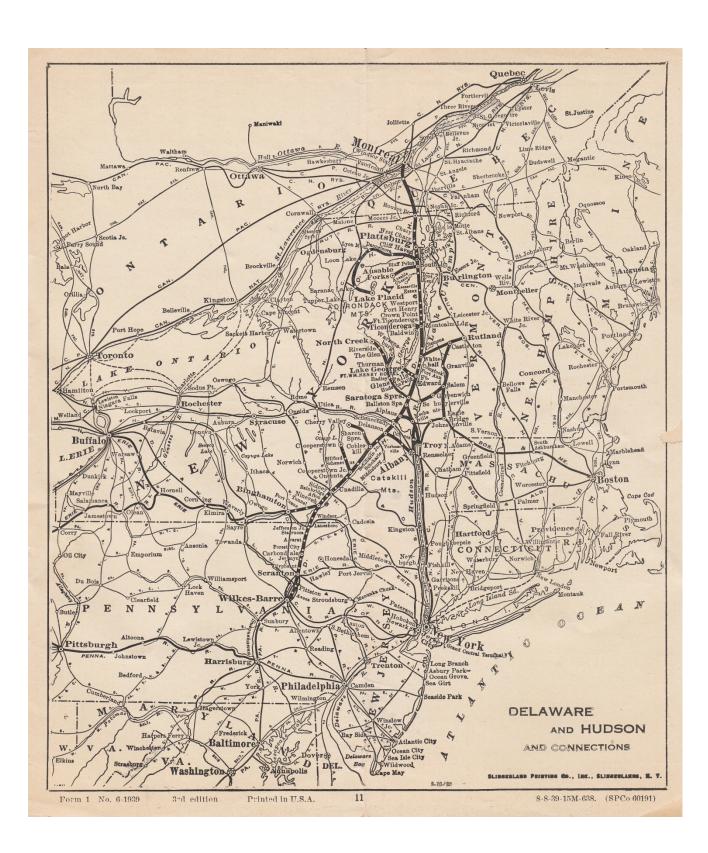
Provided that a resident of Canada shall not be entitled to the exemption herein granted within a period of four months from the date of the last exemption allowed, nor shall the exemption be allowed on alcoholic beverages in excess of one quart, or on tobacco in excess of fifty cigars, two hundred cigarettes and two pounds of manufactured tobacco.

The following regulations have been prescribed by the Minister:

- (a) All goods in respect of which exemption is claimed shall be declared in writing on Customs Declaration Form E. 24, in duplicate, unless the total value upon which exemption claimed does not exceed \$5.00.
- (b) The exemption granted shall be limited to goods actually accompanying a returning resident of Canada and brought in by him as baggage at the time exemption is claimed, and shall not be granted to any person under the age of fourteen years.
- (c) A person who has been allowed exemption of less than \$100 on a previous occasion shall not be entitled to add to the amount of a subsequent exemption the difference between the amount previously allowed and full exemption.
- (d) Goods belonging to one person cannot be included in the exemption claimed by another.
- (e) Returning residents must state in their declaration in Canadian currency the price actually paid for all goods purchased abroad in respect of which exemption is claimed, and must state a fair market value for all goods obtained abroad other than by purchase, such as gifts. The purchase price or value stated is not necessarily the value for duty, but must be declared in order to assist the appraising officer in arriving at the correct value for duty. To facilitate appraisal invoices or sales slips should be presented where possible.
- (f) If personal effects or goods taken abroad be advanced in value or improved in condition while abroad by reason of repairs or alterations further than that necessarily incidental to wear and use while abroad, the cost or value of such repairs or alterations is subject to duty. Such cost or value may, however, be included with the \$100 exemption.
- (g) In cases where goods for which exemption is claimed have a combined value of over \$100, exemption is to be applied, where practicable, to the goods subject to the highest rates of duty.
- (h) All dutiable goods obtained from abroad for which exemption cannot be allowed, must be entered at customs and duty paid in the ordinary course.
- (i) These regulations shall not be construed to be applicable to any goods the importation of which is otherwise prohibited.

Baggage may be examined at Toronto, London, Ottawa, Montreal and Quebec by American customs officials and checked through to destination in the United States, thus avoiding inconvenience of examination at port of entry on the frontier. Baggage from the United States destined to cities just named will be carried through Canada in bond to destination, avoiding examination at Canadian frontier.

SPECIAL ARRANGEMENTS FOR ORGANIZED PARTIES—Members of organizations or organized parties contemplating trips will find it greatly to their interest to consult any representative of the Delaware and Hudson Railroad who will be pleased to quote fares and make all arrangements for the trip.



Delaware and Hudson Time Table, Effective June 19, 1949

New York, Saratoga Springs, Glens Falls, Adirondacks, Lake George, Lake Champlain, Plattsburg, Montreal



Montreal Limited

The Laurentian





OVERNIGHT

MONTREAL LIMITED

EASTERN STANDARD TIME

Leave NEW YORK (Grand Central Terminal) 10:20 P. M. 7:30 A.M. Leave MONTREAL . 9:15 P. M. (Windsor Station)
Arrive NEW YORK (Grand Central Terminal) 6:20 A.M.

RESTFUL OVERNIGHT TRIP—Luxurious travel in a superbly appointed, air conditioned, non-stop train. Sleeping cars with private bedrooms, roomettes, drawing-rooms, compartments and open sections. Lounge car—breakfast served northbound. Reclining seat coach. Quebec and Ottawa connections.



LAURENTIA

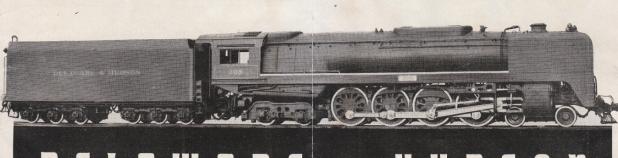
EASTERN STANDARD TIME

Leave NEW YORK 9:00 A.M. (Grand Central Terminal) Arrive MONTREAL . 7:00 P. M. (Windsor Station)

9:20 A.M.

7:30 P. M.

MAGNIFICENT SCENIC BEAUTY—Picturesque daylight trip along the Hudson River, Lake Champlain and Adirondack wonderland in air conditioned comfort. Modern, all-steel equipment. Dining car serving excellent meals. Reclining seat coaches.



E. T. GILLOOLEY, General Passenger Agent

THE DELAWARE AND HUDSON RAILROAD	SLEEPING AND DINING CAR SERVICE
Passenger representatives of the D. & H. R. R. located at the offices named below.	Coaches on all trains unless otherwise noted
ALBANY (1), N. Y.—Telephone 3-1141 E. T. GILLOOLEY. General Passenger Agent D. & H. Building J. B. HOPKINS Assistant to General Passenger Agent D. & H. Building	Equipment on trains indicated hereunder operates through between D. & H. R. and the N. Y. C. R. In both directions. On all other trains transfer is necessary at Albany Station.
MONTREAL, P. O.—Telephone Marquette 8400	* Regularly assigned cars are air-conditioned
N. J. Ferguson General Canadian Passenger Agent 116 Drummond Bldg. W. DeW. Stott Canadian Passenger Agent	SARATOGA AND CHAMPLAIN DIVISION
NEW YORK (17), N. Y.—Telephone Murray Hill 4-0552, 4-0553 W. F. Sheehan General Agent, Passenger Dept	*Sleeping Car
SCRANTON (3), PA.—Telephone 2-8751 L. E. DONOHOE. Division Passenger Agent D. & H. Passenger Station E. F. ROSEN Traveling Passenger Agent D. & H. Passenger Station	*Sleeping CarMontreal to New York.
E. T. GILLOOLEY, General Passenger Agent	*Sleeping Car
Albany (1), N. Y.	*Sleeping CarMontreal to New York. 14 Single Bedrooms (Car No. 32).
TICKETS AND PULLMAN ACCOMMODATIONS	*Sleeping CarMontreal to New York. 17 Roomettes (Car No. 33). *Through Coach (Reclining Seat)Montreal to New York.
May be secured at the following city ticket offices:	No. 32—Weekdays
IN MONTREAL	Last trip September 10
CITY TICKET OFFICE, 106 Drummond Bldg., 1117 St. Catherine Street, West	★Through Coach (Reclining Seat)Lake George to Albany.
Montreal—Phone Marquette 8400	No. 34—The Laurentian—Daily Dining CarServing all meals.
CANADIAN PACIFIC RY. CITY TICKET OFFICE, 201 St. James Street, West and Windsor Station	*Through Coach (Reclining Seat)Montreal to New York.
230 Park Ave., Room 848, New York (17), N. Y. 17 John Street, New York (7), N. Y.	No. 44—Sundays only
17 John Street, New York (7), N. Y. 4 West 33rd Street near Fifth Avenue, New York (1), N. Y. 3 West 47th Street near Fifth Avenue, New York (19), N. Y.	Last trip September 4 *Through CoachNorth Creek to New York.
155 Pierrepont Street, Brooklyn (2), N. Y.	*Through CoachLake George to New York.
	*Dining ServiceLake George to New York.
SLEEPING AND DINING CAR SERVICE	No. 7—Daily
Coaches on all trains unless otherwise noted	*Sleeping Car (12 Section D. R.) New York to Montreal (Car No. 2101).
Equipment on trains indicated hereunder operates through between D. & H. R. R. and the N. Y. C. R. R. in both directions. On all other trains transfer is necessary at Albany Station.	★Sleeping Car No. 2102 (8 Section-1 D. R3 Double Bedrooms) New York to Plattsburg, Fridays, July 1 to September 2, inclusive. ★Through Coach (Reclining Seat)New York to Montreal.
* Regularly assigned cars are air-conditioned	No. 9-Montreal Limited-Daily
SARATOGA AND CHAMPLAIN DIVISION	*Lounge Car
No. 2—Weekdays ★Through CoachNorth Creek to New York.	*Sleeping Car
Last trip September 10.	2 Drawing Rooms (Car No. 610).
No. 8—Daily	*Sleeping CarNew York to Montreal. 14 Single Bedrooms (Car No. 611).
★Sleeping Car (12 Section D. R.)Montreal to New York (Car No. 125). ★Sleeping Car No. 126 (12 Section D. R.) Plattsburg to New York, Sundays only, June 26; July 10 to August 28, inclusive; also on Mondays, July 4th and September 5th, and on Tuesday, September 6th.	*Sleeping Car
★Sleeping Car No. 127 (8 Section-1 D. R3 Double Bedrooms) Plattsburg to New York, Monday, July 4th; Sundays, July 10 to August 28, inclusive, and Monday, September 5th. Cars available for occupancy 0.00 PM.	*Sleeping Car
	*Sleeping Car
★Sleeping Car No. 135 (10 Sections) Fort Ticonderoga to New York, Mondays only, July 11 to 25, inclusive; August I to 29, inclusive; Tuesdays, July 5th and September 6th. Cars available for occupancy 9:00 P. M. previous evening.	*Sleeping Car
*Sleeping Car No. 145 (12 Section-D. R.) Whitehall to New York, Mondays only, July 11 to August 29, inclusive; also Tuesdays, July 5th and September 6th. Car available for occupancy 9:00 P. M. previous evening.	*Sleeping CarNew York to Montreal 17 Roomettes (Car No. 617). *Through Coach (Reclining Seat)New York to Montreal.
September 6th. Car available for occupancy 9:00 P. M. previous evening. *Through Coach (Reclining Seat)Montreal to New York.	No. 35—The Laurentian—Daily
(Above cars operate via Troy-not via Albany.)	★Dining CarServing all meals. ★Through Coach (Reclining Seat)New York to Montreal.
CoachMontreal to Albany.	*Through Coach
No. 16-Montreal Limited-Daily	*Through Coach
★Lounge CarMontreal to New York. 6 Double Bedroom-Buffet (Car No. 27).	Last trip September IO. *Through CoachNew York to Saratoga Springs.
★Sleeping Car	No. 43—Weekdays
	*Through CoachNew York to Lake George (Fridays only).
14 Single Bedrooms (Car No. 28).	Last trip September 2.



GRAND CENTRAL TERMINAL (N.Y.C.R.R.) NEW YORK

WINDSOR STATION (C. P. RY.) MONTREAL



SERVICE BETWEEN TWO OF THE FINEST TERMINALS IN AMERICA NEW YORK AND MONTREAL

THE POPULAR SHORT WAY

Lv. New York City from Grand Central Terminal

Lv. Montreal from Windsor Station

R	EAD DOWN				READ	UP	
Daily	The Laurentian Daily	Montreal Limited Daily		The Laurentian Daily	Daily	Montreal Limited Daily	1
NYC 21 D&H 7 CPR	NYC 143 D&H 35 CPR	NYC 61 D&H 9 CPR	TRAIN NUMBERS N. Y. C. R. R.—D. & H. N. J. Ry.—C. P. Ry.	NYC 144 D&H 34 CPR	NYC 72 D&H 8 CPR	NYC 62 D&H 10 CPR	
r8 38 PM r9 03 PM	9 00 AM r9 10 AM r9 26 AM r9 49 AM 10 45 AM	10 20 PM	NEW YORK Lv. (N.Y.E.) Grand Central Term. (N.Y.E.) Ar. L25th Street. Yonkers. Harmon. Peekskill. Poughkeepsle. Hudson. Ar. (N.Y.E.) ALBANY Lv. Troy.	7 30 PM t7 18 PM t6 58 PM 6 31 PM 6 15 PM 5 36 PM 4 55 PM 4 10 PM	6 40 AM t6 29 AM 6 10 AM 5 42 AM 5 32 AM 4 57 AM	6 20 AM t5 50 AM t5 22 AM	
11 50 PM 1 03 AM 1 10 AM 1 14 AM 2 15 AM 3 28 AM 3 28 AM 5 07 AM 6 13 AM 6 22 AM 7 42 AM 7 42 AM	12 20 PM 1 12 PM 1 20 PM 1 43 PM 1 43 PM 2 15 PM 3 00 PM 3 23 PM 4 29 PM 4 455 PM 6 45 PM 6 45 PM 6 65 PM 7 00 PM	Air Conditioned equipment between New York and Montreal 6 12 AM 7 15 AM 7 22 AM 7 30 AM	Lv. (D. & H.) ALBANY. (D. & H.) Ar Ar. Saratoga Springs. Av. Lv. Saratoga Springs. Ar. Ar. Fort Edward. Lv. Lv. Fort Edward. Ar. Port Henry. Port Henry. Westport. Port Ret. Platsburg. Lv. Rouses Point. Lv. Lv. Rouses Point. Ar. Westmount.	3 55 PM 2 55 PM 2 55 PM 2 53 PM 2 23 PM 2 23 PM 2 13 FM 1 12 26 PM 11 39 AM 11 12 AM 10 38 AM 10 38 AM 9 34 AM 9 37 AM 9 27 AM	3 55 AM 3 00 AM 2 02 AM 1 56 AM 1 31 AM 1 20 AM 1 2 14 AM 11 42 PM 10 35 PM 10 05 PM 9 08 PM 7 59 PM 7 45 PM	Air Conditioned equipment between Montreal and New York 10 34 PM 10 29 PM 9 30 PM 9 32 PM 9 15 PM 9 15 PM	
	7230 11200 7245 11215 5225	9A30 7A27	Via Can. Pac. Ry. Lv. Montreal, Windsor Sta. Ar. " Montreal West Ar. Quebec, Palais Sta Ly. " OTTAWA Union Sta	6A15 6A30		9800 6805 9825	

For Explanation of Reference Marks, see Page 4.

• United Traction Co. Bus leaves Troy Station 4:00 P. M. for Albany.

• Note A: Passengers on D. & H. train No. 9 en route to Ottawa should transfer at Montreal West to C. P. Ry train No. 503 leaving at 7:27 A. M. and arriving at Ottawa 10:10 A. M. daily.

• Note B: Passengers on C. P. Ry, train leaving Quebec 5:15 P. M. en route New York, should transfer at Montreal West to D. & H. train No. 10, leaving at 9:30 P. M.

TA	ABLE 1	IY A	ND 1		TO	MON	NTRE	0.00				SA	RATOGA SP		S TO	NO			
Miles		NYC61 D&H9 Daily	D&H1 Ex. Sun.	NYC163 D&H33 Sun. only	NYC55 D&H3 Daily	NYC143 D&H 35 Daily	17 Sun only	D&H5 Ex. Sun.	NYC167 D&H 43 Ex. Sun.	NYC21 D&H7 Daily		Miles			1 New	10	63 aily	Da See rei	13 ily erenc
	NEW YORK (N.Y.C. RR.) Lv. Grand Central Term	PM 10 20	12 05 r12 15	12 05 r12 15	7 10 r7 20	9 00 r9 10	-	PM 12\00 r12 10	PM 3 48			0.0 4.2 14.5	Lv. New York (G.C. " 125th St. " Yonkers. " Harmon. Ar. Albany. Ar. Troy.	T.)		10	M 05 15 29 54	9 19	M 00 10 26 49
0 3.3 3.9	ALBANY, D & H. Lv. Menands (+)	AM	6 25 f6 32		11 15	PM	PM	PM 4 15 4 22	7 15	PM 11 50		142.2 148.4	Ar. AlbanyAr. Troy			3 A	30 M	12 P	
7.8	TROY "	1 26	6 40	6 58		-		4 26 4 32	7 28	12 03			D. & H.	RR.		Ex. Sun.	Sun. only (See note)	Daily	Dail Dail
31.6	Cohoes. " West Waterford. " Mechanicville. " Round Lake " Ballston Spa. " Saratoga Springs. Ar.		6 47 6 52 7 07 7 17 7 32 7 45	7 05 7 10 7 23 7 33 7 45 7 57	11 35 j11 40 11 52 j12 01 12 13 12 25	The Lauren- tian		4 40 4 46 5 00 5 10 5 23 5 35	7 35 7 40 7 52 8 01 8 13 8 25	12 11 12 29 f12 38 12 51 1 03		38.3	Lv. Albany Lv. Troy Ar. Saratoga Springs			6 25 7 45 AM	6 45		12 1 PM
38.3	Saratoga SpringsLv. Fort EdwardAr.		7 50 8 15		12 30 12 53	1 20 1 43		5 40 =6 04	8 28 8 52	1 10 1 34			D. & H.	RR.		Ex. Sun.	only (See note)	Da (See	ily note)
32.9 36.8 70.8 77.4	Fort Edward Lv. Smith's Basin (+) a Fort Ann. a Comstock a Whitehall Ar.	real	8 25 8 37 8 43 8 49 9 02		1 01 f1 15 f1 21 1 35	1 48		6 08 f6 22 f6 27 6 40	ed	f2 03 2 15		16.9 21.9 29.5 35.2	" Thurman (+)	izerne).		8 05 8 41 8 52 9 07 9 17 9 35	8 05 8 41 8 52 9 07 9 17 9 35 9 50	P 1 2 2 2 2 2 2 2 2 2 2 2 2 2 2 2 2 2 2	35 10 20 36 47 05 25 45
87.2 92.2	Whitehall Lv. Dresden		9 12 9 30 9 40 9 57	September 4th)		3 00		x6 50 x7 f06 x7 f13 x7 26	haggage carri	f2 39		43.6 49.5 57.1 Notes	" Riverside " North Creek			10 15 AM	10 10 AM		
16.6 27.6 37.1 41.8 49.8 54.3	(Ticonderogs) Crown Point Fort Henry Westport Easer Baser Douglass (4-) Port Kent Plattsburg Ar.		10 29 10 52 11 09 11 20 f11 49 12 04 12 34	(Last trip Sep		3 23 3 43 f4 05 4 29 4 55		XXIbb	No	3 28 3 50 f4 05		-	: (a) Through coach Train No. 183—la Train No. 185—la Train No. 33—la	RANC	HL	INE			
67.4 76.7	PlattsburgLv. West Chazy	6 07	12 55 1 12 1 22 1 35			5 01	5 55 f6 15 f6 24 6 40			5 22 f5 35 5 43 6 13			(Also connection	s from			and A		167
	Rouses Point (D. & H.)Lv. Montreal West (N. J. Ry.)" Westmount" MONTREAL" (Windsor Station)	6 12 7 15 7 22 7 30	PM	AM	PM	5 40 6 45 6 52 7 00 PM		PM	PM	6 22 7 35 7 42 7 50		0.0	Lv. New York (6.6.1.	D:	aily (M 2 05 2 15		AM 9 00		Dai
E	XPLANATION OF REF	EREN	CE M	IARK	S SHO	NWO			TABI			4.2 14.5 32.7 142.2 148.4	" 125th St. " Yonkers. " Harmon. Ar. Albany. Ar. Troy.	12	2 29 2 54 3 30	600 620 620 600 620 620 600 620 620	r9 26 r9 49		r4 6
6	Daily, Through coach New York Stops to pick up passeng f Stops on signal.	ers for	orth C Bing	hamto	on this	train				Sunday			D. & H. RR.	Ex. Sun. (See note)	Sun. only (See note)	3 Daily	35 Daily		43 Ex Sur
1	g Stops on signal to receive passengers from Troy, a n Stops only to discharge p j Stops on signal Monday g Stops to pick up passeng	to Fri	day in Dela	nclusiv	ve; reg or bey	and D gular ond.	stop S	on. Saturda	ay and	Sunda		0.0	Lv. Albany Lv. Troy Ar. Fort Edward	AM 6 25 8 15 AM	AM 6 45	AM 11 15 12 53 PM	12 20	*** *** *** *** *** ***	PN 7 8 PN
1 1	o Stops only to discharge restops only to receive past to Stops only to discharge passer for Binghamton.	senger passen ngers	e pass s. gers. from A	Albany	from and	point Delan	son a	nd to			ngers		D. & H. RR.	161 Ex. Sun.	33 Sun. only (See note)	163 Daily (See note)	165 Ex. Sun. (See note)		43 Ex Sur
3	v Stops on signal to receive No baggage service on the Y Stops on signal Sundays. Convenient bus service Hudson Transportation Through coach equipment	his tra	in non	rth of	White	hall.			Lake	e Georg	e via	2.4 5.5	Lv. Fort Edward " Hudson Falls (+ " Glens Falls Ar. Lake George	8 33 f8 41 8 51 9 15 AM	8 33 f8 41	PM 1 55 f2 03 2 15 2 40 PM	PM		PN 8 9 9 9 PN
	Through coach equipment, United Traction Co. bus Non-Agency Station. No Through coach equipment Through coach equipment											Notes	Through coach N Train No. 33—las George. Train No. 163—las Train No. 1—Th Train No. 165—fin	ew York	k to Lak lept. 4th	e Georg	ge on th	is train.	

TABLE 4			MAIN									TA	BLE 5 BRANCH L	INE			
M	ONTRE	EAL	TO T	ROY	AND	ALB	ANY					NO	RTH CREEK TO SAR	ATOG	A SI	PRIN	GS
		D&H2 NYC146		D&H34	D&H 12 NYC 96	NYC 56	D&H 14 NYC196	NYC196	NYC 56	D&H 8 NYC 72			(Also connections to Alban	y and	New '	York)	
files	Ex. Sun.	Ex. Sun.	Daily	Daily	Daily	Ex. Sun.	Sun. only	Sun. only	Ex. Sun.	Daily	Daily			180 Ex.	18	36	184 Ex
MONTREAL (N.J. RY.) Windsor Station	AM	AM	AM	AM 9 20	PM	PM	PM	PM	PM	PM 7 45	PM 0.15	Miles	D. & H. RR.	Sun. (See	Da (See	ily	Sur (Se
Windsor Station Lv. Westmount				9 34						7 45 7 52 7 59 9 08	9 15 9 22 9 30 10 29		Lv. North Creek	AM		M	PN
Rouses Point (D.&H.) Ar.			8 45	10 30					******	9 08	10 29	7.6	" Riverside	9 00 9 20 9 35	33	05 25 40	2000MMMAP
7.7 Chasy. 4.0 West Chazy 4.3 3 Plattsburg Ar. 3.3 Plattsburg Lv.			8 57 9 20 9 40	11 07	1th))th)	th)	th)	10th)	9 18 9 32 f9 42 9 58		21.9 27.6	"Thurman (+). "Stony Creek (+).	9 35 9 52 10 02 10 17	3	59 10	23
3.3 PlattsburgLv.				11 12 11 39	ber 1	l rac	ber 4	ber 4		10 05		35.2 40.2 57.1	The Glen (Friends Lake) (+). Thurman (+). Stony Creek (+). Hadley (Lake Luzerne). Corinth. Ar. Saratoga Springs.	10 17 10 27 11 00	4 5	59 10 26 36 10 M	34
6.4 Port Kent. " 0.9 Douglass (+) " 8.9 Willsboro. " 3.6 Essex. " 3.1 Wastroot "				f12 03	September 11th)	September 10th	September 4th)	(Last trip September 4th)	September	10 35 f10 43 10 59 f11 07 11 23 11 45 11 57 12 14				AM 2	32		Pi
3.6 Essex		7 34 7 42 8 02 8 25 8 40 9 02	11 11 11 30	12 26 12 47	b Sei	p Ser	trip Se	p Se	a Ser	11 23	P J		D. & H. RR.	Ex. Sun.	Ex. Sun.	Sun. only	
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9.9 Comstock	o bag	10 11 10 17			3 10 3 20 3 27 3 33 3 45				5 10 5 16 5 22	f1 09	Mont		N.I.C. RR.	Daily		only	
3.9 Fort Ann	No	10 32		2 29	3 33 3 45		4 47		5 34	1 26		0.0	Lv. AlbanyAr. Harmon	PM 12 50		PM 7 15 t9 24	
5.3 Fort Edward Lv. 2.4 Saratoga Springs Ar	5 35	10 39 11 05		2 55	3 53 4 18	4 53 5 20	5 02 5 27	5 08 5 32		1 31 1 56		127.7	Ar. Yonkers. Ar. 125th St. Ar. New York (G.C.T.).	t4 41		t10 07	
2.4 Saratoga SpringsLv 9.1 Ballston Spa"	6 00 6 10	11 12 11 25 11 36		3 05	4 28 4 42 4 53 5 06 5 19 5 30	5 25 5 39		5 40	6 15 6 28 6 38	2 02 2 15		142.5	Ar. New York (G.C.T.)	4 52 PM		10 19 PM	
2.4 Saratoga Springs Lv 9.1 Ballston Spa. " 5.3 Round Lake " 1.8 Mechanicville. " 0.0 West Waterford. " 1.9 Cohoes "	6 23 6 34 6 40	11 47		The	5 06	6 01	6 16 6 29		6 50	2 35		Notes	Train No. 180 — last trip Sept. 1 York.				
1.9 Cohoes		12 05			5 30	6 19	6 34		7 03	3 00	3 15		Train No. 186 — last trip Sept. 10t on Sundays. Train No. 184 — first trip Sept. 12		igh coac	h to Ne	w Y
A 7 Watervliet Lv	6 46	12 13			5 37	6 27	6 42		7 17				Train 110. 10x miss unp popt. 12	ш.			
86.0 Colonie (+)	f6 53 6 56 7 05	12 25			5 50	6 40	6 55	6 40	7 30	3 55		TA	BLE 6 BRANCH L	INE			
	AM	PM	PM	PM	PM	PM	PM	PM	PM	AM	AM		LAKE GEORGE TO F	DRT	EDW	ARD	
(N.Y.C. RR.) NEW YORK, 125th St. Ar. Grand Central Term		t4 41 4 52		t7 18 7 30	t9 08 9 20	11±11 11 25	10±07	10±07	11t11	t6 29			(Also connections to Alban	y and	New	York)	
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M United Traction Co. Bus leaves Troy Station 4:00 P. M. for Albany. Sun. (See (See note) (note note)													Lv. Lake George		3 00 3 25	4 05 4 32	4
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TABLE 7 MAIN LINE **ALBANY AND BINGHAMTON** READ DOWN READ UP

208 Daily	202 Except Sun.	Mls.		205 Except Sun.	209 Daily	
4 4 5 6 7 6 7 6 7 7 7 7 7 7 7 7 7 7 7 7 7 7	9 50 10 00 10 14 10 20 10 28 10 40 10 48 10 57 11 05 11 10 11 23 11 31	0 5.5 10.8 17.2 26.8 36.2 44.7 49.9 61.5 66.5 76.3 81.7 89.6 94.0 98.8 103.3 108.4 114.3 118.8 120.3 127.5	Richmondville East Worcester Worcester Schenevus Colliers + Ar Oneonta Lv Lv Oneonta Ar	10 27 10 12 10 06 9 57 9 34 9 26 9 18 8 55 8 44 8 323 8 15 8 062 7 522 7 35	 PM 30770 10248332787 7660 1024833278 7660 1024833278 7660 1024833278 1444 1462 1442 1442 1442 1442 1442 1442	

Trains 202, 205 and 209 will stop at former site of Esperance Station (located between Delanson and Central Bridge), to pick up and discharge passengers.

TABLE 8

BRANCH LINE PLATTSBURG — LYON MOUNTAIN

READ DO	WN			1	REA	DU	P	
	82 Ex. Sun.	Mls.		81 Ex. Sun.				
	8 00	14.0 19.6	Lv. PLATTSBURGAr. " Cadyville" Dannemora" Ar. Lyon Mountain. Lv.	11 01				

TABLE 9 MAIN LINE — CARBONDALE AND SCRANTON

	RE	AD	DOW	N	The state of the s	RE	EAD	UP	
512 Except Sun.	510 Except Sun.	508 Except Sun.		Mls.		501 Except Sun.	505 Except Sun.	507 Except Sun.	509 Except Sun.
4 44	PM 12 00 12 06 12 10 12 15 12 15 12 21 12 25 12 29 12 34 PM	AM 7 00 7 06 7 10 7 15 7 18 7 21 7 25 7 29 7 34 7 36 7 40 AM	6 09 6 14 f6 17 6 20 6 24 6 28 6 33 6 36	11.6 13.8 14.4	Lv. Carbondale Ar. " Mayneld " Jermyn " Archbald " Jessup-Peckville " Jolyphant " Dlokson City " Providence (Market St.) + Green Ridge (Poplar St.) + Scranton Lv.	AM 8 10 8 04 8 00 7 55 7 52 7 49 7 45 7 36 17 34 7 30 AM	PM 3 30 3 24 3 20 3 15 3 05 3 05 2 56 f2 2 50 PM	PM 500 4 54 4 50 4 452 4 335 4 216 4 20 PM	PM 6 15 6 09 6 6 00 5 5 5 4 6 5 5 35 PM
F	or Ex	plana	ation	of R	eference Marks, see Page 4.				

TABLE 10 NAPIERVILLE JUNCTION RAILWAY

REA	D D	NWC		RE	AD	UP	
34 Daily	8 Daily	10 Daily		7 Daily	9 Daily	35 Daily	
 AM 9 20	PM 7 45 8 17 8 25	PM 9 15	Lv. (N.J. RY) MONTREAL (N.J. RY.) Ar. "Windsor Station" "Delson (1)"	AM 7 50 7 15	AM 7 30		
 K 9 56 K10 02 10 10	8 34 8 44		"St. Mathleu " "St. Edouard " "Napierville "	7 05 6 58 6 49		N6 20 N6 13 6 05	
 10 20 10 30	8 58 9 08	10 29	"Lacolle (2) "ROUSES POINT (3) Ar (D. & H. R.R.) I.v	6 37 6 22	6 12	5 54 5 40	

K Stop on signal Saturdays and Sundays to discharge passengers from Montreal West, Westmount and Montreal.

N Stop on signal Sundays to receive passengers for Montreal West, Westmount and Montreal.

TABLE OF CONNECTIONS

(1) With C. P. Ry. Joint Stations. (2) With C. N. Ry. Separate Station. (3) D. & H. R. R. (Joint Station), also with C. N. Ry., and Rutland R. R. Separate Station.

RUTLAND BUS COMPANY, INC.

Service between Whitehall, N. Y. and Rutland, Vt.

4-24-49	Ex. Sun.	Daily	Daily	Daily	Sun.	70
Whitehall (Bus Terminal). Lv. Fair Haven. 4 Hydeville. 5 Castieton Corners (Lake Bomoseen). 6 Castieton. 6 West Rutland. Ar. Rutland Ar.	9 55 10 20 10 25 10 30 10 35 10 50 11 05	AM 11 55 12 15 12 20 12 25 12 30 12 45 1 00	PM 2 30 2 55 3 00 3 05 3 10 3 25 3 40	PM 7 15 7 35 7 40 7 45 7 50 8 05 8 20	PM 10 00 10 20 10 25 10 30 10 35 10 45	

Rutland, Vt. to Whitehall, N. Y.

	Ex. Sun.	Daily	Daily	Daily	Sun.	
Rutland Lv. West Rutland a Castleton castleton Corners (Lake Bomoseen) a Hydeville. Fair Haven. Whitehall (Bus Terminal) Ar.	7 45 8 00 8 15 8 20 8 25 8 30 8 55	AM 8 50 9 05 9 20 9 25 9 30 9 35 9 55	PM 12 15 12 30 12 45 12 50 12 55 1 00 1 25	PM 4 35 4 50 5 05 5 10 5 15 5 20 5 40	PM 7 25 7 40 7 50 7 55 8 00 8 05 8 25	



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6



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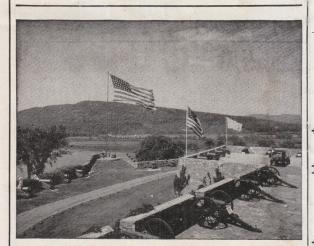
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Manager of Real Estate and Industrial Development. The Delaware and Hudson Railroad Corporation,
Albany 1, N. Y.



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WESTERN UNION EVERYWHERE

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⁺ Non-Agency Station. Baggage for stations where there is no agent or a train arriving at destination when station is closed, must be claimed at baggage car door immediately on arrival at station; otherwise baggage will be carried to next station where agent is on duty and held for further orders.

INFORMATION FOR TRAVELERS

REGARDING TIME TABLES—This Railroad is not responsible for errors in time tables, inconvenience or damage resulting from delayed trains or failure to make connections; schedules herein are subject to change without notice.

BUY TICKETS before boarding trains and avoid payment of extra charge.

CHILDREN under five years of age, when accompanied by parent or guardian, will be carried free, except that if the sole occupant of Puliman space, they will be charged half railroad fare and regular Fuliman fare; children between the ages of five and and over will be charged training and are five that fit has a special to the state of the same and over will be charged training that fare, and children twelve years of age and over will be charged training that face, and children twelve years of age and over will be charged training that face, and children twelve years of age and over will be charged training that the control of the same face of the same fac

value and size.

BAGGAGE LIMBILITY LIMITED—Excess value to be declared and paid for at

Value and size.

BAGGAGE LIABILITY LIMITED—Excess value to be declared and paid for at time of checking.

BAGGAGE FOR STATIONS WHERE NO AGENT IS ON DUTY must be claimed at baggage car door immediately on arrival of train at that station, otherwise it will be a supported in baggage car door immediately on arrival of train at that station, otherwise it will be compared in baggage cars subject to tariff regulations. Doos (except lap dogs in closed receptacles) will not be permitted in passenger coaches.

DOGS AND SMALL HOUSEHOLD PETS are transported in baggage cars subject to tariff regulations. Bous (except lap dogs in closed receptacles) will not be permitted in passenger objectionable when carried in baskets or containers, may be taken by purchasers of drawing rooms, compartments or bedrooms into the rooms they occupy.

Canada is prohibited.

LOST ARTICLES—When articles are iost on trains, or left in waiting rooms at stations, owners should apply at once to Agent at station where they leave the train, or communicate with E. T. Gillooley, General Passenger Agent, Albany, N. Y. No responsibility is assumed for unchecked articles left in stations or cars.

INFORMATION FOR PERSONS ENTERING CANADA FROM THE UNITED STATE

Immigration Regulations

American tourists and visitors are cordially invited to Canada and do not require passports to enter the Dominion. Citizens of the United States still retain their preventions of other countries should be prepared to establish their legal admission to the United States either by record of admission or certificate of reentry.

of other countries should be prepared to establish their legal admission to the United States either by record of admission or certificate of reentry.

CUSTOMS REGULATIONS

General Provisions—Wearing Travelers Baggage
and similar personal effects of peopons or the control of the provision shall not be provided by the control of the co

such as gifts.

INFORMATION FOR PERSONS ENTERING THE UNITED STATES
FROM CANADA
American citizens do not require passports either to leave or recenter the United States.
It is recommended however that they be in possession of such personal papers and other documents as may be helped to the parties of the certificate is the best form of evidence.
Citizens of Canada or British subjects domiciled in Canada may be admitted to the United States for not more than 29 days upon presentation of evidence of their citizenship, such as a Passport, Birth Certificate. Non Resident Border Crossing Identification Card. All alless park to the United States for more than 29 days are required to be in possession of a Passport and Vise of the nearest united States Consultate.

Card. All allens going to the United States for more than 29 days are required to be in possession of a Passport and Visa of the nearest United States Consulate.

Residents of United States may take into Canada their wearing apparel and other personal effects and upon return to United States are entitled to have the same admitted free of duty. Residents of United States upon return must declare all articles acquired to the states upon return must declare all articles acquired by gift, or otherwise, and whether dutiable or free of duty. Beach returning resident of the United States is entitled to an exemption from duty on articles up to a total of One Hundred Dollars (\$100,000) in value acquired outside the United States upon the following conditions, if declared when the resident arrives at the United States port of entry side the United States when the resident arrives at the United States port of entry side the United States are the states under the following conditions, if declared when the resident arrives at the United States port of entry side the United States over forty-eight hours.

2. Provided they have had no previous exemption for the past thirty days.

3. Provided they have had no previous exemption for the past thirty days.

4. Provided they are acquired as an incident of the foreign journey.

5. Foodstuffs, eighertes, tobacco, 100 cigars, and not over one wine gallon of distilled spirits, wines or mait fluores provided the quantity is not prohibited by law.

6. Each member of a family is entitled to the \$100 exemption for articles acquired abroad of the character described in the third provise to Paragraph 1798, Tariff Act of 1930, as amended, subject to the conditions prescribed therein. Articles beloinging to one passenger cannot be included in the exemption of another, except that when members of a family residing in one household travel together on their return to the United States, the exemptions to which the several members

of the family are entitled may be grouped and allowed without regard to which member is the owner, provided that the grouped exemption shall not include that of any person who is not entitled to such an exemption, nor be allowed for the property of any such person.

7. An additional exemption of \$300 applies if person has been in Canada for over 12 days, which exemption may not be claimed oftener than once in each six months period.

days, which exemption may not be cautieu of the trial of the control of the period.

The owner of valuable effects of foreign origin, sealskin garments, plumage, or other articles, prior to his departure from the United States, may make an application to the collector or his representative on Customs Form 4455 for the registration of such articles to facilitate their identification on return to the United States.

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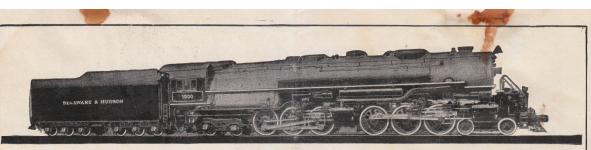
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	Rail	Ticket Fa	res	Pull	man			Rail Tic	ket Far	es	Pull	lman	R	ates fo	or Othe	r
BETWEEN	In Coach		ullman	Lower	Upper	BETWEEN		Coaches	In Pu		wer	per			dations	
	Way Ti	ip Way	Round	Bel	Up		Way	Round Trip	One Way	Round Trip	Lower	Upper	7000000			
Montreal, P. Q. and	\$7.53 \$14 6.55 12 10.08 17 7.23 14 4.23 8 3.39 6 5.88 11 4.53 8 6.03 11 10.83 18 8.35 16	\$9.66 75 8.44 555 13.00 11 9.3 11 5.3 43 4.2 41 7.4 771 7.6 65 14.0 35 10.8 15 8.0 51 8.9	16.40 25.15 4 18.28 4 10.28 5 14.52 7 10.94 7 14.94	3.50 3.50 3.50 3.50	\$2.60 2.60 2.60 2.60 2.60 2.60 2.60 2.60	Fort Ticonderoga, N.Y. and Albany N.Y. Beacon N.Y. Hudson N.Y. Lake George N.Y. Montreal P.Q. New York (G.C.T.) N.Y. New York (126th \$5.) N.Y. Foughkeepsle N.Y. T. W.Y. N.Y. Yonkers N.Y.	\$3.00 5.54 6.30 3.90 1.80 4.53 7.28 7.13 5.10 2.84 6.90	\$8.71 13.50 13.50	5.67 9.74 9.54 6.80	\$10.94 19.05 18.70	3.50 3.75 3.75	$\frac{2.80}{2.80}$		Ca	Sleepin rs Lower Berth	
Lase George N. Y. Mechanicy (I.C. T.) N. Y. New York (125th St.) N. Y. North Creek N. Y. Plattsburg N. Y. Port Henry N. Y. Port Kent N. Y. Port Kent N. Y. Foughkeepsle N. Y. Rouses Point N. Y. Saratoga Springs N. Y. Westport N. Y. Westport N. Y. Westport N. Y. Westport N. Y. Willieboro N. Y. Willieboro N. Y. Y. Millieboro N. Y. Y. Millieboro N. Y. Y. Y. Y. Y. Y. Y. Saratoga Springs N. Y. Whitehall N. Y. Willieboro N. Y. Y. Whitehall N. Y. Y. M. Y. Y. Onkers N. Y. Y. Y. Saratoga Springs N. Y. Y. Millieboro N. Y. Y. Y. Saratoga Springs N. Y. Y. Millieboro N. Y. Y. Whitehall N. Y. Y. N. Saratoga Springs N. Y. Y. Millieboro N. Y. Y. Y. Y. N. Saratoga Springs N. Y. Y. N. Saratoga Springs N. Y. Y. M. Saratoga Springs N. Y. Y. Millieboro N. Y. Y. N. Saratoga Springs N. Y. Y. Whitehall N. Y. Y. Y. N. Saratoga Springs N. Y. Y. N. Saratoga Springs N. Y. Y. Whitehall N. Y. Y. Y. N. Saratoga Springs N. Y. Y. Whitehall N. Y. Y. Y. N. Saratoga Springs N. Y. Y. Whitehall N. Y. Y. Y. N. Saratoga Springs N. Y. Y. Whitehall N. Y. Y. Y. N. Saratoga Springs N. Y. Y. Whitehall N. Y. Y. W. Saratoga Springs N. Y. Y. W.	6.25 12 6.93 11.79 20 11.65 19 8.05 15 2.49 7 2.88 5 9.63 15 6.78 13 1.75 3 6.33 12 7.29 14	23 9.4	29.45 29.10 20.40 4 5.48 4 9.62 6 6.52 7 24.05 4 19.88 6 16.92 3.60 4 15.88	3.50 3.50 3.50 3.50 3.50	2.60	Whitehall N. Y. and Albany N. Y. Beacon N.Y. Harmon N.Y. Lake George N. Y. Montreas (G. T.) Y. Y. New York (G. T.) Y. Y. Poughkeepsle N.Y. Troy N.Y.	2.33 4.88 5.63 3.23 1.13 5.19 6.61 6.53 4.43 2.18 6.23	10.03	1.46 6.54 8.87 8.68	12.68	3.35 3.35 3.00 3.50 3.35 3.35 3.35 3.35	2.55 2.55 2.30 2.60 2.55 2.35 2.30 2.55 2.55	* Rates shown apply when space is occupied by one accompanied by a child or children under 5 years of 9% These Rates apply for interstate travel only.	\$2.30 2.55 10.05 2.60 10.50 13.3 2.80 11.25 14.3	Upper * For Berth * Two Or Or More Persons	Rate
		7.6° 7.6° 11.0° 8.8°	8.68 12.68 7.60 28.35 7.21.55 7.23.45	3.50 3.50 3.50 3.35 3.75 3.75 3.75 3.75 3.00	2.60 2.60 2.55 2.80 2.80 2.80 2.30	Albany N.Y. Beacon. N.Y. Harmon N.Y. Hudson N.Y. Hudson N.Y. Montreal. P.Q. New York (G.C.T.) N.Y. New York (G.C.T.) N.Y. Poughkeepste N.Y. Troy N.Y. Yonkers N.Y.	1:73 4.28 5.03 2.63 5.88 6.01 5.93 3.83 1.50 5.63	11.41	5.67 6.66 3.41 .63 7.46 8.00 7.81 5.06	14.52	3.00 3.00 3.50 3.00 3.00 3.00	2.30 2.30 2.30 2.30 2.30 2.30 2.30 2.30	hen space is occupied r children under 5 ye or interstate travel o	0 \$6.75 \$8.85 0 7.55 9.55 0 7.90 9.60 0 8.15 10.30	For One Person	Rate
Albany N. Y. and Albany N. Y. Beacon N. Y. Harmon N. Y. Hudson N. Y. Lake George N. Y. Lake George N. Y. New York (G. C. T.) N. Y. New York (125th St.) N. Y. Poughkeepsle N. Y. Saratoga Springs N. Y. Troy N. Y. Plattsburg, N. Y. and Albany N. Y.		30 12.80	25.85 25.50 7 20.45 4 24.75	3.75 3.75 3.75 3.35 3.35 3.75		Glens Falls, N. Y. and Albany N. Y. Beacon. N. Y. Harmon. N. Y. Hollone N. Y. Hollone N. Y. Hollone N. Y. New York (G.C.T.). N. Y. New York (G.C.T.). N. Y. New York (125th 8t.). N. Y. Poughkeepsle N. Y. Troy. N. Y. Yonkers. N. Y.	1.88 4.43 5.18 2.78 6.03 6.17 6.08 3.98 1.64 5.78	11.71	3.61 7.67 8.21 8.01 5.27 2.21	14.94	3.50 3.35 3.35 3.00	2.55 2.60 2.55 2.55 2.30 2.30	age.	\$6.60 7.35 7.70 8.25	Two Or More Persons	Rate For Bedroom
Albany N. Y. Beacon. N. Y. Beacon. N. Y. H. Misson. N. Y. H. Misson. N. Y. Lake George. N. Y. Montreal. P. Q. New York (125th St.) N. Y. Poughkeepsie N. Y. Saratoga Springs. N. Y. Yonker S. N. Y. Yonkers. N. Y. Yonkers. N. Y. Yonkers. N. Y.	5.03 7.58 13 8.33 14 5.93 2.49 4 9.30 15 9.14 15 7.13 13 3.90 4.88 8.93 15	7.80 5.00 .62 2.94 .85 12.44 .60 12.20 .50 9.55	5.48 7 24.10 23.75 18.70	3.35 3.00 3.75 3.75 3.75	2.80 2.80 2.80 2.80	Lake George, N. Y. and Albany. N. Y. Beacon. N. Y. Harmon. N. Y. Montreal. P. Q. New York (G.C.T.). N. Y. New York (G.C.T.). N. Y. Poughkeepsle N. Y. Troy. N. Y. Yonkers. N. Y.	2.10 4.64 5.40 6.25 6.38 6.30 4.20 1.94 6.00	12.15	6.21 7.20 8.00 8.54 8.34	15.60	3.00 3.35 3.50 3.25 3.35 3.35 3.35	2.55 2.60 2.55 2.55 2.30	one s	\$4.45 \$5.10 4.80 5.45 5.50 5.20 5.90	For Or Or Or Persons	Rate For Roomette
Port Kent, N. Y. and Albany. N.Y. Beacon. N.Y. Harmon N.Y. Hudson. N.Y. Lake George. N.Y. Lake George. N.Y. New York (G.C.T.). N.Y. New York (125th St.) N.Y. New York (125th St.) N.Y. Poughkeepsie. N.Y. Trovoga Springs. N.Y. Trovoga Springs. N.Y. N.Y. Yonkers. N.Y.	4.64 7.20 13 7.94 13 5.54 2.88 5 8.93 15 8.84 15 6.74 3.53	41 3.46 25 11.94 20 11.78	18.95 20.75 6.52 23.20 22.85 18.00	3.35	2.30	Saratoga Springs, N. Y. and Albany N.Y. Beacon N.Y. Harmon N.Y. Montreal P. Q. New York (G.C.T.) N.Y. New York (125th St.) N.Y. Poughkeepsle N.Y. Troy N.Y.			5.94 8.14 7.27 7.06 4.34	15.88	3.00 3.00 3.50 3.00 3.00 3.00		Railros	d ticket ive occup of Pullma	nber of s require ancy of va an accomm	ed for arious moda-
Saratoga Springs		80 11.35	22.15	3.00	2.30 2.80	Riverside, N. Y. and Albany. N.Y. Harmon. N.Y. Montreal. C.C.T.). N.Y. New York (2.5th St.). Y. Poughkeepsie N.Y. Troy. Yonkers. N.Y. North Creek, N. Y. and Albany. N.Y. Montreal. P.O.		15.31 13.50 13.50	3.34 8.68	20.40	3.35	2.55	Compa servi Drawin servi	artment, lice artment, sice (sleeping Room,	seat ing car) 1 berth	1/10
New York (125th St.) N Y Poughkeepsie. N Y Saratoga Springs. N Y Troy. N Y Yonkers. N.Y.	8.03 13 6.00 4.58 3.68 7.80 13	95 10.66 7.94 6.14 4.87 65 10.26	20.80	3.75 3.35 3.00 3.75	2.80 2.55 2.30 2.80	North Creek, N. Y. and Albany N.Y. Harmon N.Y. Montreak (G.C.T.). N.Y. NewYork (125th St.). N.Y. NewYork (125th St.). N.Y. Troy N.Y. Yonkers N.Y.	7.21 7.13 5.03 2.70 6.83	13.50 13.50 13.50	9.60 9.41 6.66 3.60 9.01	18.90 18.50 18.00	3.35	2.55 2.60 2.80 2.80 2.55	serv		ing car) 2	



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Boston & Maine	Via Mechanicville, N. Y.
N. Y. C.—B. & A	Via Schenectady, N. Y.
Pennsylvania Lehigh Valley Central R. R. of Penn.	Via Wilkes-Barre, Pa.
Erie	Via Binghamton, N. Y.
Lehigh Valley	

AND THEIR CONNECTIONS

Traffic Agencies are maintained by the Delaware and Hudson as listed below, to serve the public and provide information concerning rates, routes and service. The nearest representative will, if given the opportunity, render you helpful service.

DELAWARE & HUDSON FREIGHT TRAFFIC AGENCIES

ALBANY 1, N. Y. C. F. Beck, General Eastern Freight Agent, D. & H. Building Phone 3-1141

ATLANTA 3, GA. Monte Pickens, Jr., Southeastern Freight Agent, 930 Healey Building Phone Walnut 5464

BOSTON 14, MASS. E. T. Cate, New England Freight Agent, Room 1101, North Station Office Building, 150 Causeway Street Phone Capitol 7-7175

BUFFALO 2, N. Y. V. R. St. John, General Agent, Freight Depart-ment, 1924 Liberty Bank Building Phone Washington 0853

CHICAGO 4, ILL.
C. H. CLARK, General Western Freight Agent,
838-839-840 Utilities Building, 327 South LaSalle St. Phone WA 2-8994

CLEVELAND 13, OHIO J. F. Hartman, General Agent, Freight Dept., 1236-7-8 Terminal Tower Bldg. Phone Main 1405

DETROIT 2. MICH. F. L. Dunn, General Agent, Freight Department, 7-225 General Motors Bldg. Phone Madison 5291

MONTREAL, QUE. N. J. Ferguson, General Canadian Freight Agent, 106 Drummond Bldg., 1117 St. Catherine St., West Phone Marquette 7424

NEW YORK 17, N. Y. C. G. Howland, General Agent, Freight Dept., Room 848, 230 Park Avenue Phone Murray Hill 4-0555 and 4-0556

PHILADELPHIA 2, PA. A. A. Gallagher, General Southern Freight Agent, 1604-05 Finance Bldg., South Penn Square Phone Rittenhouse 6-1271

PITTSBURGH 19, PA. Fred Sorbe, General Agent, Freight Department, 2818 Koppers Building Phone Grant 5516

V ST. LOUIS 1, MO. A. P. Lehman, General Agent, Freight Department, 2083-2084 Railway Exchange Building
Phone Chestnut 9178

SAN FRANCISCO 5, CALIF. G. C. Seaman, General Agent, Freight Department, 775 Monadnock Bidg., 681 Market Street Phone Garfield 1-6499

SCRANTON 3, PA. E. Donohoe, Division Freight Agent, D. & H.
Passenger Station Phone 2-8751

WINSTON-SALEM 3, N. C.

J. J. Kaelin, General Agent, Freight Department,
Room 502, Reynolds Building Phone 3-4451

A. W. Ackley, Freight Traffic Manager, Albany 1, N. Y.

F. W. Nyland, General Coal Traffic Manager, Albany 1, N. Y.

M. V. Beckstedt, Vice-President, Traffic,

Albany 1, N. Y.

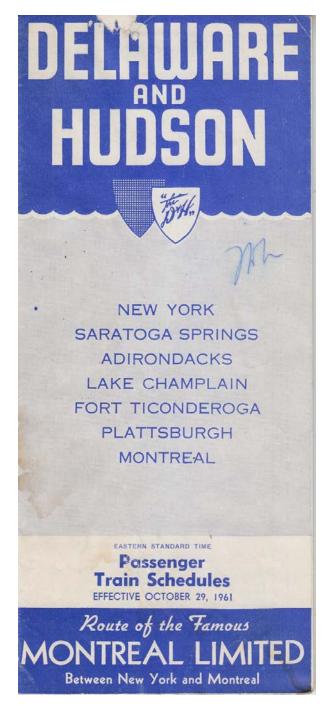


The cover of the 1953 Directory of Hotels, Dude Ranches, Guest Houses, Cabins, Cottages in the Summer Paradise The Adirondacks, Lake George, Lake Champlain, Saratoga Springs shown below was posted on Facebook on February 15, 2015 by George Ford, Jr.



Delaware and Hudson Passenger Train Schedules, Effective October 29, 1961:

New York, Saratoga Springs, Adirondacks, Lake Champlain, Fort Ticonderoga, Plattsburg, Montreal



NEW YORK—ALBANY—SARATOGA SPRINGS—PLATTSBURGH TO MONTREAL

Connecting trains to Ottawa-Quebec.

Miles	Light Face — A.M. Bold Face — P.M.	NYC 95 Daily	NYC 51 D & H 35 Daily Through Train	NYC 39 Daily D&H 5 Ex. SatSun. Note	NYC 61 D&H 9 Daily Through Train	QUALITY FOODS
	(N,Y,C,R,R,)					and
	(Grand Central	AM	AM	AM	PM	ATTENTIVE
0.0 4.2 14.5 32.7	Wt "	6 50 r7 00 r7 17 r7 51	8 45 r8 55 r9 38	11 55 r12 05 r12 22 12 46 1 25 1 45	11 45 r12 36	SERVICE
58.3 72.8 88.4 113.7 142.2	Yonkers " Harmon (Newburgh Ferry) " Beacon (Newburgh Ferry) " Poughkeepsle " Rhinecliff " Hudson " ALBANY (N.Y.C.R.R.) Ar.	8 25	11 28	1 25 1 45 2 03 2 36 3 15	1 30	•
	ALBANY(D.&H.R.R.) Ly.	1	AM 11 44	♦ N.B. PM 5 15	AM 3 02	Something
3.4 4.7 6.1 8.9	Menands (+)		N11 54	5 21 5 25 5 28		to suit
10.8 19.0 25.5	Mechanicville (+)"		12 11	PM 5 15 15 25 25 25 33 5 34 9 5 6 07 Ar 6 15	Limited	every
31.5 37.1 55.5	Ballston Spa (+)		12 36 12 58	6 07 Ar 6 15	Montreal Limited	taste.
70.8	Fort Edward { Glens Falls				4 33 4 48	Miles
99.6 116.7	Fort Ticonderoga(Ticonderoga) " Port Henry"		The Laurentian 13 1 25 1 25 2 22 2 2 2 2 2 2 2 2 2 2 2 2	not run 25, 1961 22, 1962		slip by
127.7 141.9	Westport Lake Placid Saranac Lake Willsboro.		£ 2 45	ll not c. 25,	B5 56 B6 16	like
154.4 167.5 190.8	Fort Ticonderoga. (Ticonderoga) " Port Henry. (Elizabethtown) Westport Ltake Placid (Saranac Lake) Willsboro. Port Kent (+) " Plattsburgh. (D.&H.R.R.) {Ar. Rouses Point. (D.&H.R.R.) {Ar.		3 50 4 20	Note—Will 1 Nov. 23-Dec. 2 Jan. 1, Feb. 2	6 57 7 26 7 31 7 44	magic.
100.0	Lacolle, P.Q. (N.J.Ry) " Napierville, P.Q. (N.J.Ry) " Montreal West (N.J.Ry) " Westmount (N.J.Ry) P.Q. Westmount (N.J.Ry) Windsor Station N.J.Ry "		3 50 4 20 4 30 4 43 64 54 a5 30 a5 37	Not Nov. Jan.	7 31 7 44 f7 55 a8 35	Join us,
	MONTREAL Westmount (N.J.Ry) " P.Q. Windsor Station N.J.Ry "		a5 37 a5 45 PM	PM	a8 42 a8 50 AM	won't you
						in the
	Connecting Trains Via Can. Pac. Ry.		Ex. Sat. Note		· •	D. & H.
	Montreel Windom Ste		DM DM DM		AM AM	Dining
	Montreal West. " Quebec, Palais Sta. Ar. Ottawa, Ont. "		5 35 7 25 5 45 5 50 7 36 5 56 9 35 11 25 PM PM PM		1 30 PM 12 50	
	Note—Quebec and Ottawa Montreal West from D&H	passen Train	gers change at No. 35.	+		

REFERENCE MARKS FOR ABOVE SCHEDULE

- B Stops to discharge passengers from Albany and points south on notice to conductor and will stop on station signal to receive passengers for Rouses Point and points north.
- N Stops to discharge passengers from points south of Albany and stops on signal to receive passengers for Saratoga Springs and points north.

 a Passengers not carried locally between these stations.
- a Passengers not carried locally between these stations.
 f Stops to discharge passengers on notice to conductor and will stop on station signal to receive passengers. r Stops only to receive passengers.

- t Stops to discharge passengers.

 ♦ Change trains—same station.

 This train does not carry checked baggage New York to Albany.
- -No checked baggage. N.C.B .- No facilities for handling checked baggage at this station.

EQUIPMENT

NORTHBOUND TRAINS

Coach service available on all passenger trains.

THE LAURENTIAN - Daily

(Daylight trip) N. Y. C. 51-D. & H. 35

*Coaches — New York to Montreal
Through Reclining Seat Coaches (Unreserved)
*Diner — New York to Albany
*Dining Car — Albany to Rouses Point (Note)

★Parlor Car-New York to Montreal (Railroad owned) -Car 5135.

MONTREAL LIMITED - Daily

(Night trip) N. Y. C. 61 — D. & H. 9

★Coaches — New York to Montreal
Through Reclining Seats (Unreserved)
★Sleeping Cars — New York to Montreal (open 10:00 P.M.) No.
(Railroad owned)

(Railroad owned)

10 Roomette — 6 Double Bedroom. 610

10 Roomette — 6 Double Bedroom. 611

10 Roomette — 6 Double Bedroom. 612

★Dining Car — Whitehall to Montreal (Breakfast)

6 Double Bedroom — Lounge. 613
10 Roomette — 6 Double Bedroom — 614
4 Compt. — 4 Double Bedroom — 2 Drawing Room
(Ex. Sat.) 615

*Assigned cars are air-conditioned

Note—When conditions warrant, dining car will operate to Montreal. Consult Train Conductor.

INFORMATION FOR PERSONS ENTERING CANADA FROM THE UNITED STATES

Immigration Regulations

American tourists and visitors are cordially invited to Canada and do not require passports to enter the Dominion. Citizens of the United States still retain their prewar freedom to enter and travel in Canada without new restrictions. In order to facilitate crossing the international boundary in each direction, citizens of the United States by naturalization should be prepared to present their naturalization certificates and citizens of other countries should be prepared to establish their legal admission to the United States either by record of admission or certificate of reentry.

CUSTOMS REGULATIONS Travelers' Baggage

General Provisions — Wearing apparel, articles of personal adornment, toilet articles and similar personal effects of persona arriving in Canada may be passed free, without entry at Customs as travelers, baggages, under the control of the control

MONTREAL TO PLATTSBURGH—SARATOGA SPRINGS—ALBANY—NEW YORK

Connecting trains from Ottawa-Quebec.

	Connecting Trains Via Can. Pac. Rv. OTTAWA, Union StaLv. Quebec, Palais StaWhontreal West. Montreal WestAr. Montreal, Windsor Station		Note AM 8 10 10 50 11 05 AM	6 40	6 00 9 45 10 00 PM
Miles	Light Face — A.M. Bold Face — P.M.	D&H 40 Ex. Sat. Sun. Note NYC 54 Daily	D&H 34 NYC 99 Daily through Train	D&H 10 NYC 46 Daily Ex. Sat. through Train	D&H 12 NYC 6 Sat only through Train
		AM	AM	PM	PM
.00 23.5 36.4 48.9 63.1 74.1 91.2 113.3 120.0 135.3 165.3 171.8 180.0 181.9 180.0 181.9 184.7 186.1 187.4	Port Kent (+) " Willsboro." Westport { Lake Placid { Saranac Lake } " Port Henry." Fort Ticonderoga (Ticonderoga) " Whitehall (Rutland) { Lr. Comstock (+) " Fort Edward { Glens Falls } { Saratoga Springs. Ballston Spa (+) " Mechanicville (+) " West Waterford (N.C.B.) "	Note—Will not Nov. 23—Dec. 25, Jan. 1-Feb. 22,	all 10 all 17 all 25 fill 59 12 12 12 12 12 12 12 12 12 12 13 12 12 14 12 12 15 12 12 16 14 14 12 17 18 18 18 18 18 18 18 18 18 18 18 18 18 1	MM Montreal Limited 700 HW 820	M1 09 M1 30
.0 28.5 53.8 69.4 83.9 109.5 127.7 138.0 142.2	ALBANY (N.Y.C.R.R.) Lv. Hudson Ar. Rhineeliff a Poughkeepsle a Beacon (Newburgh Ferry) a Harmon a Yonkers 125th St a NEW YORK Grand Central (N.Y.C.R.R.) Terminal Ar.	8 10 8 46 9 11	7 17	4 25 5 35 5 50 5 6 22 t 6 49 7 20 AM	4 55 5 28 6 08 t 6 57 t 7 24

REFERENCE MARKS FOR ABOVE SCHEDULE

- M Stops to discharge passengers from Rouses Point and points north on a conductor and will stop on station signal to receive passengers for and points south.
- and points south.
 P Stops Sat, and Sun., also Nov. 23-Dec. 25, 1961-Jan. 1-Feb. 22, 1962.
 K Stops Sun., also Nov. 23-Dec. 25, 1961- Jan. 1-Feb. 22, 1962.
- R Stops Sun, also Rot. 25, 1901. Sail. 1740. 22, 1902.

 R Stops to discharge passengers from Saratoga Springs and points north and stops on signal to receive passengers for points south of Albany.

 a Passengers not carried locally between these stations.
- f Stops to discharge passengers on notice to conductor and will stop on station signal to receive passengers.

 t Stops only to discharge passengers.

- Change trains—same station.
 Non-Agency Station. No baggage handled.
 Advance seat reservation necessary on this train.
 Stops Saturday only.
- N.C.B .- No facilities for handling checked baggage at this station.

EOUIPMENT

SOUTHBOUND TRAINS

Coach service available on all passenger trains.

THE LAURENTIAN - Daily

- THE LAURENTIAN Daily
 (Daylight trip) D. & H. 34 N. Y. C. 90

 *Coaches Montreal to New York
 Through Reclining Seat Coaches (Unreserved)

 *Dining Car Rouses Point to Albany

 *Diner-Lounge Albany to New York

 *Parlor Car Montreal to New York (Railroad owned) Car 340

MONTREAL LIMITED - Daily

(Night trip) D. & H. 10 — N. Y. C. 46 — Ex. Sat. D. & H. 12 — N. Y. C. 6 — Sat. Only

★Coaches — Montreal to New York
Through Reclining Seats (Unreserved)

*Dining Car — Montreal to Whitehall (Beverages-Supper)
6 Double Bedroom — Lounge.
10 Roomette — 6 Double Bedrooms...
4 Compt.— 4 Double Bedroom — 2 Drawing Room.

* Assigned cars are air-conditioned.

INFORMATION FOR PERSONS ENTERING THE UNITED STATES FROM CANADA

Immigration Regulations

Immigration Regulations
United States citizens do not require passports when reentering the United States after a visit to Canada. It is recommended, however, that they be in possession of such personal papers or other documents as may be helpfu in establishing that they are citizens of the United States. The most common documents which help to establish United States citizenship are birth certificates showing place of birth, baptismal certificates, naturalization certificates showing place of birth, baptismal certificates, naturalization certificates, and united States passports, valid or expired.

Passports and visas are waived for Canadian citizens and British subjects who reside in Canada and who are entering temporarily across the Canadian border. Such persons may be admitted as visitors for an appropriate period not to exceed six months. All other aliens seeking to enter the United States are required to be in possession of a valid passport and appropriate visa issued by an American Consul.

Aliens returning to legal residence in the United States must be in possession of their Alien Registration Receipt and Border Crossing Card (FORM 1-151) or a valid re-entry permit.

Customs Regulations

Customs Regulations

Residents of United States may take into Canada their wearing apparel and other personal effects and upon return to United States are entitled to have the same admitted free of duty. Residents of United States upon return must declare all articles acquired abroad, in their baggage, on their person or to be forwarded to them, whether by purchase, by gift, or otherwise, and whether duitable or free of duty.

Each returning resident of the United States is entitled to an exemption from duty on articles up to a total of One Hundred Dollars (\$100.00) in value acquired outside the United States under the following conditions, if declared when the resident arrives at the United States port of entry:

1. Provided they have been outside the United States over forty-eight hours.

2. Provided they have bean outside the United States over forty-eight hours.

3. Provided they have had no previous exemption for the past thirty days.

3. Provided they aricles are not intended for sale, or as an order for others, or prohibited by law.

4. Provided they are acquired as an incident of the foreign journey.

5. Foodstuffs, eigarettes, tobacco, 100 eigars, and not over one wine gallon of distilled spirits, wines or malt liquors (provided the quantity is not prohibited by State law) may be included in this exemption.

6. Each member of a family is entitled to the \$100 exemption for articles acquired abroad of the character described in the third proviso to Paragraph 1798. Tariff Act of 1930, as amended, subject to the conditions prescribed therein. Articles belonging to one passenger cannot be included in the exemption of another, except that when members of a family residing in one household travel together on their return to the United States, the exemption of another, except that when members of a family residing in one household travel together on their return to the United States, the exemption of another, except that when members of a family residing in one household travel together on their return to the

The owner of valuable effects of foreign origin, sealskin garments, plumage, or other articles, prior to his departure from the United States, may make an application to the collector or his representative on Customs Form 4455 for the registration of such articles to facilitate their identification on return to the United States.

ALBANY AND BINGHAMTON

PM 4 15 4 26 5 5 5	208 Except Sun.	Miles	Coaches on all Trains Shown		205 Except Sun.
7 16 103.3 " Sidney. " 7 25 108.5 " Bainbridge. " 7 33 114.3 " Afton. " 7 41 120.3 " Harpursville.+ " 8 25 " 142.5 Ar. BINGHAMTON Ly.	4 15 6 4 26 4 4 4 4 4 4 4 4 5 5 5 5 5 5 4 8 0 0 7 5 6 6 6 6 6 2 4 2 2 4 4 4 4 4 4 4 5 5 5 5 5 5 4 8 0 0 7 5 6 6 6 6 6 6 6 6 6 6 6 6 6 6 6 6 6 6	26.8 36.2 39.2 44.7 50.0 56.9 61.5 76.4 81.7 89.8 103.3 108.5 114.3 120.3	Delanson. Central Bridge. Howes Cave Cobleskill Richmondville. East Worcester. Worcester. Schenevus. Colliers. Uncollin. Uncollin. Sidney. Bainbridge. Afton.	READ UP	AM 11 30 f11 18 f11 10 10 45 10 30 10 21 10 12 21 10 12 9 58 f9 45 9 30 f9 17 9 10 68 48 8 37 8 30 8 17 7 20

f Stops to discharge passengers on notice to conductor and will stop on station signal to receive passengers.

+ Non-Agency Station. No baggage handled.

MONEY SAVING ROUND TRIP FARES

FAMILY FARES-Apply to husband and wife or to either or both parents and accompanying children—one parent pays the regular round-trip adult fare; the accompanying parent, if any, and each child 12 and under 22 pay the regular one way adult fare while each child 5 and under 12 is charged 1/2 of the regular one way adult fare. Children over 22 are not to be included in the round trip family ticket; children under 5 are carried free except when occupying parlor car seat. Liberal baggage privileges . . . Six-month limit . . . On sale every day . . . For travel in coaches or sleeping and parlor cars.

GROUP ECONOMY FARES—Apply for parties of three or more adults or their equivalent (two children 5 and under 12 to be considered the equivalent of one adult) ... Group must travel together on both going and return journeys . . . Good only in coaches . . . Liberal baggage privileges . . . 30 day limit.

WHITEHALL AUTOBUS CO.

READ DOWN	Except Sun.	Railroad tickets not accepted	Except Sun.	READ UP
Bus will not operate on Holidays	PM 2 35 2 55 2 58 3 02 3 08 3 35 PM	Lv. Ar. Whiteball, (Bus Terminal) N. Y. Fair Haven. Hydeville. Castleton Corners (Lake Bomoseen) Castleton Lv.	12 28	

DELAWARE & HUDSON FREIGHT TRAFFIC OFFICES

Freight traffic offices are maintained by the Delaware and Hudson R. R. Corporation listed below, to serve the public and provide information concerning rates, routes and service. The nearest Manager with years of "Know-How", will assist you with all Freight service.

ALBANY 1, N. Y.

J. M. O'Donnell, Division Sales Manager, D. & H. Building
Phone Hobart 3-1141

ATLANTA 3, GA.

Raymond O. Beebe, Southeastern Sales Manager, Room 701, 101 Marietta St. Bldg. Phone Jackson 2-5464

BOSTON 14, MASS.

A. P. Lehman, New England Sales Manager, Room 1101, North Station Office Building, 150 Causeway Street Phone Capitol 7-7175

BUFFALO 2, N. Y.

J. J. Kaelin, District Sales Manager, 601 Bank of Buffalo Phone TL 6-4646

CHICAGO 4, ILL.

D. E. Wilkinson, District Sales Manager, Room 840, 327 South LaSalle Building Phone WA 2-5574

CLEVELAND 13, OHIO

J. F. Hartman, District Sales Manager, 1236-7-8 Terminal Tower Bldg. Phone Main 1-1405

MONTREAL 2, QUE.

G. C. Seaman, Canadian Sales Manager, 106 Drummond Bldg., 1117 St.
Catherine St., West Phone Victor 9-7424

NEW YORK 17, N. Y.

B. J. Carlin, District Sales Manager, 360 Lexington Avenue
Phone YUkon 6-2886

PHILADELPHIA 3, PA.

A. A. Gallagher, District Sales Manager, 308 Transportation Center Building Phone LOcust 3-1271

PITTSBURGH 19, PA.

J. A. Quinn, District Sales Manager, 2818 Koppers Building
Phone GRant 1-5516 and GRant 1-5517

PORTLAND 3, ME.

I. W. Kelley, District Sales Manager, 310 Congress Building, 142 High Street Phone Spruce 3-1072

ST. LOUIS 1, MO.

J. H. Reddy, District Sales Manager, 2084 Railway Exchange Building
Phone Chestnut 1-9178

SAN FRANCISCO 5, CALIF.

A. E. Duprey, District Sales Manager, 457 Monadnock Bldg., 681 Market Street Phone Garfield 1-6499

SCRANTON 9, PA.

H. E. Andrews, Division Sales Manager, 703 Wyoming Avenue
Phone Diamond 2-8751

WINSTON-SALEM 3, N. C.

W. H. Hartman, District Sales Manager, Room 1410, Reynolds Bldg.
Phone PArk 3-4451

Paul F. Robinson, Vice-President Traffic, D. & H. Bldg., Albany 1, N. Y.

C. J. Connolly, Coal Traffic Manager, D. & H. Bldg., Albany 1, N. Y.

W. R. St. John, Freight Traffic Manager, D. & H. Bldg., Albany 1, N. Y.

Leonard Unger, General Sales Manager, D. & H. Bldg., Albany 1, N. Y.

RAILROAD — SLEEPING — PARLOR CAR FARES

Consult Ticket Agent about round-trip Family Fares. Also Group Economy coach excursion fares for parties of three or more adults (or equivalent).

Whitehall, N. Y. to

Albany Beacon Harmon

Albany N.Y. 3.05 6.10 4.10 8.20 8.50 1.30 Beacon N.Y. 6.67 13.34 9.87 19.74 1.90 Harmon N.Y. 7.80 15.60 11.63 22.68.50 2.13 10.00 10.

(SUBJECT TO CHANGE)

Tickets Sold In United States Subject To Fede

▲—Indicates rail tickets honored in Sleeping cars or Parlor Cars on Payment of charges for space occupied.

	Rail Ticket Fares			Rates in			Rail Ticket Fares				Rates in	Sleeping Car Fares		
Coaches, Sleeping and Parlor Cars are Railroad operated	In C	oaches	In Slee Parlor	Round	mette ne enger	or	Coaches, Sleeping and Parlor Cars are Railroad operated	In Coaches In Sleeping Parlor Car		ping or Car A	10	Montreal P.Q. and New York and		
12-60	One Way	Round Trip	One Way	Round Trip	Roor O Pass	Parlor Seat		One Way	Round Trip	One Way	Round Trip	Parlor	intermediate points	
Montreal, P. Q. to Albany N. Y. Beacon N. Y. Fort Edward N. Y. Fort Ticonderoga N. Y. Hudson N. Y. Mechanieville N. Y. New York (G.CT.) N.Y. New York (G.CT.) N.Y. Plattsburg N. Y.	\$9.28 12.93 7.13 5.37 14.04 10.57 8.50 15.32 15.43 2.72	\$18.21 24.90 13.91 10.39 26.65 20.79 16.65 28.55 28.80 5.09 6.09	\$11.99 17.84 9.11 6.79 19.52 14.14 11.04 21.47 21.81 3.39	21.68 41.65 42.25 6.38	8.50 8.50	3.00 2.45 3.95 4.20 1.30	New York (125th St.) N.Y. Poughkeepsie N.Y. Rhinecliff N.Y. Yonkers N.Y.	\$3.92 7.54 8.67 5.17 5.37 10.08 9.86 6.93 6.25 9.44	\$7.84 15.08 17.34 10.39 19.90 19.50 13.86 12.50 18.80	10.99		\$1.35 2.10 2.40 1.60 1.65 2.75 2.55 2.00 1.85 2.50	Roomette, one person \$8.50	
Harmon N.Y. Hudson N.Y. Hudson N.Y. Mechanicville N.Y. Mechanicville N.Y. New York (125th 8t) N.Y. New York (G.T.) N.Y. Plattsburg N.Y. Port Henry N.Y. Port Henry N.Y. Port Kent N.Y. Port Kent N.Y. Roratoga Springs N.Y. Watervillet N.Y. Watervillet N.Y. Westport N.Y. Westport N.Y. Willsboro N.Y. Willsboro N.Y. Vonkers N.Y.	3.22 12.29 11.64 1.75 7.71 8.96 4.27 6.22 3.70 14.88	6.09 23.80 22.70 3.15 15.07 17.57 8.19 12.09 7.05 27.90	5.96 4.03 16.84 15.88 2.00 10.00 11.66 5.37 7.92 4.70 20.77	7.66 33.00 31.35 3.60 19.60 22.92 19.34 15.44 9.00 40.45	8.50 8.50 8.50 8.50 8.50	1.60 1.30 3.40 3.25 1.30 2.40 2.75 1.50 2.00 1.40 3.90	Fort Edward, N. Y. to Atbany N. Y. Beacon N. Y. Harmon N. Y. Hudson N. Y. Horron N. Y. Wow York (G.C.T.) Y. Y. New York (125th 8t.) N. Y. Poughkeepsie N. Y. Rhinecliff N. Y. Yonkers N. Y.	2.18 5.81 6.94 3.43 7.13 8.35 8.13 5.20 4.52 7.71	13.88 6.86 13.91 16.70 16.26	12.80 12.44 7.85 6.81	17.56 21.08 10.38 17.82 25.55 24.88 15.70	1.30 1.60 1.90 1.30 2.20 2.05 1.50 1.35 2.00	*Rates shown apply when sp is occupied by one adult or by adult acompanied by a child children under 5 years of age. Minimum Number of ad Railroad tickets required exclusive occupancy of Sleing accommodations:	
Albany, N. Y. N. Y. Beacon. N. Y. Harmon. N. Y. Hudson. N. Y. Montreal P. Q. New York (G.C.T.) N. Y. New York (G.C.T.) N. Y. New York (125th St.) N. Y. Poughkeepile. N. Y. Rhineeliff. N. Y. Saratoga Springs. N. Y. Yonkers. N. Y.	7.47 11.09 12.22 8.72 1.75 13.63 13.41 10.48 9.80 5.97 12.99	14 .94 21 .60 23 .40 3 .15 25 .55 25 .15 20 .55 11 .94 24 .60	9.95 15.73 17.49 12.14 2.00 19.75 19.39 14.81 13.77 7.97 18.72	19.90 31.10 34.35 24.28 3.60 38.45 37.95 29.35 27.50 15.94 36.60	8.50 8.50 8.50	2.20 3.00 3.80 2.45 1.30 3.65 3.40 2.95 2.75 1.85 3.35	Saratoga Springs, N. Y. to Albany N.Y Beacon N.Y Harmon N.Y Hudson N.Y Montreal P.Q. New York (G.C.T.) N.Y New York (125th St.) N.Y Pougrikeepise N.Y Rimneeliff N.Y Yonkers N.Y	1.53 5.16 6.29 2.78 7.71 7.70 7.48 4.55 3.86 7.06	3.06 10.32 12.58 5.56 15.07 15.40	2.16 7.94 9.70 4.35 10.00 11.96 11.60	4 . 32 15 . 88 19 . 40 8 . 70 19 . 60 23 . 92 23 . 20 14 . 02	1.30 1.50 1.65 1.30 2.40 2.10 1.80 1.35 1.30	Roomette	
Albany N. Y. to	6.56 10.19 11.32 7.81 2.72 12.73 12.51 9.58 8.89 5.04 12.09	13.12 20.10 21.90 15.62 5.09 24.10 23.80 19.10 17.78 10.08 23.20	8.76 14.53 16.30 10.94 3.39 18.55 18.20 13.61 12.57 6.73 17.52	17 .52 28 .90 32 .15 21 .88 6 .38 36 .35 35 .60 27 .05 25 .14 13 .46 34 .45	8.50 8.50 8.50 8.50 8.50	1.90 2.90 3.00 2.40 1.30 3.30 3.15 2.70 2.60 1.50 3.10	Dining service i	s fe	THE Dail	LAU y (Da d or	JREN ylight 'Th	TIAN trip) ne Laur and si	L PASSENGERS	
BeaconN.Y.	5.01 8.63 9.77 6.26 4.27 1.17 0.95 8.02 7.34 3.52 0.53	10.02 17.26 19.30 12.52 8.19 21.70 21.35 16.04 14.68 7.04 20.65	12.44	13 .34 24 .88 28 .25 17 .70 10 .34 32 .45 31 .80 23 .04 20 .96 9 .36 30 .65		$\frac{2.45}{2.70}$	Rouses Point and beverages at reas	Allonal can d tri	ole proried properties plant	with ices. througase of	delici Whe gh to consul	ous luncen opera Windso t train o	cheons and all legal ting conditions war- or Station, Montreal	

ERS

MONTREAL LIMITED

On the "Montreal Limited" northbound, dining car is attached at Whitehall serving complete breakfasts into Windsor Station, Montreal. Southbound on this train, dining car is operated between Windsor Station, Montreal and Whitehall, serving light supper, snacks and all legal beverages.

WE SINCERELY APPRECIATE YOUR PATRONAGE



THE DELAWARE AND HUDSON RAILROAD

CORPORATION

Passenger representatives of the D. & H. R. R. located at the offices named below.

G. T. ALTHISAR, General Passenger Agent 360 Lexington Avenue New York 17, N. Y. Telephone YUkon 6-2742

TICKETS AND SLEEPING CAR RESERVATIONS

May be secured at following offices:

IN MONTREAL

D. & H. R. R. Corp., 1117 St. Catherine St., West, Phone Victor 9-3614 Note: After 5:00 P.M. and on Saturdays, Sundays and Holidays, Phone UN 1-6811.

C. P. Ry., Windsor Station, Phone UN 1-6811 C. P. Ry., 201 St. James St., West, Phone UN 1-6811

IN NEW YORK

D. & H. R. R. Corp., 360 Lexington Avenue Phone YUkon 6-1788 and 6-1789

Phone YUKON 0-1788 and 0-1789

Note: After 5:00 P.M. and on Saturdays, Sundays and Holidays, for reservations only, Phone MUrray Hill 7-6600.

N. Y. C. R. R., Reservation Bureau, Phone MUrray Hill 7-6600

N. Y. C. R. R., 165 Broadway, Room 2200, Zone 6, NYC-NY, Co.7-0400 or through your local ticket agent.

INFORMATION

Air-conditioned equipment is assigned as far as possible but the right is reserved to employ non air-conditioned cars as necessitated by volume of traffic or emergencies.

When regular trains are operated in Sections, only one Section may have Dining Car and Sleeping Cars.

REGARDING TIME TABLES—This Railroad is not responsible for errors in time tables, inconvenience or damage resulting from delayed trains or failure to make connections; schedules herein are subject to change without notice.

BUY TICKETS before boarding trains and avoid payment of extra charge.

CHILDREN under five years of age when accompanied by parent or guardian, will be carried free, except that if the sole occupant of Sleeping Car space, they will be charged half railroad fare and regular Sleeping Car fare; children between the ages of five and eleven years, inclusive, will be charged half fare, and children twelve years of age and over will be charged full fare.

ADJUSTMENT OF FARES—In case of misunderstanding with Conductors or Agents pay fare required, take receipt and communicate with General Passenger

Agent.

REDEMPTION OF TICKETS—Tickets unused or partly used, will be redeemed under tariff regulations at proper value.

BAGAGE—This Railroad does not guarantee to forward baggage on same train with passenger, or within a given or specified time limit, but reserves the right to forward it upon a preceding or following train.

BAGGAGE MAXIMUMS—No single piece of baggage exceeding 300 pounds in weight or 72 inches in greatest dimension or single shipment exceeding \$2,500.00 in value will be checked. Free allowance subject to tariff stipulations as to contents, weight, value and size.

contents, weight, value and size.

BAGGAGE LIABILITY LIMITED—Excess value to be declared and paid for at time of checking.

BAGGAGE FOR STATIONS WHERE NO AGENT IS ON DUTY must be claimed at baggage car door immediately on arrival of train at that station, otherwise it will be carried to the first station where agent is on duty, and held for inrither orders.

for further orders.

BICYCLES (not Motorcycles), BABY CARRIAGES AND GUNS are transported in baggage cars subject to tariff regulations.

DOGS AND SMALL HOUSEHOLD PETS are transported in baggage cars subject to tariff regulations. Small animals (not vicious or otherwise objectionable) when carried in baskets or containers, may be taken by purchasers of drawing rooms, compartments or bedrooms into the rooms they occupy. Dogs (except lap dogs in closed freecptacles) will not be permitted in passenger coaches. Exception.—The carrying of dogs or household pets of any nature in passenger accommodations to or from or between Canadian points is prohibited. "Seeing Eye" dogs will be allowed in any type private room or coaches, with totally blind person in Canada.

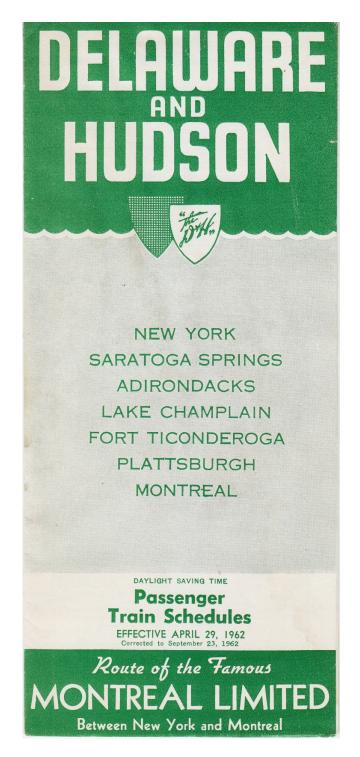
blind person in Canada.

LOST ARTICLES—When articles are lost on trains, or left in waiting rooms at stations, owners should apply at once to Agent at station where they leave the train, or communicate with General Passenger Agent. No responsibility is assumed for unchecked articles left in stations or cars.

GPA-26 Form 1

Delaware and Hudson Passenger Train Schedules, Effective April 29, 1962

New York, Saratoga Springs, Adirondacks, Lake Champlain, Fort Ticonderoga, Plattsburg, Montreal



NEW YORK—ALBANY—SARATOGA SPRINGS—PLATTSBURGH TO MONTREAL

Connecting trains to Ottawa-Quebec.

					1
Miles	Light Face — A.M. Bold Face — P.M.	NYC 95 Daily	NYC 51 D & H 35 Daily Through Train	NYC 61 D&H 9 Daily Through Train	QUALITY FOODS and
0.0 4.2 14.5 32.7 58.3 72.8 88.4 113.7 142.2	Harmon. " Beacon (Newburgh Ferry). " Poughkeepsie. " Rhinecliff " Hudson. "	6 55 r7 05 r7 22 r7 46 8 30 8 45 9 03 9 32 10 10	9 00 19 10 19 27 19 50	PM 11 45 r12 36 1 30	ATTENTIVE SERVICE
3.4.4.7 6.1.1 8.10.8 10.8 19.0 25.5.5 70.8 77.5 6116.7 127.7 141.9 154.4 167.5 190.8	ALBANY (D.&H.R.R.) Ly. Menands (+). Colonie (+). Watervillet(+). Conoes (N.C.B.) Met Waterford (N.C.B.). Meshanicville (+). Ballston Spa (+). Saratoga Springs. Fort Edward { Giene Falls Comstock (+). Like Ceorge Comstock (+). Like Ceorge Art Hield (Henry Comstock (+). Like Ceorge (Henry Comstock		PM 12 01	AM 3 02 Paper I Testuc W 4 27 4 4 42 B5 52 B6 12 6 57 7 31 7 36 7 48 7 7 48 7 7 59 a8 47 8 47 4 88 55 AM	Something to suit every taste. Miles slip by like magic. Join us, won't you in the
	Connecting Trains Via Gan. Pac. By. Montreal, Windsor Sta. Lv. Montreal West. Ar. Ottawa, Ont. Ar. Note—Quebec and Ottawa I Montreal West from D&H		Ex. Sat. Note PM PM 6 05 7 30 5 45 6 20 7 40 5 56 10 05 11 15 8 45 PM PM Sc change at 35.	AM AM 10 10 9 50 10 25 10 01 2 00 12 50 PM	D. & H. Dining Car.

REFERENCE MARKS FOR ABOVE SCHEDULE

- B Stops to discharge passengers from Albany and points south on notice to conductor and will stop on station signal to receive passengers for Rouses Point and points north.

 N Stops to discharge passengers from points south of Albany and stops on signal to receive passengers for Saratoga Springs and points north.
- Passengers not carried locally between these station
- f Stops to discharge passengers on notice to conductor and will stop on station signal to receive passengers.

 r Stops only to receive passengers.
- t Stops to discharge passengers.
- Non-Agency Station. No baggage handled. • Change trains-same station.
- This train does not carry checked baggage New York to Albany.
- N.B.—No facilities for handling checked baggage at this station.

EQUIPMENT

NORTHBOUND TRAINS Coach service available on all passenger trains.

THE LAURENTIAN - Daily

(Daylight trip) N. Y. C. 51-D. & H. 35

*Coaches — New York to Montreal
Through Reclining Seat Coaches (Unreserved)
*Diner — New York to Albany
*Dining Car — Albany to Rouses Point (Note)
*Parlor Car—New York to Montreal (Railroad owned)—Car 5135.

MONTREAL LIMITED - Daily

(Night trip) N. Y. C. 61 - D. & H. 9

*Coaches — New York to Montreal
Through Reclining Seats (Unreserved)
*Sleeping Cars — New York to Montreal (open 10:00 P.M.) No.
(Railroad owned)

*Assigned cars are air-conditioned

Note-When conditions warrant, dining car will operate to Montreal. Consult Train Conductor.

INFORMATION FOR PERSONS ENTERING CANADA FROM THE UNITED STATES

Immigration Regulations

American tourists and visitors are cordially invited to Canada and do not require passports to enter the Dominion. Citizens of the United States still retain their prewar freedom to enter and travel in Canada without new restrictions. In order to facilitate crossing the international boundary in each direction, citizens of the United States by naturalization should be prepared to present their naturalization certificates and citizens of other countries should be prepared to establish their legal admission to the United States either by record of admission or certificate of reentry.

CUSTOMS REGULATIONS

Travelers' Baggage

General Provisions — Wearing apparel, articles of personal adornment, toilet articles and similar personal effects of persona arriving in Canada may be passed free, without entry at Customs as travelers, baggage, under the provisions of the Customs Tarifi, but the provisions of the Customs Tarifi, but the provisions of the Customs Tarifi, but the provisions of the Customs are necessary and appropriate companies of the customs of the Customs are necessary and appropriate companies of the provisions of the Customs in the same and are in the use of such persons for the immediate purpose of the journey and present comfort and convenience, and shall not be held to apply to merchandise or articles intended for other persons or for sale. Merchandise for sale, when brought into Canada as baggage is subject to duty and to entry at Customs in the same manner as goods imported by freight or express.

Cigars and Cigarettes in personal baggage of non-residents — Fifty cigars or less, two hundred cigarettes or less and two pounds of manufactured tobacco or less, in open packages, or in cartons, 1 Quart Alcoholic Beverage, may delivered free of duty.

Returning Canadian Resident Exemption—Goods valued at not more than twenty-five dollars included in the baggage accompanying residents of Canada returning from abroad after an absence from Canada of not less than 48 hours and acquired by them for personal or household use or as souvenirs or gifts, but not bought on commission or as an accommodation for other persons or for sale, will be admitted free, including 200 cigarettes or less than 50 cigars or less, and two pounds of manufactured tobacco or less. Providing that a resident of Canada shall not be entitled to the exemption herein granted within a period of four months from the date of the last exemption allowed.

Returning residents must state in their declaration in Canadian currency the price actually paid for all goods purchased abroad in respect of which exemption is claimed, and must state a fair market value for all g

9

MONTREAL TO PLATTSBURGH—SARATOGA SPRINGS—ALBANY—NEW YORK

Connecting trains from Ottawa-Quebec.

	Connecting Trains Via Can. Pac. Ry. OTTAWA, Union Sta	AM 8 15 10 15 10 30 AM	PM 7 40 9 30 9 45 PM	6 00 9 45 10 00 PM
Miles	Light Face — A.M. Bold Face — P.M.	D&H 34 NYC 90 Daily through Train	D&H1(NYC 46 Daily Ex. Sat. through Train	D&H 12 NYC 6 Sat only through Train
	Windsor Sta. (N.J.Ry) . Lv. MONTREAL Westmount (N.J.Ry)	a10 50 a10 57 a11 05 f11 39 11 49 11 59	EX. Sat. 11 22 Sat. 12 S	a10 45 a10 52 a11 00 f11 36 ts 11 47 11 57-
23.5	Rouses Point(D.&H.R.R.) { Ar. Lv. Plattsburgh "	12 12 12 41	11 22 XI 11 28 11 58	12 03 -
36.4 48.9	Willsboro (Flizabethtown)	f 1 20 =	M12 37_	M1 13
63.1	Wilsooro Westport { Elizabethtown} Port Henry Fort Ticonderoga (+) / Ticonderoga Whitehall (Rutland).	1 40 1 1 57 2 anneutia	M12 58	M1 34
74.1 91.2	Port Henry	1 57 2 5 2 5 2 5 5 6 5 6 5 6 6 6 6 6 6 6 6 6	bu	
13.3	Whitehall (Rutland)	2 53 -	Montreal Limited 88.00	2 42 Aluo
20.0	Comstock (+)		mol	Montreal Limited
35.3	Comstock (+). Fort Edward Glens Falls Lake George	3 26 3 50	sal Limit	Montreal Limited
53.7 59.3	Ballston Spa (+)	3 00	al L	al L
65.3 71.8	Mechanicville (+)	4 13	Montrea not run S	itre
80.0	West Waterford (N.C.B.)" Cohoes (N.C.B.)"		Mor ot r	Mon
84.7 86.1	Waterviet (+)	R4 33	III n	
87.4	Menands (+)		≥	
90.8	ALDANT(D.a.n.n.n.)	4 45 PM	4 05 AM	4 40 AM
_	ALBANY (N.Y.C.R.R.) Lv.	F 00	4 25	
28.5		5 00 G 5 33 P 5 56 6 15 J 6 30	4 58	5 00
53.8	Poughkeepsle	P 5 56 6 15	5 35	6 10
33.9	Beacon (Newburgh Ferry) " Harmon	6 15 J 6 30 t 7 07	□5 50 t 6 32	t 6 57
27.7 38.0	Yonkers	t 7 33	t 6 32 t 6 59	t 7 24
42.2	Hudson. Ar. Rhinecilf. Poughkeepsle. Beacon (Newburgh Ferry). " Harmon. 125th St. " NEW YORK Grand Central (N.Y.C.R.R.) Terminal Ar.	8 05	7 30	7 55
		PM	AM	AM

REFERENCE MARKS FOR ABOVE SCHEDULE

M Stops to discharge passengers from Rouses Point and points north on notice to conductor and will stop on station signal to receive passengers for Albany and points south.

G Stops Saturdays, Sundays and May 30, July 4 and Sept. 3, 1962.

J Stops daily, except Saturday and Sunday.
P Stops Sundays and May 30, July 4 and Sept. 3, 1962.
R Stops to discharge passengers from Saratoga Springs and points north and stops on signal to receive passengers for points south of Albany.

a Passengers not carried locally between these stations.

f Stops to discharge passengers on notice to conductor and will stop on station signal to receive passengers.

t Stops only to discharge passengers.

t Stops only to discharge passengers.

Non-Agency Station. No baggage handled.

Stops Saturday only.

Advance seat reservation necessary on this train.

N.C.B.—No facilities for handling checked baggage at this station.

SOUTHBOUND TRAINS

Coach service available on all passenger trains.

THE LAURENTIAN - Daily

(Daylight trip) D. & H. 34 - N. Y. C. 90

*Coaches — Montreal to New York
Through Reclining Seat Coaches (Unreserved)
*Dining Car — Rouses Point to Albany
*Diner-Lounge — Albany to New York
*Parlor Car — Montreal to New York (Railroad owned) — Car 340

MONTREAL LIMITED - Daily

 $\begin{array}{c} {\rm (Night\ trip)\ D.\ \&\ H.\ 10-N.\ Y.\ C.\ 46-Ex.\ Sat.} \\ {\rm (Night\ trip)\ D.\ \&\ H.\ 12-N.\ Y.\ C.\ 6-Sat.\ Only} \end{array}$

★Coaches — Montreal to New York Through Reclining Seats (Unreserved)

★Sleeping Cars — Montreal to New York (open 9:45 P.M.).	No.	No.	
(Railroad owned.)	Ex. Sat.	Sat. Only	
10 Roomette — 6 Double Bedroom	1029	1229	
10 Roomette — 6 Double Bedrooms	1030	1230	
*Dining Car - Montreal to Whitehall (Beverages-Supper)			
6 Double Bedroom — Lounge	1031	1231	
10 Roomette — 6 Double Bedrooms	1039	1929	

* Assigned cars are air-conditioned.

INFORMATION FOR PERSONS ENTERING THE UNITED STATES FROM CANADA

Immigration Regulations

Immigration Regulations
United States citizens do not require passports when reentering the United States after a visit to Canada. It is recommended, however, that they be in possession of such personal papers or other documents as may be helpful in establishing that they are citizens of the United States. The most common documents which help to establish United States citizenship are birth certificates showing place of birth, baptismal certificates, naturalization certificates, and united States passports, valid or expired.

Passports and visas are waived for Canadian citizens and British subjects who reside in Canada and who are entering temporarily across the Canadian border. Such persons may be admitted as visitors for an appropriate period not to exceed six months. All other aliens seeking to enter the United States are required to be in possession of a valid passport and appropriate visa issued by an American Consul.

Allens returning to legal residence in the United States must be in possession of their Alien Registration Receipt and Border Crossing Card (FORM 1-151) or a valid re-entry permit.

Customs Regulations

Customs Regulations

Residents of United States may take into Canada their wearing apparel and other personal effects and upon return to United States are entitled to have the same admitted free of duty. Residents of United States upon return must declare all articles acquired abroad, in their baggage, on their person or to be forwarded to them, whether by purchase, by gift, or otherwise, and whether dutiable or free of duty.

Each returning resident of the United States is entitled to an exemption from duty on articles up to a total of One Hundred Dollars (\$100.00) in value acquired outside the United States under the following conditions, if declared when the resident arrives at the United States port of entry:

esident arrives at the United States port of entry:

1. Provided they have been outside the United States over forty-eight hours.

2. Provided they have had no previous exemption for the past thirty days.

3. Provided they have had no previous exemption for the past thirty days.

3. Provided they are had no previous exemption for the past thirty days.

4. Provided they are acquired as an incident of the foreign journey.

5. Foodstuffs, eigarettes, tobacco, 100 eigars, and not over one wine gallon of distilled spirits, wines or malt liquors (provided the quantity is not prohibited by State law) may be included in this exemption.

6. Each member of a family is entitled to the \$100 exemption for articles acquired abroad of the character described in the third provise to Paragraph 1798, Tariff Act of 1930, as amended, subject to the conditions prescribed therein. Articles belonging to one passenger cannot be included in the exemption of another, except that when members of a family residing in one household travel together on their return to the United States, the exemptions to which the several members of the family are entitled may be grouped and allowed without regard to which member is the owner, provided that the grouped exemption shall not include that of any person who is not entitled to such an exemption, nor be allowed for the property of any such person.

The owner of valuable effects of foreign origin, sealskin garments, plumage, or other articles, prior to his departure from the United States, may make an application to the collector or his representative on Customs Form 4455 for the registration of such articles to facilitate their identification on return to the United States.

ALBANY AND BINGHAMTON

208 Except Sun.	Miles Coaches on all Trains S	Shown 205 Except Sun.
PM 4 4 4 5 5 5 5 5 5 6 6 6 6 6 7 7 7 7 7 7 8 P P 4 4 2 5 5 5 5 5 6 6 6 6 6 6 5 7 7 7 7 7 8 P P 4 2 5 5 5 5 5 6 6 6 6 6 6 6 6 6 5 7 7 7 7 7	0 Lv. ALBANY 5.5 "Delmar + 10.9 "Voorheesville - 117.2 "Altamont. 26.8 "Delanson + 36.2 "Central Bridge 39.2 "Howes Cave. 44.7 "Cobleskill. 50.0 "Richmondville. 56.9 "East Worcester. 66.5 "Worcester. 66.5 "Worcester. 76.4 "Colliers + 76.4 "Colliers + 78.6 "Otego + 98.8 "Unadilla 103.3 "Sidney. 108.5 "Bainbridge. 114.3 "Afron. 120.3 "Harpursville - 120.3 "Harpursville - 142.5 Ar. BINGHAMTON	#

f Stops to discharge passengers on notice to conductor and will stop on station signal to receive passengers.

+ Non-Agency Station. No baggage handled.

MONEY SAVING ROUND TRIP FARES

FAMILY FARES—Apply to husband and wife or to either or both parents and accompanying children-one parent pays the regular round-trip adult fare; the accompanying parent, if any, and each child 12 and under 22 pay the regular one way adult fare while each child 5 and under 12 is charged 1/2 of the regular one way adult fare. Children over 22 are not to be included in the round trip family ticket; children under 5 are carried free except when occupying parlor car seat. Liberal baggage privileges . . . Six-month limit . . . On sale every day . . . For travel in coaches or sleeping and parlor cars.

GROUP ECONOMY FARES—Apply for parties of three or more adults or their equivalent (two children 5 and under 12 to be considered the equivalent of one adult) . . . Group must travel together on both going and return journeys . . . Good only in coaches . . . Liberal baggage privileges . . . 30 day limit.

WHITEHALL BUS

Except Sun.	Railroad tickets not accepted	Except Sun.
PM 1 45 2 00 2 05 2 15 2 30 2 45 PM	Lv. Ar. Whitehall, (Bus Terminal) N. Y. Fair Haven. Hydeville. Castleton Corners (Lake Bomoseen) Castleton. Rutland, Vt. Lv. Lv.	12 45 T 12 40 T

DELAWARE & HUDSON FREIGHT TRAFFIC OFFICES

The freight traffic offices listed below are well-equipped to furnish information and assistance to the shipping public. Any representative will be grateful for the opportunity of serving you.

ALBANY 1, N. Y.

J. M. O'Donnell, Division Sales Manager, D. & H. Building
Phone Hobart 3-1141

ATLANTA 3, GA.

Raymond O. Beebe, Southeastern Sales Manager, Room 701, 101 Marietta St. Bldg. Phone Jackson 2-5464

BOSTON 14, MASS.

A. P. Lehman, New England Sales Manager, Room 1101, North Station Office Building, 150 Causeway Street Phone Capitol 7-7175

BUFFALO 2, N. Y.

H. E. Andrews, District Sales Manager, 601 Bank of Buffalo
Phone TL 6-4646

CHICAGO 4, ILL.

D. E. Wilkinson, District Sales Manager, Room 840, 327 South LaSalle Building Phone WA 2-5574

CLEVELAND 13, OHIO

J. F. Hartman, District Sales Manager, 1236-7-8 Terminal Tower Bidg. Phone Main 1-1405

MONTREAL 2, QUE.

G. C. Seaman, Canadian Sales Manager, 106 Drummond Bldg., 1117 St. Catherine St., West Phone Victor 9-7424

NEW YORK 17, N. Y.

J. A. Quinn, District Sales Manager, 360 Lexington Avenue Phone YUkon 6-2886

PHILADELPHIA 3, PA.

J. J. Kaelin, District Sales Manager, 308 Transportation Center Building Phone LOcust 3-1271

PITTSBURGH 19, PA.

W. H. Hartman, District Sales Manager, 2818 Koppers Building Phone GRant 1-5516 and GRant 1-5517

PORTLAND 3, ME.

I. W. Kelley, District Sales Manager, 310 Congress Building, 142 High Street Phone Spruce 3-1072

ST. LOUIS 1, MO.

J. H. Reddy, District Sales Manager, 2084 Railway Exchange Building Phone Chestnut 1-9178

SAN FRANCISCO 5, CALIF.

A. E. Duprey, District Sales Manager, 457 Monadnock Bldg., 681 Market Street Phone Garfield 1-6499

SCRANTON 9, PA.

B. J. Carlin, Division Sales Manager, 703 Wyoming Avenue
Phone Diamond 2-8751

WINSTON-SALEM 3, N. C.

R. G. Surridge, District Sales Manager, Room 1410, Reynolds Bldg.
Phone PArk 3-4451

Paul F. Robinson, Vice-President Traffic, D. & H. Bldg., Albany 1, N. Y.

C. J. Connolly, Coal Traffic Manager, D. & H. Bidg., Albany 1, N. Y.

W. R. St. John, Freight Traffic Manager, D. & H. Bldg., Albany 1, N. Y. Leonard Unger, General Sales Manager, D. & H. Bldg., Albany 1, N. Y.

RAILROAD — SLEEPING — PARLOR CAR FARES

Consult Ticket Agent about round-trip Family Fares. Also Group Economy coach excur-sion fares for parties of three of flore adults (or equivalent).

(SUBJECT TO CHANGE)

Tickets Sold In United States Subject To Federal Tax

▲—Indicates rail tickets honored in Sleeping care or Parlor Cars on Payment of charges for space occupied.

Sleeping Car Fares Montreal P.Q. and

New York and intermediate points

one person... \$8.50

*Rates shown apply when space is occupied by one adult or by one adult accompanied by a child or children under 5 years of age.

Minimum Number of adult Railroad tickets required for exclusive occupancy of Sleeping accommodations: (See Note below for Family Fare ticket requirements.) Roomette 1

Bedroom 1 Note: The minimum ticket requirements for Family Fare tickets.

Two or More Persons

\$18.05

Roomette.

One \$14.75

Rates in

Parlor

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464	kson 2-		Rail Tic	ket Fa	res	Rat	es in			Rail Tic	eket Far	res
ion 175	Coaches, Sleeping and Parlor Cars are Railroad operated	In C	oaches	In Slee	eping or Car	Roomette One Passenger	Į.	Coaches, Sleeping and Parlor Cars are Railroad operated	In C	oaches	In Slee	ping or Car A
	12-60	One Way	Round Trip	One Way	Round	Roon Ol Passe	Parlor		One Way	Round Trip	One Way	Round Trip
1116	Albany N.Y. Beacon N.Y. Fort Edward N.Y. Fort Edward N.Y. Albany N.Y. Fort Edward N.Y. Fort Edward N.Y. Albany N.Y. Albany N.Y. Albany N.Y. Mechanieville N.Y. New York (125th St. N.Y. New York (125th St. N.Y. N.Y. Albany N	\$9.28 12.93 7.13 5.37 14.04 10.57 8.50 15.32 15.43 2.72 4.67	\$18.21 24.90 13.91 10.39 26.65 20.79 16.65 28.55 28.55 28.80 5.09	\$11.99 17.84 9.11 6.79 19.52 14.14 11.04 21.47 21.47 21.81 3.39 5.96	\$23.58 34.95 17.82 13.18 21.68 21.68 41.65 42.25 6.38 11.52 7.66 33.00 31.35 3.60 19.60 22.92 10.34 15.44 9.00 40.45	8.50 8.50 8.50	\$2.75 3.65 2.20 1.65 3.90 2.45 3.95 4.20 1.30	Fort Ticonderoga, N. Y. to Albany	\$3.92 7.54 8.67 5.17 5.37 10.08 9.86 6.93 6.25 9.44	\$7.84 15.08 17.34 10.39 19.90 13.86 12.50 18.80	10.99 12.76 7.40 6.79 15.02 14.66 10.07 9.03	21.98 25.50 14.80 13.18 29.80 29.15 20.14 18.06
14.	FOFF Refit N. Poughkeepsle N. N. Rhineelin N. Y. Rouses Point N. Y. Sagatoga Springs N. Y. Y. Wilson N. Y. Wilson N. Y. Wilson N. Y. Wilson N. Y. Yonkers N. Y. Yonkers N. Y. Yonkers N. Y. Point N. Y. Yonkers Y. Y	3.422 12.29 11.64 1.75 7.71 8.96 4.27 6.22 3.70 14.88	23 .80 22 .70 3 .15 15 .07 17 .57 8 .19 12 .09 7 .05 27 .90	4.03 16.84 15.88 2.00 10.00 11.66 5.37 7.92 4.70 20.77	7.66 33.00 31.35 3.60 19.60 22.92 10.34 15.44 9.00 40.45	8.50 8.50 8.50 8.50 8.50	1.30 3.40 3.25 1.30 2.40 2.75 1.50 2.00 1.40 3.90	Fort Edward, N. Y. to	2.18 5.81 6.94 3.43 7.13 8.35 8.13 5.20 4.52 7.71	4 . 36 11 . 62 13 . 88 6 . 86 13 . 91 16 . 70 16 . 26 10 . 40 9 . 04 15 . 42	3.00 8.78 10.54 5.19 9.11 12.80 12.44 7.85 6.81 11.77	6.00 17.56 21.08 10.38 17.82 25.55 24.88 15.70 13.62 23.54
2	Rouses Point, N. Y. to Albany, N. Y. N. Y. Beacon. N. Y. Y. Hudson. N. Y. Hudson. N. Y. Hudson. N. Y. Hudson. N. Y. Y. Hudson. N. Y. Y. N. Y.	7.47 11.09 12.22 8.72 1.75 13.63 13.41 10.48 9.80 5.97 12.99	14.94 21.60 23.40 17.44 3.15 25.55 25.55 20.55 19.45 11.94 24.60	9.95 15.73 17.49 12.14 2.00 19.75 19.39 14.87 7.97 18.72	31.10 34.35 24.28 3.60 38.45 37.95 29.35 27.50	8.50 8.50 8.50 8.50	2.20 3.00 3.80 2.45 1.30 3.65 3.40 2.95 2.75 1.85 3.35	Saratada Sariade N. V. to	1.53 5.16 6.29 2.78 7.71 7.70 7.48 4.55 3.86 7.06	3.06 10.32 12.58 5.56 15.07 15.40 14.96 9.10 7.72 14.12		4.32 15.88 19.40 8.70 19.60 23.92
Salaran and Salaran Salaran and Salaran Salara	Plattoburgh, N. Y. to - Abbäny N. Y. Beacon N. Y. Harmon N. Y. Montreal P.Q. Hew York (G.C.T.) N. Y New York (G.C.T.) N. Y. Ribinecilit N. Y. Ribinecilit N. Y. Yorkers N. Y Yorkers N. Y Yorkers N. Y Yorkers N. Y I Mel N. Y.	6.56 10.19 11.32 7.81 2.72 12.73 12.51 9.58 8.89 5.04 12.09	13.12 20.10 21.90 15.62 5.09 24.10 23.80 19.10 17.78 10.08 23.20	8.76 14.53 16.30 10.94 3.39 18.55 18.20 13.61 12.57 6.73 17.52	17.52 28.90 32.15 21.88 6.38 36.35 35.60 27.05 25.14 13.46 34.45	8.50	1.90 2.90 3.00 2.40 1.30 3.35 2.70 2.60 1.50 3.10	D. & H. DINI Dining service is between Albany	fe and	THE Dail	LAU ly (Da	JRENT hylight to "The Point,
	Westport, N. Y. to Albany, N. Y. Beacon, N. Y. Harmon, N. Y. Hudson, N. Y. New York (G.CT.), N. Y. New York (G.CT.), N. Y. New York (G.CT.), N. Y. Poughkeepsie, N. Y. Rajaicelin, N. Y. Yonkees, N. Y. Yonkees, N. Y. Yonkees, N. Y.	5.01 8.63 9.77 6.26 4.27 11.17 10.95 8.02 7.34 10.53	16.04	6.67 12.44 14.21 8.85 5.37 16.46 16.11 11.52 10.48 4.68 15.44	13 .34 24 .88 28 .25 17 .70 10 .34 32 .45 31 .80 23 .04 20 .96 9 .36 30 .65	8.50 8.50 8.50 8.50 8.50	1.50 2.45 2.70 1.85 1.50 2.95 2.95 2.20 2.10 1.30 2.80	Rouses Point and beverages at reas- rant, dining car is on the northbound	onak car tri	ole pried p; ple	throusease	Whe
ł	Whitehall, N. Y. to M Ahāny	3.05 6.67 7.80 4.29 6.22 9.21 8.99 6.06	6 . 10 13 . 34 15 . 60 8 . 58 12 . 09 18 . 40 17 . 98 12 . 12 10 . 76 17 . 14	4.10 9.87 11.63 6.28 7.92 13.89 13.53 8.95 7.91 12.86	8.20 19.74 23.26 12.56 15.44 27.75 27.00 17.90 15.82	8.50 8.50 8.50 8.50 8.50	1 20	On the "Montreal Whitehall serving Montreal. South tween Windsor S supper, snacks and WE SINCER	co coun ctation	mple d on on, N legal	te b this Montr I bev	reakfa train, eal ar erages

E FOR ALL PASSENGERS

RENTIAN ight trip)

"The Laurentian" northbound oint, and southbound between elicious luncheons and all legal When operating conditions war-h to Windsor Station, Montreal insult train conductor.

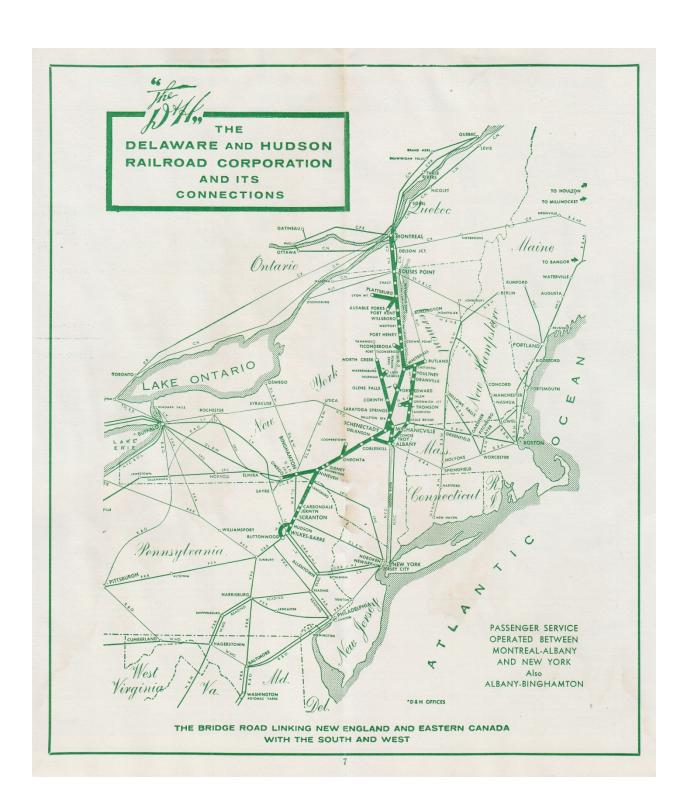
LIMITED

ght trip)

bound, dining car is attached at eakfasts into Windsor Station, Montreal. Southbound on this train, dining car is operated be-tween Windsor Station, Montreal and Whitehall, serving light supper, snacks and all legal beverages.

WE SINCERELY APPRECIATE YOUR PATRONAGE

6



THE DELAWARE AND HUDSON RAILROAD CORPORATION

Passenger representatives of the D. & H. R. R. located at the offices named below.

NEW YORK (17), N. Y.— Telephone YUkon 6-1788 and 6-1789.

J. M. Kane....... City Passenger Agent............................. 360 Lexington Avenue, 2nd floor

G. T. ALTHISAR, General Passenger Agent 360 Lexington Avenue New York 17, N. Y. Telephone YUkon 6-2742

TICKETS AND SLEEPING CAR RESERVATIONS

May be secured at following offices:

IN MONTREAL

D. & H. R. R. Corp., 1117 St. Catherine St., West, Phone Victor 9-3614 Note: After 5:00 P.M. and on Saturdays, Sundays and Holidays, Phone UN 1-6811.

C. P. Ry., Windsor Station, Phone UN 1-6811 C. P. Ry., 201 St. James St., West, Phone UN 1-6811

IN NEW YORK

IN NEW YORK
D. & H. R. R. Corp., 360 Lexington Avenue
Phone YUkon 6-1788 and 6-1789
Note: After 5:00 PM. and on Saturdays, Sundays and Holidays,
for reservations only, Phone MUrray Hill 7-6600
N. Y. C. R. R., Reservation Bureau, Phone MUrray Hill 7-6600
N. Y. C. R. R., 165 Broadway, Room 2200, Zone 6, NYC-NY, Co.7-0400
or through your local ticket agent.

INFORMATION

Air-conditioned equipment is assigned as far as possible but the right is reserved to employ non air-conditioned cars as necessitated by volume of traffic or emergencies.

When regular trains are operated in Sections, only one Section may have Dining Car and Sleeping Cars.

REGARDING TIME TABLES—This Railroad is not responsible for errors in time tables, inconvenience or damage resulting from delayed trains or failure to make connections; schedules herein are subject to change without notice. BUY TICKETS before boarding trains and avoid payment of extra charge. CHILDREN under five years of age when accompanied by parent or guardian, will be carried free, except that if the sole occupant of Sleeping Car space, they will be charged half railroad fare and regular Sleeping Car fare; children between the ages of five and eleven years, inclusive, will be charged half fare, and children twelve years of age and over will be charged full fare.

ADJUSTMENT OF FARES—In case of misunderstanding with Conductors or Agents pay fare required, take receipt and communicate with General Passenger Agent.

Agent,
Agent,
REDEMPTION OF TICKETS—Tickets unused or partly used, will be redeemed under tariff regulations at proper value.

BAGGAGE—This Railroad does not guarantee to forward baggage on same train with passenger, or within a given or specified time limit, but reserves the right to forward it upon a preceding or following train.

BAGGAGE MAXIMUMS—No single piece of baggage exceeding 300 pounds in weight or 72 inches in greatest dimension or single shipment exceeding \$2,500.00 in value will be checked. Free allowance subject to tariff stipulations as to contents, weight, value and size.

BAGGAGE LIABILITY LIMITED—Excess value to be declared and paid for at time of checking.

at time of checking.

BAGGAGE FOR STATIONS WHERE NO AGENT IS ON DUTY must be claimed at baggage car door immediately on arrival of train at that station, otherwise it will be carried to the first station where agent is on duty, and held for further orders.

BICYCLES (not Motorcycles), BABY CARRIAGES AND GUNS are transported in baggage cars subject to tariff regulations.

ported in baggage cars subject to tariff regulations.

DOGS AND SMALL HOUSEHOLD PETS are transported in baggage cars subject to tariff regulations. Small animals (not vicious or otherwise objectionable) when carried in baskets or containers, may be taken by purchasers of drawing rooms, compartments or bedrooms into the rooms they occupy. Dogs (except lap dogs in closed receptacles) will not be permitted in passenger coaches. Exception.—The carrying of dogs or household pets of any nature in passenger accomodations to or from or between Canadian points is prohibited. "Seeing Eye" dogs will be allowed in any type private room or coaches, with totally blind person in Canada.

LOST ARTICLES. When articles.

COST ARTICLES—When articles are lost on trains, or left in waiting rooms at stations, owners should apply at once to Agent at station where they leave the train, or communicate with General Passenger Agent. No responsibility is as-sumed for unchecked articles left in stations or cars.

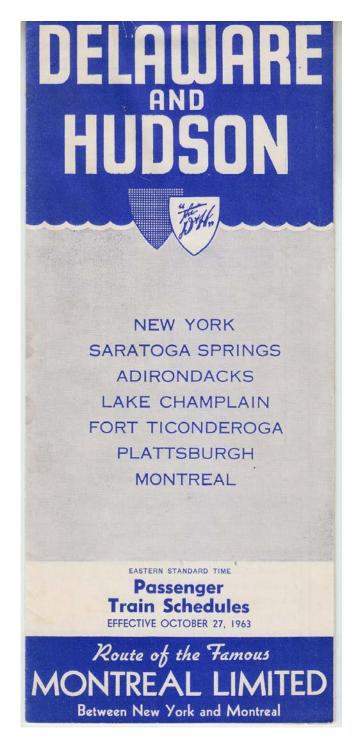
GPA-18

Form 1 No. 3

Printed in U.S.A.

Delaware and Hudson Passenger Train Schedules, Effective October 27, 1963

New York, Saratoga Springs, Adirondacks, Lake Champlain, Fort Ticonderoga, Plattsburg, Montreal



NEW YORK—ALBANY—SARATOGA SPRINGS—PLATTSBURGH TO MONTREAL Connecting trains to Ottawa—Quebec.

Mile	Light Face — A.M. Bold Face — P.M.	NYC 95 Daily	NYC 51 D & H 35 Daily Through Train	NYC 61 D&H 9 Daily Tar ough Train	QUALITY
	N.Y.C R,R,)	AM	AM	PM	and
0.0	NEW YORK Grand Central	6 40	8 45	10 15	
4.2 14.5 32.7	Centon-Harmon "	r6 50 r7 08 r7 31	r8 55 r) 12 r) 35		ATTENTIVE
58.3 72.8	Baacon (Bus from Newburgh)" Pougakeepsie. " Rhinecilft " Hudson. "	8 15 8 30		r11 06	SERVICE
88.4 113.7	Rhineclift	8 48 9 17			
142.2	ALBANY(N.Y.C.R.R.) Ar.	9 5 6	11 30	1 15	
	ALBANY(D.&H.R.R.) Lv.	1	AM 11 44	AM	Something
19.6	Watervliet(+). (Troy). " IMechanicvlire(+). " Saratoga Springs. "		N11 53	1 35	to suit
37.1 55.5	Saratoga Springs		12 32 12 54		every
70.8	Comstock (+)		t 1 09		taste.
77.5 99.6	Whitehall (Rutland)		1 18 1 23 1 53	3 10	
116.7	Fort Ficonderoga(+) (Ticonderoga) " Port Henry"		2 19	3 10 3 10	Miles
127.7	Westport Lake Placid Saranac Lake		The Laurentian 5 2 3 3 5 2 5 2 5 2 5 2 5 2 5 5 5 5 5 5	B4 27	slip by
141.9 154.4	Port Hanry			B4 48 B4 48	like
167.5	Plattsburgh		3 43 4 15	5 39 6 19	magic.
	Lacolle, P.Q(N.J.Ry) " Napierville, P.Q(N.J.Ry) "		4 23 4 23 4 35 6 4 40 a5 227 a6 35	6 27 6 41 f6 53	Join us,
	MONTREAL Westmount (N.J.Ry)Ar.		a5 20	a7 35 a7 42	
	P.Q. (Windsor Station N.J.Ry "		a6 35 PM	a7 50	won't you
	0		1 1 2	10	in the
	Connecting Trains //a Can. Pac. Ry.		Ex. Sat.		D. & H.
	Montreal, Windsor Sta. Lv. Montreal West		6 00 7 45 5 35 6 15 7 55 5 46	9 35 8 10 9 50 8 22 1 30	Dining
	Quebec, Palais Sta		10 00 11 30 8 35 PM PM PM	1 30 10 18 PM AM	Car.
	①Via Vankleek Hill ②Ottawa passenger change at Mon	treal We			

REFERENCE MARKS FOR ABOVE SCHEDULE

- B Stops to discharge passengers from Albany and points south on notice to conductor and will stop on station signal to receive passengers for Rouses Point and points north.
- N Stops to discharge passengers from points south of Albany and stops on signal to receive passengers for Saratoga Springs and points north.
- a Passengers not carried locally between these stations.
- f Stops to discharge passengers on notice to conductor and will stop on station signal to receive passengers.
- r Stops only to receive passengers.
- t Stops to discharge passengers.
- Change trains-same station.
- This train does not carry checked baggage New York to Albany.
- N.C.B .- No facilities for handling checked baggage at this station.
- + Non-Agency Station. No baggage handled.
- N.A.-Non-Agency station, Baggage handled.

EOUIPMENT

NORTHBOUND TRAINS

Coach service available on all passenger trains.

THE LAURENTIAN - Daily

(Daylight trip) N. Y. C. 51-D. & H. 35

*Coaches — New York to Montreal
Through Reclining Seat Coaches (Unreserved)
*Diner — New York to Albany
*Dining Car — Albany to Rouses Point (Note)

★Parlor Car—New York to Montreal (Railroad owned)—Car 5135.

MONTREAL LIMITED - Daily

(Night trip) N. Y. C. 61 - D. & H. 9

★Coaches — New York to Montreal (Rented pillows available)
Through Reclining Seats (Unreserved)

*Assigned cars are air-conditioned

Note—When conditions warrant, dining car will operate to Montreal. Consult Train Conductor.

INFORMATION FOR PERSONS ENTERING CANADA FROM THE UNITED STATES

Immigration Regulations

American tourists and visitors are cordially invited to Canada and do not require passports to enter the Dominion. Citizens of the United States still retain their prewar freedom to enter and travel in Canada without new restrictions. In order to facilitate crossing the international boundary in each direction, citizens of the United States by naturalization should be prepared to present their naturalization certificates and citizens of other countries should be prepared to establish their legal admission to the United States either by record of admission or certificate of reentry.

CUSTOMS REGULATIONS Travelers' Baggage

General Provisions — Wearing apparel, articles of personal adornment, toilet articles and similar personal effects of persona arriving in Canada may be passed free, without entry at Customs as travelers, baggage, under the provisions of the Customs Tarifi, but this provision shall include only such articles as actually accompany and are in the use of and as are necessary and appropriate for the wear and use of such persons for the immediate purpose of the journey and present comfort and convenience, and shall not be held to apply to merchandise or articles intended for othe persons or for sale. Merchandise for Sale, when brought into Canada as baggage is subject to duty and to entry at Customs in the same manner as goods imported by freight or express.

Cigars and Cigarettes in personal baggage of non-residents — Fifty cigars or less, two hundred cigarettes or less and two pounds of manufactured tobacco or less, in open packages, or in cartons, 1 Quart Alcoholic Beverage may be included free of duty.

Returning Canadian Resident Exemption—Goods valued at not more than twenty-five dollars included in the baggage accompanying residents of Canada returning from abroad after an absence from Canada of not less than 48 hours and acquired by themore the personal or household use or as souvenirs or gifts, but not bought on commission or as an accommodation for other persons or for sale, will be admitted free, including 200 cigarettes or less than 16 personal or household use or as souvenirs or gifts, but not bought on commission to as an accommodation for other persons or for sale, will be admitted free, including 200 cigarettes or less than 16 personal or household use or as souvenirs or gifts, but not bought on commission to as an accommodation for other persons or for sale, will be admitted free, including 200 cigarettes or less than 16 personal or household use or as souvenirs or gifts, but not bought on commission or as an accommodation for other persons or for sale, with the personal or household use or as souven

Low cost, round trip, First Class and Coach Family Fares

MONTREAL TO PLATTSBURGH—SARATOGA SPRINGS—ALBANY—NEW YORK

Connecting trains from Ottawa-Quebec.

				A
	Connecting Trains Via Can. Pac. Ry. OTTAWA_Union Stal. Guebec, Palais Sta. Montreal West. Ar Montreal, Windsor Station.	AM 7 15 9 21 9 35 AM	PM 6 40 8 30 8 45 PM	PM 6 00 9 45 10 00 PM
Miles	Light Face — A.M.; Bold Face — P.M.	D&H 34 NYC 90 Daily through Train	D&H 10 NYC 46 Daily Ex. Sat. through Train	D&H 12 NYC 6 Sat only through Train
.0 23.5 36.4 48.9 63.1 74.1 91.2 113.3 120.0 135.3 153.7 171.8	Plattaburgh. Port Kent(+). Willsboro. Elizabethown. Westport Lake Placid Saranac Lake. Port Henry. Fort Ticonderoga (+) (Ticonderoga) Whitehali (Rutland). Lv. Comstock (+). Fort Edward (Glens Falls Fort Edward (Glens Falls Saratoga Springs.	The Laurentian 25 and 2	Montreal Limited ### April	Montreal Limited 7 2 7 7 8 11 2 12 13 14 15 15 15 15 15 15 15 15 15 15 15 15 15
190.8 -0 28.5 53.8 69.4 83.9 109.5 127.7 138.0 142.2	ALBANY (N.Y.C.R.R.) Lv. Hudson Ar. Rhinsellif. Poughkeepsle. Beacon (Bus to Newburgh) Croton-Harmon Yonkers [128th St. (N.C.B.)	4 50 W5 22 P 5 45 6 05 J 6 20 t 6 55 t 7 21	4 05 AM 4 25 A 4 58 A 4 58 C 16 5 32 C 16 6 59 AM	4 35 mg 4 55 m

REFERENCE MARKS FOR ABOVE SCHEDULE

- M Stops to discharge passengers from Rouses Point and points north on notice to conductor and will stop on station signal to receive passengers for Albany conductor and will and points south.
- J Stops, except Saturday.
- P Stops Sundays and Nov. 28, Dec. 25, 1963 Jan. 1, 1964.
- R Stops to discharge passengers from Saratoga Springs and points north and stops on signal to receive passengers for points south of Albany.
- W Stops Saturdays and Sundays and Nov. 28, Dec. 25, 1963 Jan. 1, 1964.
- a Passengers not carried locally between these stations.
- f Stops to discharge passengers on notice to conductor and will stop on station signal to receive passengers.
- t Stops only to discharge passengers.
- +Non-Agency Station. No baggage handled.
- Stops Saturday only.
- N.C.B .- No facilities for handling checked baggage at this station.
- · Change trains-same station.
- Advance seat reservation necessary on this train.
- N.A.-Non-Agency Station. Baggage handled.

EOUIPMENT

SOUTHBOUND TRAINS

Coach service available on all passenger trains.

THE LAURENTIAN - Daily

- (Daylight trip) D. & H. 34 N. Y. C. 90

 *Coaches Montreal to New York
 Through Redining Seat Coaches (Unreserved)

 *Dining Car Rouses Point to Albany
 *Diner-Lounge Albany to New York
 *Parior Car Montreal to New York (Railroad owned) Car 340

MONTREAL LIMITED - Daily

(Night trip) D. & H. 10 — N. Y. C. 46 — Ex. Sat. D. & H. 12 — N. Y. C. 6 — Sat. Only

**Coaches — Montreal to New York (Rented pillows available)
Through Reclining Seats (Unreserved)

**Sleeping Cars — Montreal to New York (open 9:45 P.M.), No. No. (Railroad owned.)
10 Roomette — 6 Double Bedroom. 1029 1229
10 Roomette — 6 Double Bedrooms. 1930 1230
6 Double Bedroom — Lounge. (Beverages). 1031 1231

**Buffet Breakfust Service into New York
10 Roomette — 6 Double Bedrooms. 1032 1232

* Assigned cars are air-conditioned.

Sleeping Cars may be occupied at New York (G.C.T.) until 8:00 A.M., D.S.T.

INFORMATION FOR PERSONS ENTERING THE UNITED STATES FROM CANADA

Immigration Regulations

Immigration Regulations

United States citizens do not require passports when reentering the United States after a visit to Canada. It is recommended, however, that they be in possession of such personal papers or other documents as may be helpful in establishing that they are citizens of the United States. The most common documents which help to establish United States citizenship are birth certificates, showing place of birth, baptismal certificates, and United States passports, valid or expired.

Passports and visas are waived for Canadian citizens and British subjects who reside in Canada and who are entering temporarily across the Canadian border. Such persons may be admitted as visitors for an appropriate period not to exceed six months. All other aliens seeking to enter the United States are required to be in possession of a valid passport and appropriate visa issued by an American Consul.

Aliens returning to legal residence in the United States must be in possession of their Alien Registration Receipt and Border Crossing Card (FORM 1-151) or a valid re-entry permit.

Customs Regulations

Customs Regulations

Residents of United States may take into Canada their wearing apparel and other personal effects and upon return to United States are entitled to have the same admitted free of duty. Residents of United States upon return must declare all articles acquired abroad, in their baggage, on their person or to be forwarded to them, whether by purchase, by gift, or otherwise, and whether dutiable or free of duty.

Each returning resident of the United States is entitled to an exemption from duty on articles up to a total of One Hundred Dollars (\$100.00) in value acquired outside the United States under the following conditions, if declared when the resident arrives at the United States port of entry:

outside the United States under the following conditions, if declared when the resident arrives at the United States port of entry:

1. Provided they have been outside the United States over forty-eight hours.

2. Provided they have had no previous exemption for the past thirty days.

3. Provided they have had no previous exemption for the past thirty days.

4. Provided they are acquired as an incident of the foreign journey.

5. Foodstuffs, cigarettes, tobacco, 100 cigars, and not over one wine gallon of distilled spirits, wines or mait liquors (provided the quantity is not prohibited by State law) may be included in this exemption.

6. Each member of a family is entitled to the \$100 exemption for articles acquired abroad of the character described in the third proviso to Paragraph 1798, Tariff Act of 1930, as amended, subject to the conditions prescribed therein. Articles belonging to one passenger cannot be included in the exemption of another, except that when members of a family residing in comptions to whether together on their return to the United States, thay be grouped and allows severa members of ine family are entitled to such an exemption. In or had any person who is not entitled to such an exemption, nor be allowed for the property of any such person.

The owner of valuable effects of foreign origin, sealskin garments, plumage, or other articles, prior to his departure from the United States, may make an application to the collector or his representative on Customs Form 4455 for the registration of such articles to facilitate their identification on return to the United States.

Low cost, round trip, group Economy Coach fares, for three or more

MONEY SAVING ROUND TRIP FARES

FAMILY FARES-Apply to husband and wife or to either or both parents and accompanying children-one parent pays the regular round-trip adult fare; the accompanying parent, if any, and each child 12 and under 22 pay the regular one way adult fare while each child 5 and under 12 is charged 1/2 of the regular one way adult fare. Children over 22 are not to be included in the round trip family ticket; children under 5 are carried free except when occupying parlor car seat. Liberal baggage privileges . . . Six-month limit . . . On sale every day . . . For travel in coaches or sleeping and parlor cars.

GROUP ECONOMY FARES—Apply for parties of three or more adults or their equivalent (two children 5 and under 12 to be considered the equivalent of one adult) . . . Group must travel together on both going and return journeys . . . Good only in coaches . . . Liberal baggage privileges . . . 30 day limit.

TRI-LAKES TRANSIT CORP. Lake Placid, N. Y.

Daily Bus	(Railroad tickets not accept	ed)	Daily Bus
P.M.	Leave	Arrive	P.M.
2 50	Westport, N. Y.	1	1 20
3 05	Elizabethtown, N. Y.		1 05
3 30	Keene, N. Y.		12 40
3 45	Lake Placid, N. Y.		12 15
4 03	Raybrook, N. Y.		12 07
4 07	Raybrook San., N. Y.		12 03
5 05	Saranac Lake, N. Y.		11 55
5 17	Lake Clear Jct., N. Y.		11 43
5 21	Lake Clear, N. Y.		11 39
5 26	Saranae Inn, N. Y.		11 34
5 31	Fishcreek, N. Y.		11 29
5 45	Sunmount, N. Y.		11 15
6 05	Tupper Lake, N. Y.		10 55
P.M.	Arr ve	Leave	A.M.

WHITEHALL BUS

Railroad tickets not accepted.
OPERATES SATURDAY ONLY

Leave Whitehall 1:45 P.M.—Fair Haven 2:00—Hydeville 2:05—Castleton Corners 2:15—Castleton 2:30—Arrive Rutland 2:45 P.M.

Leave Rutland 12:00 Noon—Castleton 12:15—Castleton Corners 12:30—Hydeville 12:40—Fair Haven 12:45—Arrive Whitehall 1:05 P.M.

DELAWARE & HUDSON FREIGHT TRAFFIC OFFICES

The freight traffic offices listed below are well-equipped to furnish information and assistance to the shipping public. Any representative will be grateful for the opportunity of serving you.

ALBANY, N. Y. 12207

A. E. Duprey, Division Sales Manager, D. & H. Building
Phone 518 Hobart 3-1141

ATLANTA, GA. 30303

Raymond O. Beebe, Southeastern Sales Manager, Room 701, 101 Marietta St. Bldg. Phone 404 Jackson 2-5464

BOSTON, MASS. 02114

F. J. Smith, New England Sales Manager,
W. E. Barrows, District Sales Manager,
Room 1101 No. Station Office Bldg., 150 Causeway Street
Phone 617 Capital 7-7175

BUFFALO, N. Y. 14202

H. E. Andrews, District Sales Manager, 601 Bank of Buffalo Building Phone 716 TL 6-4646

CHICAGO, ILL. 60604

D. E. Wilkinson, District Sales Manager, Room 840, 327 South LaSalle Building Phone 312 WA 2-5574

CLEVELAND, OHIO 44113

J. F. Hartman, District Sales Manager, 1236-7-8 Terminal Tower Bldg.
Phone 216 Main 1-1405

MONTREAL 2, QUE.

G. C. Seaman, Canadian Sales Manager, 106 Drummond Bldg., 1117 St.
Catherine St., West
Phone 514 Victor 9-7424

NEW YORK, N. Y. 10017

J. A. Quinn, District Sales Manager, 360 Lexington Avenue
Phone 212 YUkon 6-2886

PHILADELPHIA, PA. 19103

J. J. Kaelin, District Sales Manager, 308 Transportation Center
Building Phone 215 LOcust 3-1271

PITTSBURGH, PA. 15219

W. H. Hartman, District Sales Manager, 2818 Koppers Building
Phone 412 GRant 1-5516 and 412 GRant 1-5517

PORTLAND, ME. 04101

I. W. Kelley, District Sales Manager, 310 Congress Building, 142 High Street Phone 207 Spruce 3-1072

ST. LOUIS, MO. 63101

P. J. O'Toole, District Sales Manager, 2084 Railway Exchange Building
Phone 314 Chestnut 1-9178

SAN FRANCISCO, CALIF. 94105

A. E. Olson, District Sales Manager, 457 Monadnock Bldg., 681 Market Street Phone 415 Garfield 1-6499

SCRANTON, PA. 18509

T. E. O'Brien, Division Sales Manager, 703 Wyoming Avenue Phone 717 Diamond 2-8751

WINSTON-SALEM, N. C. 27101

R. G. Surridge, District Sales Manager, Room 1410, Reynolds Bldg.
Phone 919 PArk 3-4451

Paul F. Robinson, Vice-President Traffic, D. & H. Bldg. Paul F. Robinson, vice-resident vianager, D. &. H. Bldg.
Ralph R. Wiltsey, Coal Traffic Manager, D. &. H. Bldg.
12207 Richard H. George, Freight Traffic Manager, D. & H. Bldg. Leonard Unger, General Sales Manager, D. & H. Bldg.

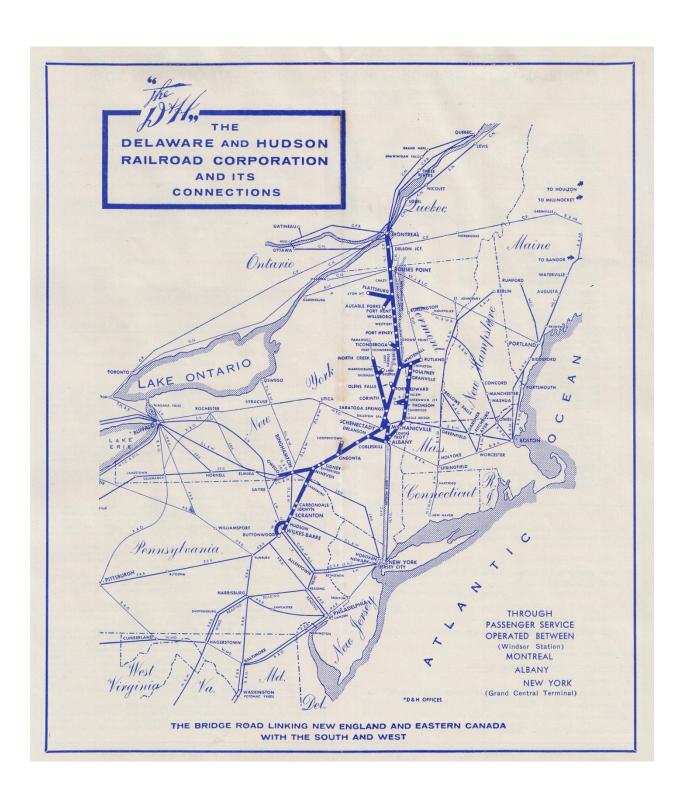
SHIP AND TRAVEL VIA D. & H.

RAILROAD-SLEEPING-PARLOR CAR FARES

(SUBJECT TO CHANGE)

Consult Ticket Agent about round-trip Family Fares. Also Group Economy coach excursion fares for parties of three or more adults (or equivalent).

Coaches, Sleeping and Parlor Cars are	IN COACHES			IN SLEEPING OR PARLOR CARS §				Coaches, Sle	eping and	IN COACHES			IN SLEEPING OR PARLOR CARS §				
Railroad operated	One- Way	30-Day Round- Trip	6-Mo. Round- Trip	One- Way	30-Day Round- Trip	6-Mo. Round- Trip	Room- ette	Parlor Car	Parlor Cars are Railroad operated		30-Day 6-Mo. One- Round- Round- Way Trip Trip		One- Way	30-Day Round- Trip	6-Mo. Round- Trip	Parlor Car	
Montreal, P. Q. to									Fort Ticonder								
Albany N. Y. Beacon N. Y. Beacon N. Y. Beacon N. Y. Fort Edward N. Y. Fort Tleonderoga N. Y. Harmon N. Y. Harmon N. Y. Hidson S. Y. Hidson N. Y. New York (125th St. N. Y. New	\$10.03 14.05 7.67 5.73 15.27 11.45 9.18 16.67 16.80 2.82 4.96	\$15.80 21.65 12.10 9.05 23.15 18.05 14.45 24.80 25.00 4.45 7.80	\$19.71 27.08 14.99 11.11 29.00 22.55 18.01 31.09 31.37 5.29 9.57	19.44 9.82 7.27 21.28 15.36 11.94 23.42 23.53 6.36	\$18.90 28.00 14.25 10.55 30.45 22.30 17.35 33.35 33.80 5.10 9.25 6.15	38.14 19.24 14.14 41.50 30.32 23.48 45.46 46.12 6.66 12.32	1.14 1.50 9.35 1.32 1.48 1.46 1.12 9.35 1.66 9.35 1.32		Albany. N. Y. Beacon. N. Y. Harmon. N. Y. Hudson. N. Y. Montreal. N. Y. New York (G.C.T.). N. Y. New York (125th S. N. Y. Poughkeepsle. N. Y. Rthineeliff. N. Y. Yonkers. N. Y.		\$4.31 8.29 9.55 5.69 5.73 11.09 10.84 7.62 6.87 10.38	\$6.80 13.10 15.10 9.00 9.05 17.30 16.95 12.05 10.85 16.35	†\$8.62 †16.58 †19.10 †11.38 11.11 21.89 21.45 †13.74 20.68	10. 02	23.85	†\$11.48 †24.20 28.00 †16.28 14.14 32.78 32.07 †22.16 †19.86 30.69	2.31 2.64 1.76 1.82 3.03 2.81 2.20
	13.34 12.63 1.75 8.31 9.68 4.52 6.67 3.90 16.19	20.65 19.70 2.50 13.10 15.25 7.10 10.50 6.15 24.25	6.39 25.87 24.66 3.15 16.27 19.01 8.69 12.99 7.45 30.38	18.33 17.28 2.00 10.80 12.66 5.71 8.51 4.97	26 . 40 25 . 10 2 . 65 15 . 70 18 . 35 8 . 30 12 . 35 7 . 20 32 . 40	8.06 35.94 34.13 3.60 21.20 24.92 11.02 16.62 9.54 44.14	9.35 9.35 9.35 9.35 9.35 9.35 9.35	1.43 3.74 3.58 1.30 2.64 3.03 1.65 2.20 1.54 4.29	Fort Edward, Albany Beacon Harmon Hudson Montreal New York (12 Poughkeepsie Rhineeliff Yonkers		2.40 6.38 7.64 3.78 7.67 9.18 8.93 5.71 4.96	3.80 10.10 12.05 6.00 12.10 14.50 14.10 9.05 7.85	† 4.80 †12.76 †15.28 † 7.56 14.99 †18.36 †11.42 † 9.92 †16.94	9.66 11.59 5.70 9.82	4.80 14.05 16.90 8.30 14.25 21.60 20.80 12.55 10.90	† 6.60 †19.32 †23.18 †11.40 19.24 28.11 †27.36 †17.28 †14.98	1.43 2.42 2.42 2.26 1.65
Rouses Point, N. Y. to									Yonkers	N.Y.	8.47	13.40	†16.94	12.95	19.30	†25.90	2.20
Albany. N.Y. Beacon. N.Y. Harmon. N.Y. Hudson. N.Y. Montreal. P.Q. New York (G.C.T.). N.Y. New York (125th st.). N.Y. Poughkeepsle. N.Y. Rhineellf. N.Y. Saratoga Springs. N.Y. Yonkers. N.Y.	8.22 12.20 13.46 9.60 1.75 15.00 14.75 11.53 10.78 6.57 14.29	20.35 15.15 2.50	†16.44 23.76 25.80 †19.20 3.15 28.11 27.67 22.61 21.40 †13.14 27.06	$ \begin{array}{c} 2.00 \\ 21.73 \\ 21.33 \\ 16.29 \end{array} $	15.95 24.90 27.50 19.40 2.65 30.80 30.40 23.50 22.00 12.75 29.30	†21.90 34.32 37.79 †26.70 42.30 41.75 32.29 30.20 †17.54 40.32	9.35 9.35 9.35 9.35 9.35 9.35	2.42 3.30 4.18 2.70 1.43 4.02 3.74 3.25 3.03 2.04 3.69	Saratoga Springs, N. Y. to Albany N. Y. Beacon N. Y. Harmon N. Y. Hudson N. Y.			2 10.95 †13.86 6 4.85 † 6.17 1 13.10 16.27 6 13.60 †16.97 1 13.10 †16.97 9 7.90 † 9.97 4 6.70 † 8.47		8.74 10.67 4.78 10.80 13.16 12.76 7.72 6.57	3.50 12.75 16.60 6.95 15.70 21.60 20.80 11.25 9.55	† 4.76 †17.48 †21.34 † 9.56 21.20 †26.32 †25.52 †15.44 †13.14	1.82 1.43 2.64 2.31 1.98 1.49 1.43
Plattsburgh, N. Y to									Yonkers	N.Y.	7.75	12.25	†15.50	12.03	19.30	†24.06	1.93
Harmon N.Y. Hudson N.Y. Montreal P.Q. New York (G.C.T.) N.Y.	$\begin{array}{c} 7.22 \\ 11.20 \\ 12.46 \\ 8.60 \\ 2.82 \\ 14.00 \\ 13.75 \\ 10.53 \\ 9.78 \\ 5.54 \\ 13.29 \end{array}$	19.00 13.60 4.45	†14.44 22.11 24.09 †17.20 5.29 26.51 26.18 20.96 †19.56	1 16.00 23.15 17.93 25.75 0 12.04 17.50 0 3.53 5.10 1 20.42 29.10 8 20.02 28.50 1 14.98 21.65		†19.28 31.85 35.37 †24.08 6.66 40.04 39.16 29.81 †27.66 †14.80	9.35 9.35	3.47	Sleeping Car fares — Montreal, Que, and New York and all intermediate points							Control	
Saratoga SpringsN.Y.	9.78 5.54	15.45 8.75 20.15	†19.56 †11.08 25.41	13.83 7.40 19.29	20.10 10.80	†27.66 †14.80	9.35	$\frac{2.86}{1.65}$	Roomette		Suite Bed		Bedro	Two		Two	
TORKETS	13.29 20.15 25.4		25.41	19.29	27.50	37.90	9.35	3.41	One Person	or more Persons		One		or in Pers	ore	or mo	re
Westport, N. Y. to	E E1	0 70	411 62	7 01	10.70	111 60	0.05	1.0-	\$9.35	\$16.94		\$13.70		\$16.94		\$27.45	
Albany N.Y. Beacon N.Y. Harmon N.Y. Hudson N.Y. Montreel P.O.	5.51 9.49 10.75	8.70 15.00 16.75	†11.02 †18.98 21.23	7.34 13.70 15.63	10.70 19.90 22.60	†14.68 †27.40 31.08	9.35	$\frac{1.65}{2.70}$ $\frac{2.70}{2.97}$									
New York (G.C.T.). N.Y. New York (125th St.) N.Y. Poughkeepsie N.Y. Rhinecliff N.Y. Saratoga Springs N.Y.	6.89 4.52 12.29 12.04 8.82 8.07 3.87 11.58	7.10	†13.78 8.69 23.87 23.49 †17.64 †16.14 † 7.74 22.72	15.63 9.74 5.71 18.12 17.72 12.68 11.53 5.15 16.99	14 . 15 8 . 30 26 . 00 25 . 45 18 . 45 16 . 80 7 . 50 24 . 55	†19.48 11.02 35.75 34.98 †25.36 †23.06 †10.30 33.72		2.04 1.65 3.25 3.14 2.42 2.31 1.43 3.08	** Rates shown apply when space is occupied by one adult or by one adult accompanied by a child or children under 5 years of age. Minimum number of adult Railroad tickets (valid in sleeping car accomodations) required for occupancy of various types of rooms are: Roomette 1 Bedroom 1 Bedroom Suit 2								
Albany . N.Y. Beacon . N.Y. Harmon . N.Y. Hudson . N.Y. Montreal . P.Q.	3.36 7.34 8.60 4.74 6.67		† 6.72 †14.68 †17.20 † 9.48 12.99 20.24	4.51 10.87 12.80 6.91 8.51 15.29 14.89 9.85 8.70	6.55 15.80 18.65 10.05 12.35 22.20 21.60	† 9.02 †21.74 †25.60 †13.82 16.62 30.53 29.70	9.35 9.35 9.35 9.35 9.35	1.43 2.09 2.31 1.49 2.20 2.70 2.48	Reference notes † — Limit one year. \$ — Indicates tickets honored in Sleeping Cars or Parlor Cars on payment of charges for space occupied. † — Will not be made valid in Sleeping Cars or Parlor Cars.								
Poughkeepsie N.Y. Rhinecliff N.Y. Yonkers N.Y.	9.89 6.67 5.92 9.43	15.55 10.55 9.35 14.90	20.24 †19.78 †13.34 †11.84 †18.86	9.85 8.70 14.16	14.35 12.65 20.55	†19.70 †17.40 28.27	9.35	1.82 1.76 2.42					Cars or	Parlor	Cars.	•	



THE DELAWARE AND HUDSON RAILROAD

CORPORATION

Passenger representatives of the D. & H. R. R. located at the offices named below.

J.A.N. LEWIESSUMAN AND THE PROPERTY OF THE PRO

G. T. ALTHISAR, General Passenger Agent 360 Lexington Avenue New York, N. Y. 10017 Telephone YUkon 6-2742

TICKETS AND SLEEPING CAR RESERVATIONS

May be secured at following offices:

IN MONTREAL

D. & H. R. R. Corp., 1117 St. Catherine St., West, Phone Victor 9-3614 Note: After 5:00 P.M. and on Saturdays, Sundays and Holidays, Phone UN 1-6811.

C. P. Ry., Windsor Station, Phone UN 1-6811 C. P. Ry., 215 St. James St., West, Phone UN 1-6811

IN NEW YORK

D. & H. R. R. Corp., 360 Lexington Avenue Phone YUkon 6-1788 and 6-1789

Phone YUkon 6-1788 and 6-1789

Note: After 5:00 P.M. and on Saturdays, Sundays and Holidays, for reservations only, Phone MUrray Hill 7-6600.

N. Y. C. R. R., Reservation Bureau, Phone MUrray Hill 7-6600.

N. Y. C. R. R., 165 Broadway, Room 2200, NYC-NY, Co.7-0400.

or through your local ticket agent.

INFORMATION

Air-conditioned equipment is assigned as far as possible but the right is reserved to employ non air-conditioned cars as necessitated by volume of traffic or emergencies.

When regular trains are operated in Sections, only one Section may have Dining Car and Sleeping Cars.

REGARDING TIME TABLES—This Railroad is not responsible for errors in time tables, inconvenience or damage resulting from delayed trains or failure to make connections; schedules herein are subject to change without notice. BUY TICKETS before boarding trains and avoid payment of extra charge. CHILDREN under five years of age when accompanied by parent or guardian, will be carried free, except that if the sole occupant of Sleeping Car space, they will be charged half railroad fare and regular Sleeping Car fare; children between the ages of five and eleven years, inclusive, will be charged half fare, and children twelve years of age and over will be charged full fare.

ADJUSTMENT OF FARES—In case of misunderstanding with Conductors or Agents pay fare required, take receipt and communicate with General Passenger Agent.

Agent.

RDEMPTION OF TICKETS—All tickets issued by this railroad, unused or partly used, will be redeemed under tariff regulations at proper value.

BAGGAGE—This Railroad does not guarantee to forward baggage on same train with passenger, or within a given or specified time limit, but reserves the right to forward it upon a preceding or following train.

BAGGAGE MAXIMUMS—No single piece of baggage exceeding 300 pounds in weight or 72 inches in greatest dimension or single shipment exceeding \$2,500.00 in value will be checked. Free allowance subject to tariff stipulations as to contents, weight, value and size.

BAGGAGE LIABILITY LIMITED-Excess value to be declared and paid for

at time of checking.

BAGGAGE FOR STATIONS WHERE NO AGENT IS ON DUTY must be claimed at baggage car door immediately on arrival of train at that station, otherwise it will be carried to the first station where agent is on duty, and held in the carried to the first station where agent is on duty, and held

BICYCLES (not Motorcycles), BABY CARRIAGES AND GUNS are trans-

BICYCLES (not Motorcycles), BABY CARRIAGES AND GUNS are transported in baggage cars subject to tariff regulations.

DOGS AND SMALL HOUSEHOLD PETS are transported in baggage cars subject to tariff regulations. Small animals (not vicious or otherwise objectionable) when carried in baskets or containers, may be taken by purchasers of drawing rooms, compartments or bedrooms into the rooms they occupy. Dogs (except lap dogs in closed receptacles) will not be permitted in passenger caches. Exception.—The carrying of dogs or household pets of any nature in passenger accommodations to or from or between Canadian points is prohibited. "Seeing Eye" dogs will be allowed in any type private room or coaches, with totally blind person in Canada.

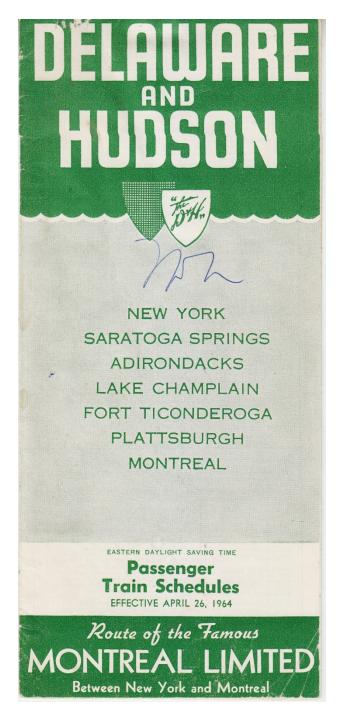
LOST ARTICLES—When articles are lost on trains, or left in waiting rooms at stations, owners should apply at once to Agent at station where they leave the train, or communicate with General Passenger Agent. No responsibility is assumed for unchecked articles left in stations or cars.

GPA-17

Printed in U.S.A.

Delaware and Hudson Passenger Train Schedules, Effective April 26, 1964:

New York, Saratoga Springs, Adirondacks, Lake Champlain, Fort Ticonderoga, Plattsburg, Montreal



NEW YORK—ALBANY—SARATOGA SPRINGS—PLATTSBURGH TO MONTREAL Connecting trains to Ottawa-Quebec.

-					-		
Miles	Light Face — A.M. Bold Face — P.M.	NYC 95 Daily		NYC 5 D & H Daily Through Train	35	NYC 61 D&H 9 Daily Through Train	QUALITY
	(N.Y.C.R.R.)				188		
	(Grand Central	AM		AM		PM	and
0.0	NEW YORK Terminal Lv. 125th Street (N.C.B.)	6 25 r6 35		8 45 r8 55		10 15	ATTENTIVE
14.5	Yonkers	r6 53		r9 12			ALIENTIAL
32.7 58.3	Beacon (Bus from Newburgh) "	r7 16 8 00		r9 35		r11 06	SERVICE
72.8 88.4	Poughkeepsie " Rhinecliff. "	8 15				12 01	
113.7	Hudson	8 33 9 02					
142.2	ALBANY(N.Y.C.R.R.) Ar.	9 41		11 30		1 15	
				AM		AM	Something
6.1	ALBANY (D.&H.R.R.) Lv. Watervliet(+) (Troy)	'→		11 44		1 35	
19.0	wiechanicville(+) "			N11 53 f12 10			to suit
37.1 55.5	Saratoga Springs " Fort Edward { Glens Falls } "			12 32			every
70.8	Comstock(+)			12 54			every
77.5	Whitehall (Rutland) { Ar. Lv.			t1 09 1 18		3 00	taste.
99.6	Fort Ticonderoga (+) (Ticonderoga) "		ian	1 25		3 10	Miles
116.7	Port Henry		aurentian	2 18		Ē	Miles
127.7	Westport Lake Placid (Note). "		Laur	2 39		B4 27	slip by
141.9 154.4	Willsboro		The	f2 59 W3 20		B4 48	like
167.5			-	3 44		5 39	III.O
190.8	Rouses Point(N.A.) (D.&H.R.R.) { Ar. Lv.			4 15 4 23		6 19	magic.
	Lacolle, P.Q. (N.J.Ry) " Napierville, P.Q. (N.J.Ry) "			4 35 f4 46		6 41 f6 53	Join us.
	MONTREAL (Montreal West (N.J.Ry) Ar. Westmount (N.J.Ry)"			a5 20 a5 27		a7 35	
	P.Q. (Windsor Station N.J.Ry "			a5 27 a5 35 PM		a7 42 a7 50	won't you
			-	PIVI	_	AM	in the
	Connecting Trains				2	• •	D. & H.
	Via Can, Pac, Ry.		Ex. S	PM	PM	AM AM	υ. α Π.
	Montreal, Windsor Sta. Lv. Montreal West "		6 0		5 45 5 56	10 10 8 05 10 25 8 17	Dining
	Quebec, Palais Sta		10 0			2 00	-
	Ottava, Ont((via Montebello)"		PM	PM	8 45 PM	PM AM	Car.
	DVia Vankleek Hill						
10	Ottawa passenger change at Montr	eal We	st fro	m D&H	No.	35	

REFERENCE MARKS FOR ABOVE SCHEDULE

-See bus service between Westport, N. Y. and Saranac Lake and intermediate points, connection with Train No. 35.

- B Stops to discharge passengers from Albany and points south on notice to conductor and will stop on station signal to receive passengers for Rouses Point and points north.
- N Stops to discharge passengers from points south of Albany and stops on signal to receive passengers for Saratoga Springs and points north,
- W Stops to discharge passengers from Albany and points south and stops to receive passengers on signal for Montreal May 10th to October 25th inclusive.
- a Passengers not carried locally between these stations.
- f Stops to discharge passengers on notice to conductor and will stop on station signal to receive passengers.
- r Stops only to receive passengers.
- t Stops to discharge passengers.
- ♦ Change trains-same station.
- This train does not carry checked baggage New York to Albany.
- N.C.B .- No facilities for handling checked baggage at this station.
- + Non-Agency Station. No baggage handled.
- N.A.-Non-Agency station. Baggage handled.

EOUIPMENT

NORTHBOUND TRAINS

Coach service available on all passenger trains.

THE LAURENTIAN - Daily

(Daylight trip) N. Y. C. 51-D. & H. 35

★Coaches — New York to Montreal Through Reclining Seat Coaches (Unreserved)

*Parlor Car-New York to Montreal (Railroad owned) -Car 5135.

MONTREAL LIMITED - Daily

(Night trip) N. Y. C. 61 - D. & H. 9

★Coaches — New York to Montreal (Rented pillows available)
Through Reclining Seats (Unreserved)

*Assigned cars are air-conditioned

INFORMATION FOR PERSONS ENTERING CANADA FROM THE UNITED STATES

Immigration Regulations

American tourists and visitors are cordially invited to Canada and do not require passports to enter the Dominion. Citizens of the United States still retain their prewar freedom to enter and travel in Canada without new restrictions. In order to facilitate crossing the international boundary in each direction, citizens of the United States by naturalization should be prepared to present their naturalization certificates and citizens of other countries should be prepared to establish their legal admission to the United States either by record of admission or certificate of reentry.

CUSTOMS REGULATIONS Travelers' Baggage

Travelers' Baggage

General Provisions — Wearing apparel, articles of personal adornment, toilet articles and similar personal effects of persons arriving in Canada may be passed free, without entry at Customs as travelers, baggage, under the provisions of the Customs Tariff, but this provision shall include only such articles as actually accompany wear and use of such personal convenimediate purpose of the wear and use of such personal convenimediate purpose of the object of the provisions of the Customs of the Customs and the provision of the Customs and the provisions of the Customs and the provision of the Customs and the provision of the Customs and the convenimental properties of the customs and the customs and the customs and the customs and the customs are customs and customs are customs and customs and cus

Low cost, round trip, First Class and Coach Family Fares

MONTREAL TO PLATTSBURGH—SARATOGA SPRINGS—ALBANY—NEW YORK

Connecting trains from Ottawa-Quebec.

				1
	Connecting Trains Via Can, Pac, Ry, Vankleek OTTAWA,Union Sta, Guebec, Palais Sta. Montreal West. Montreal, Windsor Station.	AM 8 15 7 50 10 15 11 15 10 30 11 30 AM AM	PM 7 40 9 30 9 45 PM	6 00 9 45 10 00 PM
Miles	Light Face — A.M.; Bold Face — P.M.	D&H 34 NYC 50 Daily through Train	D&H 10 NYC 46 Daily Ex. Sat. through Train	D&H 12 NYC 6 Sat only through Train
		PM	PM	PM
	MONTREAL Weindoor Sta. (N.J.Ry) Lv.	a12 05 a12 12 a12 20 f12 53 1 03	a10 20	a10 45— a10 52 a10 52 a11 47 67 a11 57 a11 5
.0	Rouses Point(D.&H.R.R.)(N.A.)	1 13	11 32 7	11 57-
23.5 36.4 48.9	Plattsburgh. " Port Kent(+). " Willsboro. "	1 52 K 2 07 f 2 30	12 08_ M12 47	12 33 M1 12
63.1	Westport Lake Placid (Note)"	g 2 50	B M 1 08	% M1 33
74.1 91.2	Westport { Elizabethtown } Lake Piacid (Note) " Port Henry Fort Ticonderoga (+) (Ticonderoga) Ar. Whitehall (Rutland)	The Laurentian 2 20 2 20 4 10 4 10 1 1 1 1 1 1 1 1 1 1 1 1 1 1	Montreal Limited Control Control Montreal Limited To be 1 M Montreal Limited	Montreal Limited To Surday morning only
113.3	Whitehali (Rutland) \ \ \ \ \ \ \ \ \ \ \ \ \ \ \ \ \	9 f 4 17	Montreal Lim	2 45 E
120.0	Fort Edward (Glens Falls)	E 4 42	lont	font
153.7 171.8	Saratoga Springs	5 02 f 5 30	M run Su	Nuday
184.7 190.8	Comstock (+) " Fort Edward Glens Falls	R5 48 6 05 PM	4 05 AM	4 35 g
28.5	ALBANY(N.Y.C.R.R.) Lv. Hudson		4 25 M 4 58 I	4 55
53.8	Rhinecliff"		5 35	6 05
69.4 83.9	Beacon (Bus to Newburgh)		□5 50	
109.5 127.7 138.0	Poughkeepsle. " Beazon (Bus to Newburgh). " Croton-Harmon. " Yonkers. [125th St. (N.C.B.) * N.Y.C.R.R.) Terminal. Ar	t 8 21 t 8 50 t 9 08	t 6 30 t 6 59	t 6 55 t 7 23
142.2	NEW YORK (N.Y.C.R.R.) Grand Central Terminal Ar	9 20 PM	7 30 AM	7 55 AM

REFERENCE MARKS FOR ABOVE SCHEDULE

Note—See bus service between Westport, N. Y. and Saranac Lake and intermediate points, connection with Train No. 34.

- M Stops to discharge passengers from Rouses Point and points north on notice to conductor and will stop on station signal to receive passengers for Albany and points south.
- K Stops to discharge passengers from Montreal and to receive passengers for Albany and points south, May 10th to October 25th 1964 inclusive.
- R Stops to discharge passengers from Saratoga Springs and points north and stops on signal to receive passengers for points south of Albany.
- a Passengers not carried locally between these stations.
- f Stops to discharge passengers on notice to conductor and will stop on station signal to receive passengers.
- t Stops only to discharge passengers.
- +Non-Agency Station. No baggage handled.
- Stops Saturday only.

N.C.B .- No facilities for handling checked baggage at this station.

- Change trains-same station.
- Advance seat reservation necessary on this train.
- This train does not carry checked baggage Albany to New York.

N.A.-Non-Agency Station. Baggage handled.

EQUIPMENT

SOUTHBOUND TRAINS Coach service available on all passenger trains.

THE LAURENTIAN - Daily

(Daylight trip) D. & H. 34 - N. Y. C. 50

★Coaches — Montreal to New York Through Reclining Seat Coaches (Unreserved)

★Dining Service. ★Parlor Car — Montreal to New York (Railroad owned) — Car 340

MONTREAL LIMITED - Daily

(Night trip) D. & H. 10 — N. Y. C. 46 — Ex. Sat. (Night trip) D. & H. 12 — N. Y. C. 6 — Sat. Only

* Assigned cars are air-conditioned.

Sleeping Cars may be occupied at New York (G.C.T.) until 8:00 A.M., D.S.T.

INFORMATION FOR PERSONS ENTERING THE UNITED STATES FROM CANADA

Immigration Regulations

Immigration Regulations
United States citizens do not require passports when reentering the United States after a visit to Canada. It is recommended, however, that they be in possession of such personal papers or other documents as may be helpful in possession of such personal papers or other documents as may be helpful in documents which help to establish United States after most common documents which help to establish United States this making are birth certificates showing place of birth, baptismal certificates showing place of birth, baptismal certificates showing place of birth, baptismal certificates and British subjects who reside in Canada and who are entering temporarily across the Canadian border. Such persons may be admitted as visitors for an appropriate period to to exceed six months. All other aliens seeking to enter the United States are required to be in possession of a valid passport and appropriate visa issued by an American Consul.

Aliens returning to legal residence in the United States must be in possession of their Alien Registration Receipt and Border Crossing Card (FORM 1-151) or a valid re-entry permit.

Customs Regulations

Customs Regulations

Residents of United States may take into Canada their wearing apparel and other personal effects and upon return to United States are entitled to have the same admitted free of duty. Residents of United States upon return must declare all articles acquired abroad, in their baggage, on their person or to be forwarded to them, whether by purchase, by gift, or otherwise, and whether dutiable or free of duty.

Each returning resident of the United States is entitled to an exemption from duty on articles up to a total of One Hundred Dollars (\$100.00) in value acquired outside the United States under the following conditions, if declared when the resident arrives at the United States port of entry:

1. Provided they have been outside the United States over forty-eight hours.

2. Provided they have been outside the United States over forty-eight hours.

3. Provided they have had no previous exemption for the past thirty days.

3. Provided they aricles are not intended for sale, or as an order for others, or prohibited by law.

4. Provided they aricles are not intended for sale, or as an order for others, or prohibited by Isaw.

5. Foodstuffs, cigarettes, tobacco, 100 cigars, and not over one wine gallon of distilled spirits, wines or malt liquors (provided the quantity is not prohibited by State law) may be included in this exemption.

6. Each member of a family is entitled to the \$100 exemption for articles acquired abroad of the character described in the third provise to Paragraph 1798. Tariff Act of 1930, as amended, subject to the conditions prescribed therein. Articles belonging to one passenger cannot be included in the exemption of another, except that when members of a family resident of the exemption of another, except that when members of a family are entitled in the exemption of another, except that when members of a family are entitled may be grouped and allowed without expers of the family are entitled to such an exemption, nor be allowed for the property of any such person.

The own

Low cost, round trip, group Economy Coach fares, for three or more

Plan Your Trip Now to the Exciting

WHITE STATE STEEL

WHITE STATE STEEL

WHITE STATES STEEL

FAIR DATE TO DATE

ORDONATION

NEW YORK WORLD'S FAIR





- * Economy Family Plan
- * Economy Group Plan
- * Low Cost Round Trip Fares
- * Excellent, Reasonably Priced

Dining Service Coaches, Parlor and Sleeping Cars Avoid Highway Congestion and Traffic Hazards. Tension free and delightfully restful.

For information, reservations tickets, apply

DELAWARE & HUDSON

TRI-LAKES TRANSIT CORP.

Saranac Luke, N. Y.

Buses operate Sat, and Sun, until June 28th 1964 and will operate Daily from June 29th 1964.

P.M.	Leave	Arrive	P.M.
2 50	Westport, N. Y.	1	2 20
3 05	Elizabethtown, N. Y.		2 05
3 35	Keene, N. Y.		1 25
4 10	Lake Placid, N. Y.		1 00
4 18	Raybrook, N. Y.		12 52
4 22	Raybrook San., N. Y.		12 48
4 30	Saranac Lake, N. Y.		12 40
P.M.	Arrive	Leave	P.M.

LAKE CHAMPLAIN TRANSPORTATION CO., INC. Burlington, Vermont

Adults .55¢ — Children (5 to 12) .40¢
FROM
PORT KENT, N. Y.
3:00 P.M.
4:10 P.M.
12:40 P.M.
May 10 to June 24, also Sept. 8 to Oct. 25, 1964 incl.

June 25 to Sept. 7, 1964, incl.

WHITEHALL BUS

Railroad tickets not accepted.

OPERATES SATURDAY ONLY

Leave Whitehall 1:45 P.M.—Fair Haven 2:00—Hydeville 2:05—Castleton
Corners 2:15—Castleton 2:30—Arrive Rutland 2:45 P.M.

Leave Rutland 12:00 Noon—Castleton 12:15—Castleton Corners 12:30—
Hydeville 12:40—Fair Haven 12:45—Arrive Whitehall 1:05 P.M.

DELAWARE & HUDSON FREIGHT TRAFFIC OFFICES

The freight traffic offices listed below are well-equipped to furnish information and assistance to the shipping public. Any representative will be grateful for the opportunity of serving you.

ALBANY, N. Y. 12207

A. E. Duprey, Division Sales Manager, D. & H. Building Phone 518 Hobart 3-1141

ATLANTA, GA. 30303

Raymond O. Beebe, Southeastern Sales Manager, Room 701, 101 Marietta St. Bldg. Phone 404 Jackson 2-5464

BOSTON, MASS. 02114

F. J. Smith, New England Sales Manager,
W. E. Barrows, District Sales Manager,
Room 1101 No. Station Office Bidg., 150 Causeway Street
Phone 617 Capital 7-7175

BUFFALO, N. Y. 14202

H. E. Andrews, District Sales Manager, 601 Bank of Buffalo Building Phone 716 TL 6-4646

CHICAGO, ILL. 60604

D. E. Wilkinson, District Sales Manager, Room 840, 327 South LaSalle Building Phone 312 WA 2-5574

CLEVELAND, OHIO 44113

J. F. Hartman, District Sales Manager, 1236-7-8 Terminal Tower Bldg. Phone 216 Main 1-1405

MONTREAL 2, QUE.

G. C. Seaman, Canadian Sales Manager, 106 Drummond Bldg., 1117 St.
Catherine St., West Phone 514 849-7424

NEW YORK, N. Y. 10017

J. A. Quinn, District Sales Manager, 360 Lexington Avenue Phone 212 YUkon 6-2886

PHILADELPHIA, PA. 19103

J. J. Kaelin, District Sales Manager, 308 Transportation Center Building Phone 215 LOcust 3-1271

PITTSBURGH, PA. 15219

W. H. Hartman, District Sales Manager, 2818 Koppers Building
Phone 412 GRant 1-5516 and 412 GRant 1-5517

PORTLAND, ME. 04101

I. W. Kelley, District Sales Manager, 310 Congress Building, 142 High Street Phone 207 Spruce 3-1072

ST. LOUIS, MO. 63101

P. J. O'Toole, District Sales Manager, 2084 Railway Exchange Building
Phone 314 Chestnut 1-9178

SAN FRANCISCO, CALIF. 94105

A. E. Olson, District Sales Manager, 457 Monadnock Bldg., 681 Market Street Phone 415 Garfield 1-6499

SCRANTON, PA. 18509

T. E. O'Brien, Division Sales Manager, 703 Wyoming Avenue Phone 717 Dlamond 2-8751

WINSTON-SALEM, N. C. 27101

R. G. Surridge, District Sales Manager, Room 1410, Reynolds Bldg.
Phone 919 PArk 3-4451

Paul F. Robinson, Vice-President Traffic, D. & H. Bldg. Paul F. Robinson, vice-Freshult Canada, D. &. H. Bldg. Albany, N. Y. Raiph R. Wiltsey, Coal Traffic Manager, D. &. H. Bldg. (12207) Richard H. George, Freight Traffic Manager, D. & H. Bldg. Leonard Unger, General Sales Manager, D. & H. Bldg.

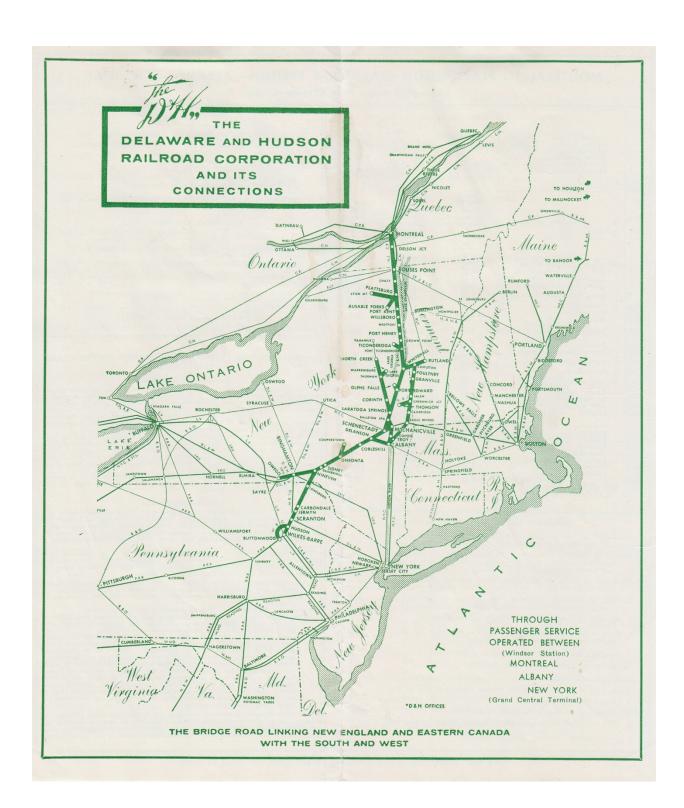
SHIP AND TRAVEL VIA D. & H.

RAILROAD-SLEEPING-PARLOR CAR FARES

(SUBJECT TO CHANGE)

Consult Ticket Agent about round-trip Family Fares. Also Group Economy coach excursion fares for parties of three or more adults (or equivalent).

Coaches, Sleeping and Parlor Cars are	IN	IN COACHES IN SLEEPING OR PARLOR CARS §				Coaches, Sleeping and Parlor Cars are	11	IN COACHES			IN SLEEPING OR PARLOR CARS §					
Railroad operated	One- Way	30-Day Round- Trip	6-Mo. Round- Trip	One- Way	30-Day Round- Trip	6-Mo. Round- Trip	One Way Room- ette	One Way Parlor Car	Railroad operated	One- Way	30-Day Round- Trip	6-Mo. Round- Trip	One- Way	Round-	6-Mo. Round- Trip	One Way Parlor Car
Montreal, P. Q. to									Fort Ticonderoga, N.Y.							
Albany N. Y. Beacon N. Y. Fort Edward N. Y. Fort Edward N. Y. Fort Honderoga N. Y. Harmon N. Y. Hudson N. Y. Mechanieville N. Y. New York (125th St.) N. Y. New York (125th St.) N. Y. New York (125th St.) N. Y. Port Henry N. Y. Port Henry N. Y. Port Kent N. Y.	\$10.03 14.05 7.67 5.73 15.27 11.45 9.18 16.67 16.80 2.82 4.96 3.37	23.15 18.05 14.45 24.80 25.00 4.45 7.80 5.30	29.00 22.55 18.01 31.09 31.37 5.29 9.57 6.39	7.27 21.28 15.36 11.94 23.42 23.80 3.53 6.36 4.23	\$18.90 28.00 14.25 10.55 30.45 22.30 17.35 33.35 33.80 5.10 9.25 6.15	\$25.58 38.14 19.24 14.14 41.50 30.32 23.48 45.46 46.12 6.66 12.32 8.06	\$9.35 9.35 9.35 9.35	\$3.03 4.02 2.42 1.82 4.18 3.30 2.70 4.35 4.62 1.43 1.76	HudsonN.	5.69 5.77 11.09	9.00 9.05 17.30	21.89	8.14 7.27 16.52 16.12 11.08 9.93	\$8.35 17.60 20.40 11.85 10.55 23.85 23.35 16.10 14.45 22.35	†\$11,48 †24,20 28,00 †16,28 14,14 32,78 32,07 †22,16 †19,86 30,69	\$1.49 2.31 2.64 1.76 1.82 3.03 2.81 2.20 2.04 2.75
Harmon. N. Y. Hudson. N. Y. Mechanieville. N. Y. Mechanieville. N. Y. Mew York (125th St.) N. Y. New York (125th St.) N. Y. Port Kent. N. Y. Port Henry. N. Y. Port Kent. N. Y. Port Kent. N. Y. Bratoga Springs. N. Y. Saratoga Springs. N. Y. Westport. N. Y. Willisboro. N. Y. Willisboro. N. Y. Willisboro. N. Y. Rouses Point. N. Y. Rouses Point, N. Y. Rouses Point, N. Y. Rouses Point, N. Y.	13.34 12.63 1.75 8.31 9.68 4.52 6.67 3.90 16.19	19.70 2.80 13.10 15.25 7.10 10.50 6.15	24.66 3.15 16.27 19.01 8.69 12.99	17.28 2.00 10.80 12.66 5.71 8.51 4.97	26 . 40 25 . 10 3 . 00 15 . 70 18 . 35 8 . 30 12 . 35 7 . 20 32 . 40	35.94 34.13 3.60 21.20 24.92 11.02 16.62 9.54 44.14	9.35 9.35 9.35 9.35 9.35 9.35	3.74 3.58 1.30 2.64 3.03 1.65 2.20 1.54 4.29	Hudson N. N. Montreal	7.64 7.64 7.67 9.18 9.18	3,80 10,10 12,05 6,00 12,10 14,50 14,10 9,05 7,85	† 4.80 †12.76 †15.28 † 7.56 14.99 †18.36 †17.86 †11.42 † 9.92	9.66 11.59 5.70 9.82 14.08 13.68 8.64 7.49	4.80 14.05 16.90 8.30 14.25 21.60 20.80 12.55 10.90	† 6.60 †19.32 †23.18 †11.40 19.24 28.11 †27.36 †17.28 †14.98	1.43 1.76 2.09 1.43 2.42 2.42 2.26 1.65 1.49
Albany N. Y. Beacon N. Y. Harmon N. Y. Hudson N. Y. Montreal P. Q. New York (G.C.T.). N. Y. New York (125th St.) N. Y. Poughkeepsle N. Y. Rhineeliff N. Y. Saratoga Springs N. Y. Yonkers N. Y. Yonkers N. Y.	8.22 12.20 13.46 9.60 1.75 15.00 14.75 11.53 10.78 6.57	20.35 15.15 2.50 22.20 21.85 17.85	3.15 28.11 27.67 22.61 21.40 †13.14	17.31 19.24 13.35 2.00 21.73 21.33 16.29 15.14	15.95 24.90 27.50 19.40 2.65 30.80 30.40 23.50 22.00 12.75	†21.90 34.32 37.79 †26.70 3.60 42.30 41.75 32.29 30.20 †17.54	9.35 9.35 9.35 9.35 9.35	2.42 3.30 4.18 2.70 1.43 4.02 3.74 3.25 3.03 2.04	Saratoga Springs, N. Y. t Albany. N. y Beacon. N. y Harmon. N. y Hudson. N. y Montreal. N. y Now York (C. C. T.)	7. 1.68 7. 5.66 7. 6.92 7. 3.06 9. 8.31 8.46	9.00 10.95 4.85 13.10 13.60	+16 92	2.38 8.74 10.67 4.78 10.80	3.50 12.75 16.60 6.95 15.70 21.60 20.80	† 4.76 † 17.48 † 21.34 † 9.56 21.20 † 26.32 † 25.52	1.43 1.63 1.82 1.43 2.64 2.31
Plattsburgh, N. Y. to	14.29	21.33	27.06	20.60	29.30	40.32	9.35	3.69	New York (125th St.) N.Y Poughkeepsie N.Y Rhinecliff N.Y Yonkers N.Y	4.99	6.70	†16.42 † 9.98 † 8.48 †15.50	$7.72 \\ 6.57 \\ 12.03$	11.25 9.55 19.30	†15.44 †13.14 †24.06	1.49 1.43 1.93
Albany , N.Y. Beacon , N.Y. Harmon , N.Y. Hudson , N.Y. Montreal , P.Q. New York (G.C.T.) , N.Y. New York (125th St.) N.Y.	7.22 11.20 12.46 8.60 2.82 14.00 13.75	19.00 13.60 4.45 20.95	†14.44 22.11 24.09 †17.20 5.29 26.51	12.04 3.53 20.42	14.05 23.15 25.75 17.50 5.10 29.10 28.50	†19.28 31.85 35.37 †24.08 6.66 40.04 39.16	9.35 9.35 9.35 9.35	3.19 3.30 2.64 1.43 3.63	One Way Sleeping Car fares — Montreal, Que. and New York and all intermediate points							
Poughkeepsie N.Y. Rhinecliff N.Y. Saratoga Springs N.Y.	Y. 10.53 16.60 20.96 14.9		14.98 13.83	21.65 20.10 10.80	.65 29.81 .10 +27.66	9.35	$\frac{2.97}{2.86}$	Roomette Roomette	Suite		Bedroo	om		Bedroom	Suite	
YonkersN.Y. Westport, N. Y. to	13.29	20.15	25.41	19.29	27.50	37.90	9.35	1.65	One Or mor Person Person		One		or m Pers	ore	Two or mor Person	re
Albany N.Y. Beacon N.Y. Harmon N.Y.	5.51 9.49 10.75	8.70 15.00 16.75	†11.02 †18.98	7.34 13.70	10.70 19.90 22.60	†14.68 †27.40 31.08	9.35	1.65 2.70 2.97	\$9.35 \$16.9		\$13.7	0	\$16.	94	\$27.4	5
Hudson N. Y. Montreal P. Q. New York (G.C.T.), N. Y. New York (125th St.) N. Y. Poughkeepsle N. Y. Rhineciliff N. Y. Saratoga Springs N. Y. Yonkers N. Y.	6.89 4.52 12.29 12.04 8.82 8.07 3.87	10.90 7.10 18.85 18.55 13.95 12.75 6.10 17.95	21.23 †13.78 8.69 23.87 23.49 †17.64 †16.14 †7.74 22.72	5.71 18.12 17.72 12.68 11.53	14.15 8.30 26.00 25.45 18.45 16.80 7.50 24.55	†19.48 11.02 35.75 34.98 †25.36 †23.06 †10.30 33.72	9.35 9.35 9.35 9.35	2.04 1.65 3.25 3.14 2.42 2.31 1.43 3.08	* Rates shown apply when panied by a child or childre Minimum number of adul required for occupancy of v Roomette 1 Family fare ticket require	t Railre arious t Be ments	ad ticke ypes of i droom	of age. ts (valid rooms ar	in slee: e: Bedi	ping car	accomod	
Whitehall, N. Y. to									Bedroom 1	1/4		Bedro	oom Su	ite 2½		
Albany N Y Beacon N Y Harmon N Y MONTreal P P New York (G.C.T.) N Y	4.74 6.67 10.14 9.89 6.67 5.92	5.30 11.60 13.55 7.50 10.50 16.00 15.55 10.55 9.35 14.90	† 6.72 †14.68 †17.20 † 9.48 12.99 20.24 †19.78 †13.34 †11.84 †18.86	9.85	6.55 15.80 18.65 10.05 12.35 22.20 21.60 14.35 12.65 20.55	† 9.02 †21.74 †25.60 †13.82 16.62 30.53 29.70 †17.40 28.27	9.35 9.35 9.35 9.35 9.35	1.43 2.09 2.31 1.49 2.20 2.70 2.48 1.82 1.76 2.42	Reference notes † — Limit one year. ‡ — Indicates tickets honored in Sleeping Cars or Parlocharges for space occupied. ‡ — Will not be made valid in Sleeping Cars or Parlor Ci							



THE DELAWARE AND HUDSON RAILROAD CORPORATION

Passenger representatives of the D. & H. R. R. located at the offices named below.

MONTREAL (2), P. Q.—Telephone 849-3614. 106 Drummond G. C. Seaman, Canadian Sales Manager, Bldg. 1117 St. J.A.N. LEMESSURIER.Canadian Passenger Agent. Catherine St., West

R. P. KEATING, General Passenger Agent 360 Lexington Avenue New York, N. Y. 10017 Telephone YUkon 6-2742

TICKETS AND SLEEPING CAR RESERVATIONS

May be secured at following offices:

IN MONTREAL

D. & H. R. R. Corp., 1117 St. Catherine St., West, Phone 849-3614
Note: After 5:00 P.M. and on Saturdays, Sundays and Holidays,
Phone UN 1-6811.
C. P. Ry., Windsor Station, Phone UN 1-6811
C. P. Ry., 215 St. James St., West, Phone UN 1-6811

IN NEW YORK

IN NEW YORK

D. & H. R. R. Corp., 360 Lexington Avenue
Phone YUkon 6-1788 and 6-1789

Note: After 5:00 P. M. and on Saturdays, Sundays and Holidays,
for reservations only. Phone MUrray Hill 7-6600.

N. Y. C. R. R., Reservation Bureau, Phone MUrray Hill 7-6600

N. Y. C. R. R., 165 Broadway, Room 2200, NYC-NY, Co.7-0400
or through your local ticket agent.

INFORMATION

Air-conditioned equipment is assigned as far as possible but the right is reserved to employ non air-conditioned cars as necessitated by volume of traffic or emergencies.

When regular trains are operated in Sections, only one Section may have Dining Car and Sleeping Cars.

REGARDING TIME TABLES—This Railroad is not responsible for errors in time tables, inconvenience or damage resulting from delayed trains or failure to make connections; schedules herein are subject to change without notice.

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ADJUSTMENT OF FARES—In case of misunderstanding with Conductors or Agents pay fare required, take receipt and communicate with General Passenger

Agent.

REDEMPTION OF TICKETS—All tickets issued by this railroad, unused or partly used, will be redeemed under tariff regulations at proper value.

BAGGAGE—This Railroad does not guarantee to forward baggage on same train with passenger, or within a given or specified time limit, but reserves the right to forward it upon a preceding or following train.

BAGGAGE MAXIMUMS—No single piece of baggage exceeding 300 pounds in weight or 72 inches in greatest dimension or single shipment exceeding \$2,500.00 in value will be checked. Free allowance subject to tariff stipulations as to contents, weight, value and size.

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of further orders.

BICYCLES (not Motorcycles), BABY CARRIAGES AND GUNS are transported in baggage cars subject to tariff regulations.

DOGS AND SMALL HOUSEHOLD PETS are transported in baggage cars subject to tariff regulations. Small animals (not vicious or otherwise objectionable) when carried in baskets or containers, may be taken by purchasers of drawing rooms, compartments or bedrooms into the rooms they occupy. Dogs (except lap dogs in closed receptacles) will not be permitted in passenger coaches. Exception.—The carrying of dogs or household pets of any nature in passenger accommodations to or from or between Canadian points is prohibited. "Seeing Eye" dogs will be allowed in any type private room or coaches, with totally blind person in Canada.

LOST ARTICLES—When articles are lost on trains, or left in waiting rooms at stations, owners should apply at once to Agent at station where they leave the train, or communicate with General Passenger Agent. No responsibility is assumed for unchecked articles left in stations or cars.

GPA-3

Form 1

Printed in U.S.A.

Delaware and Hudson Passenger Train Schedules, Effective October 31, 1965

New York, Saratoga Springs, Adirondacks, Lake Champlain, Fort Ticonderoga, Plattsburg, Montreal



MONTREAL-PLATTSBURGH-SARATOGA SPRINGS-ALBANY-NEW YORK Connecting Trains Via Can. Pac. Ry. ALL EQUIPMENT-RAILROAD OPERATED 7 40AM 7 40PM •OTTAWA, Union Sta...... 9 45PM 10 20AM SOUTHBOUND TRAINS 7 50AM Ex. Sun. 6 00PM 6 00PM Quebec, Palais Sta..... 10 00PM (Ex. Sat. 1 25PM NO. 34-LAURENTIAN-DAILY THROUGH COACHES AND LOUNGE PARLOR CAR NYC 35 D&H 9 Daily through Train D&H 34 NYC 50 NYC 95 D&H 35 Daily through Train **D&H** 10 D&H 12 (Ex. Sat.) NYC 6 (Ex. Sun.) through train Montreal to New York NYC 8 Light Face—A.M. Bold Face—P.M. (Sandwiches and Beverages) Montreal to Albany in Lounge (Dining Service) Albany to New York (Sun. only) through train 5 PM a6 30 a6 22 a6 15 f5 38 5 27 5 14 5 09 4 35 AM a8 50 a8 42 a8 35 f7 46 7 32 7 17 7 10 6 36 a12 01 a12 07 a12 15 f12 50 1 01 1 11 1 19 1 52 a 9 30 a 9 37 a 9 45 f10 21 10 33 10 44 10 50 11 21 a11 05) a11 12 a11 20 f11 56) 1: 08 12 19 12 26 12 57 | Windsor Sta. (N.J.Ry). Ar | Westmount (N.J.Ry). Ar | P.Q. | Westmount (N.J.Ry). | | Napierville, P.Q. (N.J.Ry). | | Lacolle, P.Q. (N.J.Ry). | MONTREAL LIMITED D & H-NO. 10 (Ex. Sat.) NO. 12 (Sat. only) Ex. THROUGH COACHES AND SLEEPING CARS Rouses Point (D&HRR) (NA).. | Rouses Point (Dennin) (No.) | Ar| Plattsburgh | Port Ken(+) | | Port Ken(+) | | Westport Lake Placid | | Saranac Lake | | Port Henry | | Fort Ticonderoga | Ticonderoga | | Whitehall (Rutland) | IV | | Ar-WITH ROOMETTES-BEDROOMS-LOUNGE MONTREAL TO NEW YORK f2 32 M12 01 M 1 37 f3 52 BUFFET BREAKFAST INTO NEW YORK B 5 48 IN LOUNGE CAR M12 22 M 1 58 2 52 3 32 B 5 26 3 11 3 37 4 08 4 14 f4 21 3 14 2 47 2 17 2 11 t2 02 NORTHBOUND TRAINS 윤 The Lau NO. 35-LAURENTIAN-DAILY 1 32 1 38 3 08 3 14 4 06 3 56 THROUGH COACHES AND LOUNGE PARLOR CAR Comstock (+) Fort Edward [Glens Falls Saratoga Springs Mechanicvilla (+) Water villet (+) (Troy) ALBANY (D&HRR) Montreal New York to Montreal 4 40 1 46 (Diner Lounge) New York to Albany (Sandwiches and Beverages) Albany to Montreal in Lounge 5 05 f5 35 R 5 51 6 05 PM 1 24 f12 52 N12 34 12 25 PM 3 35 AM 5 20 AM 2 10 AM Lv ALBANY (NYCRR). A Hudson Rhinecliff Poughkeepsie Beacon (Busto 1/2ewburgh). Croton-Harmon Yonkers NEW YORK (Grand Central Terminal (NYCRR) **4** 10 **6 30** 6 05 12 10 11 30 11 05 10 47 10 33 r9 51 r9 28 r9 10 9 00 AM 1 42 12 56 MONTREAL LIMITED-DAILY D & H-NO. 9 5 19 5 34 t6 07 6 35 12 15 THROUGH COACHES AND SLEEPING CARS t8 21 t8 50 t9 08 9 20 PM WITH ROOMETTES—BEDROOMS—LOUNGE NEW YORK TO MONTREAL BUFFET BREAKFAST INTO MONTREAL 17 58 11 21 9 00 AM 10 30 PM IN LOUNGE CAR IN COACHES Coaches, Sleeping Cars are Railroad operated IN SLEEPING CARS REFERENCE MARKS Fares 6-Mo. Round-Trip B Stops to discharge passengers from Albany and points south on notice to conductor and will One-Way Room-ette \$9.35 Round-Trip One-Way Montreal, P. Q. to stop on station signal to receive passengers for Rouses Point and points north \$19.71 27.08 14.99 11.11 29.00 22.55 18.01 31.09 31.37 5.29 9.57 6.39 9.57 24.66 3.15 16.27 19.01 8.20 19.20 10.20 10.20 10.20 10.20 10.20 10.20 10.20 10.20 10.20 10.20 \$12.99 19.44 9.82 7.27 21.28 15.36 15.36 23.42 23.80 6.36 6.36 6.36 17.28 2.00 10.80 12.66 5.71 8.51 4.23 18.53 \$25.58 38.14 19.24 14.14 41.50 30.32 23.48 45.46 46.12 6.66 12.32 8.06 33.94 34.13 3.60 21.20 24.92 11.02 9.54 M Stops to discharge passengers from Rouses Point and points north on notice to conductor and Fort Edward . . . Fort Ticonderoga . will stop on station signal to receive passengers for Albany and points south. Harmonolideroga. Hudson Mechanicville New York (126th St.). New York (3.2th St.). New York (6.C.T.). Port Henry. Port Henry. Port Henry. Port Henry. Rouses Point. Rouses Point. Saratoga Springs. Wateryi et. Westport. Willsboro. Vonkers. N Stops to discharge passengers from points south of Albany and stops on signal to receive 9.35 passengers for Saratoga Springs and points north. 9.35 9.35 R Stops to discharge passengers from Saratoga Springs and points north and stops on signal to receive passengers for points south of Albany. 9.35 a Passengers not carried locally between these stations. 8.31 9.68 4.52 6.67 3.90 16.19 f Stops to discharge passengers on notice to conductor and will stop on station signal to receive 9.35 9.35 9.35 9.35 9.35 passengers. t Stops only to discharge passengers Yonkers. r Stops only to receive passengers. One Way Sleeping Car fares — Montreal, Que. and New York and all intermediate points + Non-Agency Station. No baggage handled. C Stops Saturday only. Roomette Roomette Suite Bedroom N.C.B.—No facilities for handling checked baggage at this station: Two or more Persons One One Person • Advance seat reservation necessary on this train. \$9.35 \$16.94 \$13.70 \$16.94 \$27.45 * Attess shown apply when space is occupied by one adult or by one adult accompanied by a child or children under 5 years of age. This train does not carry checked baggage Albany to New York; N.A.-Non-Agency Station. Baggage handled. Minimum number of adult Railroad tickets (valid in sleeping car accommodations) required for occupancy of various types of rooms are: Roomette 1 Bedroom 1 Bedroom Suite 2 Bedroom Suite 2

Low cost, one way and round trip, First Class and Coach Family Fares Low cost, round trip, group Economy Coach fares, for three or more

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N. Y. C. R. R., 30 Church St. (Hudson Ter.), NYC-NY, Co.7-0400 or through your local ticket agent.

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LOST ARTICLES—When articles are lost on trains, or left in waiting rooms at stations, owners should apply at once to Agent at station where they leave the train, or communicate with General Passenger Agent. No responsibility is assumed for unchecked articles left in stations or cars.

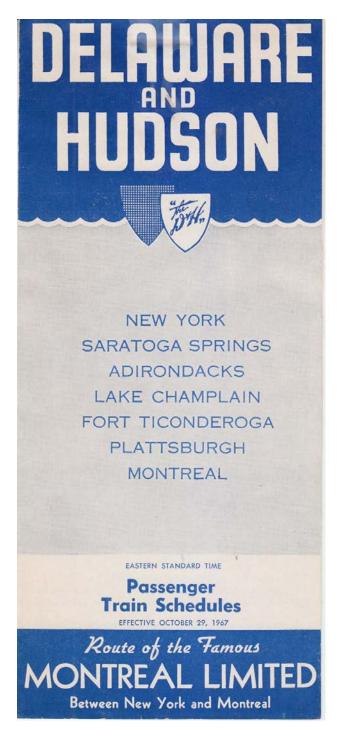
G.P.A.-13

Form 1

Printed in U.S.A.

Delaware and Hudson Passenger Train Schedules, Effective October 29, 1967:

New York, Saratoga Springs, Adirondacks, Lake Champlain, Fort Ticonderoga, Plattsburg, Montreal



MONTREAL-PLATTSBURGH-SARATOGA SPRINGS-ALBANY-NEW YORK Connecting Trains Via Can. Pac. Rv. 7 25PM 8 00AM 5 50PM .OTTAWA, Union Sta..... 12 55PM 9 30PM ALL EQUIPMENT—RAILROAD OPERATED 6 00PM 6 00PM . Quebec, Palais Sta..... 9 30PM 1 30PM assigned cars are air-conditioned — coaches are unreserved D&H 10 NYC 6 Daily (Ex. Sat.) through Train D&H 12 NYC 8 Sat. only through Train D&H 34 NYC 90 NYC 59 D&H 9 Daily through Train SOUTHBOUND TRAINS NO. 34-LAURENTIAN-DAILY Daily through Train THROUGH COACHES AND LOUNGE PARLOR CAR Montreal to New York (Dining Service) Montreal to Albany Buffet Lounge Car (Beverages and Light Meals) Albany to New York AM a10 30 a10 37 a10 45 f11 21 11 31 11 42 11 48 12 20 PM a9 30 a9 37 a9 45 10 21 Ex 84t Ex 84t 10 503 11 22 AM a8 40 a8 32 a8 25 f7 41 7 32 7 21 7 16 6 43 MONTREAL Windsor Sta. (N.J. Ry)... Westmount (N.J. Ry)... Montreal West (N.J. Ry)... Napierville, P.Q. (N.J. Ry). Lacolle, P.Q. (N.J. Ry). a5 50 a5 42 a5 35 f4 55 4 45 4 34 4 28 3 52 MONTREAL LIMITED—DAILY Rouses Point (D&HRR) (NA). Plattsburgh. Port Kent(+). Willsboro. Westport(+)(①) Port Henry. Fort Ticonderoga(+) Ticonderoga. D & H-NO. 10-EX. SAT. M11 59 | E M12 21 The Laurentian D&H—NO. 12—SAT. ONLY THROUGH COACHES AND SLEEPING CARS WITH ROOMETTES—BEDROOMS MONTREAL TO NEW YORK f12 57 1 19 1 37 2 33 2 33 2 38 f2 44 3 03 3 28 f4 03 R4 23 4 40 PM 13 09 2 48 2 30 2 205 1 35 1 31 1 23 1 06 1 51 2 07 N11 49 11 40 AM B6 02 B5 42 4 27 4 22 Comstock(+)... Fort Edward(3). Saratoga Springs. Mechanicville(+) NORTHBOUND TRAINS NO. 35-LAURENTIAN-DAILY THROUGH COACHES AND LOUNGE PARLOR CAR Watervliet (+)(4)... ALBANY (D&HRR). 2 30 AM New York to Montreal (Dining Service) New York to Albany Albany to Montreal 4 15 6 05 ALBANY (NYCRR) 11 10 1 47 MONTREAL LIMITED—DAILY t7 58 r11 51 D & H-NO. 9 NEW YORK 125th St. (NCB) (NYCRR) Grand Central Terminal. THROUGH COACHES AND SLEEPING CARS WITH ROOMETTES—BEDROOMS 8 25° 7 25 AM 9 00 AM 11 00 PM NEW YORK TO MONTREAL REFERENCE MARKS IN COACHES IN SLEEPING CARS & and PARLOR CAR ① Nearest Station to Elizabethtown, Lake Placid and Saranac Lake, N. Y. Fares 6-Mo. Round Trip ② Nearest Station to Rutland, Vt. One-Way Montreal, P. O. to 3 Nearest Station to Glens Falls and Lake George, N. Y. \$9.35 \$10.03 14.05 7.67 5.73 15.27 11.45 9.18 16.67 16.80 2.82 4.96 3.37 13.34 12.63 1.75 8.31 9.68 4.52 6.67 3.90 16.19 \$19.71 27.08 14.99 11.11 29.00 22.55 18.01 31.09 31.37 5.29 9.57 6.39 25.87 24.66 3.15 16.27 19.01 8.29 9.27 19.01 8.30 10.20 Albany Beacon Fort Edward Fort Ticonderoga. Harmon Mechanicville. New York (126th 8t.) New York (G.C.T.) Plattsburgh Port Henry Port Kent. Rinner Rouses Point Saratoga Springs Watervillet Westport Whitehall Willsburo \$12.99 19.44 9.82 7.27 21.28 15.36 11.94 23.40 3.53 6.36 4.23 18.33 17.28 2.00 10.80 12.66 5.71 8.51 4.97 22.66 \$25.58 38.14 19.24 14.14 41.50 30.32 23.48 45.46 46.12 32.32 6.66 35.94 34.13 3.60 21.20 24.92 11.02 9.54 44.14 (4) Nearest Station to Troy, N. Y. 9.35 B Stops to discharge passengers from Albany and points south on notice to conductor and will stop on station signal to receive passengers for Rouses Point and points north. N Stops to discharge passengers from points south of Albany and stops on signal to receive passengers for Saratoga Springs and points north. R Stops to discharge passengers from Saratoga Springs and points north and stops on signal to receive passengers for points south of Albany. 9/35 9.35 a Passengers not carried locally between these stations. b Daily Except Saturday. f Stops to discharge passengers on notice to conductor and will stop on station signal to receive z Stops Saturday and Sundays and Nov. 23; Dec. 25; Jan. 1 and Feb. 22 only: One Way Sleeping Car fares — Montreal, Que. and New York and all intermediate points n Stops Sunday and Nov. 23; Dec. 25; Jan. 1 and Feb. 22 only. t Stops only to discharge passengers. r Stops only to receive passengers. Roomette | Roomette Suite || Bedroom Bedroom Suite ☐ Stops Saturday only. • Stops Sunday only to discharge passengers. N.C.B.—No facilities for handling checked baggage at this station. \$9.35 \$16.95 \$13.70 \$16.95 These trains do not carry checked baggage between Albany and New York. *Rates shown apply when space is occupied by one adult or by one adult ac by a child or children under 5 years of age. by a child or children under o years of age. Minimum number of adult Railroad tickets (valid in sleeping car accomodations) required for occupancy of various types of rooms are: Roomette 1 Bedroom 1 Bedroom Suite 2 N.A.—Non-Agency Station. Baggage handled. + Non-Agency Station. No baggage handled.

Low cost, one way and round trip, First Class and Coach Family Fares Low cost, round trip, group Economy Coach fares, for three or more

THE DELAWARE AND HUDSON RAILROAD CORPORATION

Passenger representatives of the D. & H. R. R. located at the offices named below.

R. P. KEATING, General Passenger Agent 360 Lexington Avenue New York, N. Y. 10017 Telephone YUkon 6-2742

TICKETS AND SLEEPING CAR RESERVATIONS

May be secured at following offices:

IN MONTREAL

D. & H. R. R. Corp., 1117 St. Catherine St., West, Phone 849-3614
Note: After 5:00 P.M. and on Saturdays, Sundays and Holidays,
Phone 861-6811.
C. P. Ry., Windsor Station, Phone 861-6811
C. P. Ry., 215 St. James St., West, Phone 861-6811

IN NEW YORK

D. & H. R. R. Corp., 360 Lexington Avenue Phone YUkon 6-1788 and 6-1789

Note: After 5:00 P.M. and on Saturdays, Sundays and Holidays, for reservations only. Phone MUrray Hill 7-6600.

N. Y. C. R. R.. Reservation Bureau, Phone MUrray Hill 7-6600 or through your local ticket agent.

INFORMATION

Air-conditioned equipment is assigned as far as possible but the right is reserved to employ non air-conditioned cars as necessitated by volume of traffic or emergencies.

When regular trains are operated in Sections, only one Section may have Sleeping Cars.

REGARDING TIME TABLES—This Railroad is not responsible for errors in time tables, inconvenience or damage resulting from delayed trains or failure to make connections; schedules herein are subject to change without notice.

BUY TICKETS before boarding trains and avoid payment of extra charge.

CHILDREN under five years of age when accompanied by parent or guardian, will be carried free, except that if the sole occupant of Sleeping Car space, they will be charged half railroad fare and regular Sleeping Car fare; children between the ages of five and eleven years; inclusive, will be charged half fare, and children twelve years of age and over will be charged full fare.

ADJUSTMENT OF FARES—In case of misunderstanding with Conductors or Agents pay fare required, take receipt and communicate with General Passenger Agent.

Agent.

REDEMPTION OF TICKETS—All tickets issued by this railroad, unused or partly used, will be redeemed under tariff regulations at proper value.

BAGGAGE—This Railroad does not guarantee to forward baggage on same train with passenger, or within a given or specified time limit, but reserves the right to forward it upon a preceding or following train.

BAGGAGE MAXIMUMS—No single piece of baggage exceeding 300 pounds in weight or 72 inches in greatest dimension or single shipment exceeding \$2,500.00 in value will be checked. Free allowance subject to tariff stipulations as to contents, weight, value and size.

BAGGAGE LIABILITY LIMITED—Excess value to be declared and paid for

BAGGAGE FOR STATIONS WHERE NO AGENT IS ON DUTY must be claimed at baggage car door immediately on arrival of train at that station, otherwise it will be carried to the first station where agent is on duty, and held for further orders.

BICYCLES (not Motorcycles), BABY CARRIAGES AND GUNS are trans-

BICYCLES (not Motorcycles), BABY CARRIAGES AND GUNS are transported in baggage cars subject to tariff regulations.

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G.P.A. 6

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Form 1

Printed in U.S.A.

JUNE, 1969 102nd YEAR

President

John Findlay Allen

Vice-President and Secretary John A. Pattee

Treasurer

Warren W. Schaffner

COMPILED AND EDITED UNDER THE DIRECTION

R. J. Laidlaw, Manager F. Coyle, Asst. Manager

PUBLISHED MONTHLY

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THE OFFICIAL GUIDE OF THE RAILWAYS

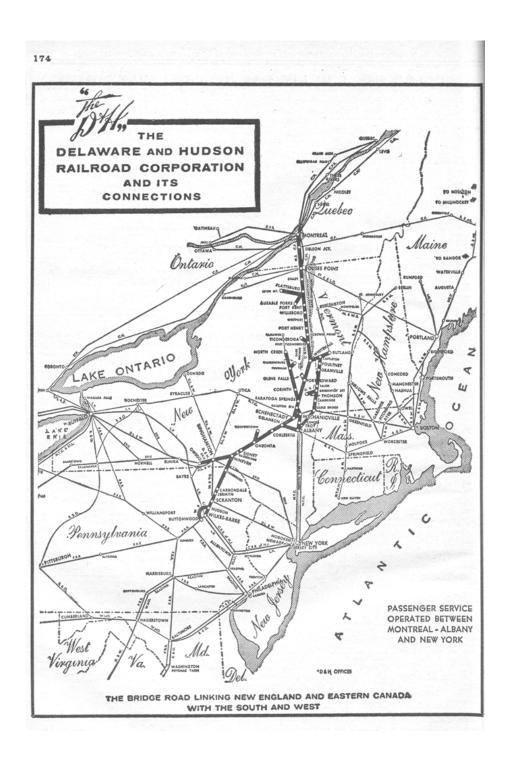
AND STEAM NAVIGATION LINES OF THE UNITED STATES, PUERTO RICO, CANADA, MEXICO AND CUBA

ALSO TIME-TABLES OF RAILROADS IN CENTRAL AMERICA

AIR-LINE INFORMATION

PUBLISHERS AND PROPRIETORS

NATIONAL RAILWAY PUBLICATION COMPANY... 255



DELAWARE AND HUDSON RAILWAY COMPANY

The Delaware & Hudson Building, Albany, N.Y. 12207 — Phone 518 Hobart 3-1141.

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Albany, N. Y. 19907

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Albany, N. Y. 12207 | -JOHN H. ENGLISH, General Attorney,

General Claims Attorney, Albany, N. Y. 12207

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R. J. BROGAN, Assistant to Vice-President-Traffic, Albany, N. Y. 12207

PASSENGER

R. P. KEATING, General Passenger Agent,
Eric Lackawanna Torminal Building, Hoboken, N. J. 07030

Eric Lackawanna Torminal Building, Hoboken, N. J. 07030

R. H. GEORGE, Freight Taff, Mgr. (Rates), Albany, N. Y. C. H. HOUSE, Jr., Asst. General Freight Agent (Rates), Building, Company, Compa Albany, N. Y. 12207

TRAFFIC DEPARTMENT REPRESENTATIVES

Albany, N.Y. 12207-Phone 518 Hobart 3-1141-Telex No. 14-5416- | Philadelphia, Pa. 19103-309 Transportation Center Building-A. E. DUPREY, Division Sales Manager.

Atlanta, Ga. 30303-Room 701, 101 Marietta Street Building-Phone 404 Jackson 2-5464-Telex No. 54-2230-R. O. BEEBE, Southeastern Sales Manager.

Boston, Mass. 02114-Room 225, 150 Causeway Street-Phone 617 227-7175-F. J. Smith, New England Sales Manager.

W. E. BARROWS, District Sales Manager. Buffalo, N.Y. 14217-2411 Delaware Avenue-

Phone 716 876-1613-H. E. Andrews, District Sales Manager.

Chicago, III. 60604-Room 1731, 327 South La Salle Building-Phone 312 Wabash 2-5574-D. E. WILKINSON, District Sales Manager,

Cleveland, O. 44115-1334 Midland Building-Phone 216 621-7140-W. G. Tierney, District Sales Manager.

Hoboken, N.J. 07030-Eric Lackawanna Terminal Building-Phone 212 349-5395 (96) (New York)— 201 653-6645 (New Jersey)—

I. A. OUINN, District Sales Manager. Montreal, P. Q .- xxx7 St. Catherine Street, West-Phone 514 849-7424-G. C. SEAMAN, Canadian Sales Manager.

J. J. KAELIN, District Sales Manager.

Pittsburgh, Pa. 15219-2118 Koppers Building-Phone 412 471-5516— W. H. HARTMAN, District Sales Manager.

Portland, Me. 04101-310 Congress Building-Phone 207 Spruce 3-1072-I. W. Kelley, District Sales Manager,

St. Louis, Mo. 63101-309 Chemical Building, 721 Olive Street-Phone 314 Chestnut 1-9178-Telex No. 44-7223-P. J. O'Toole, District Sales Manager.

San Francisco, Cal. 94105-621 Monadnock Building, 681 Market Street-Phone 415 421-6499-A. E. Olson, District Sales Manager.

Scranton, Pa. 18503-Erie Lackawanna Station Phone 717 342-8751-J. W. THOMAS, Division Sales Manager.

Winston-Salem, N.C. 27101-Dalton Hege-Lewis Building, 1020 Brookstown Avenue— Phone 919 PArk 3-4451-Telex No. 80-6417-R. G. SURRIDGE, District Sales Manager,

176 Schedules shown on this page are in terms of "LOCAL TIME" (advanced or daylight) as observed by each city.

DELAW	/AR	E AND H	UDS	ON R	AILW	AY C	MPA	NY		
Local time.	nea i	AND CHAMPLAI	N DIV	ISIONS			EXPL	ANATI	ON OF SI	ons.
Table I—SARA	UGA	IND CHAMPLAI	N DIV	isions.	-	_	* Daily,			
April 6, 1969.	Mls.	PC 71 D&H 35		P C 63 D & H 9			these statio	ns. o dischar	ge passeng	ers fro
(PC) [LEAV New York (Grand Central Terminal) Proton-Harmon	20.7	*8 30 A M d'925 * d1000 *	4	1030 Р м 1125 Р м	,		receive pas points north	sengers fo		Point an
PoughkeepsieRhinecliff	. 88.4	10 35 9	-	-			d'Stops o	nly to rec	eive passen	gers.
lidson libany-Rensselaer (D. & H.) (LEAV	E	II IO A M	:	1 25 A M			points sout	ops to discharge passengers south of Albany and stops on s rive passengers for Saratoga Sp		
libany-Rensselaer	6.1 19.0	*II25 A M #II47 A M #I205 P M = 1241 *	ein.	2 00 A M			and points	north.		
aratoga Springs ort Edward + \(\)		105 .	- to -	-			notice to con signal to rec	nductor as		on static
Whitehall(Rutland)an Whitehall+	t. 83.7	141 » 213 » 238 »	£ .	13 =			points south passengers	of Alban on signal	for Mont	to receiv
Port Henry Elizabethtown Cake Placid Saranac Lake Oot Kent County Cou	160,6	£ 257 * £ 13 18 *	5 8	5 48 #				ly to disc	harge passe	
Plattsburgh	173.5 197.0 197.0	13 18 # 18 18 # 19 13 40	ontro	30 # 707 # 712 A M			handled.		ation; no	
(Napierville Junction Ry.) LEAV. acolle Applorville Montreal West Westmount		5 00 P M 5 10 * 4555 *		22 A M 33 *			gage New Y	ork to Al	bany.	
Montreal West Westmount. Windsor Station		a555 * a602 * a610 * M	a!	32 # 30 A M			+ Coupon			
Connecting Trains via Can. Pac. Ry. ARRIV. Duebec (Palais Station) ttawa (Union Station)		9 20 P M 9 30 P M		20 г м 255 г м			each stati Index of Guide.	on is loc	able upor ated, see in back	Genera
All Equipment—Railroad Opera Air-Conditioned. Coache	ted.	Assigned Cars a Unreserved.	re	Coach are I	nes, Sleep Railroad o	ing Cars perated	COACHES		LEEPING CA	RS @
NORTHBOUNE D&H NO. 35-LAURE				Fares Montrea	I P O	to	One- Way	One- Way First Class	One- Way Roomette	
THROUGH CO. NEW YORK TO M				Albany-R	ensselaer		\$11.06	\$14.79	\$9.35	
BUFFET BAI				Croton-H	armon		16.81	23.79	9-35	
(Beverages and Li	eht Me	als)		Fort Edw	ard		8.46			:::::
NEW YORK TO ALBAN		NSSELAER		Hudson		7.))	12.56	******		
(Dining Ser RENSSELAER-ALBANY		ONTREAL.		New York	k (125th S	7.)	18.31			
MONTREAL LIMIT				Plattsburg	k (G.C. 7.)	18.56	26.79	9.35	:::::
D&H NO.	. 9			Port Hen	rv		0.0			
THROUGH COACHES AND WITH ROOMETTES - BEDROOM	SLE	EPING CARS		Poughkee	psie		3.73 14.81		9-35	:::::
NEW YORK TO M	ONT	REAL.	(£2)	Rouses Pe	oint		13.81	2.15	9-35	
Light Breakfast into Mont	real in	Lounge Car		Watervlie			to 6#			
				Westport			5.00	6.42	9.35	
SOUTHBOUND	TE	AINS		Willsboro			4.32	9.64	9+35	
D&H NO. 34-LAURE				Yonkers .			. 17.81	******		
THROUGH CO. MONTREAL TO M	ACHES	5		for space	ates ticke occupied.	ets honored	in Sleeping	Cars on	payment o	f charge
MONTREAL TO ALBAN	Y-RE	NSSELAER		One Wa	ay Sleep		res-Mont termediat			v York
BUFFET BAI (Beverages and Li		va /e)		Roomet		mette Suite	Bed	room	Bedroom	m Sulte
RENSSELAER-ALBANY	TO N	EW YORK		One Peri	son	Persons	Person ®	Two or mo	Per	r more sons
MONTREAL LIMIT D&H NO. THROUGH COACHES AND WITH ROOMETTES—BEDROOF MONTREAL TQ	IO SLEI MS-L	EPING CARS	ree)	Minimu	shown a	pply when by a child o	space is occor children u ailroad ticken uncy of varie	upied by	one adult o	

DELA	WARE	AND HU	DSO	N RA	ILWA	Y CO	MPANY
Local time.							
Table I—CH	AMPLAIN	AND SARATOG	A DIVIS	IONS.			EXPLANATION OF SIGNS.
		D & H 34	١	H 10			4 Th. 11
April 6, 1969.	Mls.	P G 72		64			* Daily,
					1		e Passengers not carried locally between these stations.
Connecting Trains via Can. Pac.	PAUP						f Stops to discharge passengers of
ttawa (Union Station)uebec (Palais Station)			6.0	00 P M			notice to conductor and will stop on static
(Madieumilla Varaction Pu) (1)	DATE I	€#8 30 A M		0 Р м			signal to receive passengers.
Windsor Station Windsor Station Montreal Westmount Montreal West		€6 38 × €6 45 ×	€93	7 »			iStops to discharge passengers fro
apierville		fg 21 ×	f10 2	21 ×	*********		Saratoga Springs and points north as stops on signal to receive passengers f
acolle[AR	RIVE	9 33 A M		2 P M			points south of Albany.
ouses Point (D. & H.)	RIVE	*943 A M	29 *104	4 гм			/Stops to discharge passengers fro
ouses Pointort Kent	.lve. o	£ 948 *	F 105				Montreal and to receive passengers f Albany and points south from May a
		1035 ×		9 рм			1969 to October 12, 1969.
Elizabethtown Lake Placid Saranac Lake	63.1	£ 11 23 *		MAI			f Stops only to discharge passengers.
ort Henry	74.1						\$ Stops Sundays only to discharge ps
ort Henry ort Ticonderoga (Ticonderoga). /hitehall (Rutland) /hitehall.	s.re gr.a	≡ 12 08 Р м - 12 40 э	ě - ·	- 13 *			
omstock	1 1 20.0	12 46 » e f 253 »	E 14	0 9			g Connecton made at Montreal West,
ort Edward Glen Falls	135.3	114 >		-			This train does not carry checked ba
aratoga Springslechanicville	153-7	1 39 » 9 f2 14 »	ontreal				gage Albany to New York.
atervliet (Troy)	190.9	= 1232 × 310 ₽ M		NA M			Non-agency station; no bagga
(D. & H.) [AR	RIVE			*****			handled.
Ibany-Rensselaer[L	o	3 30 P M		O A M			 Stops to discharge passengers from Rouses Point and points north on notice
udsonhinecliffoughkeepsie	53.8	415 *		-			to conductor and will stop on station sign
roton-Harmon	109.5	£5 20 =	#6 a	io #			to receive passengers for Albany as points south.
onkers	(al) . 142.2	45 45 W 6 10 P M		OAM			
(AX	RIVE	For Equi		see page			▲ Non-agency station; baggage handle
	F	REIGHT	SER	VICE	ON	LY.	
Table 2.		Table 5.			Table 7		Table II.
ALBANY AND BINGHAMTON.	NINEVEH	, N.Y., AND WIL BARRE, PA.	KES-	FORT T	CONDER	OGA, N.	V FACIF RRIDGE N V AND
oAlbany	Mls.	1	1	Als.	STATI Fort Tico	ONS.	Mls. STATIONS.
o.oVoorheesville	2118.	STATIONS.	- 11-	2.0	. Ticond	eroga	6.1
7.2 Altamont	0	Nineveh Centre Village					zo.8Shushan t5.4 Greenwich Junction
6.2 Central Bridge 9.2 Howe's Cave	8.4	. East Windsor		MECHAN	Table !	3. Ν.Υ Δ	ND 23.7West Rupert
4.7Cobleskill	20.5	Windsor Lanesboro		Mis.	LANSON		25.2Rupert
6.9 East Worcester 1.5Worcester	60.6	Carbondale Mayfield		0	Mechan	icville	38.cMiddle Granville
6.5Schenevus 6.4Colller's	64.2	Jermyn Archbald Jessup-Peckville.		7 . 5	Ushe	ra	44.4Poultney
0.6 OneontaOtego	68.1	Olyphant Dickson City		0.8	South Sche	nectady	58.4 West Rutland
8.8Unadilla	71.6	Providence	******	2.01	Delan	son	62.4Rutland
8.5 Bainbridge	74.0	Green Ridge Seranton .South Scranton			T-11		Table 13.
0.3Harpursville	80.0	·····Moosic		BALLST	Table S ON SPA, ENECTAI	N.Y., AI	PLATTSBURGH, N.Y., AND AUSABLE FORKS, N.Y.
able 3-SARATOGA SPRINGS-	84.1	Avoca Pittston Yatesville		Mls.	STATI	ONS	Mis. STATIONS.
NORTH CREEK.	87.1	Laffin		6.4	.Ballston	Spa	6.tSalmon River 8.8Lapham's Mills
oSaratoga Springs	92.4	Parsons Wilkes-Barre		4. I	Schene	ctady	10.9
6.9 Corinth					Table :		23.5 Ausable Forks
9.5Stony Creek		Table 6.		WHIT	Table IC	Y., AND	Table 14.
9.6Riverside	COOP	NTA, N.Y., AND ERSTOWN, N.Y.	_	fls.	STATI	ONS.	PLATTSBURGH, N.Y., AND LYON MOUNTAIN, N.Y.
able 4 — FORT EDWARD AND GLENS FALLS.	Mls.	STATIONS.		8.4	White	hall	oPlattsburgh
Ils. STATIONS.	0	Oneonta		0.1	Hydev	ille	To.4 Morrisonville
o Fort Edward	£. 2	Colliers Milford Cooperstown .	1	0.7	. West Ru	tland	Donnemore
Glens Falls.,	22.0	. Cooperstown .		3.7	Rutla	nd	36.3 Lyon Mountain

Adirondack: D&H Train 68

The photo of the arrival of the *Adirondack* in Plattsburgh, NY in August 1974 that is given below was posted by Arthur House on the Delaware and Hudson Railroad *Facebook* page on July 13, 2015, with the following caption:

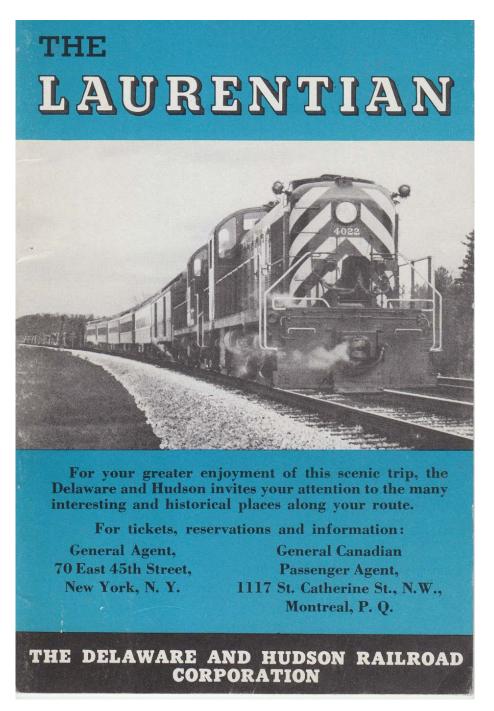
"August 1974: The first revenue run of Amtrak's Montreal-New York City 'Adirondack,' gloriously equipped with Delaware and Hudson motive power and rolling stock---and a leased Canadian Pacific dome-buffet car--arrives in Plattsburgh, N.Y., southbound. (The winged memorial to the War of 1812 Battle of Plattsburgh, in which a British invading force from Canada was defeated and sent packing, is in the background.) The train's D&H equipment includes former Santa Fe PA locomotives, and former DL&W, Erie, and Rio Grande cars."

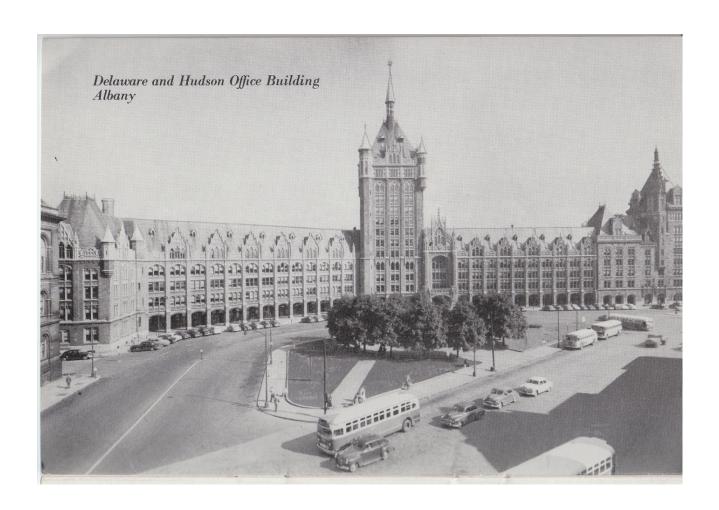


Amtrak: a government-run passenger service. In early 1971, Amtrak took over all intercity passenger service, and announced that effective July 1, 1971, the remaining D&H passenger service would be discontinued. This did not sit well with the cities served by the D&H. Amtrak would, therefore, run a passenger train (the "Adirondack" / D&H train 68) between New York City and Montreal.

The Laurentian

Given below is the booklet on *The Laurentian* in the holdings of the Carbondale D&H Transportation Museum:



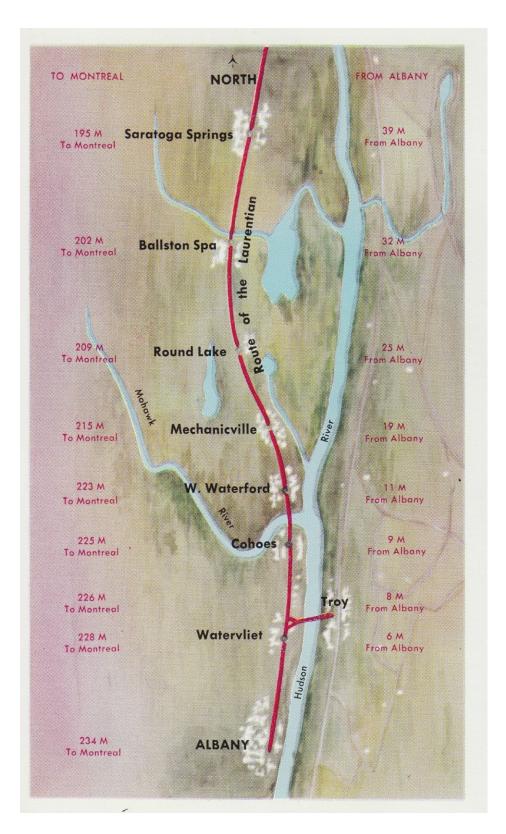


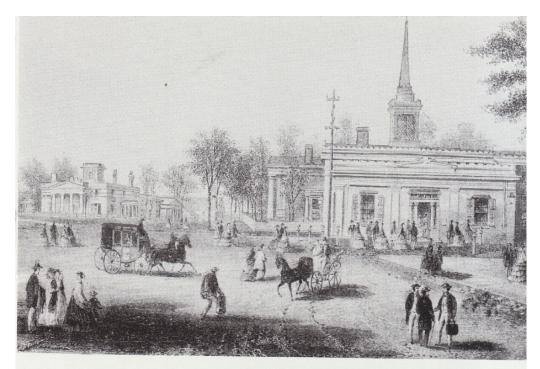
THE LAURENTIAN, the daylight fast train over the D&H between New York and Montreal, offers the most interesting, picturesque and scenic one-day trip in America. The route of the D&H between Albany and Montreal follows the Hudson River and the Champlain Canal to Whitehall, and then follows the shore of Lake Champlain for over one hundred miles to the Canadian border. This lake of magnificent distances, superb mountain views and important historic associations, lies midway between the long ranges of the Adirondacks and the Green Mountains. It is one hundred and thirty miles in length, and from one to thirteen miles in width. Its forest walled shores and many islands with their mountain background create a constantly changing scenic picture of unsurpassed beauty.

The military history of Lake Champlain extends from its discovery by Samuel de Champlain in 1609 to the Battle of Plattsburg in 1814. The waterway from the St. Lawrence River through the Richelieu River, Lake Champlain and Lake George played an important part in early Colonial history, as it was the invasion route of the French under General Montcalm, and later of the British under General

Burgoyne.

Some of the points of interest along your route between Albany and Montreal are pointed out in the following pages.





Old print of former Saratoga Terminal

LEAVING Albany, the Capital City of the Empire State and an inland seaport, The Laurentian begins its journey northward. It passes at Watervliet the U. S. Army Arsenal and at Cohoes a fine view is had of the Mohawk River as it flows into the Hudson.

Mechanicville, with its paper mills, is also the junction where the D&H interchanges with the Boston & Maine R. R. for freight operations to and

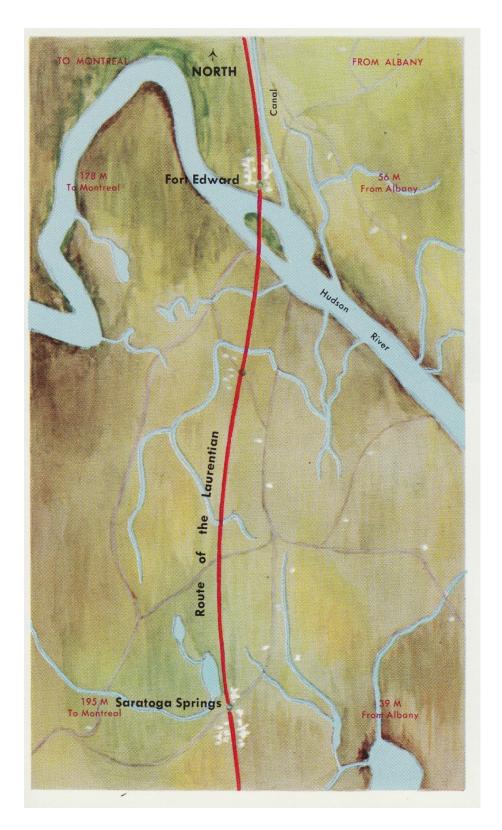
from New England.

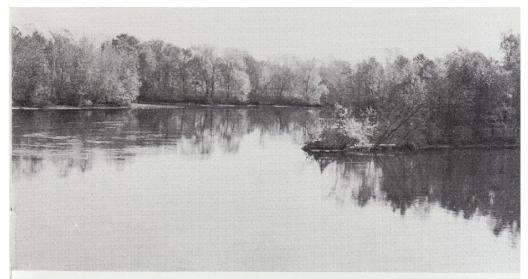
Saratoga Springs is widely known as a health resort. Its variety of mineral waters are world famous. The State of New York has developed a magnificent reservation with baths, drinking halls, bottling plant and a fine hotel with every recreational facility.

During the month of August horse racing brings visitors from far and near to one of the oldest and one of the most beautiful tracks in America. Night Harness Racing is also conducted on another track,

the Saratoga Raceway.

[3]





Hudson River, Fort Edward

THE TRAIN crosses the Hudson at the "Great Carrying Place" which was the beginning of the war-

path that led from the River to Canada.

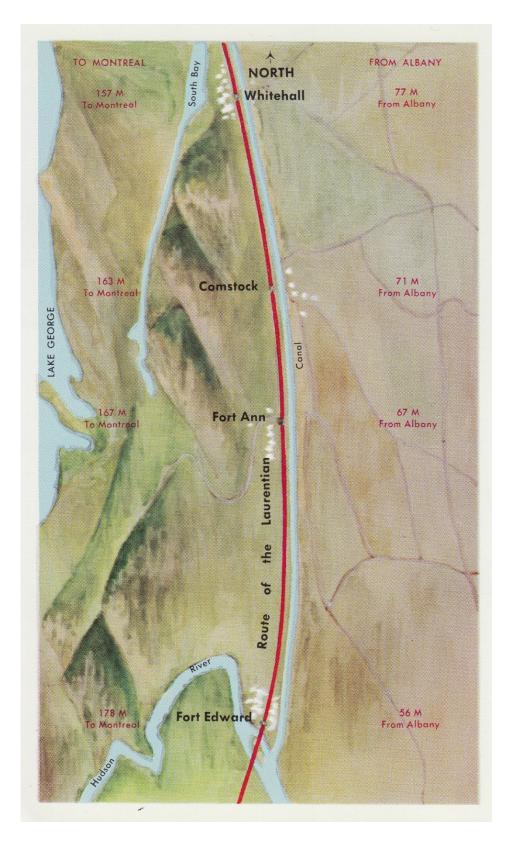
The fort — first named Fort Lyman, afterwards, Fort Edward — was one of the chain of block houses that protected the trail, and there is not a mile that has not heard the Indian war cry, and the firing of the contending armies in the French and Indian War and the Revolutionary War.

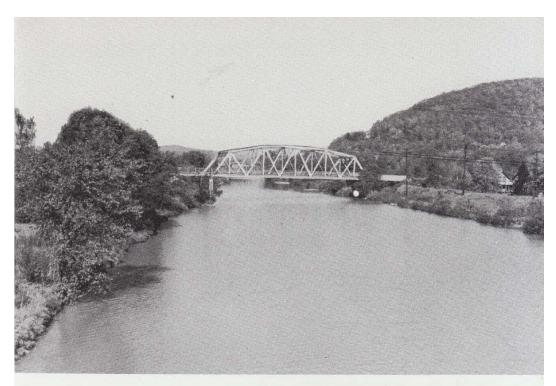
It was at Fort Edward that Jane McCrae was tomahawked and scalped by an Indian in the employ of the British. Though she was of a Loyalist family, the rage aroused by the murder caused reinforcements to pour into General Gates' army from all the Northern Colonies, and aided in the victory over the British at Saratoga.

The D&H and the present canal follow roughly the original wilderness path, and the next block house at Fort Ann—eleven miles north—exists today only

as grassy mounds east of the track.

Large paper, cement and manufacturing plants in this area and on the branch line to Glens Falls are served by the D&H.

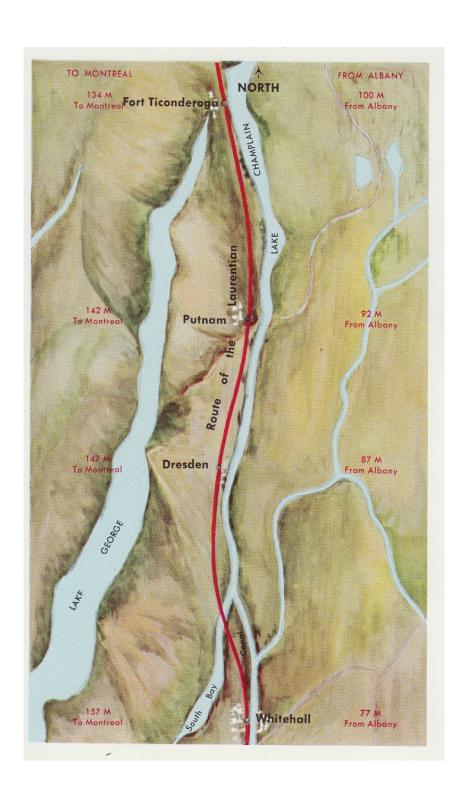


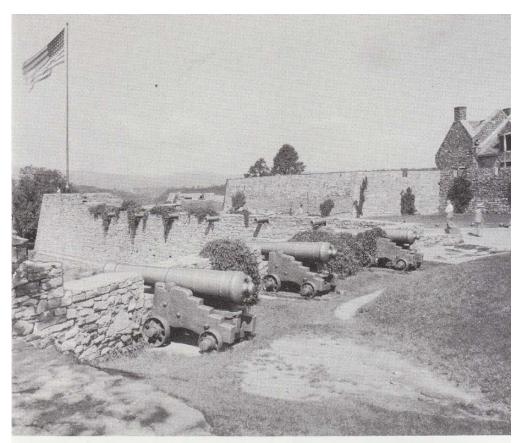


Champlain Canal, Whitehall

W HITEHALL—the Skenesboro of early days—and the birthplace of the Navy where Arnold built his fleet figured largely in the tide of war that rolled back and forth from Canada to the Hudson. Controlling the head waters of the lake, it changed hands repeatedly—from French—to British—to American—and was the main camp for Burgoyne's army during his disastrous campaign that ended at Saratoga.

Major Philip Skene, a Tory officer, had grandiose ideas of a great manor at the head of the lake. Legend has it that he persuaded Burgoyne to take the old trail to Fort Edward rather than the easier way up Lake George, as it would open up roads on his estates. Another legend states that his mother received an annuity "so long as she remained above ground," and that when she died, he put the body in the cellar and continued to collect the annuity.



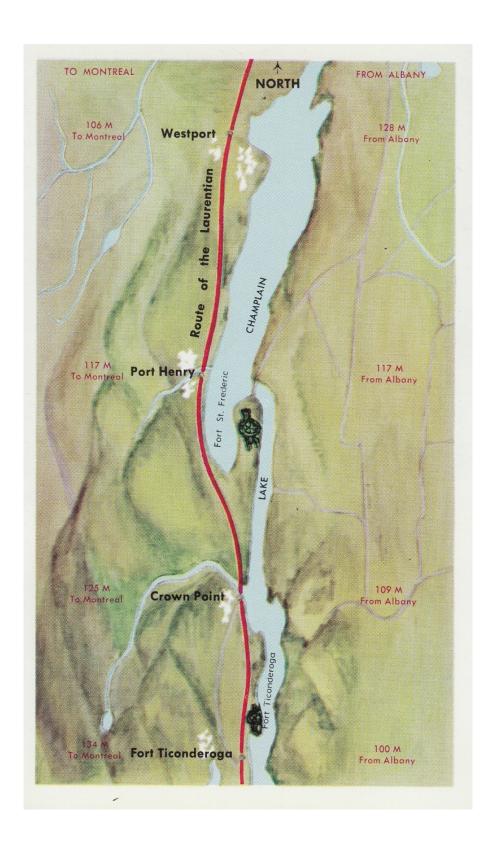


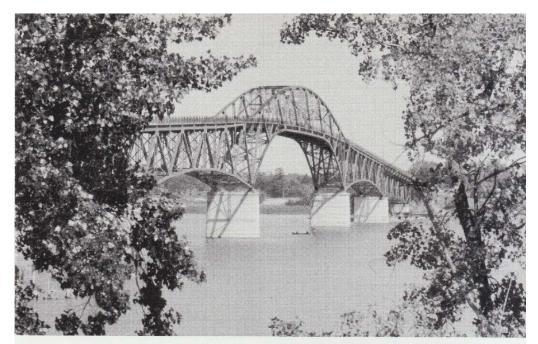
Fort Ticonderoga

FROM WHITEHALL north, the train crosses the entrance to Great South Bay, the source of the lake, and follows the winding waterway to Montcalm Landing-now Fort Ticonderoga-where Montcalm stepped ashore on his way to capture Fort William Henry at Lake George during the French and Indian Wars. On a bluff east of the train is Fort Ticonderoga, beautifully restored by the Pell family and containing a complete collection of war matériel. The Fort was held in turn by France-Britain-and the United States, and was originally named Fort Carillon. It was the strongest of the fortified posts on the lake.

Great paper mills constitute the principal indus-

try in this area.

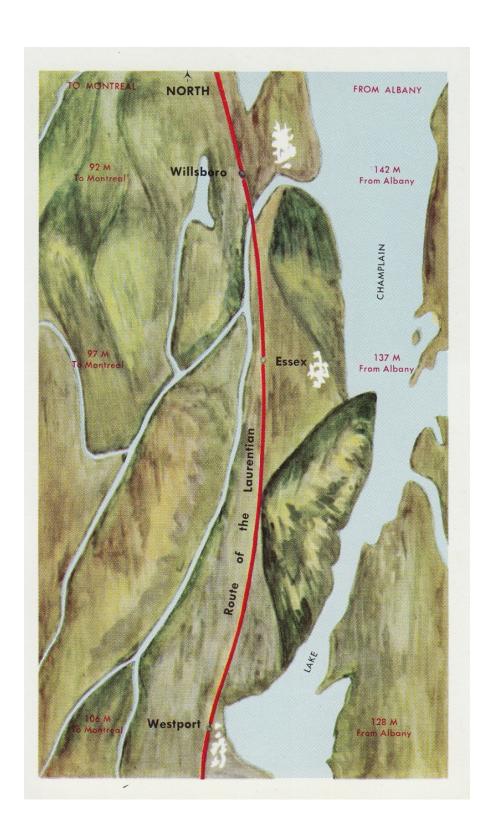




Highway Bridge at Port Henry

AT PORT HENRY, shipping point for Adiron-dack iron ore the lake widens, and on Crown Point, at the New York end of the highway bridge, stands Fort St. Frederic, one of the first fortified posts on the lake. Indications are that a block house was built there about 1730, and later a substantial fort whose stone walls still stand. The site is a State Park and the lighthouse is a memorial to Samuel de Champlain who was the first white man to see the lake in 1609. The French called it "Fort à la Chevelure" or Scalp Point, from the massacre there of the refugees from the burning of Schenectady who were fleeing to Canada.

From Port Henry to Westport, the historical interest centers on the Vermont shore. There—in creeks and bays—Macdonough built his fleet which won such a decisive victory over the Britist under Captain Downey at Cumberland Head in September 1814.

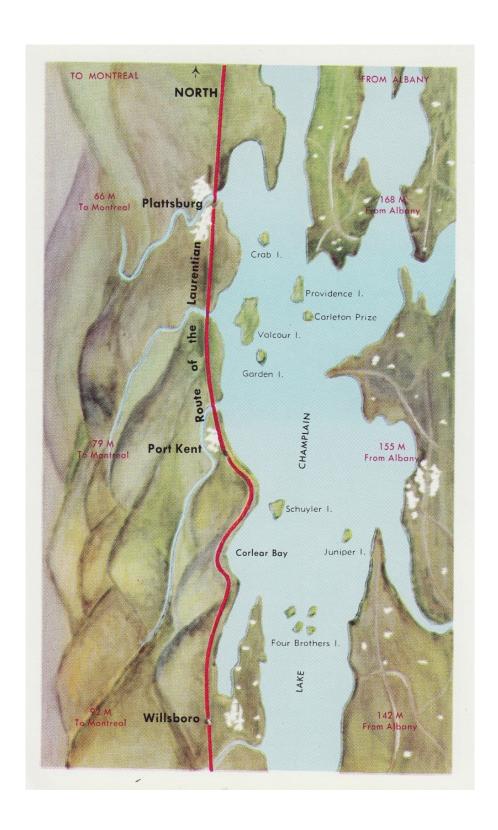


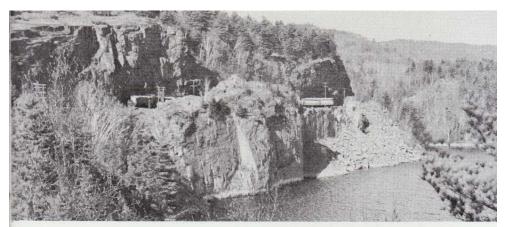


Westport Bay

WESTPORT was settled about 1765, and is located on one of the most beautiful bays in the lake. It is a charming resort village and popular yachting base. The train leaves the lake there to join it again at Willsboro, and for fourteen miles goes through fertile farming country watered by the Bouquet River, on its way to join the lake at Willsboro. The high peaks of the Adirondacks are directly west, and Westport has long been the principal gateway to the mountain resorts, Elizabethtown, Keene Valley, Lake Placid and Saranac Lake.

[13]



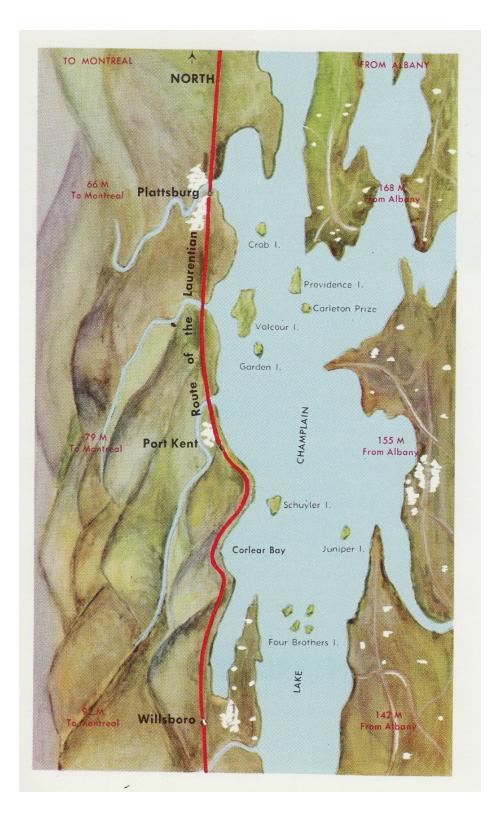


Through the Rocks, Willsboro

THE TRAIN joins the lake again at Willsboro, and for thirteen miles goes through some of the most spectacular scenery in the State. Willsboro Bay cuts four miles inside. Willsboro Point, and the railroad is built into the side of the mountains that rise abruptly from the bay. The building of the road in 1875 was one of the great engineering feats of the decade. Red Rock Tunnel cuts through the mountain, and halfway down the bay is the small cove called Port Misery—attesting to the hardships of the barges that took shelter there in rough weather. Wollastonite and pulp are the principal products of this area.

Over Willsboro Point and across the lake can be seen Burlington, the largest city in Vermont; and behind and against the sky, the range of the Green Mountains with the two high peaks of Camel's Hump and Mount Mansfield most prominent. Juniper Island stands like a ship in Burlington Harbor, and to the south the four Brothers Islands—now a bird sanctuary—are plainly seen. The track makes a great curve around Corlear Bay where Arendt van Corlear, the founder of Schenectady, was drowned; and in the bay sits beautiful Schuyler Island where Arnold collected and repaired his sinking ships after the battle of Valcour.

[15]

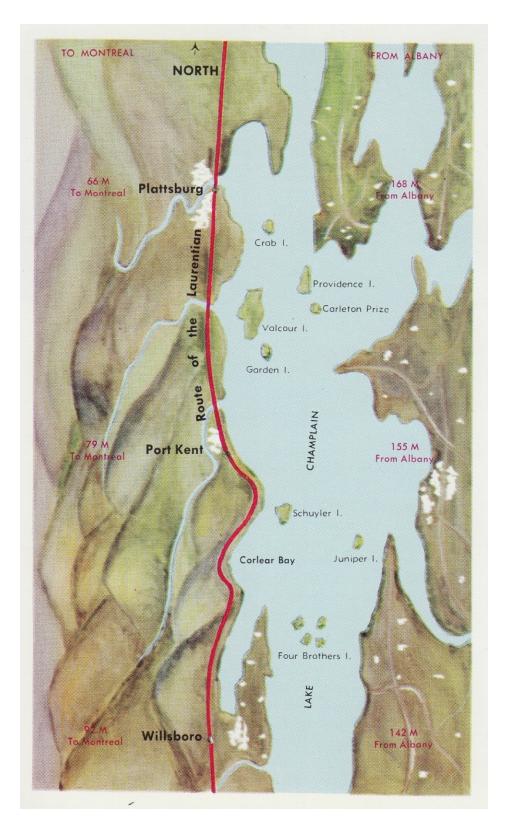


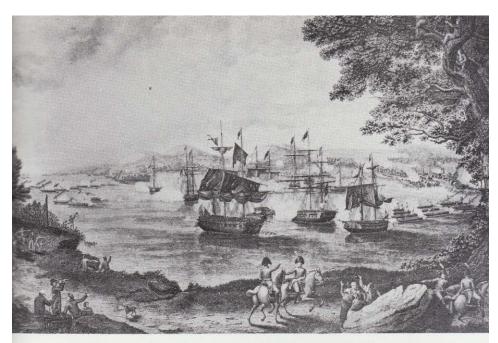


Port Kent Ferry

AT THE LITTLE VILLAGE of Port Kent, ferry service crosses the lake to Burlington, and off the end of Providence Island—on the Vermont shore—is the island called "Carleton's Prize." There is a legend that the British mistook it for a hostile ship and bombarded it heavily before they saw their mistake. Garden Island, off the south end of Valcour, was also often mistaken for the enemy, and there is a story that the British, seeing it in the fog, halted their pursuit of Arnold, thinking that he had been reinforced.

A few miles north of Port Kent, the track crosses the two branches of the Au Sable River and then Valcour Island appears quite near the shore. The great fight between the British and Americans in 1776 was off this island, and Arnold's anchorage, about where the lighthouse can be seen, has yielded many relics of the battle. For many years, the wreck of the "Philadelphia" could be seen from a small boat in the clear water. In 1935, it was raised, and is now permanently exhibited on Highway 22 between Essex and Willsboro. Cannon balls art frequently found today in the shallow water.





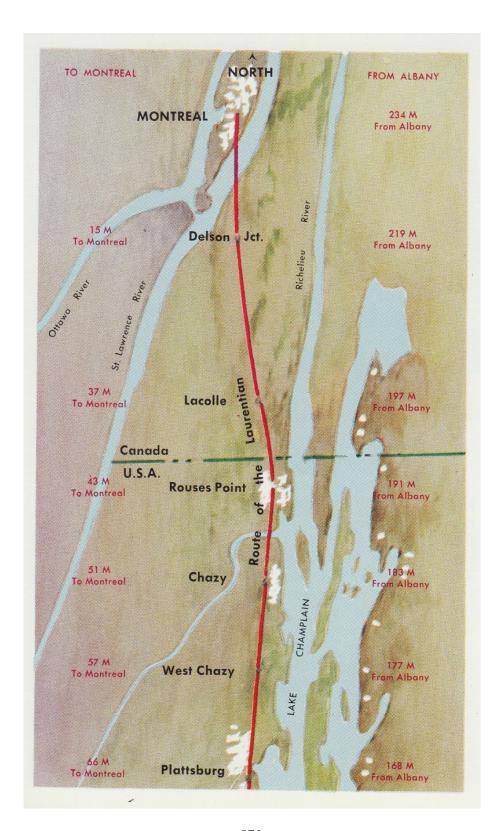
Naval Engagement between the American and British Fleets September, 1814

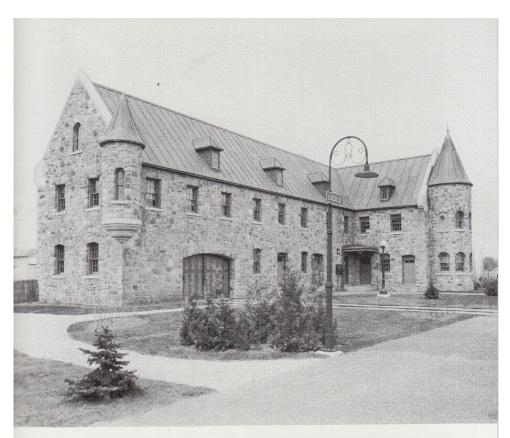
JUST NORTH OF VALCOUR lies Crab Island—a government cemetery for British and Americans killed in the Battle of Lake Champlain—and then the train approaches Plattsburg—the largest city on the west side of the lake and site of a great base of the Strategic Air Command of the U. S. Air Force. The old stone barracks of the Army post can be seen still standing to the west of the train at the south end of the modern buildings. Plattsburg is a busy city with a number of active industries. Iron ore from Lyon Mountain, 13 miles to the west, is brought to the main line for shipping to the steel mills.

Commodore Macdonough and General Macomb defeated the British on lake and land on September 18, 1814—a decisive victory which, more than any other action, decided the War of 1812-14. A monument to Macdonough can be seen to the west of the

train in the square facing the Lake.

[19]



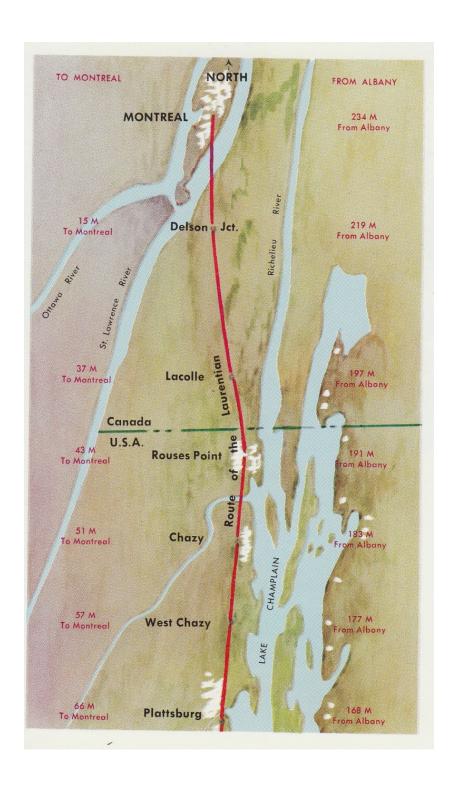


Railroad Station and Canadian Customs and Immigration Office at Lacolle

FROM PLATTSBURG to the border, the country flattens out, and signs of French Canada appear in the villages and habitant farms. At the border town of Rouses Point, settled about 1793 by Jacques Rouse, the lake flows into the Richelieu River, and on into the St. Lawrence River at Sorel. Here Canadian Customs and Immigration inspectors board the train and complete their work before arrival in Montreal.

Rouse is said to have had twenty-six children, the family forming the first pioneer settlement in north-

ern New York.





Windsor Street Station, Montreal

FROM ROUSES POINT to Montreal, the train proceeds through the Province of Quebec and reaches the Canadian Pacific Railway at Delson Junction, 28 miles north of the border.

The flat country which is traversed between Rouses Point and Delson Junction is cut up into small farms and it will be noted that there are many divisions as outlined by fences. These farms are owned by French Canadians and the fences denote parcels of land that have been set aside for the children of the owner.

The train crosses the St. Lawrence River on one of the longest railroad bridges in the Dominion of Canada and just west of the south end of this bridge is the Indian Village of Caughnawaga. The Indians who own this village, which is upon a reservation granted them by the Canadian Government, are skilled structural steel workers.

The Windsor Street Station, which is pictured above, in addition to being the train terminal of the Canadian Pacific Railway, houses the general offices of that railway system and is one of the finest structures of its kind on the continent.

Montreal is the second largest French-speaking city in the world. It is gay, historic, beautiful and dynamic, worthy of being the metropolis of Canada. We very much appreciate your patronage and hope that this little historical booklet of the route traveled will lend added zest and interest to your trip. Keep it, please, as a memento.

We also operate a fast, non-stop overnight service—the Montreal Limited—between Montreal and New York. This train carries reclining seat coaches, Pullman club car and Pullman sleeping cars, offering wide choice of accommodations, including drawing rooms, compartments, double or single bedrooms, roomettes and section space.

Dining car is operated serving breakfast

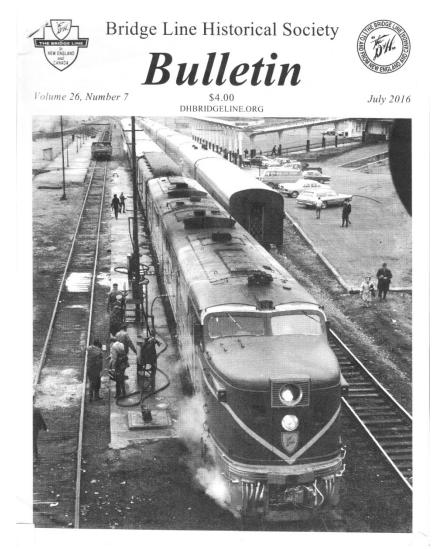
northbound.



Villooler



At Whitehall, in mid-afternoon, the north- and south-bound *Laurentians* passed each other. Here is a photo of the meeting of the last two *Laurentians* at Whitehall on April 30, 1971. This is the cover photo of the July 2016 issue of the *Bridge Line Historical Society Bulletin*.



"The D&H's last two *Laurentians*, with the northbound led by D&H #16, meet at the Whitehall, NY station April 30, 1971 photo by Hugh Strobel."

For more on the *Laurentian* and the *Montreal Limited*, see the August 2015 issue of the *Bridge Line Historical Society Bulletin* (Volume 25, Number 8), pp. 22-27, for a reprint of Karl R. Zimmerman's article from *Railroadiana*, No. 41, Nov/Dec 1971, titled "Dumaine's Trains *Of The Laurentian, The Montreal Limited, and the PA's.*"



The Laurentian. Photo by Jim Shaughnessy

"THE LAURENTIAN"

The Delaware & Hudson, the only railroad that offers passenger service between New York and Montreal, now operates this modern passenger equipment with high speed diesel locomotives on its daylight train, "The Laurentian." The train is pictured passing through Mechanicville, N. Y. on the Southbound run.

Post card in the collection of the Carbondale D&H Transportation Museum.

Wilkes-Barre Connecting Railroad

Wilkes-Barre, the Southern Terminus of the Pennsylvania Division

Having had a good look around at the northern end of the D&H system in upstate New York, we must now journey to the South and have a look around in the Wilkes-Barre area, the extreme southern end of the D&H Pennsylvania Division.

It will be useful here to recall what we presented about the Union Railroad in Volume X in this series. Therein, we reported the following:

Union Railroad line from Green Ridge to Wilkes-Barre

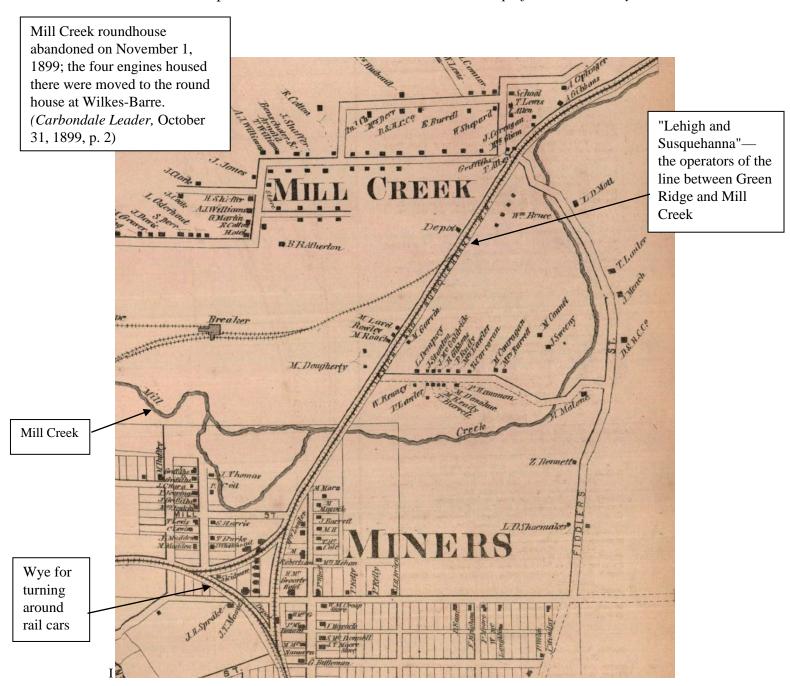
In 1867, the Union Railroad Company of Wilkes-Barre completed the 15-mile long standard-gauge rail line from Green Ridge to Union Junction (near Hudson, formerly known as Mill Creek).

Where was Union Junction? In A Century of Progress, pp. 198-200, we read:

"The Union Coal Company, after it had merged the Howard company in February, 1867, completed railroad construction northward from Union Junction, in the Lackawanna Valley, to Green Ridge. The location of Union Junction may be stated as approximately one and one-half miles north of the present station at Hudson [formerly known as Mill Creek] on the Pennsylvania division of the company's railroad, so that the railroad from Union Junction to Green Ridge covered a distance of nearly fifteen miles. As Hudson is only three and one-half miles north of Wilkes-Barre, it is apparent that a long step toward the latter had been taken." (In 1886, the D&H extended the line three miles to the south to the Lehigh Valley depot in South Wilkes-Barre.)

The line from Green Ridge to Union Junction opened on June 18, 1867, and the Union Coal Company leased the exclusive right to run passenger and merchandise between Union Junction and Green Ridge, for a period of twenty years, 1867-1887, to the Lehigh Coal & Navigation company, the latter company afterward transferred this lease to the C. R. R. of N. J. The D&H could use the line (but not for passengers and merchandise) but did not control the line. The D&H would gain control of the line in 1887.

Shown below is a map of Mill Creek for the 1873 D. G. Beers Map of Luzerne County.



In Passenger and Freight Stations Delaware and Hudson ("Inspection of Lines, June 7-10, 1928), page 14, we read: "Hudson and the stream flowing through it were formerly called Mill Creek. Five thousand acres of wood land were originally purchased and settled by two brothers, Henry and Richard Drinker. / In 1891, by petition of the citizens of the town, its name was changed to 'Hudson,' taken from the word 'Hudson' of the Delaware and Hudson Canal Company. / It is the junction point of the Wilkes-Barre connecting Railroad and The Delaware and Hudson Company. / Population 3,800."

Hudson used to be called Pumpkin Hollow:

George Cotton, was born at Blenavon, Wales, in 1851, the son of a mining foreman. When George Cotton was a small boy, the family moved to America and settled in Bloomsburg, PA. In 1867, the Cotton family moved from Ashland, PA to a community that was known at the time as Pumpkin Hollow, which was later known as Mill Creek, and then as Hudson, where George worked for the Hudson Coal Company.

That we know from the biographical portrait of the man (" 'Twas Called 'Pumpkin Hollow' ") that was published in the April 1, 1932 issue (pp. 99-100, 108) of the *Delaware and Hudson Railroad Bulletin*, wherein we read: "In 1867 the Cotton family again moved, this time to Pumpkin Hollow, later known as Mill Creek and now Hudson, Penna. Here George was employed in the Union Slope, now operated by the Hudson Coal Company as part of the Loree Breaker's workings, as a mule driver, moving the diminutive coal cars in and out of the mines. Shortly after his arrival he was promoted to the rank of runner, a position which paid \$3.25 a day. As a runner he was in charge of the drivers. It was his responsibility to make sure that each miner had sufficient cars in his chamber at all times and also to give each miner exactly eight cars per day." (p. 100).

From George Cotton's biographical portrait, we learn some remarkable facts about the D&H engines that were assigned to Hudson in 1875. In his biographical portrait, we read: "One of the most attractive Delaware and Hudson engines ever to turn a wheel on the Pennsylvania Division, according to George Cotton, retired Wilkes-Barre roundhouse machinist, was the old No. 7, the *Quintard*, which was stationed at Hudson back in 1875. There were hand carved grapes on her tender so realistically done that one was tempted to pick them. The engine itself, moreover, was painted in a variety of colors in addition to the conventional black, and the numerous brass bands, handles, bell and flag staffs. The top of the smokestack was bright red, while dashes of gold and blue completed her gaudy dress. / The other two engines then assigned to Hudson, Nos. 6 and 8, named *Mill Creek* and *J. J. Albright*, respectively, were also painted in bright hues. Numbers 7 and 8, both of which had four drivers, were known as road engines, and operated between Olyphant and Hudson; No. 9, a six wheeler, was used exclusively on mine runs." (p. 99)

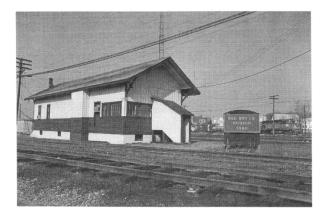
On February 1, 1880, George Cotton became an engineman with engine 22, named the *Fuller*. This position called for runs between Plymouth and Hudson and various mine switching work. Working on the single track between Plymouth and Hudson was not only difficult but dangerous, as Cotton explains in his biographical portrait: "Operation of the single track line between Plymouth and Hudson was made doubly difficult and hazardous because of the fact that Delaware and Hudson crews ran over the tracks of four different railroads in that short distance: their own, the Lehigh Valley, Central Railroad of New Jersey, and the Delaware, Lackawanna and Western. The crews therefore had not only to be familiar with the books of rules of all these lines but they had to watch the schedule of each to avoid collisions." (pp. 100, 108)

On July 1, 1880, Seth V. Colvin ("They Had No Caboose," *The Delaware and Hudson Company Bulletin*, October 1, 1932, pp. 259-60) was named conductor on the *Mark L. Fuller* on the run between Plymouth and Hudson. In his biographical portrait, we read: "On July 1, 1880, Mr. Colvin was promoted to the rank of conductor and was placed in charge of locomotive 22, the *Mark L. Fuller*, a 20-ton engine with her water supply in tanks placed along the sides of the boiler instead of in a tender. This engine was used to do mine switching at Plymouth and to move cars between that point and Hudson. On that short run the crew ran over four different single track railroads: The Delaware and Hudson, Lehigh Valley, Jersey Central, and Lackawanna; today the Wilkes-Barre Connecting Railroad, owned and operated jointly by the Pennsylvania and Delaware and Hudson, links Plymouth and Hudson."

The Yard Master at Hudson was Milot Shiffer. Seth Colvin married his daughter, Ruth, and they were the parents of five children. Their son Louis became a passenger train conductor, running between Scranton and Carbondale.

Milot Shiffer: In Tunkhannock, PA, on October 21, 1853, Milot Shiffer was born. In May, 1871, he became a brakeman in the yard at Mill Creek. In the biographical portrait of the man ("Yardmaster For Fifty Years") that was published (pp.371-372) in the December 15, 1929 issue of *The Delaware and Hudson Company Bulletin*, we read: "The yard [at Mill Creek] was small, there were only two or three tracks when he first began, yet with the two different gauges of track, broad and narrow, a yard brakeman had his hands full at all times keeping the cars moving and on the rails. After two years as a brakeman, Mr. Shiffer was promoted to yardmaster at Mill Creek in which capacity he served for the remainder of his sixty years of service." (pp. 371-372)

On page 33 of the *Bridge Line Historical Bulletin* for April 2015, there is a photograph by Mike Bischak of the "D&H RWY Co. Hudson Yard office." The caption on the photo reads as follows: "D&H's SX Cabin, the Hudson, Pa. yard office, was home of the Hudson Operator, who controlled the CTC from SC Cabin in South Wilkes-Barre to DF Cabin in Carbondale. March 7, 1980 photo by Mike Bischak." Here is that photograph:



Back to the Union Railroad:

About this rail line, the Union Railroad, we read the following in the 1890s Summary:

"In 1867*, the Union Railroad, between Green Ridge and Union junction, near Mill Creek, was completed. Up to this time, the gauge of the [D&H] track was 4 ft. 3 inches, but in constructing the line to Union Junction, a third rail was laid, making two gauges, the one 4 ft. 3 inches, the other 4 ft. 8 ½ inches, from Valley Junction to Union Junction. The Gravity, or 4 ft. 3 in. gauge, was extended to the Baltimore mines, near [north of] Wilkes-Barre, over the tracks of the Lehigh Coal & Navigation company, and Gravity coal cars now ran between Honesdale and the Baltimore mines."

*1866 or 1867? In Passenger, Freight and Work Equipment on the Delaware and Hudson, The Delaware and Hudson Company BOARD OF MANAGERS INSPECTION OF LINES, June 2, June 5, 1927, p. 21, we read: "The Union Coal Company opened, in 1866, the line between Green Ridge and Union Junction. (15 miles)." 1866 or 1867? Construction may well have begun in 1866, with the line being completed in 1867.

Before the publication of the first five volumes in this series on the D&H, it was not generally known that Gravity-gauge tracks extended south of the foot of Plane No. 23 in Olyphant. Jim Shaughnessy, for example, in *Delaware & Hudson* says (pp. 68-69) the following:

"In the meantime in 1870, a locomotive-operated road was built between Carbondale and Olyphant, near Scranton, and a second track was added to the original flat stretch from Valley Junction near Olyphant to Green Ridge, adjoining Scranton. Strangely enough, a third rail was laid on this line too, only this one was at a 4'-3" gauge to accommodate gravity equipment. On many stretches of the lines below Carbondale four rails were used: one common running rail, a 4'-3" gravity rail, a standard gauge rail and six-foot rail for Erie equipment."

Shaughnessy's use of "strangely enough" suggests, perhaps, that he was not aware of the fact that a third rail for the movement of Gravity cars to and from the Baltimore mines was installed in the Union Railroad line to Wilkes-Barre. Perhaps, as well, he was not aware of the Gravity passenger service that took place south of Providence?

The extension of Gravity-gauge tracks from Providence to Green Ridge to the Baltimore mines in Wilkes-Barre was a part of a concerted program of expansion of D&H mining operations undertaken by President George Talbot Olyphant (elected president on March 15, 1858; took office on March 31, 1858) and Vice President Thomas Dickson. In order to buy the desired coal lands, the D&H had to get a supplement to its charter to allow it to hold an additional 3,000 acres of coal lands.

The extraordinarily important relationship between the Union Coal Company and the D&H began in 1866. In the D&H centennial book, *Century of Progress*, pp. 196-197 and following, up to 200, we read the following about that relationship:

"The buying of additional coal properties, which in a measure antedated entrance into the general railroad field, began with relations with The Union Coal Company entered into in 1866. . . At first the business arrangements made with this coal company contemplated nothing more than transporting for it coal that it produced. A little later it was deemed advisable to aid The Union Coal Company in the project it then entertained of buying the property of the Baltimore Coal Company situated near Wilkes-Barre. To furnish the financial assistance that was thought necessary, the Managers, in 1867, loaned the Union company \$1,300,000, obtaining the funds by an issue of bonds, all of which were taken at par and chiefly by the stockholders. But by January, 1868, additional financing on the part of the Union company had become requisite and its absorption in some way by the Delaware and Hudson Company seemed highly probable. In fact, it seemed desirable to obtain control of both the Union company and the Baltimore company, since these properties embraced over three thousand acres of coal lands owned in fee, and eleven hundred acres held under favorable leases; four coal breakers with all appurtenances, capable of turning out more than two thousand tons of anthracite daily, and seventeen miles of railroad connecting with the company's line in the Lackawanna valley near Scranton. The properties of the two companies also included four locomotives, five hundred and fifty coal cars and one hundred canal boats. / Towards the close of 1867 an opportunity offered to purchase an important 'Coal Estate' at Plymouth, opposite Wilkes-Barre. President Olyphant and a committee of the Board visited this property, and on their unanimous recommendation it was purchased for \$1,575,000 bringing to the enterprise eight hundred and three additional acres of coal lands owned in fee; two hundred and twenty-five acres held on favorable leases; three mine openings; one breaker ready for operation and one in course of construction; store properties, houses, a grist mill, and mine houses; also a first-class railroad bridge over the Susquehanna, with two miles of railroad connecting the property with the main line of traffic 'on the east side of the river.' [emphasis added]. By the Spring of 1868, not only had the purchase of this coal property at Plymouth been completed and title taken in the name of the Northern Coal and Iron Company, but the desirability of obtaining control of the properties of the Union and Baltimore companies had assumed paramount importance. . . / .Besides this, the railroad advantages that offered were considerable. The Union Coal Company, after it had merged the Howard company in February, 1867, completed railroad construction northward from Union Junction, in the Lackawanna valley, to Green Ridge. The location of Union Junction may be stated as approximately one and one-half miles north of the present station at Hudson on the Pennsylvania division of the company's railroad [emphasis added], so that the railroad from Union Junction to Green Ridge covered a distance of nearly fifteen miles. As Hudson is only three and one-half miles north of Wilkes-Barre, it is apparent that a long step toward the latter had been taken. /.The arrangements by which the beneficial interest in the properties of The Union Coal Company and the Baltimore Coal Company were actually secured may now be summarized. The extensive coal properties of

the Baltimore Coal Company were conveyed to the Delaware and Hudson Canal Company by deed dated October 15, 1867 [emphasis added]. Because of the financial difficulties of the Union company, all its property, including coal lands and railroad, was sold under foreclosure and conveyed by deed dated February 20, 1868, to Andrew T. McClintock and Thomas Dickson, in trust. By deed dated April 2 they conveyed the property to the Baltimore Coal and Union Railroad Company, incorporated on April 8. [On page 199 of Century of Progress, there is a photograph of Edward A. Quintard, who was the first president of the Baltimore Coal and Union R. R Co.] Finally, by instrument dated December 1, 1868, the Baltimore Coal and Union Railroad Company leased its coal lands and railroad in the Lackawanna valley to the Delaware and Hudson Canal Company, conveying to the lessee the right to operate the railroad for the purpose of transporting coal, subject, however, to existing rights of the Lehigh Coal and Navigation Company to operate it for general purposes [emphasis added]. / To provide for the additional tonnage expected upon the completion of the railroad about to be constructed from Green Ridge to Union Junction [approximately one and one-half miles north of the present station at Hudson on the Pennsylvania division of the company's railroad] further alterations and improvements in the Gravity railroad were commenced in 1866." [emphasis added]

The Lehigh Coal and Navigation Company, with whom the Union Railroad Company initially entered into a lease arrangement on the rail line from Green Ridge to Union Junction, owned a canal that extended from Tunkhannock, PA to Baltimore, MD, passing through Wilkes-Barre. On the site in Wilkes-Barre to which that canal extended were located, in 1927, the D&H Wilkes-Barre yards. This we have learned from the biographical portrait of Thomas D. Winn that was published in the June 1, 1927 issue of *The Delaware and Hudson Company Bulletin*, pp. 163-64, 176, wherein we read:

"The canal basin was a part of the Lehigh Coal and Navigation Company's canal, extending from Tunkhannock, Pa., to Baltimore, Md., and the boats and coal pockets belonged to the Baltimore Coal Company, the extensive coal properties of which were conveyed by deed to The Delaware and Hudson Canal Company on October 16, 1867."

Three and a half years before October 16, 1867, Thomas D. Winn (born in Liverpool on December 25, 1854) began working for the Baltimore Coal Company, where his brother-in-law worked as barn boss and mule buyer. Thomas D. Winn, at the age of 9, came to America on the S. S. Great Western, then making its second trip across the Atlantic, arriving at Castle Garden in New York City after a voyage of 18 to 20 days. Winn's first job for the Baltimore Coal Company was "as a messenger boy at the Baltimore Coal Company's office that stood where the overhead bridge is located at the junction with the Lehigh Valley railroad. Later he became a muleteer on the trestle over the coal pockets that spanned the canal basin, hauling the little cars of coal as they came down from the Black Diamond and Baltimore No. 1 breakers and returning with them empty after their contents had been deposited in the storage pockets. There were two sections of pockets, one for lump coal and one for finer sizes, and between these were located the lime kilns. Tom Milligan was the boss."

The first standard-gauge engine for the Union Railroad was named "Mill Creek."

In the 1934 list of 60 old locomotives on the Pennsylvania Division, we read the following about the Mill Creek:

"She [No. 6 in the 1934 list of 60 old locomotives on the Pennsylvania Division] was built at the Grant Locomotive Works, and was brought to the National Crossing, near the Minooka station, over the tracks of the Delaware, Lackawanna & Western company, on April 2d, 1867. She was pushed down the line a short distance, and her boiler filled with water carried form the river in pails. The fire was started by D. C. Benscoter, who acted as fireman; the engineer's name was John Bloom. When the "E. A. Quintard" now No. 7 was purchased, May 1st, 1867, engineer Bloom and fireman Benscoter were transferred to her, and the Mill Creek was sent to Mill Creek to do switching and mine work."

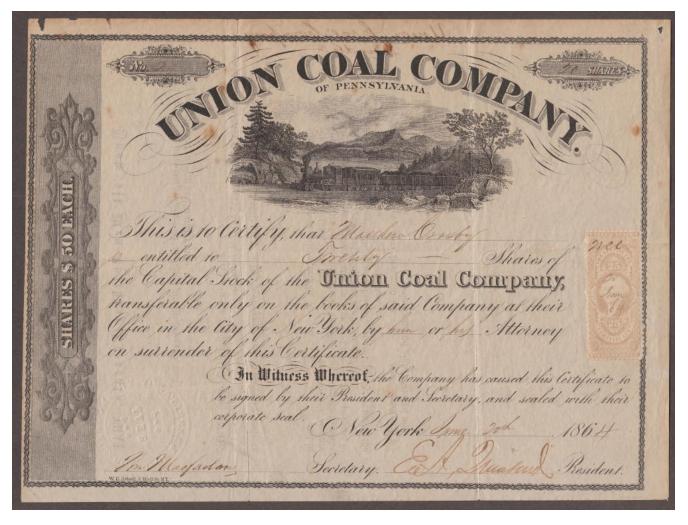
Edward A. Quintard was the first president of Baltimore Coal and Union R. R. Company. Here is the engraving of Quintard from page 199 of *A Century of Progress*:



In 1883, the D&H engine, the *E. A. Quintard*, was thoroughly repaired following an accident. This was the engine used for the celebrated "Saratoga Express."

"Engine No. 7, 'E. A. Quintard,' which was nearly smashed up a few months ago is again on the track after being thoroughly repaired. She has spent four months in the Dickson Works, and last Saturday was brought to Carbondale and placed in the round house, where under the skillful supervision of Mr. Eitel, the engine dispatcher, the necessary fixtures were added to her and on Thursday of this week, she took her old place on train 1 and 2 (Saratoga), with an entirely new steel boiler, cab. &c. Engine 7 now looks like a new engine and her appearance and general make-up are calculated to make glad the heart of her trusty engineer, Miles Biesecker." (Carbondale Leader, February 9, 1883, p. 2)

Shown below is a Union Coal Company stock certificate signed by E. A. Quintard. This certificate was donated to the Carbondale Historical Society by John V. Buberniak:



Here is an account of an excursion train that was run over this line between Wilkes-Barre and Green Ridge on May 18, 1867. Included in this account is some very interesting information about this line and about D&H engines used on this line. The engine that pulled this excursion train back to Wilkes-Barre from Green Ridge was the *E. A. Quintard*.

"On May 18th, 1867, an excursion train, of ten coaches, was run over this line between Wilkes-Barre and Green Ridge. The engine "Mauch Chunk" hauled the train from Wilkes-Barre to Green Ridge, and as the Green Ridge turntable had not then be completed, the "E. A. Quintard" was sent to Green Ridge, headed south to haul the train on the trip back to Wilkes-Barre. The "J. J. Albright" engine No. 8 was delivered in May, 1867. A six-wheel connected engine had been built by the Dickson Manufacturing company for the Ulster & Delaware Railroad, but was found to be unsuitable for that road. The "J. J. Albright" was sent to the Ulster & Delaware in exchange for the six-wheel engine, and the latter assumed the name and number of the No. 8. [In the biographical portrait of Seth V. Colvin that is presented on pages 259-260 of October 1, 1932 issue of The Delaware and Hudson Railroad Bulletin, we read the following: "One day he [Seth V. Colvin | met Amos Carl, Superintendent of the company's railroad, and immediately made application for a position on a train crew. Seth was told that he could begin braking on April 1, 1873, and on that date he became a member of the crew which operated locomotive J. J. Albright, No. 8, at Mill Creek. For seven years he was on this 'cabooseless' run, working from 12 to 20 hours a day for \$1.73. . . Flagging the rear end of a freight train today [1932] is like riding in a Pullman compared with conditions at Mill Creek (Hudson), Pa., in 1873, when SETH COLVIN began his 51-year career with the Delaware and Hudson. His crew had no caboose and the youngest man, instead of riding the head end as at present, had to find as comfortable a position as he could on the last car and, despite wind, rain, snow, or hail, protect and brake the rear end of the train. / Derailments were frequent, yet there was no wrecking crew to be called on: the train crew had to get the engine or cars back on the track as best they could. When a car went 'off the iron' they collected pieces of rail, ties, boards, stones, and any other available material which might be used in prying the wheels out of the cinders, raising them inch by inch on blocking, and eventually getting them back on the light iron rails, only too frequently to have them go on the ground again after traveling a short distance. . . "] The next engine procured was the Baltimore, No. 9, built in Baltimore, Md.; she is now in the scrap pile. Then the "R. Manville," No. 10, which was built by the Dickson Manufacturing company about 1865, for the use of the United States Government in prosecuting the war, was turned over to the Delaware & Hudson in 1868, for use on the Union Railroad. Her first engineer was named Isaac N. Street, and George Shiffer, was fireman, both of Scranton. (1890s Summary)

The first wreck on his line took place in May, 1867. Here is an account of that accident:

"The first wreck on this railroad occurred in May 1867. The construction gang was going north to Green Ridge; a heavy rainstorm was raging. When about 300 feet north of the Minooka station

they came upon a washout. The engine was stopped on what seemed to be a safe, but the water had undermined the roadbed; the engine rolled down the embankment, and rested on her side at the bottom. The engineer had reversed her and shut off steam, but a splinter of wood probably pushed the throttle lever open, for she took steam into her cylinders and thrashed for about five minutes, until the lever could be gotten at and closed." (1890s Summary)

Joseph Gallagher was the first employee to lose his life on this rail line:

The first employee to lose his life on the railroad was named Joseph Gallagher. The *James Dickson*, engine No. 11, engineer D. C. Benscoter, was working at the Baltimore mines. Gallagher and another brakeman were seated on the bumper beam in front of the engine. Gallagher's companion was to throw the first switch and Gallagher the second. When the latter stepped off the engine, he slipped and fell, the pilot rolled him over and crushed his life out. (1890s Summary)

The interest of the D&H in this rail line was made very clear in June 1867, when the annual excursion trip of the directors and officers of the D&H included a ride over the Union Railroad from Wilkes-Barre to Green Ridge before the line was officially opened:

"Del. & Hud. Canal Excursion. / The annual excursion trip of the directors and officers of the Del. & Hud. Canal Co. came off on the first week in June. This party this year consisted of / Thos. Dickson, Acting Pres., wife and daughter, Scranton; E. W. Weston, Supt. Coal Dept., and wife, Scranton; R. Manville, Supt. R. R. Dept., and wife, Carbondale; C. F. Young, Supt. Canal Dept., and wife, Honesdale; Hon. T. P. Howell, wife and daughter, New York; P. H. Balentine, wife and daughter, New York; C. A. Sprague, wife and sister, New York; W. J. Schenck and wife of the Merchant's Hotel New York; P. J. Dubois, wife and daughter, Kingston; L. C. Fuller, Scranton; Rev. Dr. Terhune, R. D. Church, Newark; W. C. Rose, Div. Supt. and daughters, Port Jervis. / They left Rondout in the Packet *Dyberry*, on the Fourth of June, and arrived in Honesdale on the Seventh--making the trip over the canal in three days. The party reached here [Carbondale] on Sunday en route to Scranton. / At Scranton the party took the Lackawanna & Bloomsburg Railroad to Wilkes Barre, and returned to Scranton upon the *Union Railroad*. That Railroad connecting the Lehigh and Susquehanna Railroad at Wilkes Barre with the Del. & Hud. Railroad near Providence is expected to be formally opened on Tuesday next, 18th inst." (*Carbondale Advance*, Saturday, June 15, 1867, p. 3)

On June 18, 1867, the Union Railroad, connecting the Lehigh and Susquehanna at Wilkes-Barre and the D&H at Green Ridge, officially opened:

"IMPORTANT RAILROAD OPENING. Our town [Carbondale] was visited on Wednesday by the Officers of the Lehigh and Susquehanna Railroad Co., the Union Railroad Co., the Del. & Hud. Canal Co., and other excursionists, making a formal opening of the Union Railroad Co., which forms the connecting link between the Lehigh and Susquehanna at Wilkes Barre and the

Del. & Hud. at Providence. / This opening is an important one to our valley in general, facilitating communication and transportation, and will be very beneficially felt in many localities. It gives Wilkes Barre direct communication out, both ways, on its own side of the river, with choice of routes to New York and Philadelphia. It also gives us in addition to what we had before another direct line of communication with the seaboard cities. Scranton now has the advantage of Wilkes Barre, only in a direct line North." (*Carbondale Advance*, Saturday, June 22, 1867, p. 3)

The Northern Coal and Iron Company played a very useful role for the D&H, beginning in 1871:

"On August 4 [1871] the Baltimore Coal and Union Railroad Company was merged with the Northern Coal and Iron Company which, in 1864, had been incorporated chiefly for the purpose of holding, in the interest of the Delaware and Hudson Canal Company, coal in excess of the acreage allowed by the charter of the latter. As the Northern Coal and Iron Company acquired, in 1866, certain additional railroad rights and franchises, this merger not only served the business convenience of substituting one company for two, but furnished the Delaware and Hudson company with a convenient subsidiary that could be utilized as a coal-land holding company and for railroad purposes." *COP*, p. 209

In 1872-73, the D&H purchased, in the name of the Northern Coal and Iron Company, the Gibson property in Carbondale:

"Authorized on February 6, 1872, there was purchased for \$425,000 in the name of the Northern Coal and Iron Company, on January 17, 1873, nearly two thousand acres, located near Carbondale and known as the Gibson property." *COP*, p. 209:

And then in December 1873, the D&H and the Northern Coal and Iron Company entered into an even closer relationship when the D&H leased the property and franchises of the Northern Coal and Iron Company in perpetuity:

". . . on December 1, 1873, the relation between the Northern Coal and Iron Company and the Delaware and Hudson company was made closer and more permanent by a lease of the property and franchises of the former to the latter in perpetuity." (COP, p. 210)

In 1886, the D&H assumed full control of the Union Railroad from Green Ridge to Mill Creek. This opened a major window in the passenger service industry for the D&H:

"In November, 1886, the twenty-year lease for the right to run passenger and merchandise traffic over the Union railroad, by the C. R. R. of N. J. expired, and the Delaware & Hudson assumed entire control of the line through to Wilkes-Barre, the extension from Mill Creek to Wilkes-Barre having been built in this year in anticipation of the expiration of the aforesaid agreement. More passenger trains were called into service, and promotions for the trainmen were a natural sequence. (1890s Summary)

About the new (summer of 1886) three-mile extension of D&H rails from Mill Creek to the "fine new" Lehigh Valley depot in Wilkes-Barre and uniting at South Wilkes-Barre with the D & H road, by its Susquehanna bridge, to the Plymouth mines on the west side of the river, we read the following in the July 15, 1886 issue of *The Journal:*

"The New D. & H. Connection. / The Delaware and Hudson Canal Co., opened a short line route for its Plymouth coal recently. The new track is from Mill Creek to Wilkes-Barre, about three miles, connecting with the Lehigh Valley road near its fine new depot, and uniting at South Wilkes-Barre with the D & H road by its Susquehanna bridge, to the Plymouth mines on the west side of the river. This saves several miles and terminates traffic by the Lehigh and Susquehanna and Nanticoke branches of the Reading Co. The new road will have double tracks, and sometime soon we shall no doubt be able to take the comfortable D. & H. cars, which now come south only to Scranton, and ride from Wilkes-Barre to Montreal without transfer. The company has almost inexhaustible coal property in the finest part of the Wyoming field and will supply the best markets; East, North and West, where winters are long and severe, and when transporters and consumers fully realize the great changes wrought in the trade by the rapid increase of railway traffic, and recover from the alarm at loss of a heavy fall demand, confidence will return and with it prosperity." (The Journal, July 15, 1886, p. 3)

A follow-up article about the recently completed branch of the D&H Pennsylvania Division to South Wilkes-Barre was published in *The Journal* a week later:

"The D. & H. Canal Company has recently completed a branch of its Pennsylvania division from Mill Creek to South Wilkes-Barre. The branch is about three miles long and gives the company a new connection with the Lehigh Valley road and also gives it a shorter outlet from the Plymouth mines." (*The Journal*, July 22, 1886, p. 3)

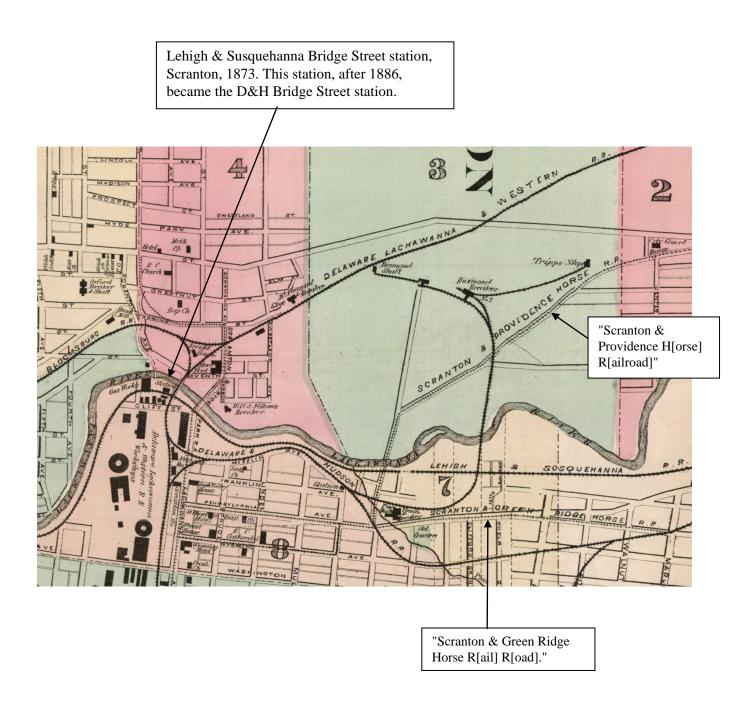
D&H passenger service daily was immediately offered between Carbondale and Wilkes-Barre (at the Lehigh Valley depot):

"The Del. & Hud. Canal Co.'s Railroads. / Next Saturday night at twelve o'clock the twenty years lease of the Union Railroad from Scranton to Mill Creek to the Lehigh Coal & Navigation

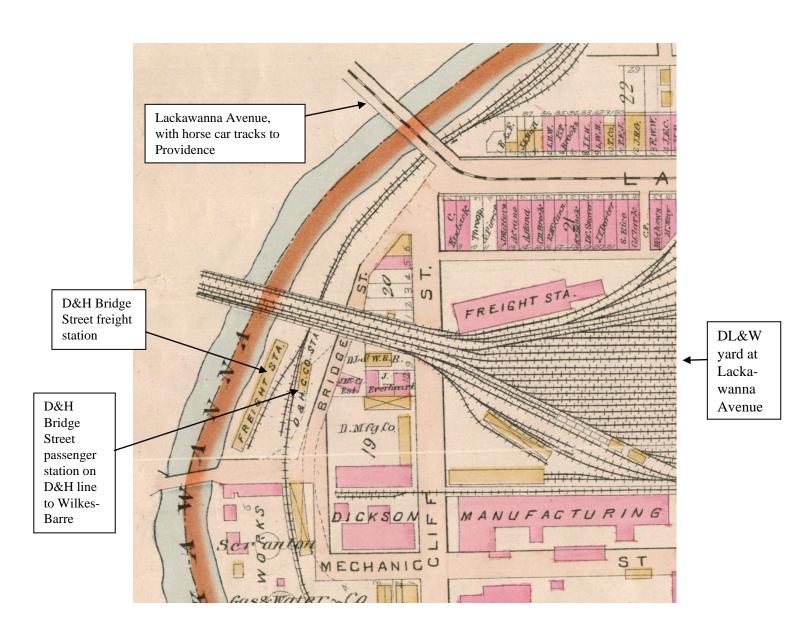
Co. expires, and the control of the road returns to its owners, the Delaware & Hudson Canal Co. During the twenty years the management of the passenger and freight business of this short but important railroad link has been operated successively by three different companies—the Lehigh & Susquehanna, the Central of New Jersey, and the Philadelphia & Reading. These twenty years have seen a wonderful growth in business enterprise and population in the two valleys which the Union Railroad connects, and what seemed then of minor importance, is now a most valuable branch of the Delaware & Hudson railroad system. The Delaware & Hudson Canal Co. will take possession of the Union Railroad, as before stated, next Sunday. They have built an extension from Mill Creek to the Lehigh Valley depot in Wilkes-Barre, and on Monday next will run their passenger and freight business from the city to Wilkes-Barre [emphasis added]. A rearrangement of the passenger trains on the new time table goes into effect next week, as follows: / On and after Monday next, passenger trains will leave Carbondale for Scranton at 7.00, 8.20, 9.50, and 11.20 a. m., 1.00, 3.46, 5.20. and 8.15 p.m. / Trains will leave Scranton for Carbondale at 7.00, 8.40, 10.00, and 11.47 a.m., 2.10, 5.00, 7.00, and 11.05 p.m. / All trains except the 7.00 a.m. from Scranton and the 9.50 a.m. from this city will run through coaches between Wilkes-Barre and Carbondale. / The train leaving here at 8.15 p.m. will run through to Wilkes-Barre, reaching that city at 9.50 p.m. Returning, it will leave Wilkes-Barre at 10.15 p.m., and reaching this city at 11.50 p.m. Passengers to and from Scranton on this train will go to the Bridge street depot, the old L. & S. station, near the Stone bridge Scranton [emphasis added]. / The principal changes from the present time of trains between here and Scranton are that the first train from Scranton in the morning will leave that city at 7.00 and reach this city at 7.50, and that the last train at night from Carbondale will leave at 8.15. The Saratoga Express will leave this city at 11.00 a.m. instead of 6.03; the present 11.25 a.m. train will leave five minutes earlier--11.20; the old evening train from Scranton will leave that city at 7.00 instead of 7.10 p.m. / The Company will run seven trains each way between Scranton and Wilkes-Barre" [emphasis added]. (The Journal, November 4, 1886, p. 3)

With regular D&H passenger service between Carbondale and Wilkes-Barre now a reality, the former L&S station near the Stone bridge in Scranton became the D&H Bridge Street depot. The exact location of the Bridge Street depot is shown on the maps on the following two pages.

The L&S Bridge Street station (which after 1886 became the D&H Bridge Street station) is shown on the detail given below from *Atlas of Luzerne County Pennsylvania From Actual Surveys by and under the Direction of D. G. Beers*, Published by A. Pomeroy & Co., Philadelphia, 1873. This map is in the collection of the Lackawanna Historical Society.



The D&H Bridge Street passenger station and the Bridge Street freight station are shown on the detail given below from *Atlas of the City of Scranton and Borough of Dunmore, Pennsylvania.* L. J. Richards & Co, Philadelphia, 1888. Map in the collection of the Lackawanna Historical Society.



With the D&H now in control of the former Union Railroad from Scranton to Wilkes-Barre, many personnel changes were made:

"The D. & H. Trains. / The connecting of the Union Railroad from Scranton to Wilkes-Barre to the Del. & Hud. Railroad system, has necessitated many changes in the crews of the passenger trains. / David R. Nicol, for several years the popular conductor of the Saratoga Express, now runs from Green Ridge to Wilkes-Barre, with Eugene Hayden, engineer, and Eugene Wonnacott, baggage master. Headquarters at Green Ridge. / William Histed, of this city, has charge of another passenger train between Green Ridge and Wilkes-Barre, with headquarters at Wilkes-Barre. Jacob Butzman, engineer. / Lewis Cook, the former baggage master of the Saratoga Express, is now conductor of the same. S. S. Cobb is still engineer, and James A. Nicol has been appointed baggage master. The headquarters of this train [the Saratoga Express] is at Green Ridge, as heretofore. As the Saratoga Express does not leave Scranton until 10.00 o'clock a.m., this crew will also run the 7.00 a.m. train from Scranton to this city, and the 8.20 a.m. train from here to Scranton. / Wm. Rosser, conductor, with D. C. Benscoter, engineer, runs the trains from this city to Scranton at 7.00, and 9.50 a.m. and 1.00 p.m. / E. Skeels, conductor, with Alex. Copeland, engineer, and Horace Daley, baggage master, run the trains from here to Scranton leaving at 11.20 a.m., 5.20 and 8.15 p.m. The last train is run through to Wilkes-Barre by this crew, returning at 11.50 p.m." (The Journal, November 11, 1886, p. 3)]

David R. Robbins operated the first D&H freight train into Wilkes-Barre on the line of the former Union Railroad. This we know from the biographical portrait of David B. Robbins that is published in the October 1, 1931 issue of *The Delaware and Hudson Company Bulletin* (pp. 291-292):

"From 1866 to 1886 the Jersey Central had leased the Delaware and Hudson's right of way between Wilkes-Barre and Scranton; Mr. Robbins operated the first freight train into Wilkes-Barre after the road reverted to Delaware and Hudson management."

With the D&H in charge of the line between Green Ridge and Wilkes-Barre, the offices of the National Express Company were then moved to the Second National Bank Building in Scranton:

"Express Change. / Yesterday [November 10, 1886] the Delaware and Hudson Railroad took possession and in the future will operate, its own road between Green Ridge and Wilkes-Barre. The National Express Company has been doing business over the Delaware and Hudson Northern lines for years, and over their local road between Honesdale and Scranton since last May, and today it operates on the line between Green Ridge and Wilkes-Barre, and has established offices at all stations. The National Express Company in this city [Scranton] has removed its office from the United States Express office to that of the Adams Express Company's office in the Second National Bank building, and after this date the business of the two companies—the Adams and the National—will be conducted at the latter place, being a joint

office for both companies.--Scranton Republican of Monday." (The Journal, November 11, 1886, p. 3)

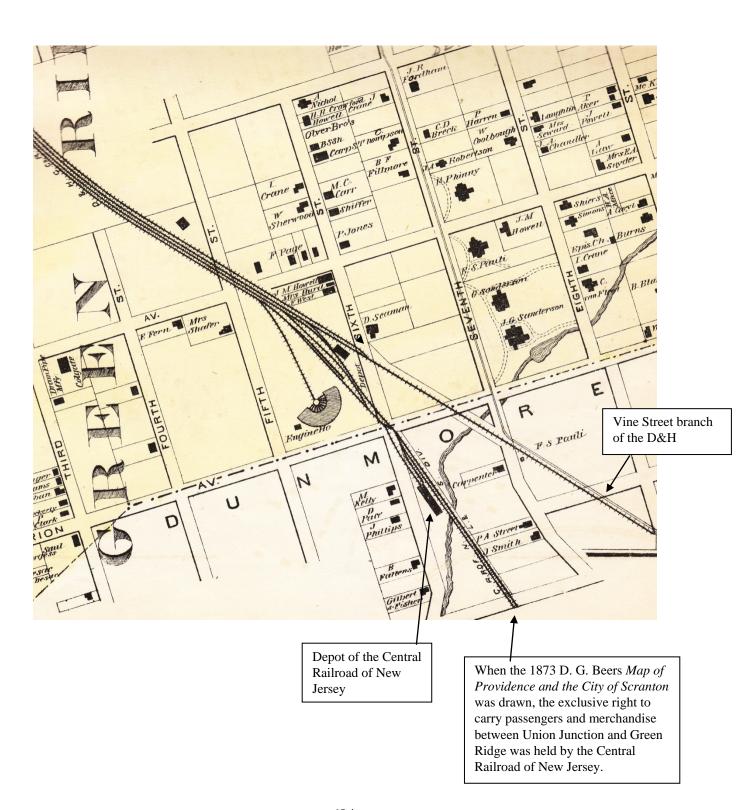
In 1890, James A. Farrell was put in charge of maintaining the new interlocking plant at Hudson, where, in the twentieth century, the Wilkes-Barre Connecting Railroad would intersect the D&H main line:

"In 1888, the company began the erection of gates at grade crossings between Plymouth and Carbondale for the protection of pedestrians and vehicles. Mr. [James A.] Farrell and Charles Johnson were chosen to erect and maintain them. Two years later Mr. Farrell's work was extended to include the maintenance of the new interlocking plant at Hudson, Pa., where the Wilkes-Barre Connecting Railroad intersects the main line. Because of the two different track gauges, standard and narrow, stub end switches were installed. Under this arrangement the ends of the three rails on the main track were moved to engage the three rails of either the continuation of the main line or the turn-out onto the Connecting Railroad's right of way. Later Mr. Farrell had charge of a similar plant erected at Carbon Street Junction, Carbondale. /With the gradual expansion of business it became necessary for someone else to take over a part of Mr. Farrell's duties; consequently, in 1907, he was made maintainer of interlocking plants and signals south of Carbondale. He had six towers to maintain: three at Carbondale, Carbon Street, 'OU' Tower, at the junction of the Honesdale Branch and the main tracks, the Delaware and Hudson-Lackawanna diamond crossing, and plants at Moosic, Hudson, and Mineral Springs, Penna. / In 1921, after 53 years with our company, Mr. Farrell was placed in charge of the Signal Department storehouse at Dundaff Street, Carbondale. He continued at this work until he was pensioned on July 1, 1931, after 63 years and 7 months in Delaware and Hudson employ." (Biographical portrait of James A. Farrell, "The Engineer Hired a Horse," pp. 195-196, 203, July 1, 1932 issue of *The Delaware and Hudson Company Bulletin*)

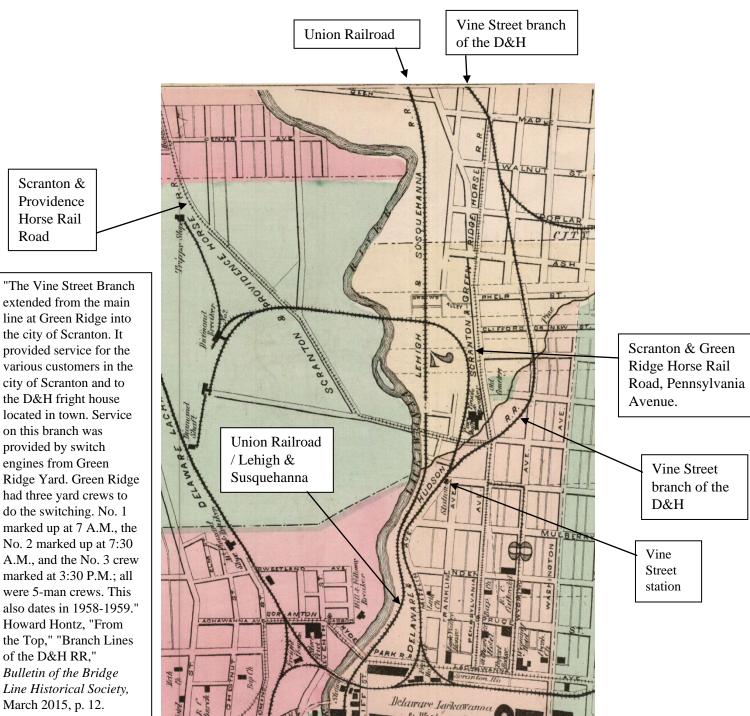
The path of the Union Railroad from Green Ridge to just south of Lackawanna Avenue in Scranton is shown on the maps on the following four pages.

When the Union Railroad from Green Ridge to Union Junction opened on June 18, 1867, the Union Coal Company leased the exclusive right to carry passengers and merchandise between Union Junction and Green Ridge, for a period of twenty years, 1867-1887, to the Lehigh Coal & Navigation company, the latter company afterward transferred this lease to the Central Railroad of New Jersey.

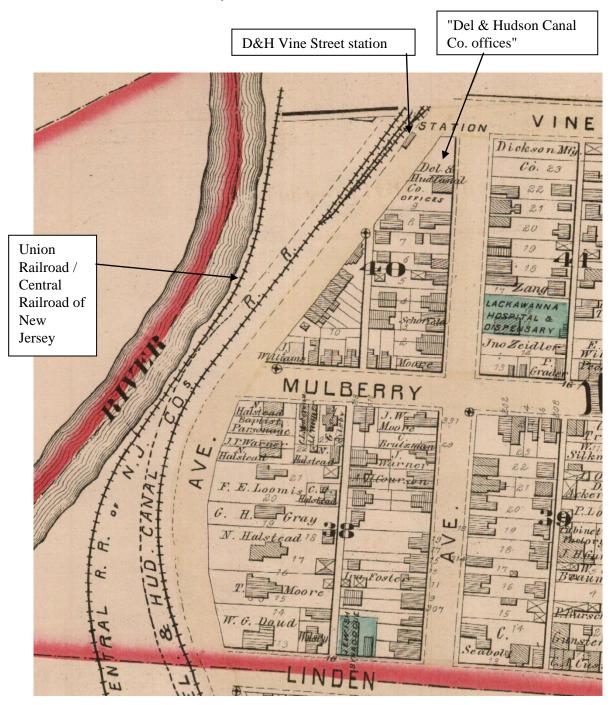
The Union Railroad is the western branch of the Y at Green Ridge, as shown on the detail given on the following page of the Green Ridge area from the 1873 D. G. Beers *Maps of Providence and the City of Scranton*. At the time that this 1873 D. G. Beers map was drawn, the exclusive right to carry passengers and merchandise between Union Junction and Green Ridge was held by the Central Railroad of New Jersey.



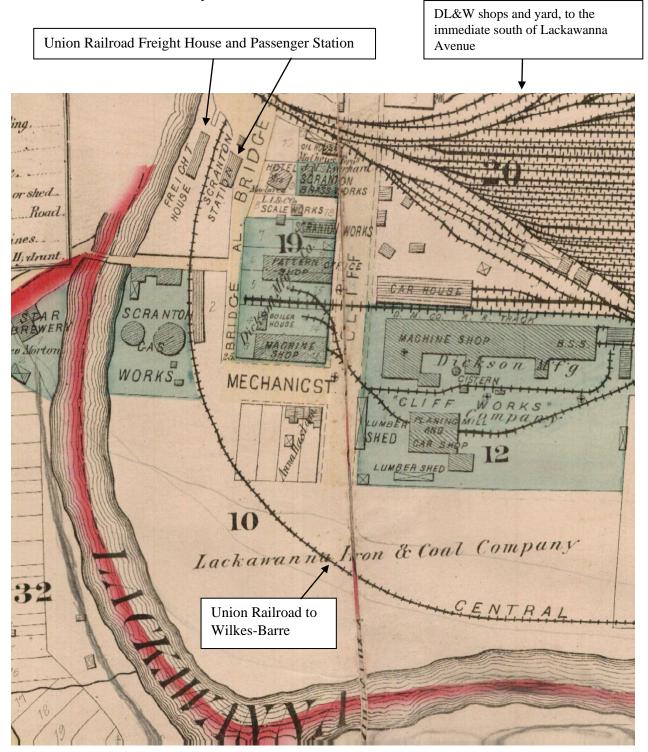
Here is the whole line of the Union Railroad from Green Ridge to below Lackawanna Avenue. This map detail is from *Atlas of Luzerne County Pennsylvania From Actual Surveys by and under the direction of D. G. Beers.* Published by A. Pomeroy &Co, Philadelphia, 1873. Map in the collection of the Lackawanna Historical Society.



Detail from *City Atlas of the City of Scranton Pennsylvania*. G. M. Hopkins, Philadelphia, 1877, showing the route of the Union Railroad through downtown Scranton. Map in the collection of the Lackawanna Historical Society.



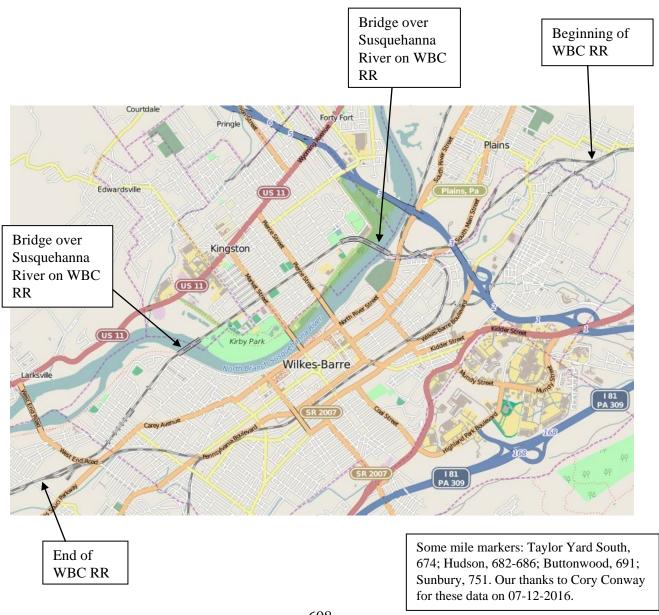
Detail from *City Atlas of the City of Scranton Pennsylvania*. G. M. Hopkins, Philadelphia, 1877, showing the Union Railroad south of Lackawanna Avenue. Map in the collection of the Lackawanna Historical Society.



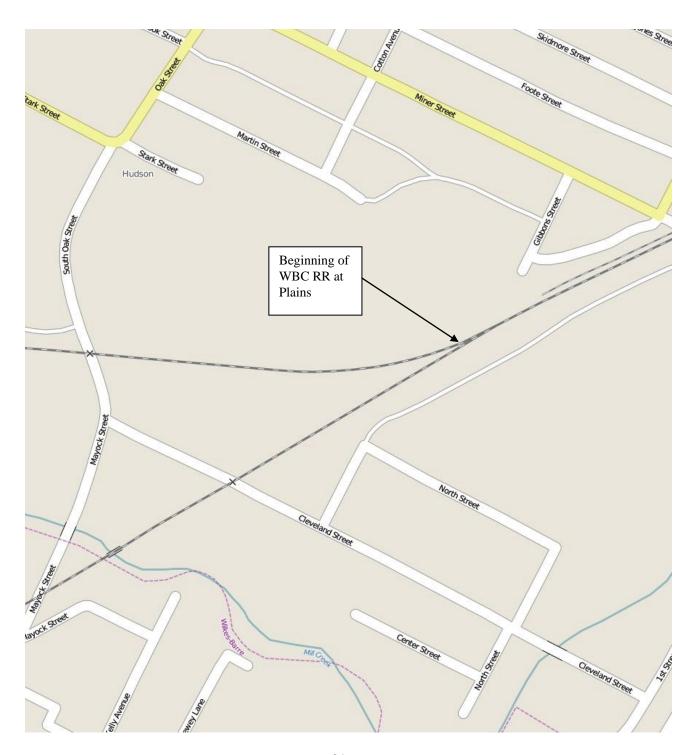
Such were the D&H rails at the southern end of the Pennsylvania Division, until 1913 when the Wilkes-Barre Connecting Railroad was constructed to make it possible for the D&H to exchange cars with the Pennsylvania Railroad at Buttonwood.

Wilkes-Barre Connecting Railroad

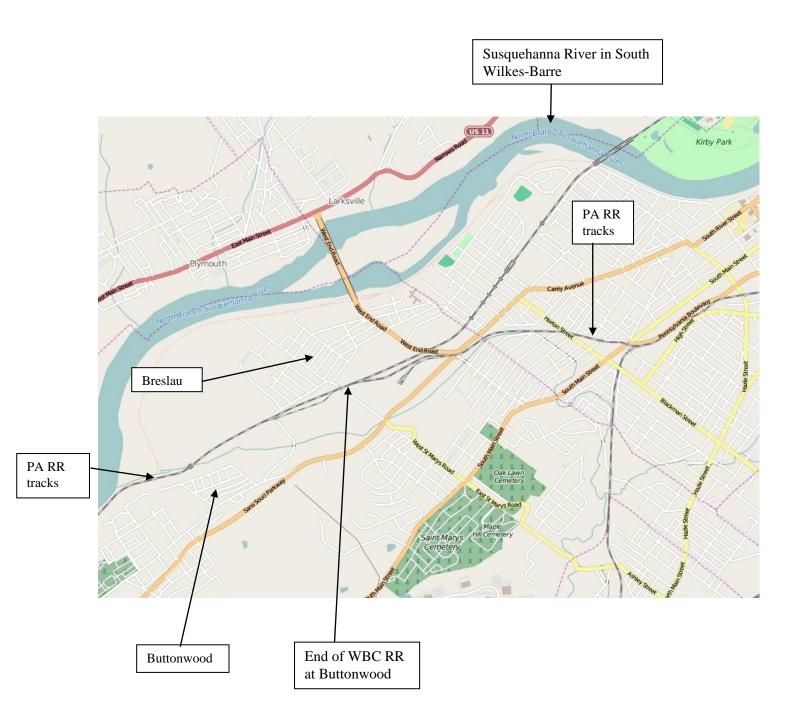
In 1912, the D&H and the Pennsylvania Railroad incorporated the Wilkes-Barre Connecting Railroad, creating an interchange between the two lines at Hanover Township, PA, thus avoiding going through downtown Wilkes-Barre. Opened in 1915, it runs north 6.65 miles to the D&H mainline at Hudson crossing the Susquehanna River twice.



Beginning of Wilkes-Barre Connecting Railroad at Plains:



End of Wilkes-Barre Connecting Railroad at Buttonwood:



Research on the history of the Wilkes-Barre Connecting Railroad led me to the following webpage:

http://rnetzlof.pennsyrr.com/corphist/wbconn.html

At that website, the corporate history of the WCR is presented, as follows:

Beginning of downloaded material from http://rnetzlof.pennsyrr.com/corphist/wbconn.html:

"Corporate Genealogy Wilkes-Barre Connecting Railroad

[All of this material was taken from the report on the Delaware and Hudson Company. Hence, "the carrier" refers to that company, not the Wilkes-Barre Connecting Railroad.]

INTRODUCTORY

The railroad of Wilkes-Barre Connecting Railroad Company, hereinafter called the Wilkes-Barre Connecting, is a double-track standard-gauge steam railroad, operated for freight service only, located in the northeastern part of Pennsylvania. The carrier owns 4.187 miles of double-track road in two parts and has trackage rights over 2.454 miles of road, making a continuous line from Buttonwood to Hudson, Pa., a distance of 6.641 miles. The carrier's line thus provides a direct connection between the tracks of the Pennsylvania Railroad Company and the Northern Coal and Iron Company. The latter is entirely controlled by the carrier. The owned mileage of the Wilkes-Barre Connecting consists of 4.187 miles of first track, 4.187 miles of second main track, and 0.149 mile of yard and side tracks, making a total of 8.523 miles of all tracks owned.

CORPORATE HISTORY

The Wilkes-Barre Connecting was incorporated under the general laws of Pennsylvania and by letters patent of the Governor of Pennsylvania dated November 18, 1912. Its organization was perfected on December 3, 1912. It has been maintained and operated since completion for the joint benefit of the Pennsylvania Railroad Company and the carrier. Operations are conducted by the Pennsylvania Railroad Company, and the carrier as agents without benefit or loss to the Wilkes-Barre Connecting, as fully explained under the caption Leased Railway Property.

DEVELOPMENT OF FIXED PHYSICAL PROPERTY

Construction was begun in 1913. Trackage rights were obtained over a portion of the railroad of the Northern Coal and Iron Company between Buttonwood and the Plymouth branch of the carrier, about 1.6 miles, and over a portion of the line of the Wilkes-Barre and Eastern Railroad Company from a point 0.75 mile west of Plains Station to a point 0.25 mile west of a bridge over the Susquehanna River, about 0.854 mile. The total of 2.454 miles of road thus obtained was rehabilitated and improved to meet the general plan of construction adopted by the Wilkes-Barre Connecting, which charged the recorded expenditures as improvements on leased railway property. The entire line of 6.641 miles was completed and placed in operation on March 29, 1915.

INVESTMENTS IN OTHER COMPANIES

The balance sheet of the Wilkes-Barre Connecting at June 30, 1916, records investments in affiliated companies amounting to \$131,384.92, which were found to cover cash advances to the Kingston Realty Company, a corporation controlled jointly by the carrier and the Pennsylvania Railroad Company through ownership of the entire capital stock outstanding.

LEASED RAILWAY PROPERTY

On date of valuation, the Wilkes-Barre Connecting controlled through leases the following property of others:

The Wilkes-Barre Connecting acquired trackage rights over a portion of the Buttonwood branch of the Northern Coal and Iron Company, consisting of 1.6 miles of main track, together with 2.26 miles of yard tracks and sidings and 0.27 mile of spur track, from March 29, 1915, for the period of the corporate existence [sic] of the Wilkes-Barre Connecting, under an agreement dated December 29, 1914. By the terms of this agreement the Wilkes-Barre Connecting is required to pay 4.5 per cent per annum on a valuation of \$520,770.20 for the railroad and facilities. The rent accrued payable for the year ended on date of valuation amounted to \$29,488.58. Under the terms of this agreement the Wilkes-Barre Connecting is required to pay all taxes and assessments, insurance, and all operating and other expenses necessary for the upkeep of the railroad; also to provide for all addition-and-betterment work, which expenditures shall be repaid by the carrier in case the agreement is terminated. The leased property is maintained and operated by the carrier acting as agent for the Wilkes-Barre Connecting.

The Wilkes-Barre Connecting also acquired trackage rights over the tracks of the Wilkes-Barre and Eastern Railroad Company from a point about 0.75 mile west of Plains Station to a point

about 0.25 mile west of the bridge over the Susquehanna River, being about 0.854 mile in length, for 99 years, with option to renew, under agreement dated December 30, 1913. The annual rental is stated to be 5 per cent per annum on the original cost of this section. The rent recorded as payable for the year ended on date of valuation was \$9,650.61.

On date of valuation the companies mentioned below had rights to use all or portions of the property of the Wilkes-Barre Connecting:

The Pennsylvania Railroad Company and the carrier each have equal right to use the tracks and the other property owned or used by the Wilkes-Barre Connecting under the agreement dated December 29, 1914, which agreement provides for the construction, operation, and maintenance of the railroad of the Wilkes-Barre Connecting. Such property is to be maintained and operated by either the Pennsylvania Railroad Company or the carrier, as agent for the Wilkes-Barre Connecting. The rental payable in equal proportions by the Pennsylvania Railroad Company and the carrier for the trackage and other rights and privileges covers the interest upon any bonds and other outstanding obligations of the Wilkes-Barre Connecting, and any sums payable by the carrier as rental for trackage rights and any sums required to pay dividends, at the rate of 5 per cent per annum upon the outstanding capital stock of the Wilkes-Barre Connecting, less any sums receivable from others using any portion of the Wilkes-Barre Connecting's property. All expenses of maintenance and operation, less portions collected from others, are to be assumed by the Pennsylvania Railroad Company and the carrier in equal proportions. The joint-facility rent income recorded as receivable for the year ended on date of valuation, from the Pennsylvania Railroad Company, the carrier, and the Wilkes-Barre and Eastern Railroad, amounted to \$39,139.19. This amount agrees with the rental recorded as payable to the Wilkes-Barre and Eastern Railroad and to the Northern Coal and Iron Company.

The Wilkes-Barre and Eastern Railroad participates with the Pennsylvania Railroad Company and the carrier in the use of the 2.41 miles of track between a point about 0.25 mile west of the upper bridge over the Schuylkill River [1] to a point of connection with the Plymouth branch of the carrier leased to the Wilkes-Barre Connecting. The rental payable by the Wilkes-Barre and Eastern Railroad is a wheelage proportion of rental it receives for the property it leases to the Wilkes-Barre Connecting. It also assumes a wheelage proportion of the maintenance and operating expenses connected with that particular property.

Note 1: I suspect the ICC confused two hard-to-spell Pennsylvania rivers and meant to write "Susquehanna" here. Original data source: Decisions of the Interstate Commerce Commission Vol. 116 pp 819-820, 825, 828-829, Interstate Commerce Commission, Washington, DC, November 1926

Although published in 1926, the report is "...as of date of valuation" which was 30 June, 1916 HTML by Robert T. Netzlof 8 July 2002

Typographical errors corrected 19 January 2008

Find an error? Have a comment? Tell Bob"

End of downloaded material from http://rnetzlof.pennsyrr.com/corphist/wbconn.html.

Great material. Thank you http://rnetzlof.pennsyrr.com/corphist/wbconn.html. One couldn't hope for more. In presenting that material here, we, too, like Robert T. Netzlof (who did the HTML for the above history of the WBCR) have corrected therein a few typographical errors: e.g., "Pennsylvania Railroad Company" not "Pennsyvana Railroad Company"; also a couple of minor format issues. The point is this: Get is down for the record. We're all in this together. Make that record the best that it can be.

In the course of our research on D&H steam lines beyond the Lackawanna Valley, we have learned data about the WBCR or about the operation of the line or about persons who worked on the Wilkes-Barre Connecting Railroad that will reconfirm or supplement what we have learned from http://rnetzlof.pennsyrr.com/corphist/wbconn.html.

In *Century of Progress*, the basic facts on the incorporation and opening of the line are reconfirmed: the WBCR was incorporated in 1912, the line was completed in 1915 (cost of construction, \$2,220,000), and placed in operation on March 29, 1915:

"The Wilkes-Barre Connecting railroad was placed in operation on March 29, 1915. Built by the Wilkes-Barre Connecting Railroad Company, incorporated in 1912, this construction was jointly undertaken by the Pennsylvania Railroad Company and The Delaware and Hudson Company for the purpose of connecting their roads. Prior to that time, such connection was afforded by trackage rights over intervening tracks of the Lehigh Valley railroad. The new road, completed in 1915, runs from Buttonwood northerly to Hudson, Pennsylvania, a distance of 6.65 miles, avoiding the business section of Wilkes-Barre and furnishing also a direct connection for the company with its railroads in the Plymouth district. The Wilkes-Barre Connecting railroad is operated by The Delaware and Hudson Company under an agreement with the Pennsylvania Railroad Company and the Northern Coal and Iron Company." (pp. 379-80)

Before the WBC was built, connection between the D&H and PRR was accomplished by trackage rights over the LVRR through the business section of Wilkes-Barre.

When the WBCR was constructed in 1913, the Roadmaster of the D&H line from Wilkes-Barre to Carbondale, including the Honesdale Branch of the D&H and the WBCR, was James D. Hevers. This we have learned from the biographical portrait of James D. Hevers that is presented in the April 1, 1929 issue of *The Delaware and Hudson Company Bulletin*, pp. 99-100. Therein we read:

"In 1900 James D. Hevers became the Track Supervisor of the Valley Road; on May 1, 1916, he was made Roadmaster of the line from Wilkes-Barre to Carbondale, including the Honesdale Branch and the Wilkes-Barre Connecting Railroad. During James Hevers' years in charge of the track on the Pennsylvania Division, many important improvements were made. Notable among these changes was the work of converting the Honesdale Branch from narrow to standard gauge track in 1886 and 1887 [No, it was in 1899-1900]; building of the yard at Honesdale, 1895; laying of the tracks inside and leading to the new Carbondale roundhouse in 1910; increasing the number of main line tracks between Providence and Carbondale from two to four; and the construction of the Wilkes-Barre Connecting Railroad in 1913. This last was intended primarily to exchange cars with the Pennsylvania Railroad at Buttonwood." (Biographical portrait of James D. Hevers, pp. 99-100, of the April 1, 1929 issue of *The Delaware and Hudson Company Bulletin*)

In 1915, James J. Barrett was transferred to the D&H Maintenance of Way Department as crossing watchman at Miners Mills on the WBCR. He worked in that position until 1931 when he was pensioned. This we know from Barrett's biographical portrait, titled "Sixty-Three Years' Service," that is published in the May 15, 1932 issue of *The Delaware and Hudson Company Bulletin*, pp. 147-148, 158. Therein, we read:

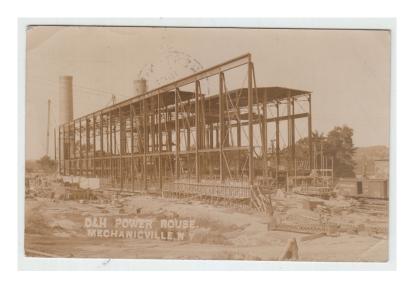
In 1915, James J. Barrett "was transferred to the Maintenance of Way Department of the [D&H] railroad as crossing watchman at Miners Mills on the Wilkes-Barre Connecting Railroad, the line which connects the main tracks at Hudson with mining operations at Plymouth, Penna., on the west shore of the Susquehanna River. He remained at that post until he was pensioned last year [1931]."

The facilities and operations of the D&H at the southern terminus of the company, Wilkes-Barre, are described in Part I of a two-part article on Wilkes-Barre, commonly known as the City of the Black Diamond, in the May 15, 1929 issue (pp. 149-151, 158) of *The Delaware and Hudson Company Bulletin*, as follows: "To form a picture of our facilities in the city [of Wilkes-Barre] let us begin with the railroad yard at that point. The Wilkes-Barre yard has five house tracks, holding an average of eighty-eight cars, ten classifying tracks, allowing for the classification of

one hundred and thirty cars, three receiving tracks from the Central Railroad of New Jersey to hold about one hundred cars, and two delivery tracks holding about forty-five cars; in addition there are ash, coal, caboose, coach, and storage tracks at Jackson Street. To provide for storage and minor repair work to locomotives, there is a roundhouse equipped with a sixty-five foot turntable, and having nine stalls. / Our interchange with other lines at Wilkes-Barre has grown so rapidly during recent years that five or more engines are used daily to handle interchange freight. During the first six months of 1928 the average number of cars interchanged each month was over 32,000. This figure includes cars going to and coming from the Pennsylvania, Central Railroad of New Jersey, and the Lehigh Valley. / Prior to 1915 interchange to and from the Pennsylvania was made over the tracks of the Lehigh Valley Railroad and through the business section of the city. As the volume of traffic increased it became a serious problem to effect this interchange due to lack of facilities and yard room, the number of busy highway crossings, and the many Lehigh Valley trains passing over the tracks. To avoid this condition, the Wilkes-Barre Connecting Railway was constructed and placed in operation in 1915. This line is owned jointly by The Delaware and Hudson Company and the Pennsylvania Railroad, and is operated and maintained by our Company. It consists of a double track system seven miles long, extending from Hudson, Pa., on our line to Buttonwood, Pa., on the Pennsylvania, running around the congested northern section of the city. There are, on an average, over 12,000 cars passing over this line in each direction every month of the year. / Upon arriving at Hudson from the Pennsylvania, the cars are inspected, repaired if necessary, and classified for movement north or south. Twice daily merchandise cars require handling at Wilkes-Barre transfer are moved to that point. Northbound cars are classified and picked up by manifest freights for movement to points north on the Susquehanna Division, to the Capital District, and to connecting lines. The Plymouth Branch, which extends from South Wilkes-Barre to Plymouth, affords a connection for the interchange of coal and other freight between The Delaware and Hudson and Central Railroad of New Jersey, and the Pennsylvania Railroad on the other side of the Susquehanna River. / To meet the demand for fast freight service, there are two manifest freights, WR-1 and WR-3, moving north from Wilkes-Barre daily to Rouses Point, the northernmost point served by the Delaware and Hudson Company, leaving Wilkes-Barre at 11:30 A.M. and 11:30 P.M. In addition, fast freight trains for the Boston and Maine at Mechanicville* leave at 1:30 A.M., 7:30 A.M., 1:30 P.M., and 7:30 P.M. and are known as WM-1, WM-3, WM-5, and WM-7. To insure prompt handling of freight for the Boston and Albany connection and the Capital District, Trains WO-1 and WO-3 depart from Wilkes-Barre at 1:30 P.M. and 7:30 P.M. for Oneonta whence they are 'main-lined' through to Albany after picking up cars at that point. In making these schedules the movement of manifest freights on connecting lines at both receiving and delivering terminals were taken into consideration with a result that cars are frequently delivered to connections within thirty minutes after their arrival in the yard." (p. 149)

*"At the turn of the century, Mechanicville had become a major junction point and gateway for rail traffic to and from New England. Both the D&H and B&M built extensive yards and servicing facilities. Together they created a sprawling complex. After the B&M and then the D&H were sold to Guilford Transportation Industries, Guilford didn't need the Mechanicville yard and the tracks were removed." Doug Barron, "Riding Amtrak's *Autumn Express*," *BLHS Bulletin*, January 2016, p. 8

Here is a post card view of the "D&H Power House, Mechanicville, NY" (under construction). Post card purchased by the author at Hawley, PA, on April 16, 2016 (Tom Kennedy Local History festival):





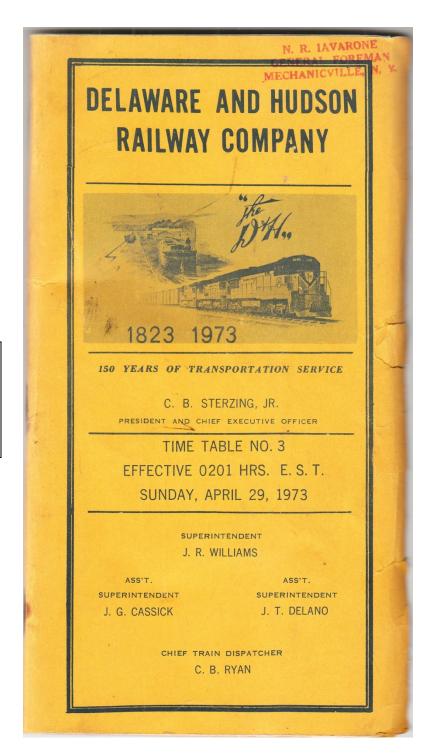
Presented in D&H time table No. 33, effective Sunday, September 25, 1938 at 12:01 A.M., for the government of employes, are interesting data about the operation of the Wilkes-Barre Connecting Railroad. In that time table, we read:

"Wilkes-Barre Connecting R. R. / The Wilkes-Barre Connecting Railroad extending between Hudson and Buttonwood will be operated as double track governed by the Delaware and Hudson Book of Rules dated November 1st, 1914. / Extra trains moving over the Wilkes-Barre Connecting Railroad will not be required to have clearance cards, but will throw off Register Slip at Buttonwood telegraph office and Hudson Yard Office, showing the engine number, initials and names of Enginemen and Conductors, time reported for duty, number of loads and empties and tonnage of trains. / The display of white flags and white lights as prescribed by Rule 21 will be omitted on all extra trains of the P. R. R. except passenger extras. / Trains of the P. R. R. may use yellow lights in markers as prescribed by Rule 19-19A and 19 B of the Book of Rules, instead of green lights. / P. R. R. engines, heavier than Class H-9S; must not be run double headed over Susquehanna River Gauntlet Bridge, without at least three cars between them. / P. R. R. engines heavier than the 'L' type, must not be run over Susquehanna River Gauntlet Bridge." (The Delaware and Hudson Railroad Corporation / Pennsylvania Division / Time Table No. 33 / Effective Sunday, Sept. 25th, 1938 at 12:01 A.M. . . For the Government of Employes Only, pp. 13-14)

In D&H Time Table No 33, effective Sunday, September 25, 1938, we also read interesting information about Fish Island Junction and the Susquehanna River Bridge Gauntlet on the Wilkes-Barre Connecting Railroad, as follows:

"Fish Island Junction, W. B. C. R. R. / Interlocking Station closed. Hand thrown switches are installed at the crossover switches and the lead to the north leg of the wye. Square end boards are retained. Trains finding a signal governing the approach to Fish Island in stop position will wait three minutes, then if signal does not indicate caution or proceed may proceed to the switches to ascertain if they are set for the straight-away route, take such action as may be necessary at the switches and may then proceed with caution to the next signal. / The normal position of the switch at the South End of the North Leg of Wye at Plymouth River Bridge will be for movement from Plymouth to Hudson. / Susquehanna River Bridge Gauntlet, Wilkes-Barre Connecting R. R. / The signal at the South end and the signal at the North end of Susquehanna River Bridge Gauntlet are operated electrically and will be controlled from the Switch-tenders Cabin, South end of Hudson yard. / Trains finding these signals in stop position and trains that are delayed, which foul the Gauntlet or track circuit, will report over telephone to Switch-tender at the South end of Hudson Yard and will be governed by instructions from the Switch-tender. / Telephones are located at the signals indicated above." (The Delaware and Hudson Railroad Corporation / Pennsylvania Division / Time Table No. 33 / Effective Sunday, Sept. 25th, 1938 at 12:01 A.M. . . For the Government of Employes Only, pp. 14-15)

The restrictions and instructions on the movement of trains on the Wilkes-Barre Connecting Railroad in 1973 are set forth in D&H Time Table No. 3, Effective 0201 hrs. E. S. T., Sunday, April 29, 1973, pp. 31-32, as follows:



This timetable is in the Alan G. Dustin collection of the Carbondale D&H Transportation Museum.

DELAWARE AND HUDSON RAILWAY COMPANY

SERVICE IS OUR BUSINESS

SAFETY IS OUR GOAL

Time Table No. 3 is the authority for the movement of trains of the First, Second, Third and Fourth Subdivision and supercedes Time Table No. 2.

J. R. WILLIAMS SUPERINTENDENT

H. W. Hontz writes a column for the *Bridge Line Historical Society Bulletin*. APPROVED

→ H. W. HONTZ GENERAL SUPERINTENDENT

WILKES - BARRE CONNECTING R.R.

General

The Wilkes-Barre Connecting Railroad between SX Hudson and Buttonwood will be operated in accordance with the Delaware and Hudson Railway Company "Rules of the Operating Department", Timetable and Special Instructions.

Trains or engines moving over the Wilkes-Barre Connecting Railroad will not be required to have Clearance Form "A", but will throw off Register Slip at Buttonwood and SX Hudson, showing the engine numbers, initials and names of Enginemen and Conductors, time reported for duty, number of loads and empties and tonnage of trains.

Speed Restriction

									M.P.H.
Between SH	Cabin	and north	end	of	Buttonwood	Yard			. 20
Bridge No.	4.35,	SC Cabin .							. 10

Movement of Trains or Engines

The movement of trains or engines between the southward signals at SX Hudson and the "End of Block" sign located 250 feet north of the switch at Carey Ave., are governed by signal indications.

The main tracks between SH Cabin and NC Cabin are numbered from west to east and will be used as follows:

No. 1 - Northward and southward trains. No. 2 - Northward and southward trains.

The main tracks between the southward interlocking signals at SC Cabin and the "End of Block" sign located 250 feet north of the switch at Carey Ave., are identified from west to east and will be used as follows:

Southward track - Southward trains. Northward track - Northward trains.

Interlocking switches and signals are controlled from SX Hudson.

The switches at NC Cabin and SC Cabin are equipped for dual control.

Buttonwood Yard

The running track between P.C. R.R. Bridge and the switch at the south end of main tracks, Wilkes-Barre Connecting Railroad, is designated as the thoroughfare track.

FIRST SUBDIVISION

WILKES - BARRE CONNECTING R.R.

Movements on the thoroughfare track are in charge of the Yardmaster, at Buttonwood, as follows:

Proceed indication on the northward home signal at P.C. R.R. Bridge will authorize northward movements from that point to Carey Avenue.

Trains or engines entering thoroughfare track at Buttonwood must obtain permission from Yardmaster before doing so.

The normal position for the switch at Carey Avenue is for movements from the thoroughfare track to the northward track.

Southward trains must stop clear of the switch north of Carey Avenue and must obtain permission from Yardmaster before making movements either into yard or on the thoroughfare track between Carey Avenue and Buttonwood.

A southward "End of Block" sign is located 250 feet north of switch at Carey Avenue.

Southward trains must not use thoroughfare track south of Buttonwood without proper authority.

Manual Control Box on Highway Crossing Signals

Manual Control Box is in service at highway crossings listed below and will be used by train crews as outlined in Item 103d.

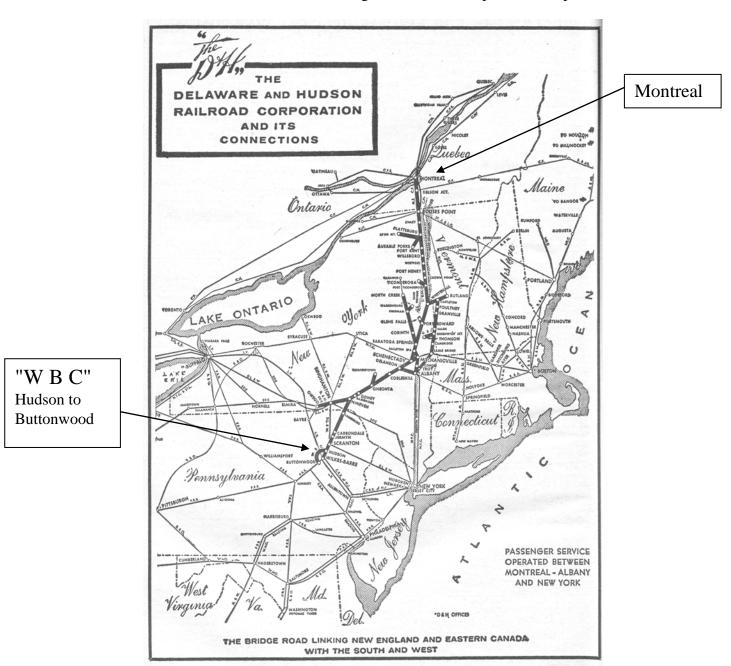
Location	Name of Crossing					
Plains, Pa.	South Main Street					
Wilkes-Barre	Mill Street - See Note 1					
Buttonwood	Old Ferry Road					

Note 1 - Trains or engines making switching movements in the vicinity of this crossing must observe that gates are down before proceeding over crossing.

Telephones

Location	Connected With
Oak St., Gate Cabin	
SH Cabin)
Mill St., Crossing)
South Main Street Crossing,	
Plains, Pa)
NC Cabin) SX Hudson
SC Cabin)
Plymouth Wye, south leg)
Carey Ave)
Buttonwood Yard Office)

On the map of the entire D&H system on page 174 of the June, 1969 edition of *The Official Guide of the Railways and Steam Navigation Lines of the United States, Puerto Rico, Canada, Mexico and Cuba*, the Wilkes-Barre Connecting Railroad, we are pleased to report, is shown:



Photos from Frank Dutton's blog of the WBCR bridges at Plains and at South Wilkes-Barre:

Thursday, May 7, 2015

Three views of the Wilkes-Barre Connecting RR Mill Creek bridge over the Susquehanna River in Plains:







Thursday, April 30, 2015 Black Diamond RR Bridge*

The Black Diamond bridge of the Wilkes-Barre Connecting Railroad in South Wilkes-Barre. The bridge is currently used by Norfolk Southern.





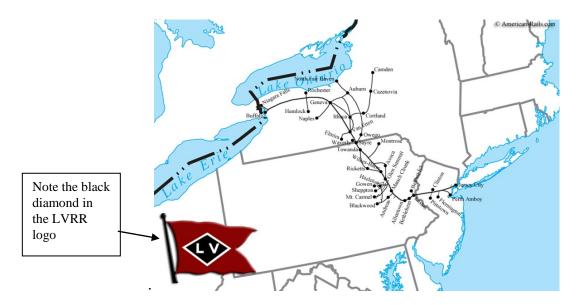


Fish Island lies in the bed of the river at Wilkes-Barre nearly opposite the junction of Crescent Avenue and Old River Road, one mile due west from Public Square. The island was named after Jabez Fish, who was an early settler in Wilkes-Barre who lived on West River Street below South Street and owned a broad tract of land that extended along the river's margin from West River Street to Old River Road.

Black Diamond Notes

Many people in the Wilkes-Barre today call the bridge shown on the cover of this volume the Black Diamond bridge and use it as a pedestrian short cut across the Susquehanna River—which is not a good idea, to be sure. In railroad circles, this bridge is known as Fish's Island Bridge.

The Lehigh Valley Railroad, one of many railroads that passed through Wilkes-Barre in the nineteenth and twentieth centuries, was known as the Route of the Black Diamond. Its 435-mile long main line between New York City and Buffalo was used for hauling anthracite coal, *black diamonds*, from the Pennsylvania coal fields to market. It's famous daylight express train between New York and Buffalo, the *Black Diamond Express* (operated between May 18, 1896 and 1959), passed through Wilkes-Barre, but neither it nor any other Lehigh Valley Railroad train crossed what is popularly called the Black Diamond Bridge in Wilkes-Barre. Here is the route map of the Lehigh Valley Railroad:



The route of the LVRR: Jersey City, Bethlehem, Allentown, Mauch Chunk, Glen Summit, Wilkes-Barre, Towanda, Sayre, Van Etten, Geneva, Buffalo

Here is a post card view of the Lehigh Valley depot at Wilkes-Barre.



Lehigh Valley Depot, Wilkes-Barre, Pa. Post card in the collection of the Carbondale D&H Transportation Museum. This passenger station, which was built by the Lehigh Valley Railroad and opened in 1886, was jointly used by the LVRR and the D&H.

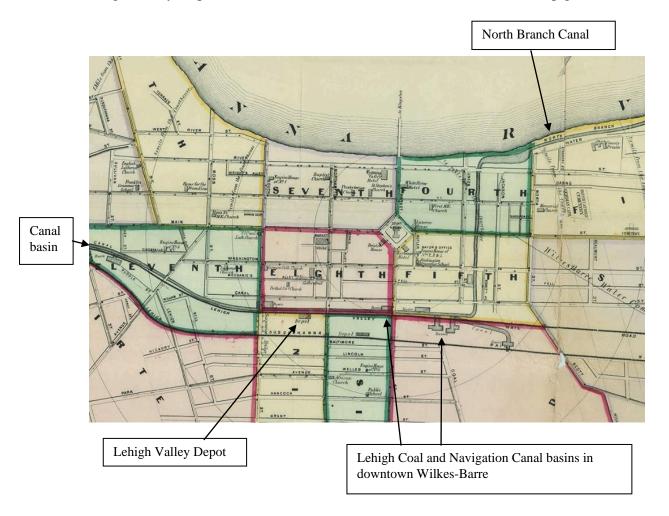
Beginning November 8, 1886, the D&H ran freight and passengers to Wilkes-Barre. From the timetable that went into effect at that time, we learn the following:

On and after Monday, November 8, 1886, passenger trains left Carbondale for Scranton at 7.00, 8.20, 9.50, and 11.20 a. m., 1.00, 3.46, 5.20. and 8.15 p.m. / Trains left Scranton for Carbondale at 7.00, 8.40, 10.00, and 11.47 a.m., 2.10, 5.00, 7.00, and 11.05 p.m. / All trains except the 7.00 a.m. from Scranton and the 9.50 a.m. from Carbondale ran through coaches between Wilkes-Barre and Carbondale. / The train that left Carbondale at 8.15 p.m. ran through to Wilkes-Barre, reached that city at 9.50 p.m. Returning, it left Wilkes-Barre at 10.15 p.m., and reached Carbondale at 11.50 p.m. Passengers to and from Scranton on this train went to the Bridge street depot, the old L. & S. station, near the Stone bridge Scranton. There were seven trains each way between Scranton and Wilkes-Barre.

Reverse of post card shown above:



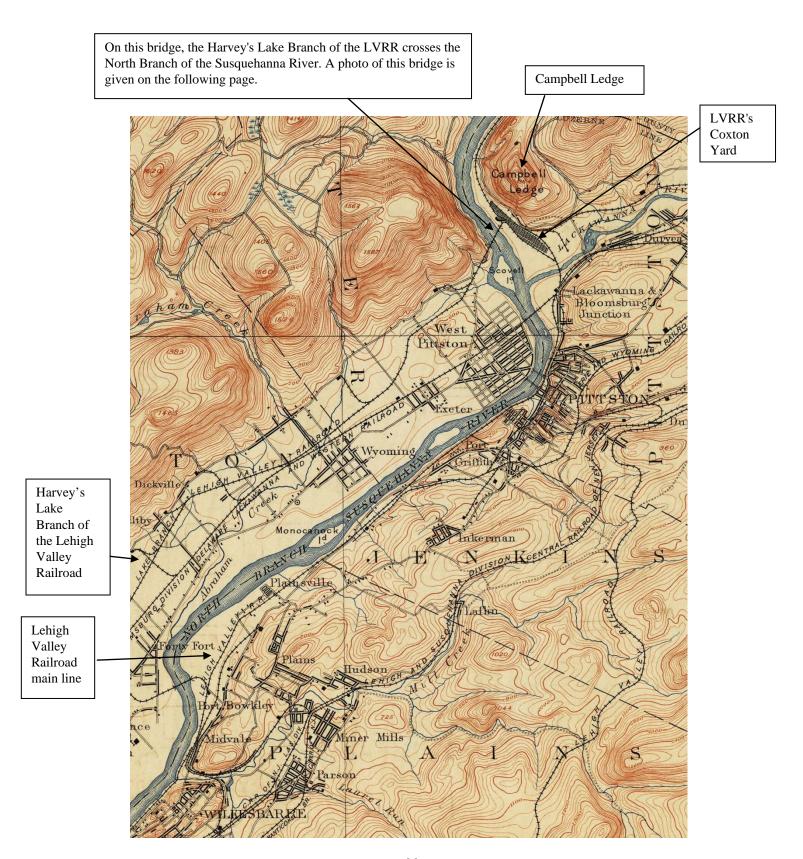
The Lehigh Valley Depot is shown on the detail of the 1882 Wilkes-Barre map given below.



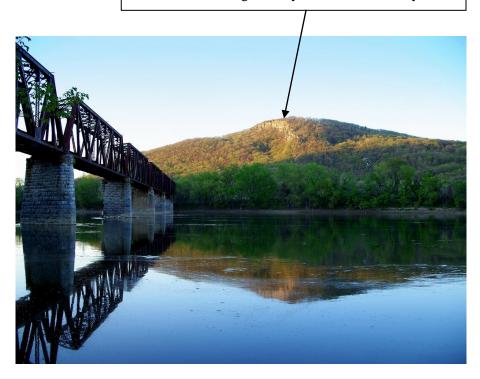
Also shown on this map are the North Branch Canal and the canal basin in downtown Wilkes-Barre. The D&H Wilkes-Barre yard was built on the site of the former Lehigh Coal and Navigation canal basin. There used to be a canal terminal in what is now the heat of Wilkes-Barre. There was a dam across the Susquehanna River at Nanticoke, eight miles below Wilkes-Barre, the locks at 'Butch' Ball's landing made it possible for boats to pass from the river into the canal basin, located on the site of the Wilkes-Barre D&H yard. D&H coal was delivered at the basin for emptying into canal boats en route to New York via the North Branch Canal.

Here is a detail of the *U. S. Geological Survey Map of Pittston, PA* (surveyed in 1890, edition of Sept. 1893, reprinted June 1913) Henry Gannett, Chief Topographer; Marcus Baker, Topographer in Charge; Triangulation by U. S. Coast and Geodetic Survey and S. S. Gannett; Topography by Robt. D. Cummin. On this detail, the LVRR in the Wilkes-Barre area is shown.

Note that the Harvey's Lake Branch of the Lehigh Valley Railroad is on the Wyoming-side of the Susquehanna River. North of West Pittston, that branch crosses the Susquehanna River to the LVRR Coxton yard below Campbell's Ledge. From there (the LVRR Coxton yard) the Lehigh Valley main line, which crosses the Lackawanna River north of Pittston in its movement north from the Wilkes-Barre area to the Coxton yard, continues north along the northern shore of the North Branch of the Susquehanna River.



Campbell's Ledge, also known as Dial Rock, which overlooks the Lehigh Valley Railroad's Coxton yard.



On this bridge, the Harvey's Lake Branch of the Lehigh Valley Railroad crosses the North Branch of the Susquehanna River.

Two 20th Century D&H Branches in the Wilkes-Barre Area:

In Howard Hontz's column in the *Bridge Line Historical Society Bulletin*, March 2015, pp.12-14 ("From the Top: Branch lines of the D&H RR") we read: **"Wilkes-Barre and Plymouth Branches**. . The Northern Coal and Iron Company built two important branches in 1906 and 1907: the Buttonwood Branch, from Buttonwood North to connect with the Plymouth Branch at South Wilkes-Barre; and the Plymouth No. 5 Branch, from a connection with the Bull Run Branch to a connection with Plymouth Branch No. 3. The traffic on these branches was heavy."

To conclude this look at the D&H steam lines beyond the Lackawanna Valley, we present the complete station list for all four subdivisions of the D&H in 1973, as presented in *D&H Time Table No. 3, Effective 0201 hrs., E. S.T., Sunday, April 29, 1973*, pp., 81-84:

GENERAL INFORMATION
STATION LISTS
(FIRST SUBDIVISION)
Station Number
Wilkes-Barre 5107 Hudson 5113 Laflin 5114 Yatesville 5115 Avoca (Dupont) 5118 Moosic 5119 Green Ridge* 5129 Dickson City 5130 Olyphant 5132 Jessup-Peckville 5133 Archbald 5135 Jermyn 5136 Mayfield 5139 Carbondale 5140 Marvine 5162 Forest City 5176 Uniondale 5178 Herrick Center 5179 Burnwood 5180 Ararat 5180 Paraton 5183 Starrucca 5182 Thompson 5183 Starrucca 5186 Lanesboro-Cascade 5114 Windsor 5201 Stringhams 5205 Nineveh 5204
Wilkes-Barre Connecting Railroad
Station Number Hudson .5113 Plymouth Wye .5150 Buttonwood .5157
Plymouth Branch
Station Number South Wilkes-Barre
(SECOND SUBDIVISION)
Station Number
Owego 5399 Binghamton. 5300 Sanitaria Springs. 5305 Tunnel 5306 Harpursville 5308 Nineveh. 5204 Afton 5309 Bainbridge. 5310 Lewis Switch 5347 Sidney. 5311 Unadilla 5313
81

(SECOND SUBDIVISION)	
Station Num Wells Bridge 53 Otego 53 Oneonta 53 Colliers 55 Cooperstown Junction 53 Maryland 53 Schenevus 53 Worcester 53 East Worcester 55 Summit Siding 53 Richmondville 55 Cobleskill 55 Howes Cave 55 Central Bridge 55 Schoharie Junction 55 JX Spur 55 Delanson 55 Wohawk-Schenectady* 55 QC Cabin 51 QS Cabin 51 Elnora 55 Ushers 55	315 316 328 321 322 323 324 327 327 327 327 327 327 327 327 327 327
Albany Main	
Station Delanson. Altamont. Voorheesville. Voorheesville Jct. Slingerlands. Delmar. Elsmere. Albany** *Station 5357 covers the Schenectady area including the South Schenectady Branch.	338 340 340 343 344 367
**Station 5400 covers the Capital District from Kenwood to Waterford including branches.	
(THIRD SUBDIVISION)	
Station Num Albany*. 51 Mechanicville 51 QG Cabin. 51 BW Cabin. 51 Ballston Lake 51 JS Cabin. 51 Ballston. 51 Saratoga. 51 Cooks. 51 Whiteheads. 51 Gansevoort 55 Fort Edward** 51 Smiths Basin. 55 Dunhams Basin. 55 Fort Ann. 55 Comstock. 51 Whitehall. 55	412 413 414 430 428 415 417 419 420 421 422 433 424

 $[\]pm \text{Station}$ 5400 covers the Capital District from Kenwood to Waterford including branches.

^{**}Station 5421 covers the Fort Edward District including branches.

(THIRD SUBDIVISION)

Adirondack Branch

Adirondack Branch
Station Number Saratoga. .54,17 King's. .54,61 South Corinth .54,62 Corinth. .54,63 Hadley .54,67 Warrensburg .54,50 The Glem .54,68 Riverside .54,69 North Creek .54,70
Rutland Branch
Station Number Whitehall .5425 Fair Haven .5500 Hydeville .5501 Castleton .5502 West Rutland .5503 Center Rutland .5505 Rutland .5507
Washington Branch
Station Number Castleton ,5502 Poultney ,5510 Middle Granville 54,91 Granville ,54,92 West Pawlet ,5512 Rupert ,5512 Salem ,54,93 Greenwich Junction ,54,85 Shushan ,54,96 Cambridge ,54,97 Eagle Bridge ,54,98
(FOURTH SUBDIVISION)
Station Number Dresden .5601 Fort Ticonderoga .5607 Crown Point .5609 Port Henry .5610 Pyrofax .5604 Howards .5606 Westport .5612 Whallonsburg .5614 Essex .5616 Willsboro .5618 Port Kent .5620 South Junction .5624 Plattsburgh* .5623 West Chazy .5628 Rouses Point .5630 *Station 5623 covers the Plattsburgh area from Bluff Point to Boyntons

GENERAL INFORMATION

(FOURTH SUBDIVISION)

Ticonderoga Branch

Station	Number
Fort Ticonderoga	5607
Ticonderoga	5641
Ausable Branch	
Station	Number
South Junction	
Salmon River Junction	5626
Peru	5652
Ausable Forks	
Chateaugay Branch	
Station	Number
Salmon River Junction	5626
Otis Junction	5639
Imperial	5648
Morrisonville	5660
Cadyville	5661
Federal Sand	
Dannemora	5662

Need a doctor? Given on the final two pages, pp. 89-90, in *Time Table No. 3, Effective 0201 hrs.*, E. S. T., Sunday, April 29, 1973, is a list of the D&H Company Physicians. Here is that list:

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COMPANY PHYSICIANS

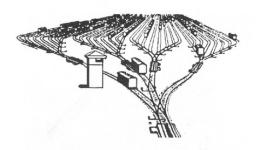
Z. Sheremeta - Medical Director - Albany, New York 463-1141 (Ext. 325 or Colonie PBX 36)

NAME	LOCATION	OFFICE	TELEPHONE	RESIDENCE	TELEPHON
. F. Cavan	Wilkes-Barre, Pa			30 Wyoming St	822-6341
. Shellman	Blakely, Pa			513 West Lackawanna Ave	487-0731
. E. Swift	Scranton, Pa			145 S. Main Ave	343-1623
. M. Howell	Scranton, Pa	Suite 609			
		Medical Arts Bldg	343-2860		
. R. Morasco	Hallstead, Pa	Hallstead Plaza	879-2145		
. L. Casey	Carbondale, Pa	58 Spring St	282-1642		
. J. Falbo	Carbondale, Pa	55 No. Church St	282-2690		
. J. Burnett	Binghamton, N.Y	151 Front St	724-3237		
. F. Carson	Oneonta, N.Y	28 Watkins Ave	432-2110		
. L. Haines	Oneonta, N.Y	Medical Arts Bldg	432-6291	154 East St	432-3390
. Phelps	Oneonta, N.Y	Fox Memorial Hosp	432-2000		
.G.S. Dougall	Cobleskill, N.Y			18 Grand St	234-3501
. J. Rourke	Schenectady, N.Y	1328 Union St	393-9507		
. Runge	Schenectady, N.Y	713 Union St	370-3090		
. Spring)					
. A. Dunham)	Schenectady, N.Y	847 Union St	393-1331		
. E. Holmblad)					
. F. Mastrianni	Mechanicville, N.Y	222 North Second Ave.	664-4711	176 So. Main St	664-4711
. H. deRouville	Albany, N.Y	Room 1, D&H Bldg	463-1141 (Ext. 255)		

COMPANY	PHYSICIANS
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NAME	LOCATION	OFFICE	TELEPHONE	RESIDENCE	TELEPHONE
Z. Sheremeta	Watervliet, N.Y	504 8th Avenue	273-2255		
C. R. Becker W. H. Moore	Troy, N.Y	2414 15th St 505 Broadway	272-2412 584-0810	•••••	
Wm. Young	Greenwich, N.Y	2 Church St	692-2240		
L. J. Butler	Glens Falls, N.Y	17 Pine St	792-4728	25 Horicon Ave	792-2723
E. Seligmann T. R. Cummins	Whitehall, N.Y Ticonderoga, N.Y	146 Montcalm St	585-4149	40 Williams St	499-1211
J. Glavin	Port Henry, N.Y	58 So. Main St	546-7111		
D. T. Kasprzak D. Weeks	Plattsburgh, N.Y Plattsburgh, N.Y	135 Broad St	561-3430 563-2910		
G. W. Clark	Chazy, N.Y		846-7161		
J. B. Southwick	Champlain, N.Y	North Country Medical Center	298-2691		

In the preceding pages we have focused on the <u>addition</u> of rail lines to the D&H. In 2015, a major <u>subtraction</u> of D&H rail lines took place when Norfolk Southern purchased from Canadian Pacific what is referred to as D&H South. An excellent account of that sale is given by Doug Barron in his column, "The Receiving Yard," in the November 2015 issue (p. 26) of the *Bridge Line Historical Society Bulletin*. Here is that column:



The Receiving Yard

by Doug Barron

Norfolk Southern assumes control of D&H South ... On Saturday, September 19, 2015 at 12:01 a.m., Norfolk Southern officially assumed control of Canadian Pacific's ex-D&H trackage between Schenectady, N.Y. and Sunbury, PA. The NS Harrisburg Division Dispatcher assumed all dispatching of this trackage. Federal regulators had approved the sale of the 283 miles of the D&H across New York and Pennsylvania to the Norfolk Southern. On Friday May 15, 2015, the U.S. Surface Transportation Board (STB) approved the \$217 million purchase of tracks owned by CP Railway.

The 282.55 miles of track includes the D&H from Sunbury, Pa., (MP752) to Schenectady, NY (MP484.85), and the 15.4 miles of the Voorheesville Running Track between Voorheesville Junction (MP A10.9) and Delanson, NY. Norfolk Southern will retain trackage rights over approximately 17.45 miles of track between Schenectady, NY (MP484.85) and in the vicinity of Mechanicville, NY (MP CPF467), including the right to use tracks within the D&H's Mohawk Yard.

Norfolk Southern has been the major user of the line in recent years. The purchase also puts Norfolk Southern in direct competition with CSX between Chicago and Albany, NY. The STB preserved Norfolk Southern trackage rights over D&H lines to Saratoga Springs and to the Mechanicville rail yard.

D&H will discontinue trackage rights over Norfolk Southern and other lines connecting to the tracks it sold to NS. The STB said those rights hadn't been exercised in more than two years, and thus the move was unlikely to affect competition. The acquired lines connect with NS's network at Sunbury, Pa., and Binghamton, N.Y., and would provide NS with a single-line route from Chicago and the southeastern U.S. to Albany, N.Y. NS also gains an enhanced connection to its joint venture subsidiary Pan Am Southern. Additionally, NS has acquired the D&H's car shop in Binghamton, along with other facilities along the corridor.

NS has hired about 150 former D&H employees who have experience working in this corridor. NS plans to add new ties, resurface 40 miles of track, and install 14 miles of new curve rail in 2016. The last Canadian Pacific train to operate over D&H South was train 253 with CP Rail 9623-8954 and 19 cars. Norfolk Southern train 12K was the first to operate over D&H South after the purchase from CP was completed on September 19, 2015. The crew was on duty at 12:01 a.m. in Binghamton, and was on standby, ready to depart after receiving the confirmation that all dispatching features were safely functional from the Harrisburg dispatching center. They departed Binghamton at 4:00 a.m. and crossed the trestle at Harpursville at 5:00 a.m. Albany Times Union, Norfolk Southern, John Sesonske 1210

Weekly Newspaper Columns of News and Notes Primarily about the D&H Steam Line (Valley Road) and the Jefferson Branch of the Erie Railroad. May 22, 1885—September 9, 1892

In Volumes VII, VIII, IX, and X in this series we published the newspaper columns of news and notes exclusively about the Gravity Railroad that were published regularly, but not weekly, in the newspapers published in Carbondale in the period September 15, 1882—October 28, 1892.

Published in those same Carbondale newspapers for the same period are 60 columns of news and notes that are primarily about the D&H steam line (Valley Road) and the Jefferson Branch of the Erie Railroad, but there are also items in these columns about the Gravity Railroad as well as about the Pennsylvania Rail Road Company.

Fourteen of these columns are titled *Railroad Matters*, forty-five are titled *Railroad Notes*, and one is titled *Some Railroad Notes*.

Railroad Matters, published in the Carbondale Leader, in the period February 9, 1883—September 4, 1883, are all unsigned.

Railroad Notes, published in both the Carbondale Leader and in three issues of the Carbondale Advance, in the period March 22, 1884—September 9, 1892, have many different signatures on them: 32 O. K., Switch Key, Step Link, Cracked Wheel, Ecnrcitec, Cracked Head, C____D H_____D, P. R. V., New Signals, and E. C. Centric.

Some Railroad News, published in the Carbondale Leader of November 13, 1890, is about the Gravity Railroad, the D&H steam line (Valley Road), the Jefferson Branch of the Erie Railroad, and the Pennsylvania Rail Road Company.

In Volume XI in this series we presented 30 of these weekly newspaper columns of news and notes that are primarily about the D&H steam line (Valley Road) and the Jefferson Branch of the Erie Railroad for the period February 9, 1883--May 19, 1885.

Presented in this volume are 30 more of those weekly newspaper columns of news and notes that are primarily about the D&H steam line (Valley Road) and the Jefferson Branch of the Erie Railroad. They cover the period May 22, 1885—September 9, 1892

Here then, are the 30 columns containing news and notes primarily about the D&H steam line (Valley Road) and the Jefferson Branch of the Erie Railroad that are published in Carbondale newspapers for the period May 22, 1885—Sepember 9, 1892:

Gossipy in-crowd chit chat from "Step Link"

RAILROAD NOTES.

ITEMS ABOUT THE BOYS ALL ALONG THE LINE.

Engineer Mack's daughter, Grace, is quite ill.

I. J. N. says trout fishing is not a success; with him, at least. His luck last week was poor.

"Silver," why those yexatious idle tears? Why not preserve or crystalize them? Water may get scarce this summer. Mr. Cole, what is the fare from Uniondale to Windsor?

Jimmy B. called on Scranton friends last week. He reports a pleasant time, visited the Salvation Army meetings and was so well pleased with them that he concluded to join, but in putting on his "Hallejuah" jacket he got it on wrong side out, bottom side up and hind side in front. The lieutenant said he was not a good subject for a soldier and gave him the g. b. (this does not stand for good boy.)

Why don't Ike sell Walker to some telephone company? He is strong enough for it and all they would have to do is to put an insulator on him. He would make a nice smooth pole.

"Eli" Gibbons says he doesn't get in a twenty ton dump coming through Thompson. Wonder if "Eli" got one of those chromos? "STEP LINK."

"The orders for coal are coming in good. . ."

"A 'bath tub' has been

[tower[and shower

baths are all the rage

there now, especially

among the coal eight

their turn comes. . ."

Coal Eight: All coal

trains departing from

Oneonta for Carbondale

between 8 A. M. and 1

P. M. were called coal

eights.

boys" who get a bath as

put in the office at J. N.

RAILROAD NOTES.

ITEMS ABOUT THE BOYS ALL ALONG THE LINE.

The orders for coal are coming in good, and some of the gangs that were running on the lower end a few days ago because of the duliness of the trade are on their regular runs again to Nineveh, and Conductor Case, from the lower end, has been added to the upper end where we can see his smiles, and Homer's, too. Engine 44 makes good time for Dickey Burnett winds up the steam guage with the scoop and she is always hot.

Who on carth is "Switch Key"? With a "Flat Wheel" and a "Switch Key" and a long order that is "32 O. K." it would seem as though a good run ought to be made even if a "double header" was forced on. But say, "Flat Wheel," if you have got that "plaster" out of your eyes yet why not shake some of the boys up or recommend one of Doc. B's 3½ story Knox hats for "Silver." If he goes to Washington he, "Silver," would need just such a hat as that to carry his medicine and extra clothing in.

Alex. Copeland's new house is completed and ready for him to go in; but he says he prefers to wait until that stained glass has been put in the door by Contractor Lec.

James Gibbons, better known as "California Jim", is soon to be married, but he is undecided yet which one of two he will marry. Now, Jim, if I were you I would marry the other one for she knows how to bake buckwheat cakes better than Miss ———.

A "bath tub" has been put in the office at J. N. and shower baths are all the rage there now, especially among the coal eight boys who get a bath as their turns come. Jack Moyles handles the concern, assisted by Jack McQuartus and Colonel Hoyle.

Hoyle.

The N. Y., L. E. & W. R. R. Co. are doing a heavy biz at present and several division gangs are here every day. The engineers that do the pushing up the hill get all the hardship they want, but being all healthy men they never loose a trip.

L. A. Rose, at one time dispatcher for the N. Y., L. E. & W. folks, and for two years dispatcher for the N. Y., W. S. & B. R. R., is back with us again, but this time with the D. & H. C. Co. The boys are all glad to see L. A. R. back again and we hope this time for a long stay. It is no use to say anything as to his qualities for he is well known among all our boys, who have the kindest regards for him.

L. A. Rose is again working for the D. & H.

C. M. T., our little light weight dispatcher, now has a helper and he will not be kept up so long nights as of yore.

We heard a man say on Palmer's train the other day that Polk found work enough at Forest City to tire Mike Kane out and that he was carried back to the coach on a board. Old man Brush finished the switching and he, becoming exhausted, sank down on the footboard, when the head brakeman tackled the levers. It has since been learned that there was only two cars there but Polk wanted to wrestle with them and he did.

Ben Dunlap, an old railroad man and for many years a brakeman on the Eric, had two fingers crushed last Monday while coupling cars at Forest City.

We hope to be able in our next to say something in regard to the order of Railway Conductors which is soon to be organized here. With such a fine looking lot of men as are the conductors on this division we see no reason why as fine a Division can not be organized as anywhere in the country.

The new, cozy dispatchers' office, lately built in place of the old one that was so mysteriously burned about two months ago, is now completed and ready for the smiling faces of Dan, George and Washington Irving.

Many of our boys heard with much re-

Many of our boys heard with much regret of the sad and untimely death of P. A. Street, traveling engineer for the P. & R. R. Co., which occurred a short distance from Solomon's Gap on that road a week ago last Saturday. Mr. Street was well known by many of our boys and was a railroad man of rare qualities. He was known to be a first-class man and of good temperate habits. He will be missed by all who knew him.

Engineer Wm. Haigh and wife were visiting relatives at Thompson and Nineveh last week. Billy is a first class man and runs a first class engine, too, the "S. A. McMullen," with Mr. Correll at the "slash dur."

"slash bar."

Toney White ran the "local" a couple of days last week. Toney is a very polite and genial conductor and is quite a favorite with many a lass along the Jeff. 32 O. K.

The engineer, conductor and train-boy, of the Saratoga express, and the night yardmaster at Nineveh, thought they would go fishing on the Susquehanna while they were waiting at the above place on Saturday. The train boy accordingly borrowed several nice trolling lines and hooks of the operator. They had the usual fishermen's luck, losing the lines and hooks and not catching anything. Every time the operator, of whom they borrowed the lines, appears in sight nowadays there is a general scramble for a good hiding place.

"Happy" Jack Hogan is never so happy nowadays as when he can sit on the smoke-stack of engine 9 and count fifty-seven woodchucks on the road between J. N. and N. V. CRACKED WHEEL.

Brakeman Ben Dunlap had two fingers crushed while coupling cars at Forest City.

The order of Railway Conductors is soon to be organized here.

"... the sad and untimely death of P. A. Street, traveling engineer for the P. & R. R. R. Co..."

Engineer William Hugh runs the *S. A. McMullen*, along with Mr. Correll.

The crew of the Saratoga Express and the night yardmaster at Nineveh go fishing.

for the D. & H.

Carbondale Leader, Railroad Notes, May 29, 1885, p.2

Engineer Milo Gardner and Conductor Ted Isger, on Engine 52, have been put on "the upper end [i. e. from Carbondale to Lanesboro]."

Frank Timmons is the Superintendent of Telegraph Repairs for the D. & H.

> Gossipy incrowd chit chat from 32 O.K.

L. A. Rose named D. & H. C. Co. train dispatcher

Engine No. 7, with a new coat of paint and varnish, has just come out of Master Mechanic Dotterer's shop, and "she looks fine."

RAILROAD NOTES.

ITEMS ABOUT THE BOYS ALL ALONG THE LINE.

One more train has been put on the upper end, which is evidence of a growing and prosperous summer's work. Conductor Isger and Engineer Gardner have charge of this train. Two better men do not sign orders than Milo and Ted. And with McCawley at the handle end of a brand new scoop, engine 52 is always "hot" and nothing short of a good run is the result.

the result.

Frank Timmons, Superintendent of telegraph repairs for the D. & H.. had occasion to ride up the line on Wednesday to give the wires a twist. Frank is a good fellow, besides being a gentleman, and is very fond of "Tarbox," which abounds in great plenty at Susquehanna.

Charlie Dow's case has been acted upon and it was decided that he is entitled to be a member of "Silver's" dudes. His white hat (not of the Knox style, either) and leather mittens make him look like a "professional" man, a hatless doctor, or a man who went to the city to study medicine the second time.

I. H. Bell and wife, of Honesdale, visited at Mr. and Mrs. J. A. Bryden's, at Nineveh, last week.

Nineveh, last week.

Quite a number of the boys have asked the question since that sad and eventful night at Nineveh with George and the Doctor, what the result would have been had Ed. Morgan been there and rendered one of his soul stirring sentimental songs and not had an organ grinder to mesmorize them and drown their sorrows. Ed., there's a chance for you to embrace notoriety. Why not size it? and by all means take P. R. V. and Gorman with you. "Fire" and "wind" are powerful elements, you know. you know.

Our friend, Gulnac, the Forest City agent for the Eric Co., has invested in a small piece of property at Peckville and thinks some of taking an agency for an oculist as so many wear "glasses" down there. What will old man Eidleman say, or 'Manda either, when they hear of such a change. a change.

a enange.

Op'r Carrick, at S. W., has returned to us after a long and happy vacation.

"Spike" Williams, the "Tom Thumb" of the operators on the N. Y., L. E. & W., has gone to A.K. for a few days. Some one has gone to Saratoga Springs, but it wasn't Carrick.

Nothing could have pleased the boys along the line better than the recent appointment of train dispatcher L. A. Rose for the D. & H. C. Co. at this place. He and C. M. T. bring the boys home in good shape.

The new Erie dispatchers' office is now being used by those officials and is a very neat and roomy one, nicely painted and in first class order.

in first class order.

Engine 7 is just out of Master Mechanic Dotterer's shop and she looks fine in her new coat of paint and varnish. Everything is all satisfactory, or, at least, we judge so, as "Jake" grow weary looking at her and fell asleep, sitting on the engineer's seat, with that old "briar-root" protruding from his mouth, and only for Bob Campbell calling "Be Jagers!" at him there's no telling how long he would have remained so. have remained so.

Uncle Billy Moyles' sand house burned to the earth last Sunday night. Sam. Moon holds an umbrella over him now. We understand it is to be rebuilt with brick.

brick.

H. A. Mace, of Green Ridge, train dispatcher at that place for the D. & H. C. Co., went to Ninevel by the Saratoga express a few days ago to enjoy the invigorating and sweet perfumed atmosphere that abounds in the Empire State. We should like to see him oftener as once a year is not enough. Strange to say he was not in his "shirt sleeves."

Lehr Gresidy, chief train dispatcher for

was not in his "shirt sleeves."

John Cassidy, chief train dispatcher for the P. & R. R. Co., with headquarters at Wilkes-Barre, passed over this Division last Monday, on train three, envout for Canada where he will remain for a time for the benefit of his health. Mr. Cassidy has been in the service for thirty-five years. Thirteen years of his service has been with the P. & R. Co. Previous to that time he was with another railroad company. Mr. C. has many warm friends here.

pany. Mr. C. has many warm friends here.

Jimmie Engle, George Walker and two bosom friends of theirs, who live in the beautiful land of Nineveh, went fishing on that majestic stream, the Susquelanna, and with "baited" hooks and wooden "bobs" they sat on the bank a little ways apart. As time and "nibbles" went by the boys began to grow "merry," but could not tell why. Growing weary of this sport, for they caught nary a fish, they betook themselves to the "uncient" city, Nineveh, to "whet" their whistles. The result was, two gates were unhinged, the town aroused, and Jimmie was taken home on a "buckboard" wagon, and George was minus his fine gold watch, which had dropped out of his pocket, with both cases badly bent. One of their friends, who, by the way, is an organ agent and had one with him, placed it on the "buckboard" behind whore poor Jimmie sut and sweetly played "Home, Sweet Home." Jimmie was found across the deorsill the next morning asking himself if there were any pickerel on his hook. Georgo slept on the "buckboard" all night with a rein grasped firmly in each hand and was only found, after diligent search by his friends, by means of the tremendious noise he made in snoring. And the organ agent, oh means of the tremendous noise as many in snoring. And the organ agent, oh where was he? His friends were dragging the river and he was given up as lost; but where was he? His friends were dragging the river and he was given up as lost; but when George was found Mr. agent was also discovered sitting with fingers still on the keys and very pale. Examination found that he was not dead but only asleep. Since that eventful night Jimmie don't like to be asked about pickerel, nor George the time of day, and they both tan up awfully easy whenever they see a gate open.

32 O. K. gate open.

H. A. Mace, train dispatcher at Green Ridge, recently visited Nineveh.

Jimmie Eagle and George Walker and two of their friends went fishing on the Susquehanna at Nineveh. No luck fishing, so they went into Nineveh to "whet" their whistles. They had quite an eventful night.

Carbondale Leader, Railroad Notes, June 5, 1885, p. 3

H. C. Butler, Esq., a former D&H employee in Carbondale, paid a surprise visit to his D&H pals in Carbondale.

"Engine 7, manned by S. Cobb, sr., and S. Cobb, jr., made her first trip on train one and two on Wednesday, and the beauty of it was that the engine (7) took the train through on time."

Gossipy, in-crowd, chit-chat of no historical value

RAILROAD NOTES.

ITEMS ABOUT THE BOYS ALL ALONG THE

Anthracite, the king of our traffic, is dull, and because of it the number of trains is somewhat diminished. But the boys have good hopes yet of a heavy summer's work.

▶ H. C. Butler, Esq., a law student of ex-Gov. Hoyt, of Philadelphia, and well known in this city as an old timer and member of the Clover Club, suprised his many friends here and on the "flats" this week by appearing in person. Harry is one of the boys, and at one time "ground out" way-bills for the D. & H. He is a favorite with all the R. R. boys.

Micky, the would be poet, went fishing under Van Bergen's store the other day. He caught two 2-inch suckers, and came near catching a ducking.

Engine 7, manned by S. Cobb, sr., and S. Cobb, jr., made her first trip on train one and two on Wednesday, and the beauty of it was that the engine (7) took the train through on time. Hope it will ever be thus.

Conductor and brakeman Nicol are away for a few days attending the funeral of Mr. Nicol's brother.

Lew Cook is doing up tickets this week and 'Gene Wonnacott is crushing baggage while Daye and Jimmie are away. 'Gene don't seem to be at home north of Carbondale.

"Paddy Grant" and Lynett, on the pick up local, have taken the "brick-yard" and "mine prop" contract off of Hankey's hands. Hankey has not given as good satisfaction since he became manager for this company as "Silver" did, hence the resignation. Hankey has got his eye on a piece of real estate at Stern's Point which he intends to purchase.

Geo. Walker is the most manly looking of any one of "Silver's" dudes. "Age and nature" have done much for him, besides the "polish", he got in the society of Susquehanna people. But the Dr. is the hardest laboring man on that train, as his duties confine him during the day to his train and he works at his trade (blacksmithing) evenings. A small "anvil" and "soft" hammer constitute his entire set of tools.

We kept out to give "Flat Wheel" a chance to say a few consoling words in the last issue of the Leader; but to our great grief and disappointment he did not show a single hair of his light colored head. Perhaps the "plaster" which nearly took his vision from him, or a sliver from an "orange box" which got into his eyes one time, has been the cause of his silence. Inever studied "medicine," though I wish I had, yet, I will say to P. R. "Flat Wheel" that if you would bind on a large "beauquett" of the "W-gate" pattern your vision might be restored to you again. Such a one as you would need will cost only \$1.08, or the price of two hundred pounds of "hay." What nom de plume will you take refuge behind in your next? A new name every time you write is indeed quite a novelty. Speak

Paddy Pidgeon has got a soft track at Lanesboro just now. That is all right Paddy, but be sure to sign your own name to all communications hereafter.

One of the engineers on coal seven, who went hunting at Nineveh a few days ago, came across "Windy" Smith, once a brakeman on coal 10. Of course "Windy" was glad to see the engineer and asked after all the boys. His honest face betrayed some battle scars which were quickly seen by the engineer, who asked "Windy" now it had happened. With a sad, downcast look poor "Windy" said that those were the scars made with a "boot box" in the hands of Jim Davis when he was still a citizen of this place. As tears began to flow from "Windy's" eyes the engineer sadly moved away all broken up.

A brakeman on Conductor Maloney's train who signs his name "Frank Bogart"—which is not by any means correct—wrote a heart-breaking note to a dashing old widow at O. N. last week and wanted to know how he stood in her affections. The good woman, who, by the way, is old enough to be "Frank's" ma, told him that she thought him perfection and extended him an invitation to call on her by all means. But a "bullhead" on the same train got onto the "racket" and wrote the dame that Bogart was "Bogus." "Frank" did not go, which was, at this time, the aged widow's wish.

Op'r Bevins, at Forest City, has made up his mind to quit telegraphing and learn the carpenter's trade. He thinks there is a good chance for him in this place to get such an education and will locate on Cemetery street where he claims to be well acquainted.

Frankie Cunningham, the "dandy" little agent and operator at Ararat Summit, Re-"Joyces" at the good luck he recently had by falling heir to a fine farm in Mt. Pleasant. Accept our congratulations and we will Re-"Joyce" too.

OUR CONDUCTORS.

On the new table, conductor Gilbert has to go to the D. L. & W. station in Scranton. This will suit Gil. and the "deacon" as they can see all the fashions as well as the handsome girls, whom we hear Gil is fond of looking at. He will look well in a brass coat with blue buttons.

This cool, invigorating atmosphere is very healthful for the "early risers," and makes the boys on later trains look hearty, too, and it exactly suits our heavy weight conductors,—Messrs. Fowler, Gilbert and Maloney.

Coal seven: All coal trains departing from Carbondale between 8 A. M. and 1 P.M. for Oneonta were called coal seven.

Coal ten: All coal trains departing from Oneonta between 1 P. M. and 4:15 A.M. the following day for Carbondale were called *coal ten*.

Gossipy, in-crowd, chit-chat of no historical value

"... our heavy weight conductors,--Messrs. Fowler, Gilbert and Maloney." (continued)

Engineers Hutchins and Gardner are running south again. "Milo is . . . a candid, frank sort of fellow but, unlike Homer. wears a brilliant smile and is ever ready to listen to or tell a good story. He is very fond of 'red lemonade' and maple sugar."

"Conductor Ed. Brandt and wife, of Green Ridge, visited at Milo Gardner's last Sunday..."

Conductors Case and Isgar, with their engineers, Hutchins and Gardner, are running south again. Homer is one of our best engineers, is very quiet and seldom smiles, and yet he enjoys a good joke as well as any one, and if put in the "corner" can tell a good anecdote or smilingly relate some of his adventures when a lad. Milo is also a candid, frank sort of a fellow but, unlike Homer, wears a brilliant smile and is ever ready to listen to or tell a good story. He is very fond of "red lemonade" and maple sugar.

Conductor Silvernell, of Wyoming Valley Division 160 of R. C., Wilkes-Barre, was among old friends on Sunday and attended the organization of Pennsylvania Division 156.

Conductor Griner, of Peckville, was a visitor in this city on Sunday. Something unusual to see him here on that day.

Conductor Rosser, of the Honesdale passenger train, was unusually happy on Sunday p. m. He found a half smoked cigar that Gill had laid aside for future use and of course he touched it off with a match which he borrowed from Jack Copeland.

Conductor Ed. Brandt and wife, of Green Ridge, visited at Milo Gardner's last Sunday. Ed. looks hale and hearty which is accounted for by his boarding at home for the last two years.

Conductor Roach from Scranton, a D. & H. boy, made the boys here a pleasant call on Sunday.

Conductors Fowler and Robbins are talking about a week's vacation down the valley, and a camp along the river for the purpose of robbing the Susquehanna of its fish. Fowler is to furnish the bread and pastry, and do all the kitchen work, while Dave brings in the vegetables, game and fire-wood. We hope they will have better success than that party at Nineveh did last week. Better take Ed. Morgan with you, boys.

Judd. Callender, of Peckville, conductor on the D. & H. at Green Ridge, spent Sunday with his son, Mel. Callender, der. We always like to meet the old timers from the lower end. Mr. C. has been a conductor for the D. & H. C. Co. many years.

THE RAILWAY CONDUCTORS ORGANIZED, The conductors of this division with two from the N. Y., L. E. & W. R. R. met in Carbondale on Sunday last and organized a division to be known as the Pennsylvania Division of the Order of Railway Conductors, No. 156. Deputy Grand Chief Conductor, C. A. Wood, of Elmira, began to organize at 1:30 p. m., and at 4:25 the organization was completed. Officers for the term were elected and installed, and the work of the first degree gone through with. Thirteen conductors were then initiated by the new officers and members of the Division, (twenty being charter members,) and at 5:45 a recess was taken for hash. At 7 o'clock the Division was called to order, and D. G. C. Wood gave the second degree to the newly initiated members, which took until nine o'clock.

The Penn. Div. O. of R. C. starts out with a membership of 33 good men and, with the good officers they have, there is no reason why it will not be as good a working division as any in the State. The officers are as follows: Chief Conductor, J. B. Kirkbride; A. C. C., O. H. Loftus; S. and T., Boyd Case; Sr. C., D. B. Robbins; Jr. C., M. H. Maloney; I. S., J. J. Lowery; O. S., M. Gorman; Cor. Sec., H. E. Johnson.

32 O. K.

Judd. Callender of Peckville is a conductor on the D&H at Green Ridge.

The Pennsylvania Division of the Order of Railway Conductors, No. 156, was organized at Carbondale on May 31, 1885. Thirteen conductors were initiated by the new officers and members of the Division (twenty being charter members). The Chief Conductor is J. B. Kirkbride.

Samuel Penwarden, conductor on the Honesdale to Carbondale Gravity passenger train when passenger service was inaugurated, April 5, 1877, has resigned on account of poor health. Si. Hobbs will take Mr. Penwarden's place.

This column from "32 O.K." is full of pointless chit-chat/gossip --which should not be a part of a public document.

RAILROAD NOTES.

ITEMS ABOUT THE BOYS ALL ALONG THE

Sam. Penwarden, conductor on the Honesdale passenger train from its first trip to the present time, resigned on account of poor health. Samuel will be greatly missed by many of our people who knew him so well. Si. Hobbs takes Mr. Penwarden's place. He is well and favorably known along the line between this place and Honesdale. Si., we wish you all the good luck and comfort there is to be had.

Conductor Skeels is the dandy conductor on the road and with his new suit looks too nice for anything.

The "aged widow," G. at O. N., so dame rumor says, is soon to move to this place; but we do not know whether Mr. "Bogart" will board with her or run the house himself. Frank, it costs money to take good care of a woman these days, but I don't see any reason why you haven't got as much right to "starve" a woman to death as any one else.

take good care of a woman these days, but I don't see any reason why you haven't got as much right to "starve" a woman to death as any one else.

Paddy Pidgeon, the "bird," and Billy Moyles take a great deal of "solid" comfort telling "consoling" anecdotes about what "hair-breadth" escapes they have been through at Lanesboro and on the "military" plains at Nineveh.

Pete Dockerty thinks that he did not take very good last week while on the passenger train because he didn't have a "white cap." But Pete is a good man and can get a shake with any of the girls while on his own train, which is drawn by Engineer "Yankee" Histed and conducted by Paddy McQuade. Much has been said already of these men and to praise them would only be repeating what has already been said.

"Tone" Cook is the dandy man with the lassies down at Hosie's. He thinks it is too bad that Hosie has stopped for the present as he is deprived of seeing them. Never mind, "Toney," they will soon start up again, and then, oh then.

The traffic on this Division is improving and the river and fish will get a rest. Hardly a fish or "polly-wog" is to be found in the Lackawanna or the pond at Uniondale.

Den Swan says he would have enjoyed his trip on C. 9 last Saturday a great deal better if "Jake" had let him go home for hash before he left, as engine 54 don't steam very good and to have to tackle a trip to Nineveh without any "chuck" is, to say the least, pretty tough.

"Shorty Ellis," the Forest City crusher, goes to Herrick on important business lately and generally gets detained all night. "Shorty," I will see that the lady lets you off easier, for a man can't count lumber all day and entertain a lady in the evening and do justice to both.

More gossip and chit-chat from "32 O.K."

"Frank Smith, chief shipping agent in the coal department, has gone to Canada for a two week's vacation. Billy Blair has full charge of the office. . ."

More gossip and chitchat from "32 O.K."

Precisely as we had expected P. R. "Flat Wheel" rushed in his "budget" in the last issue of the Leader, and has unbosomed himself to our people. He gave "32 O. K." a severe scalp wound with a telephone pole, and struck at him a second time with an old bundle of "lath" and threatened in other ways, fully as fatal, to do him up. P. R., your last nom de plume ought to hide you as it is wide enough and long enough to protect a good sized man. Now, if I were you, I would get behind a small bundle of "lath" in your next letter.

Mr. Carrick, the handsome operator at S. W., attended a ball at Herrick last week and speaks of it in glowing words. He met for the first time several handsome lassies which, he claims, he made favorable impressions upon. We-know this to be true, for he is a terrible masher and does it so very modestly too. He never seemed so happy as now.

There are two dandy operators at Herrick in the persons of Purtell and Connelly. Purtel is chuck full of life and fun and makes many inquiries after his best gal, who lives in Mt. Pleasant. Mr. Connelly is more quiet and enjoys the society of old men the best.

It has been reported that the Italians at Nineveh are very dangerous men to fool with. One of them,—who is, by the way, a first class barber—became very angry at a couple of R. R. men who live in that beautiful land and struck at one, completely severing the whole "lapel" of his coat. Since that time Charlie has been wearing his Sunday coat or an old "Jersey" that Doc. found on the river bank the last time he went fishing.

Frank Smith, chief shipping agent in the coal department, has gone to Canada for a two week's vacation. Billy Blanr has full charge of the office and the general business will be looked after by him. The water boy has charge of the "pocket" shute and will black Billy's shoes during the Dutchman's absence.

The "smoling phiz" of O. S. Stearns, once a way-bill "scratcher" on the flats, and who attempted to stand "Mickey" off on poetry in those days, shone brilliantly in diamonds last Tuesday in the office he once was so happy in. Of course the boys were all too happy to see Otis. His call was a limited one but he promised to see them later. Will Blair and Lew. Williams were moved to tears as he passed by them through the hall, and Jack Copeland wanted to know what "goose egg" coal was selling at, but Ote was too far away and did not hear it. Billy Skeels appeared to be the most "moved" of any man in the office, and wept like an infant when he saw Ote's diamonds and said they were so cheap that any one could wear them.

Mose Myers and a friend of his went fishing at Uniondale last week and caught 13 eels, 91 bull-heads and 319 sun fish. They chartered a twenty ton coal car to bring-them home. One bull head, Mose said, weighed $31\frac{1}{2}$ pounds besides his horns, which were 13 inches long.

Rene Ball and wife, of Nineveh, are visiting relatives in Honesdale. Rene returns to actual duty on Monday. Rene is one of the "white hat" dudes so famously known on the road as Silver's dudes.

We expect to give a glowing account of Messrs. Fowler and Robbin's trip down the Susquehanna when they return, and as we have learned that they took Ed. Morgan with them a full account will be easily got.

32 O. K.

"Mose Myers and a friend of his went fishing at Uniondale last week and caught 13 eels, 91 bull-heads and 319 sun fish. They chartered a twenty ton coal car to bring them home. One bull head, Mose said, weighed 31½ pounds besides his horns, which were 13 inches long."



The **American eel** (Anguilla rostrata) is a facultative catadromous fish found on the eastern coast of North America. The eel lives in fresh water and estuaries and only leaves these habitats to enter the Atlantic Ocean to start its spawning migration to the Sargasso Sea. Spawning takes place far offshore where the eggs hatch. The female can lay up to 4 million buoyant eggs a year, and dies after egg-laying. After the eggs hatch and the early-stage larvae develop into leptocephali, the young eels move toward North America where they metamorphose into glass eels and enter freshwater systems where they grow as yellow eels until they begin to mature. Like all anguillid eels, American eels hunt at night, and during the day they hide in mud, sand or gravel very close to shore, roughly 5 to 6 feet under. They feed on crustaceans, aquatic insects, small insects, and probably any aquatic organisms that they can find and eat.

D. B. Robbins and F. Fowler took a "trip down the valley" last Saturday.

"Business is better this week than for some time past. All the gangs are out."

> More in-crowd gossip and chitchat

RAILROAD NOTES.

TIEMS ABOUT THE BOYS ALL ALONG THE LINE.

D. B. Robbins and F. Fowler returned from their trip down the valley last Saturday well satisfied with the trip and what fun was connected with it. They went as far as Wilkes-Barre and there provisions ran out. They would have visited Plymouth but on learning of the epidemic prevailing there they had no desire to go further. They met several parties from Carbondale who, like themselves, was seeking amusement on the desire to go further. Iney met several parties from Carbondale who, like themselves, was seeking amusement on the banks of the Susquehanna. Jim Davis with several of his friends was camping near Wyoming and not far from Queen Esther's rock, where so many were slain by the red devils in the early days of that fair valley. Jim's canoe was a primitive one being built from the wrecks of old boot boxes and beer barrels but it answered their purpose well. He had with him the little German band and officer Moran. Robbins and Fowler had a much better band than the little German, their's being both vocal and instrumental and executed by Prof. Ed. Morgan. Both parties go again the latter part of September at which time they are to consolidate bands. Windy Smith from Nineveh will be added to its force. Windy is to play Jim's nine dollar horn.

Business is better this week than for

Business is better this week than for some time past. All the gangs are out.

The scribe of many names—some of which are "Flat Wheel," "Cracked Wheel," "Switch Key" and "Brick-bat"—must have sat on the smoke stack counting "woodchucks" again, either before or just after writing the last epistle, as in such a lofty position he could easily see all that was going on ground him. see all that was going on around him, even milliners and dressmakers; but whether he saw that "milliner" at Sum-mit or the dressmaker at Windsor we have not learned, neither does he tell us, but judging from the "scare" he got at Windsor the night it was in "flames" we do not think he could have told a "bon-net" from a bonyout conscillation. net" from a bouquet, especially one of the W-Gate pattern. But P. R. if "32 O. K." wields the mayor's club at Ninewill no doubt be easy on you and will only keep you locked up half the night in a "barn" instead of doing as that magistrate did at M. O. some four years ago. Look out while in Nineveh.

ago. Look out while in Nineveh.

"Handsome Harry" and "ShortyEllis,"
two dudes from Forest City, spent last
SundayinUniondaleandHerrick. "Handsome Harry" is always a welcome visitor
at Uniondale as is "Shorty" at Herrick.
"Shorty" says he is more anxious now then
ever before to be a shoemaker. But,
"Handsome," be careful and don't let the
tank get low at the City while you are
away or some one else will be put at the
handle.

Op'r Carrick, from S. W., attended a dance at Forest City last week. Several young ladies have fallen desperately in love with him. Oh, he's a crusher, and wherever he goes its just the same.

Messrs. Marcy and Walker went to Ninevel last week to visit Geo. Walker, Ninevel last week to visit Geo. Walker, who resides at that place, and while out one evening fishing for lobsters the boat capsized and the whole party went to the bottom. They clung to the old "schooner" which, after a time, drifted to the shore. This was another case of "fisherman's luck."

Charlie Dow is on 'the sick list this week and during his absence Jerry Moyles runs the scoop. Jerry is anxious to have the "mail" got over the road on time and

Ike Baker says he is going to try raising "canary birds" at Summit since his eating house proved a failure there. Ike, if you will keep Ammerman and Pete Ellis out you will be all right as either one of them would break any bird's heart and your canary would surely perish.

"Tough Skeels" and "Bf-Ferrell" made a pleasant call at the W. B. office on Tuesday afternoon. "Billy" was as brilliant as ever and cracked many a humorous joke, but, as usual, was all broke up with grief when they left—sand, Billy.

Pete Dougherty is doing duty on the passenger train this week while "Tough" is on the shelf. Pete's grin takes well.

Miles Cregan, at Forest City, is the heavy-weight foreman on the Eric. His net weight is 271 lbs. He never sweats.

P. R. V., politically, is a mugwump, but he thinks he ought to be appointed P. M. or mail carrier for "Bernwood," a smart little town about three miles above Herrick. All right, Pete, send up your photograph. 32 O. K.

Conductor Silvernell has been off on leave of absence this week. Conductor Isger has been ringing the punch on three and four during his absence

The crop of field strawberries is the greatest known for years. The boys have big picks along the line waiting for orders from B. Dan.

The last boquet Dan. received at C. N. was full of thorns or "prickers" as he called them.

Moran that butter milk was invigorating. Please have some more sent to the same place.

P. P. is about to buy the old soap Factory near the Tannery on Belmont-st. It will be a good investment Paddy.

M. M. tell us about the invitation you received to vamoose, the ranch on Railroad-st. Did he do it with his little No. 9 hob-nailed shoe?

Ben. says "well, I guess they must have moved the lake from the Summit dawn to Herrick, as the boys all get off at H. to do their fishing."

Jim., alias California or Cuba, is a good Latin scholar, at least he uses "high" language. Jim. get up a class in "oxnoxionsnessity."

Wally C. won the badge at the P. Vallev picnic.

Bert, and Jake did some fine piscator-Bert. and Jake did some fine piscatorial business last week on Stanton pond. They put a set-line out and the first pull they hauled in a water snake, 3 crabs, 2 clippers, 5 sun fish and 1 cork. It was a good day for fish too.

Al's. car wants it known that it is as heavy as any of the rest of the cars that run on Belmont-st. Ecnrettee.

More in-crowd gossip and chitchat from "32 O.K."

> "The crop of field strawberries is the greatest known for years. The boys have big picks along the line waiting for orders from B. Dan."

In-crowd gossip and chit-chat from "Ecnrcitec" "The double track has been lengthened 1500 feet more west of Herrick..."

"We have read with great admiration the poetry written by Wm. Williams, of Belmont st., and among the many that he has written we most admire the last one, called 'Fair Belmont.' . . . "

Jake Eitel is the foreman of the Carbondale round-house.

"Coal is running South red hot again this week."

RAILROAD NOTES.

ITEMS ABOUT THE BOYS ALL ALONG THE LINE.

The double track has been lengthened 1500 feet more west of Herrick. This ought to get all trains home on time with Fonda's yard added to it. But will it?

Dame Rumor says that D. F. is soon to take unto himself a wife. All right, Dan; send around some cake if you do.

→ We have read with great admiration the poetry written by Wm. Williams, of Belmont-st., and among the many that he has written we most admire the last one, called "Fair Belmont." Mr. W., if I were you I'd quit teaming. Don't you know that the sword is mightier than the reigns?

Gulnac and Bevans, the gentlemanly agent and operator at Forest City, are enjoying themselves with thoughts of a happy future. Gulnac favors Peckville, and Bevans Carbondale for their future homes. Bevans has found a site on Cemetery-st. that ju t suits him, but Gulnac says it don't look as well to him as Peckville does. Evidently Mr. G. is wearing spec's again.

Thos. Cunningham is working at any of the offices where an operator is off. Tommy is one of the boys, and can jerk the lightning equal to any of them.

Operators Connelly and Purtell have been removed by the N. Y. L. E. & W. R. R. Co. Not for misbehavior, but because they have 1500 feet more of new track at Herrick. This move takes them out of town—which they greatly regret.

When are the conductors going to have their Frst Annual Ball? Can any one tell us?

Toney White can't say that he never got left, for he got badly left one day last week at Starrucca. Toney was paralized, and stood with arms folded across his breast when he saw the train had gone, as he had orders to go with his "little flag" and hold a train. Hours rolled by, but his train could not move, because of it. Don't shake your flags at him boys.

→ Jake Eitel, foreman at the round-house, started for Albany on the Saratoga last Monday morning where he will join his wife who has been visiting relatives in New York for several days. Jake had his "briar root" and 7 lbs. of Clark and Snover smoking with him, and he says, "Bejaggers he calculated to have enough to last him while away."

Bob Campbell and Sam. Moon are going to spend their Fourth gloriously at No. 4 pond this year.

→ Coal is running South red hot again this week.

Several of the boys have gone fishing the past few days and have brought in some fine strings of suckers, bull-heads and pickerel. Among the most fortunate anglers are Milo Gardner, O. E. Histed, Mose Myers and Frank Colvin. Oscar and Frank fished in one place last week near Porter's Pockets for two hours, and caught a pail full of "bald-headed dispatchers."

Jake Schermerhorn says Forest City is as fine a place to live as anywhere along the "Jeff." Jake must be solid with some fair one there, or he would never speak that way. Jake how is that little farm of yours at mud pond?

We had a chance to see Ike Baker's flock of canary birds this week at Summit. Ike ought to be proud of the birds he has up there.

A freight brakeman on one of the upper end trains, took the truck from Windsor station to get some freight out of a car and forgot to return it. The agent was wild about it and poor "Rene" is all broke up if anyone says "truck."

We understand Tommie Bevans, of the D. & H. freight house, and Constable Falkner are to run a foot race on the 4th, "from the freight house to Porter's Pockets.

Paddy Pidgeon and Billy Moyles have bought a fine residence at Lanesboro and will soon move their effects there. Paddy will run a "peanut stand" and Billy has a situation in the tannery as foreman. Success to you boys.

"32 O. K."

Successful fishermen during the past few days include: Milo Gardner, O. E. Histed, Mose Myers, and Frank Colvin.

"We had a chance to see Ike Baker's flock of canary birds this week at Summit. Ike ought to be proud of the birds he has up there."



The domestic canary (Serinus canaria forma domestica) is a domesticated form of the wild canary, a small songbird in the finch family originating from the Macaronesian Islands (Azores, Madeira and Canary Islands). Canaries were first bred in captivity in the 17th century. The canaries that coal miners first took into the underground tunnels where they worked were undoubtedly of the domesticated variety. Although they didn't cost very much, they were invaluable to the miners, whose lives depended on them. For nearly a hundred years (as recently as the 1980s) before the technological age of reliable electronic air-quality sensors, miners could look for signs of distress in their caged canaries to get advance warning that dangerous coal gases, especially carbon monoxide, were building up inside the mine shafts where they were working. The small birds, with their rapid metabolism, showed the effects of poisoning much more quickly. If the birds' behavior changed, the miners knew they had only a little time before they, too, would suffer the dangerous effects of poisonous gases. The canary's call, or lack thereof, was an early warning that gave those miners the time they needed to save themselves.

ITEMS ABOUT THE BOYS ALL ALONG THE LINE.

Al. J. has a regular train on "Fair Belmont."

John P. rings the bell, but it is not a Swiss bell.

Tommy W. posed as an artist's model at the Thomson gallery.

Jimmy B. can settle a fellow in his seat so that he will stay.

Engine 53 had her draught pipe razed and other work done on her at Olyphant one day last week. It seems Conductor Isger had some work to do at Valley Junction and had detached his engine from his train at F. bridge, and the switchman at O., thinking they were dropping cars in there, changed it and the consequences were quite serious. The whole train came crashing into Conductor Lowry's train, completely demolishing the front of engine 53 and burning the cab nearly off her. Engineer Coon and Fireman Schentzer had narrow escapes.

→ Business is quite brisk at this writing, all gangs making full time on the northern division and some making extras on the lower end.

Keene, the jeweler and watchmaker at Uniondale, would like to have some of the boys come and see him. Boys, be honest and square.

The extension of double track is going on vigorously on the "Jeff." Several gangs are on duty between Forest City and Carbondale.

Our old friend, J. Hogan, stepped off or out, as it were, last Friday. Well, accept our congratulations, Jack. May your ship never strike a reef or get in troubled waters.

What's the matter "32 O. K.," did you run out of mud? There is plenty over at Brandt's.

Dan. N. had the swell rig on the 4th.

Dave C. would like to have the Saratoga lay over at Lanesboro, then his folks could move over there instead of to the Ridge. He is solid over there.

The hotel De-Stevens, at Stevens' Point, does not sell cider to boys. How is it Bert?

Our genial friend, J. E. P., has been transferred from the west end to the east end of the double track; the little giant, Williams, is doing duty at the west end.

"Business is quite brisk at this writing, all gangs making full time on the northern division and some making extras on the lower end."

at Olyphant

Collision between Engine 53 and Conductor Lowry's train

"The extension of double track is going on vigorously on the 'Jeff.' Several gangs are on duty between Forest City and Carbondale." "Wm. Muir, of
Honesdale, master
mechanic of the D. &
[H.] C. Co. shops at
that place, and his son,
Will., who is well
known in this place,
went north on train
three last Monday,
enroute for Boston."

"P. R. Vandermark, the gentlemanly engineer of engine 41, and M. Gorman thunder coal 9 and 10 over the division on time every day. . ."

"Several engineers and conductors from the Delaware Division took in Cole's great show at Binghamton last Saturday. Among those we saw were Jesse Ingraham, J. E. Salmon, I. B. Cole and Uncle Hogan..."

RAILROAD NOTES.

ITEMS ABOUT THE BOYS ALL ALONG THE LINE.

➤ Wm. Muir, of Honesdale, master mechanic of the D. & C. Co. shops at that place, and his son. Will., who is well known in this place, went north on train three last Monday, enroute for Boston.

"Gully" Harvey ran into the last end of "Scovey" Ferrels train one day last week and "Scovey" had to abandon his caboose and "Gully" his engine at Starrucca, from which point both trains were consolidated and "Scovey" took passage to J. N.in "Gully's" caboose. "Scovey," we hear, has gone fishing for a short time.

P. R. Vandermark, the gentlemanly engineer of engine 41, and M. Gorman thunder coal 9 and 10 over the division on time every day. This speaks well for these two men for its a hard train to run and there is a great deal of extra work to perform; woodchucks to count, "fires," if any are discovered, to be quenched and bouquets to be gathered. All these duties are performed by P. R., and in cases of large conflagrations, where no water is to be had, "mud" or "mortar," which he carries on his engine, is hurled on the flames until extinguished. If these remedies don't do it, Peter telegraphs to the Scranton Republican for help, and it comes.

Several engineers and conductors from the Delaware Division took in Cole's great show at Binghamton last Saturday. Among those we saw were JesseIngraham, J.E. Salmon, I. B. Cole and Uncle Hogan. The Delaware Division must have lain idle that day.

Peter Whurlo Robinson and his flagman, Lewis, are two good men to run a "gravel train." Lewis is a "dandy." Nothing ever gets by him when left with his red flag, not even the Saratoga express. Lewis says a half an hour isn't long to hold No. 2, nor an hour and a half to hold coal 9. Good boy, Lewis.

The double track grading between Carbondale and Forest City is going on with great rapidity and it will not be very long ere we shall have another track to get out of town on. Success to old Eric.

Kernie Brennan, at one time brakeman on the N. Y., L. E. & W. train which lays over at this place, is ngain among us, fat, hearty and healthy.

Geo. Walker, of local freight notoriety between this place and Nineveh, had the misfortune to split one of his middle fingers at Nineveh one evening last week. While engaged in a hotly contested game of base ball with some old farmers the gas suddenly went out as George was reaching for a high ball. He is trying to get on the shelf—but no go.

Messrs. Hobbs and Pierce, of the passenger train lying over at Honesdale, often visit the boys on the flats and are always welcome. Billy Rosser and Hubput in their extra time on the docks and Irving Cliff while resting themselves.

Daniel Fonda, dispatcher of trains for the Erie, who has been on a vacation in the western part of New York state, returned to us last Monday morning on train 46 much improved in health and appearance.

Mr. Goodrich, supervisor of bridges and carpenters for the N. Y., L. E. & W. R. R., and who resides in Owego, came over the Jefferson branch accompanied by E. A. Saxton, foreman of bridges and carpenters, on Wednesday last, inspecting the several bridges along that road.

Business is good on the road again. All gangs are making full time and many of them extras to G. R.

Paddy Pidgeon has put a bay window in his peanut stand at Lanesboro and painted the fence blue. Hammocks are numerous around the place and it will in time be quite a summer resort.

32 O. K.

Daniel Fonda, dispatcher of trains for the Erie. . .

"'Mr. Goodrich, supervisor of bridges and carpenters for the N. Y., L. E. & W. R. R., and who resides in Owego, came over the Jefferson branch accompanied by E. A. Saxton, foreman of bridges and carpenters, on Wednesday last, inspecting the several bridges along that road."

"The double track grading between Carbondale and Forest City is going on with great rapidity and it will not be very long ere we shall have another track to get out of town on. Success to old Erie."

ITEMS ABOUT THE BOYS ALL ALONG THE LINE.

Engineer Lingfelter and wife paid Thomson friends a visit last week.

Wally Copeland signs orders with Milo during Conductor Isger's enforced idleness

"32 O. K." says he never was frightened but once, and then he could not read the figures on a coal way-bill, and that was the only time his hands refused their calling. G. F., brace up or you will lose your grip.

Engineer Yarns is erecting a fine residence on his lot on Cemetery street which will be one of the neatest on that street.

J. Vandermark has resumed his position on engine 55 after a two week's idleness caused by a scalded hand.

Gorman says the meteor struck his train at State Line last Friday night.

Engineer J. Bryden, of Nineveh, stopped over Sunday in this city.

W. B. Skeels has returned to duty at the way bill office after an extended trip to Owego and other points of interest.

Jake's crop of berries are now ripe at Cotrill's, and he is sighing for a few days dull time so he can go up and see about harvesting them.

Billy Skeels and a few choice friends tried their piscatorial powers on Lake Elk. Doc. says if he could have kept W. B. on the lake they would have had better success.

Dan. is on duty again and the boys get their usual "stabs."

The crops and fruit on the line between Carbondale and Nineveh will be the largest known for years. Pete, how is it about your orchard at Nineveh?

Jake, at the round house, says, by shimming, mine frow will learn that Sam Dotter to look a leetle out. He was making himself too familiar around mine house when she got back from the old country and she caned him like blazes."

Jacob G. must have a regular girl at Scranton as he lays over there Saturday nights.

The two rival railroad correspondents are having quite an exciting time raking up each other's past histories. Go it, boys, we won't run opposition.

A party of young ladies had a pienic at the grove at Nineveh, one day last week, which was the cause of much rivalry among the young gentlemen on the Saratoga express. The newsboy thought he would fix up nice and go over and enter-tain the young ladies. He accordingly borrowed a razor and asked Lew. Cook, the baggage-master, to shave him, which he laughingly consented to do if he would tell him where to find something to shave. The boy at last convinced Lew. that there was a large growth on his face and so he began operations. The boy had never been shaved before and every time the razor came close to his neck he would clutch his throat with both hands for fear that it was going to be cut. But, after having one man sit on his legs and another to hold his hands, he was shaved. When at last he was all spruced up what was his rage and disgust to find that a couple of the other boys had got ahead of him and were even then entertaining the young ladies. Any man, woman or child who wants to spend their vacation in the hospital had better say "shave" to CRACKED HEAD. him.

A summer picnic at the grove in Nineveh, youthful enthusiasm, and rites of passage.

"The crops and fruit on the line between Carbondale and Nineveh will be the largest known for years..."

Carbondale Leader, Railroad Notes, August 4, 1885, p. 1

RAILROAD NOTES. ITEMS ABOUT THE BOYS ALL ALONG THE LINE. "... on the gravel tie Dan., alias "Bogus," says he is not on the road to ruin, but is on the gravel tie road between East road between East Windsor and Centre Windsor and Center Village. Dan., did you have a cane, or was that applied before you left Cd? Village..." Bert. Porter is to have a post office established at Stringham's, so the "escaped lunatic" can send letters direct and not depend on Keifer for his billet-I. J. Naughton visited agent Cole at Uniondale last week. P. P. says he wonders if 32 O. K. has a bay window on his crystal palace in Binghamton, familiarly known as Sis-Where are you old "Cracked Head?" Have you been run in the back shop to have "Jake" "praze" the fracture? Dave Cobb has been Dave. Cobb has been transferred to the transferred to the Valley Division. "Obadiah" takes his place in the affections of the "feather Valley Division. bag" at Lanesboro. Engineer Blake was caned by the B. of L. E. fraternity on Sunday last. 'Twas good enough for him. He deserved it. Jno. H. is under impression that it is all mine at Tuscarora. Look out J. you don't get left. California's "mash" came over from Thompson to visit him last week, but she was left; Jim's other one had superceded her. Becker, the world renowned boxer and sport, is to give an exhibition at Crystal Lake some time this summer. Jake., why don't you have them remodel the 518? She would make a firstclass steam excavator for use on the double track. Jimmy B. intends to visit Barnum's circus at Binghamton. Jim., take 32 O. K. with you. He can show you the "elephant." "Robbins' famous → Robbins' famous circus will be here Aug. 21. We have a Robinson's circus quite often on the "Jeff." It is commonly called "Whirlo's." He does some fearcircus will be here Aug. 21..." ful tumbling at times and it is not under canvass either. The tumbling he does is when the train gets detached and runs together. L. Cummings read the riot act to the ex-mayor of F. City one night lust week and perhaps would have made an alderman of him if Dispatcher Naughton had not interferred. Bob Campbell is the happiest man in this or any other town and all because he has a bouncing girl. The "Moon" shines for all but some tines it gets clouded when the boys use

"Becker, the world renowned boxer and sport, is to give an exhibition at Crystal Lake some time this summer."

Charley Dowe and H. Smith are not color blind.

too much oil and waste.

ITEMS ABOUT THE BOYS ALL ALONG THE

W. B. Skeels takes a trip to the sea shore next week.

Baker and Ammerman have settled their differences and decided to let Tom King have their aquarium at the Summit, and their bird show.

"Obadiah" says he does not care about the numerous jobs the boys put up on him, but when they tried to burn his dinner can, he had to get out.

→ Hank Schenzer is doing scoop duty for J. Bryden on trains 3 and 4.

A number of the boys are to join the G. A. R. trip to Shohola on 20th inst.

The coal trade is a little slower this week than last.

Harry has swopped with Gorman. Gorman is well pleased with the change. His man keeps awake.

The "beechers" at Cotrill's switch don't monkey with one Jimmy B. so much as they did. They get paralyzed, sometimes.

Guy Little does considerable "guying" at Stringham's.

Clum and Gillies caught a nice mess of pickerel at Lewis Lake on Tuesday.

The buttermilk fiend on Coal Nine and Ten has appropriated it all to himself. The boys tried to get some at Doraville, but Smith said he only had enough for one hog besides his own.

Dan. is a good one; he believes in having the trains moving in one direction, then will have no collision. Yaw, now; some men are large anyhow.

Engineer R. R. Simerell, of the Ridge, is the guest of his brother on Belmont street.

Conductor Rosser, of the Gravity, took charge of the valley train for Skeels on Wednesday.

The masonry on the new iron bridge at Olyphant is about completed. It will be a substantial affair.

Byron Peck, of Green Ridge, took Brakeman Cook's place yesterday on the Saratoga.

P. R. V. wants to know who "Cracked Head" is. Well, he isn't any body who runs on the upper end and tries to lay out the Saratoga fifteen or twenty minutes every time he meets it.

The Saratoga newsboy has resigned and his place is occupied by the one who has been on the valley passenger train.

C——D H——D.

"Byron Peck, of Green Ridge, took Brakeman Cook's place yesterday on the Saratoga."

"Hank Shenzer is doing scoop duty [fireman] for J. Bryden on trains 3 and 4."

"Clum and Gillies caught a nice mess of pickerel at Lewis Lake on Tuesday."

"The masonry on the new iron bridge at Olyphant is about completed. It will be a substantial affair."

ITEMS ABOUT THE BOYS ALL ALONG THE

We understand that Billy Skeels, who once wore a diamond necktie and a pair of moss-agate pants, is to leave us. He will locate at Providence, but comes home to sleep. It's a good thing Billy is so well known by the boys on the lower end, or he might' get bounced out of the caboose again. Silver always laughs when that subject is dug up. It's a tender heart that weeps or looses control of his nerves when attacked by an officer of Blackstone.

Luke McQuade, "Paddy Grant," "Hankey's Clown," or any other nigger you have a mind to call him, is going to Hot Springs during the heated season. Messrs, Lynett, White and Lynch accompany him as far as Archbald. We wish him a speedy trip, and hope his failing health will be fully restored to him. He takes the B. of R. R. B. in before he leaves. We shall expect some poetry of his trip on his return, but whether it will be of William's kid, I am not at this writing prepared to say. "Hankey" is too full for utterance.

Thomas Ayers, fireman on engine 21 between this city and Scranton, is dangerously ill. The latest reports are that his recovery is doubtful. Three doctors have at times been in attendance.

A. C. Lown, on the Nineveh train, has purchased a fine nag, carriage and harness and can ride behind as fine a traveler as goes the river roads in that lovely country. He thinks the road the best between Nineveh and Stringham's switch. Now boys, you can't guess her name; so don't try. She ain't that "escaped lunatic" either that P. R. Flatwheel sported about.

Patrick Oxen, Si. Mills, Mart. Gearity and Jake. Schemerhorn went fishing and hunting at Ararat Lake one day this week. They took a stroll over Jake's farm which lays near by mud pond; in that beautiful country. Jake. showed the boys the beauties of his place and several relies of the old ark, which Jake. says was the one used by Noah. Mr. Oxen got very happy listning to Jake's, anecdotes, and said he wished Mr. Lewis "Whirloe's" flagman and "Bull Head Kearns" was there too, for they always enjoy good and ancient gtories, when well told.

The camp meeting at Uniondale is in full blast, and several of the b'hoys are frequenters there,—more especially the telegraph operators. Op'r Gulnac went up the day it opened, probably to assist in pitching tents and other light workwhich he is so well adapted to do. Mr. Bevans is also a daily visitor there; usually going up on an Eric coal train, and always riding on the engine for he says he can more easily hide from Carrick, who would give him away were he to see him.

The grading for the new extension of double track between this place and Forest City is fast nearing completion, and it will not be very long ere we shall see trains running over it. Conductor Baldwin has charge of the gravel train, and he makes a good man for it, too.

The excursion on Tuesday last to Binghamton to see Daddy Barnum's great show did not pan out as was expected by those who were interested in its management.

Miss Carrie L. Foster is visiting at Nineveh, N. Y., with her uncle and aunt, Mr. and Mrs. E. Wilcox. She returns on Tuesday next.

Miss Clara B. Cook, of Binghamton, and Miss Hattie T. Young, of New Rochelle, N. Y., are the guests of Misses Olive and Mary Wilcox at Nineveh, N. Y.

32 O. K.

Engineer Morgan and a few choice spirits did the Stanton pond on Friday with limited success.

Gorman's flag was carried at Barnum's circus in Binghamton on the 11th inst.

Smithy •2nd says he likes skating first class but it is so difficult to get acquainted with good skaters.

Mr. Blair finished the trench for the new water main on North Cemetery street last week.

Mart says Si Mills will have to remove his kids and plug hat or the boys can never do their work.

P. Gillen was general yard-master during circus day.

Tom says: "By ginger, it is none of your business if I do get a picture painted at the 'gallery'."

Clum has shaken "Curly" and will not fish any more. He says it is better pastime to go after game at F. City.

Richard Moyles employed as a boiler maker in the locomotive shop lies very sick at his home on R. R.-st.

Please tell us where you are located, C-d H-d, so we can be more prompt and not "lay out" Train 2 so long. We suspect you're hid in the woods somewhere between F. C. and S. W., with one of F. Gelder's chronometers, since the picnic at F. City where they run so many out.

J. H. Lingfelter has the neatest and best kept lawn on Belmont.

"320k" seldom has a wreck, but they will happen; he had a car off not long since but could not find his frog and wedge. Well they must be found, so off he goes on a quiet hunt and finds the Wedge in a Barn-es up in Binghamton. The Wedges of sin were not death in this case but quite costly. P. R. V.

"The grading for the new extension of double track between this place [Carbondale] and Forest City is fast nearing completion, and it will not be very long ere we shall see trains running over it..."

"P. Gillen was general yard master during circus day."

"The camp meeting at Uniondale is in full blast, and several of the b'boys are frequenters there..."

"Thomas Ayers,

between this city

[Carbondale] and

dangerously ill. . . "

Scranton, is

fireman on engine 21

ITEMS ABOUT THE BOYS ALL ALONG THE LINE.

32 O. K. enjoys fresh milk, but not the kind given by cows.

J. Eitel, round-house foreman at Cd., is on the sick list this week; G. B. Stewart is off on a pleasure trip, and M. M. Dotterer has his hands and head full to keep both places under surveillance.

Mrs. Wm. Clum is visiting friends out of town.

Will. White was the big mash at the Uniondale camp meeting on Saturday.

Kelly, fireman on engine 17, says if the tongue of his Belle would not sound so loud the boys would not know where he was o' nights.

Chas. Scheuster caught a nice mess of fish in the lake near White's Valley last week.

Mrs. Wm. Cole, of Meadow Farm, Clinton, Wayne county, is visiting her mother, Mrs. Skeels, on Belmont-st.

Some of the R. R. boys are mad as hornets because they were left on delegate election.

"Rusty," who runs the middle end for Maloney, did duty on coal 9 & 10 last week. He is a dandy. They made a running switch at Nineveh and old Rusty said he supposed the head man would cut the engine off and the switch throw itself, consequently a balk. O, Rusty, you're a pumpkin-sucker.

"Old Muscovey" has gone into the jewelry business.

Jake Gorman had his photo taken at Mt. Para: while on an excursion. His girl objected to having his cane play so conspicuous a part in the foreground. To much varnish, Jake.

The young man, Coburn, who was found dead in a culvert near Herrick, had two wounds resembling bullet holes in his head, also a deep gash as if given by some blunt weapon. The last seen of him alive was about eleven o'clock Saturday, p. m.

M. Gorman says it would pay the company to have a dining ear placed behind the tender of the engine on his train for the use of his headman, then he would be at his post.

Ike Baker did not attend camp meeting last week at Uniondale, but will go to the one that commences to-morrow. He thinks to enjoy it all by himself.

"The young man, Coburn, who was found dead in a culvert near Herrick, had two wounds resembling bullet holes in his head, also a deep gash as if given by some blunt weapon. The last seen of him alive was about eleven o'clock Saturday, p. m."

Lots of gossip and chit-chat from 32 O. K.

RAILROAD NOTES.

ITEMS ABOUT THE BOYS ALL ALONG THE

LINE.
Conductor Nicol is on the sick list this week, and "Lew" is scalping tickets, and this change brings Jimmie from the platform to the baggage end of the car,

The two Cobbs, as usual, thunder trains one and two in every station o. k., unless "Peter Whirlo" or his best man, Lewis, shoves them in the switch. There is nothing like it Sam., keep it up.

Geo. Chapman is thinking some about starting a troupe of Swiss Bell Ringers, and is holding his rehearsals in an Erie box car at Morss' upper switch. Iu sounding his bells-he has two of themsounding his belts—he has two of them—he found one of them badly cracked and the other had no clapper. Why not make P. R. V. your advance agent? His name on your bills would be enough, and you would never want for "ringers.

Paddy McQuade thinks the B. of R.R. B. would have had a better dance and more genuine amusement if they had gone to McTighes Hall. Paddy must be laying his mud sills to some gigantic scheme; perhaps a private consultation with "P. R. Flatcar" would get you over the chasm, as he is an old excavator and stone mason, but don't know anything about "brick" or butter milk, especially of the "swill mixture."

The early closing of stores in town has knocked Michael Square Business Boland, alias Frank Bogert, out of his tobacco, as the stores are not opened early enough to suit him in the morning, and close too soon for his arrival in the evening. Why don't you start a tobacco store in Thompson, and promote that young widow to a tobacconist? Try it, anyhow. Let's see, does P. R. V. use the weed? Yaw, he do.

John Peter Dougherty, of R. R. notoriety, attended the B. of R. R. B. Ball, and to see him taking a "gal-up-on" the floor to dance, would make one think he was hauling props or a load of new mown clover. He didn't wear his "moss agates" that eve. We hear he took the switch at one o'clock, but whether he closed it behind him or not, we have not been able to learn; at all events, the night line, which was following him close, and drawn by bold Peter, engineer, was ditched near the cross-roads-Peter, and some dry steam escaped.

You were right P. R. V. when you came out last week from behind a pile of ties, or "orange boxes" and said 32 O. K. was fond of milk, but not of the "bovine mixture." How on earth did you get on to it. Anyone would suppose after seeing that spicy article that you had been a superintendent of some cheese factory or had herded a small dairy on some Western ranch. But Peter I found it in the "cloak room" in "Lackawanna hall" several years ago, and the same night my eyes were filled with "ceiling plaster.

Shorty Ellis, the Forest City crusher, is getting a pair of pants made at Herrick,—Mr. Doolittle will be his tailor. They are to be made of the best of "B sole leather. We hope they won't be

high water pants.

Ike Baker came in from Summit on Wednesday afternoon with two fine canaries in pasteboard boxes full of small They were, he said, a present from Bob Copeland.

Charlie Clark, the gentlemanly flag-man on Jno. Kirkbride's train, has taken a vacation and gone to Starucca on a hunting and fishing tour. He says he is determined to earn in some way enough to pay Silver the dollar he borrowed of him last February, while snow-bound there. That is right, Charlie, redeem yourself; you always had an honest face. Don't let it get rusty now, old boy.

Quite a number of our boys are attending the Free Methodist camp meeting at Uniondale. Among those most prominent are Whitford Coon, of O. N., Mr. Gulnac, of F. C., and Mr. Cunningham, of Mi. I. J. N. and D. F. are talking of driving up on Saturday evening. Dan will be at home when he gets at Stillwater in his own yard.

F. W. Ellis, better known as "Shorty," has been filling G. W. Johnson's position as book-keeper for the past week. We are not certain whether the honor of his promotion or his recent marriage has turned his head; at any rate he was heard to remark that he was now gen'l supt, of the H. C. & I. Co., and bracing up to the crowd said, "Them's my working clothes."

Carrick is in his element now, he can

exercise his winning powers on the pretty girls at camp meeting and his handsome face is often seen there. Go it Martin.

F. O. Cunningham is one of the converts at Uniondale.

There was a pie stolen from one of the tents on the Uniondale camp ground last Friday night. The "Dude of Herrick" claims that Connelly did it and refused to divide with him hence the exposure. We did not think Jack was that kind of 32 O. K. a boy.

"Ike Baker came in from Summit on Wednesday afternoon with two fine canaries in pasteboard boxes full of small holes. They were, he said, a present from Bob Copeland."

"Ouite a number of our boys are attending the Free Methodist camp meeting at Uniondale..."

Gossip and in-crowd chatter—all of marginal importance from an historical perspective.

RAILROAD NOTES.

ITEMS ABOUT THE BOYS ALL ALONG THE

S. Keating and the "Mayor" had quite a wreck at NS. last Friday night damaging their engine to the extent of two or three thousand dollars. What were you doing "Chubby" trying to hog out on some one?

Charley Clark tells us that the dollar Silvernell lent him was counterfeit or had been punched and filled with lead so he did not dare turn it in or pass it, so if he thought that was the best way to make himself whole, he got left.

Big-head, Blind Mose runs the dandy pusher for the Eric. Say Mose, did you find out who was down beside your eng. show night. Why didn't you get down and thrash him?

Jake is glad when it comes his turn for the gravel train. He likes that kind of pushing. Adam can keep her roaring on

Arthur says he has a better hound than Levi. Will White will return from Can-ada in a few days with one that goes ahead of anything in town. Whitey, why didn't you keep the one from Thompson? he was a fine canine.

"California" thinks the boys ought to let up on him. Well Jim, how did you make out with the justice over at Thompson? Did it cost three hundred, or did you compromise?

Moran at J. N. enjoyed Shohola with the G. A. R. excursion on the 20th.

32 G. F. S. has gone into training for a prize fight, he having challenged the champion, Sullivan. The first lesson was received on Friday last from his trainer, Mid. Hoyt, at Nineveh, and was quite severe. He thinks they should be more gentle at first. What size boot does Barnes wear at Binghamton, anyhow, about the width of your coat-tail?

Clum requested the large elephant at Robbins' circus to get his "trunk" checked for Saratoga or Troy, so says Curly.

"The Scranton Brotherhood of Railroad Brakeman are going to run an excursion to Jones' Lake [now called Lake Ariel] on the 17th day of September. All of our boys who wish to enjoy a good time should attend."

A photograph by Johnson of this new iron bridge at Olyphant is given on the following page.

"The Saratoga express has been running extra cars for the last week to accommodate the hop-pickers." See boxed text on the right of this page; see also the photograph on the following page of a hop field.

RAILROAD NOTES.

ITEMS ABOUT THE BOYS ALL ALONG THE DA TIME. THE TE

S., how is it you haven't flagged train 3 at B. lately? C. has had his boots halfsoled lately and intends to wear them out on you. What caused your necktie to be so disarranged at Nineveh? Please explain these little matters.

The Scranton Brotherhood of Railroad Brakemen are going to run an excursion to Jones' Lake on the 17th of September. All of our boys who wish to enjoy a good time should attend.

The new iron bridge at Olyphant is nearly completed. This bridge is a great improvement on the old one both in beauty and safety.'

→ The Saratoga express has been running extra cars for the last week to accommodate the hop-pickers.

A flagman has been put on the curve just north of Archbald. This makes it a great deal safer for the boys.

P. R. says something in one of his contributions about our living around Forest City or Uniondale. You are way off, old man. We don't live far from you.

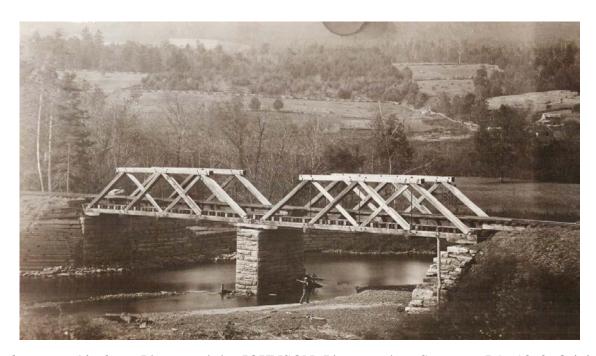
C-D H-D.



A green cone, or strobile, surrounds the female flower on a hops plant, *Humulus lupulus*. Inside these cones are golden grains that form a sticky greenish yellow to orangeyellow powder, which is used as a flavoring and stability agent in beer brewing. It is hops that gives beer its bitteraromatic quality. Growing hops in central New York was a huge industry in the nineteenth century. In 1849, New York was the national leader in hops production, and by 1855 over three million pounds of New York state hops were sold annually. A photograph of a hop yard is given on the following page.



The hop plant, *Humulus lupulus*, is a vigorous, climbing, herbaceous perennial, usually trained to grow up strings, as in the hop field or yard shown here.



Bridge Near Olyphant. Photograph by JOHNSON, Photographer, Scranton, PA, 1860. Original photograph in the collection of the Wayne County Historical Society, Honesdale, PA

"... picnic in Murdock's grove..."

"Conductor Nicol, of the Saratoga express, who was confined to the house for two weeks with malarial fever, resumed work yesterday."

"If the frosts deal gently with nature there will be an abundant harvest of all kinds of nuts along the line through New York state."

RAILROAD NOTES.

ITEMS ABOUT THE BOYS ALL ALONG THE LINE.

→ Hughie Finlan won the cake at the picnic in Murdock's grove, but he doesn't like cake.

Jno. Kirkbride's flagman don't see why so thing isn't done for him. He has bees clerking for John nearly three years. Keep cool, they are about to fill the canal soon with gravel for a new road.

S. Lingfelter, familiarly known as "Kansas Sam," is living a life of single blessedness for a short time.

32 G. F. S. says J. A. B. acted as engineer on the steamer at Cooperstown, N. Y., on Cop's excursion last week. John Moyles thinks they would have made more if all the hop-pickers had gone with them.

→ Conductor Nicol, of the Saratoga express, who was confined to the house for two weeks with malarial fever, resumed work yesterday.

H. Schenzer and T. Ayers, who have been on the sick list, resumed their duties last week.

Conductor Gorman has a soft thing. He likes to get laid out at O. N. Look out, Mike.

Levi Slocum and Herman Faatz are getting things in shape for a deer hunt on Bowman's creek in Sullivan county.

Henry Fisher, sr., of the machine shop, had a slight stroke of paraylsis last week.

→ If the frosts deal gently with nature there will be an abundant harvest of all kinds of nuts along the line through New York state.

P. Quinn, jr., said he did not run away with the circus; he and "Muck" hired out to them with the intention of being exhibited as the "Wild Australian Children."

I. J. N., our genial and thoroughgoing Eric dispatcher, is soon to become a Benedict. We wish thee well, Irv.

All of the conductors on coal ten received letters last Friday night at C. N.; they were not from their cousins or aunts either.

Will White went with the veterans' excursion to Susquehanna last week. Will, did you miss the train?

Will and Orv. Skeels took passage for Waymart last Sunday with their fair ones.

Al. Jagger said it was not so that he had to switch part of his train. They have graded Belmont for him and Dan.

Machine shop foreman Stewart has returned from his trip looking hale and hearty and weighing considerably more than when he left.

Del. Clark, alias "Stony Clo," plays authors at Forest City.

Rusty's girl procured tickets for the G. A. R. Excursion to Shohola but Rusty declined to go. Rusty you're a "gook."

P. R. and Jim B. fished in Lewis lake at Uniondale last week. They had poor luck but are going there again soon. Jim says the oil that was put on the bait kept the fish from biting, and P. R. said they would have caught more if Jim hadn't gone to sleep and let a sunfish tow them around the lake.

P. R. V.

"All of the conductors on coal ten received letters last Friday night at C. N. . . . "

The foreman of the machine shop is named Stewart. "The improvements on the line between Carbondale and Scranton are being pushed forward rapidly. Richard Kellow has a gang of men at Archbald laying new rails and ties and putting in a switch. New rails and ties have also been put in between Peckville and Olyphant and also new ties below Olyphant. The indications all point to an extension of the double track shortly."

"The Triangular excursion left Scranton last Tuesday with twelve cars. Conductor Nicol, of the Saratoga express, ran the train to Nineveh. . ."

RAILROAD NOTES.

Items About the Boys All Along the Line.

The improvements on the line between Carbondale and Scrauton are being pushed forward rapidly. Richard Kellow has a gang of men at Archbald laying new rails and ties and putting in a switch. New rails and ties have also been put in between Peckville and Olyphant and also new ties below Olyphant. The indications all point to an extension of the double track shortly.

Si., what made you stop at Uniondale so long Tuesday? Was it to take on water or steam?

Supt. Manville and a number of the other officers of the road went to Scranton on a special train Tuesday.

Lewis Cook, of the Saratoga express, who has been off attending the funeral of his brother, who was killed last week by an accident on the A. & S. division, is again on duty.

The Triangular excursion left Scranton last Tuesday with twelve cars. Conductor Nicol, of the Saratoga express, ran the train to Nineveh and then went through with them. Lew. Cook takes his place as conductor and Jim Nicol takes Lew's place as baggage-master.

Where is "32 O. K.?" We miss his interesting letters very much.

Items About the Boys All Along the Line. F. O. Cumingham is doing the office work at Uniondale in place of A. R. Cole, who is indisposed on account of a severe cold which he contracted two weeks ago.

"Always-out-of-the-office Coon" surprised his "intimates" at Susquehanna last week. The office was tenderly cared for by Prof. Beckaloff, night operator at Ararat.

A prominent fireman and an old hunter, who resides at Nineveh, went out for rabbits a few days ago but had poor luck in slaying even one. But, while laying in ambush in a small grove (the same where the "regular army was quartered") with cocked musket and a yelping hound, he waited for what he expected would be a fine white hare. But in this he was mistaken, for, when it passed him, he fixed on his glasses for a shot, but it turned out to be a light brown "hair" and wore, so he says, a No. 3 congress gaiter. Of course he didn't shoot, but since then wishes that he had. He did not follow that trail any further.

"Windy" Smith, the man who was so immercifully beaten with a boot-bex in the hands of Jim Davis, and who went to Nineveh to reside two years ago, has left for Oneonta, where he has accepted a position as general manager of a pump. forcing river water to the top of a water tank.

Geo. Walker and his amiable wife are pleasantly situated at the Hotel de Page, Ninevel.

Dr. James Engle appeared at, and took part in the Opera at Susquehanna last week. He sang, with great effect and much sweetness, the "Village Blacksmith" which more than brought down the house. He is equally as much of a "sentimental" songster as Ed. Morgan is and does it as easily.

Conductor Reardon ought to have been the happiest man in Carbondale, and to a great degree he was. He was anxious to say as much as "Breezy". could, but was disappointed just one-half. Well, Joe, there is some consolation to know that one is company and two is a crowd, and your face had just as many smiles as Breese's did, though only half as much to create them.

Mr. Moran, the watchman at J. N. is, a happy sort of a fellow and borders just a little on the dude pattern. If he will only quit drinking buttermilk, his friends predict a brilliant future for him.

Asa Lown, better known as "Tony," is rusticating in the Parlor city at present. His hopes are many and we understand are soon to be consummated. Send me a buckwheat cake embroidered.

Eli Gardner, a distant relative of the famous Lime Kiln Club man, is general manager of J. N. and all its wires. No man enjoys a joke better nor detests a poor songster more than he. He was a spectator at the opera where Jimmie Engle first made his debut, and he never tires telling about it.

Two Nicols and two Cobbs, with their cook, went North on No. 1 on Tuesday morning. Tommy McTighe, went with them as a U. S. M. waitah.

We caught a glimpse of Chauncey Fowler's face on the Saratoga last Tuesday. He was, of course, going somewhere for he wasn't alone.

Frankie Cunningham, the dandy little station agent at Arrarat, smiles frequently on his friends at Herrick, and rejoices as much as any one when everything is O. K.

Martin Carrick, of Stillwater, is contemplating a tour in the Canadas, about April, going on the express this time.

"Silver" says he would be glad to adopt the regulation "blues" if the coats were made swallow tail, and had Martha Washington sleeves—too bad.

Hanky Johnson ought to make good runs now, if paint will make any difference, as engine 43 and coach 32 are shining with beautiful colors. Now Hanky wear the "mine prop scarf pin."

Banjo Burns says Cody thinks he has it all his own way in Forest City with the darlings, but he don't for he himself has fallen in with a couple or so and they are at students either.

Opr. Cody makes tri-weekly trips to this place to attend business of not a little importance, he having badly burned his right hand lighting a kerosene lamp. His Dr. thinks his burns are not of a fatal character.

Station agent Gulnac, of F. C. is one of the handsomest R. R. men on our division and is always in the best of humor. Why don't you take a lady student Ed?

32 O. K.

Frankie Cunningham is the station agent at Ararat.

"Dr. James Eagle appeared at, and took part in the Opera at Susquehanna last week. . . " "The second serious accident happened to Wm. Hurlbert, brakeman in Nineveh yard, the early part of last week. . ."

RAILROAD NOTES.

Items About the Boys All Along the Line.

The second serious accident happened to Wm. Hurlbert, brakeman in Nineveh yard, the early part of last week. He was in the act of stepping from one car to another when they parted, he falling between them to the ground and was dragged some distance, clinging to the brake beam for dear life, which he came so near losing. He was bruised and cut quite badly but no bones broken, and is now quite comfortable.

"Old Grannie Rumor" says that ex-Mayor Pohren, of Forest City, a noted, as well as ancient place on the "Jeff," is soon to be married to an estimable lady in that city. Well, Peter can take care of a wife as well as any man we know. Besides being blessed with health, he has a good situation, that of conductor on the Erie, with headquarters in Carbondale. Don't forget the cigars nor eider, Pete.

We understand that the "dollar weekly" is having quite an extensive circulation in the homes of our Forest City brethren, and that a "juicy" correspondent tells the people there, through its columns, what he knows about farming and commenced to sub-soil for a "coop" which he expects to yield him a fortune soon. He tore down the fence belonging to "32 O. K." and thrashed him badly, and overturned the corn-crib owned by "New Signals". He writes with a bar of pig iron and ink of old "whrye."

Mart Carrick says some tolks are getting too cunning about his accordion agency. He was only selling chances for the raffling of it, and not acting as salesman.

John Cunningham, the foreman at Stillwater, is a most fortunate young man and has hosts of friends in this place, since the ball given by the raildroad conductors, Miss L. being among the most pleasing. Come down oftener, Jack.

Frank Cunningham did not attend the Thompson bean bake. Business of a too important character kept him in the office that night.

The pleasant face of Thomas Cunningham shone beautifully bright and wore heavy smiles, as he took his seat in the Mt. Pleasant stage, one cold stormy eve last week. We understand he only got as far as the hotel, kept by Mr. Riley, of that place. Jo Gould, who injured his ankle so badly at Jeff. Junction one month ago, in getting off his engine, is improving very fast and we shall soon see Jo at the throttle again.

The coal biz is unusually dull for this time of year, gangs laying in every day.

Station agent Livingstone at Centre Village, had an extra hand sent him last week, the large shipments of buckwheat flour necessitated a new W. B. clerk.

Conductor Gorman was obliged to return home last week on account of sickness in his family. One of his children being very seriously afflicted.

Messrs. Purtel and McKeevey, of Herrick, both "flys" but good "knights of the keys," attended the G. A. R. bean bake at Thompson last week. The Hon. Mr. McKeevey was called upon for a speech, by "Uncle Moses" Saxton, and was promptly responded to by the gentleman from Herrick. The brilliant smiles of Duke Leach, who happened to be there, broke up more than one poor lass that night.

Engine 49 is out with a new coat of green paint and nicely varnished. "Eaf" is very proud of her, and showed himself to be a "solid" man by putting up for the boys.

"Moses" Saxton, foreman of bridges on Jeff. branch, and his "niggers" are working on bridge 9 at present. "Mose" went to Scranton on important business not long ago, but not for his R.R. Co.—it was personal business.

Peter "Whurly" Robinson, conductor on the N. Y., L. E. & W. R.R. with head-quarters at Susquehanna, don't like any one to speak about Nineveh "roosters." Pete carried off a fine "bird" several years ago when galvanizing at that place for Pat Hogan.

Mart Carrick sent a bran new accordion with his choicest regards to one of the young ladies at J. A,

Engineer Bryden and Cond'r Silvernell, of the "fast line," are going to attend the ball at Susquehanna given by the B. of L. F., of that place. Mickey Feeaun has offered great inducements to get them, and Charles Giniman meets them at the stone bridge with his mules and bob sled.

"NEW SIGNALS."

"The coal biz is unusually dull for this time of year, gangs laying in every day."

> The station agent at Center Village is named Livingstone.

"Engine 49 is out with a new coat of green paint and nicely varnished..."

"Moses" Saxton is the foreman of bridges on the Jefferson Branch of the Erie.

The funeral of brakeman William Tillsley Items About the Boys All Along the Line.

The funeral of William Tillsley, the unfortunate brakeman who was so seriously injured a short time ago was one of the largest in this city for many years, the Odd Fellows and R. R. B. turning out in a body, of which he was a member.

Conductor Maloney, whose hand was so seriously injured while making a coupling in Nineveh yard three weeks ago, is getting on finely, and will soon be on his train again.

Levi Ames, is able to be out again. He was seen at the office of Yard Master Copeland this week looking none the worse for the loss of his hand and yet, he is forever deprived from being the first-class brakeman that he was.

Ike Baker was making a few improvements on his property, at Summit, on Thursday, fixing fence, and cleaning up in general. This is the property that he purchased of Mr. Ammerman.

Thomas Cunningham is back at Herrick again. Thompson atmosphere, he says, agrees with him all right, but Herrick suits him best.

Station Agent Frank Cunningham has adopted the "overall" regulation order, and looks fine in that kind of a garb. He looks like a woman dressed in a gossi mer on a windy day. But the Ararat folks think it's to be so, and that every agent wears them.

Rene Bull, flagman and baggage smasher on train 304 is a bran new dad and it is as fine a boy as can be found. Rene set out good cigars and urged the boys to take two or three apiece.

Engineers Bryden & O. A. Histed are going to take a trip to Florida and will start about the last of this month. They intend to buy an orange grove.

Engine 51 is to go in the shop soon for a general overhauling and will, no doubt, be one of the best engines on the road when she comes out again, and Jack will be more proud of her than ever before. She will, no doubt, be put on an orange train if that farm is purchased down in Florida.

We are sorry to learn that the gentlemanly clerk Wm. Shannon, who held the tort so acceptably in the D. & H. freight office has left the employ of said company. We hope he will succeed in his new place of business as well as he did when he sat with the toys and listened to the political episodes of "Lovelyday."

Daniel Nelson, one of the D. & H. Co's best, and steadiest brakemen, was quietly married to an estimable young lady at Thompson, two weeks ago, but it just got out among the boys, and Dan is putting up in great shape.

Conductor Silvernell was called to White Haven last week, on account of the injury to his son, who was struck by a pin, and blood poison set in. The boy is better now.

Jef. Rosengrant and his assistant foreman Frank Springteen, both of whom have met with unfortunate accidents, are at their places in the yard again.

Another "rush" with old king coal, has just passed and the gangs from the lower end, who were running north are back in their old places again.

John Peter Dougherty says its nothing to make a trip to Susquehanna now since Jo. is at the valves again, but says he would much rather have Adam push him than Jake, the "old Seluth of the Jeff"

Operators Burns and Cody are back at the keys again doing "order" work as usual. Go one eye on it this time, "Banjo" NEW SIGNALS. William Shannon, clerk in the freight office, has left the company.

Daniel Nealon, D&H brakeman, was recently married.

"baggage smasher" on train 304 is a new father.

Rene Bull.

flagman and

"Will Lawrence, assistant train despatcher under Mr. H. A. Mace at Green Ridge so long, has been promoted to dispatcher at that place."

"Uncle Mose Saxton, with his men, are doing some good work on the trestle at Starrucca."

RAILROAD NOTES.

ftems of Interest About the Boys All Along the Line.

➤ Will Lawrence, assistant train despatcher under Mr. H. A. Mace at Green Eddge so long, has been promoted to despatcher at that place.

The clerks in the D. & H. and Eric freight houses are dying to go fishing or hunting. Harry Campbell is the most eager. Schryver and Griffin should let them off a day.

Ben. Butler Cody laid off a few days last week to do his farming. The truth of the matter, is, he only did a little work in his "Garden." He complains about the hard work and says he will get him a "Gardener" and one that will suit him too.

"Uncle Mose" Saxton, with his men, are doing some good work on the trestle at Star-ucca.

Engine 51 looks fine in her new vest of green. She will not be used on trains 3 and 4, but will pull coal and freight on the upper end. Ike Baker smiles out loud, and asks the boys if she isn't a "daisy." As yet, we have seen no cigars or "canaries."

have seen no cigars or "canaries."

Conductor Cummings is back on his train.

Ass't Yard Master Day has been appointed

Yard Master at Green Ridge, which is considered a large promotion for him. Arthur was very popular wilth all the boys here, and he has left many warm friends that will inquire often after his welfare. Success to you old boy.

Engine 17, which bears the name of our esteemed citizen, J. B. Van Bergen, is just out of the shop, where she underwent a general overhauling and received a new complexion from the brush of Prof. Blocksiege, who, by the way, is a "dandy" at his profession.

Sup't Goodrich, of Owego, who has charge of trestles and bridges for the N. Y., L. E. & W. R. R. Co., was over the Carbondale Branch last Thursday.

Engineer Ammerman is justly proud of his engine. John has put up the "fine cut." All got a chew.

Thomas Francis White, "Hankie's" end man, ran coal 9 and 10 while Conductor Cummings was absent. Promotion don't make Tommie proud at all, he didn't even pull his overalls off. Dave Lynch said, in speaking of Mr. White, that he would be more successful on a night line if he would be more fashionable in Nineveh yard, and sort out P. & Rs. for "Hankey" and the lanes for stone trade.

Jimmie Engle expects to embark in the cod and whale fishery this season. His vessel will soon be completed, and when launched will be one of the finest of its kind It is to be built of what is left of the old ark near Ararat. Patrick Pidgeon will be its captain, while Billy Moyles and John McQuartus will be the first and second mates.

New Signals.

"Ass't Yard Master Day has been appointed Yard Master at Green Ridge, which is considered a large promotion for him. . ."

Engine No. 17, the *J. B. Van Bergen*, has just been overhauled and painted.

Baseball game between the *Toughs* of Carbondale and the *Bums* of Forest City last Sunday on the Forest City mule lot grounds. The *Toughs* won.

Mel. Callender, the good looking fireman on Engine 56, plays his cornet while he fires on Engine 56.

"Engine 33 is pulling trains 3 and 4 while 42 is in for repairs."

RAILROAD NOTES.

items of Interest About the Boys All Along the Line.

▶ A very exciting game of baseball was played on the Forest City mule lot grounds between the "Toughs", of Carbondale, and the "Bums", of Forest City, last Sunday which resulted in favor of the "Toughs" by a score of 21 to 20.

Frank McCrannells has purchased the house formerly occupied by John Gallagher at Forest City.

William Lewis, of the F. C. saw mill, and his friend Mr. Price, visited the Authors' Carnival at Scranton, and report having a grand time. They were very nicely treated by the Scranton people. Mr. Lewis registered at the Bastile house.

Jake "Sleuth" has a new smoke-stack on engine 518. Adam is overjoyed and says it's a darling.

→ Mel. Callendar, the good looking fireman on eng. 56, is quite au expert on the cornet. Mel. is just the one for that kind of music, he having lots of the element to sound it. He carries his horn with him and plays while running, which puts Dave and Spence asleep in the caboose.

Miles Creegan and Jimmie O'Boyle, the two best looking foremen on the Jeff., tip the scales at 489 lbs. together.

Charlie Dow uses a nickle-plated scoop shovel and wears a straw hat and has sent for a pair of tooth-pick shoes. Where are you Adam? Photograph this.

Mart Carrick, of Stillwater, has set himself up as a pugilist, and offers himself to "rap out" any one who dares to get before him. "Oney" Walker hearing of this gladiator has brustled up, and will pit himself against said Stillwaterian for a paper of "Clark-and Snover."

John Peter Dougherty, the dandy little flagman on Conductor Loftus' train, has taken vacation for two weeks and has gone to New York city. Squire McDonough acts in the capacity of flagman during his absence.

Old "Rover" went to the shop on Monday to have a flue "filled." Jake does all the "dentist" work on locomotives now, Bob Campbell does the extracting while Sam Moon makes impressions and burnishes the plates.

Engine 33 is pulling trains 3 and 4 while 42 is in for repairs.

Hughy Finlin, the "detective" and goodlooking flagman who flutters his little red flag to the elements for Cond'r Case, went north on Monday. Private business calls a shrewd detective everywhere. Hughy says his red flag does not mean anarchy but to stop short near his caboose,

Dave Nicol has a new car, No. 1 just, out of the shop, and its a dandy. Dave has reason to be pleased. New cars and Sam always get them there. New Signals.

William Lewis of the Forest City saw mill and his friend Mr. Price visited the Authors' Carnival at Scranton and had a grand time.

A boxing match will take place between Mart Carrick of Stillwater and *Oney* Walker.

Carbondale Leader, Railroad Notes, May 18, 1886, p. 2

"The Hillside Coal and Iron Company are about to begin boring, for the purpose of sinking another new shaft. Work will be commenced in about a week."

Matt Quinn, Erie station agent at Starrucca, and Mr. Sherman, of Starrucca, have purchased a large tract of land near the Northwest breaker and will enter the lumber business.

"John 'Rover' wears the jauntiest Scotch cap on the road and it is made of the pure Kentucky tobacco leaf—a new pattern."

RAILROAD NOTES.

ttems of Inverest About the Boys All Along

Ben Butler Cody has once more blossomed out, this time as an astronomer and weather prophet, and bids fair to rival Venor and Wiggins. He may now be seen on rainy days taking observations from the top of his "signal pole." He still practices his profession of gardener.

Our friend Banjo Burns has at last be-

Our friend Banjo Burns has at last become so proficient in getting on trains that he has dispensed with the services of Charley Winters and his wheel barrow, which he has been practicing with for some time past.

Who is it that interests Prof. Ed. Morgan so much when he passes F. C. station?

"Old Sleuth" is acting as agent for a maple sugar firm at Cotrill's switch. He sells on commission.

Mr. Doolittle has "Shorty's' pantaloons mostly completed. They are beauties and "assistant superintendent" feels justly proud of them.

"Windy" Bivins claims to be the champion pool player of the city. He has vanquished all who met him on the field of the green cloth. N. B.—This is not Bivins of telegraphic fame, but a distant relative.

The H. C. & I. Co. are about to begin boring, for the purpose of sinking another new shaft. Work will be commenced in about a week.

The work of building the addition to the new breaker at Forest City is being pushed forward rapidly. Mr. Lutz has the contract for building it.

Matt Quinn has resigned his position as station agent at Starrucca for the N. Y. L. E. & W. R. R. Co. on account of the large amount of personal business he is about to engage in near this city, he and Mr. Sherman, of Starrucca, having bought a large tract of timber near the Northwest breaker. Success to them, they are both worthy gentlemen.

The present wet weather is making sad havoc with gardens necessitating great care and much attention on the part of those having such property to look after. B. B. Cody has more than he can do just now weeding out the grass and thistles, and says a good gardener now is everything to him. The vegetables will soon be marketed.

Elder Gulnac does a great deal of reading evenings, and sometimes when an interesting article is at hand does not retire until a late hour. He also wears "glasses" when he works at his trade, "carpenter work."

About the two happiest men in or about the "flats" are Jimmie Gallaghy and uncle Den Swar, They fill up their "cobh" pipe', w. have been in use over four years, and smoke and tell anecdotes, mostly of what has occurred to them while in R. R. service standing in the upper end of the yard.

Paddy Pidgeon has made a contract with a party at Summit to furnish him with heavy "gloves" to knock out heavy weights with. The sample pair is now in his hands and are satisfactory. Billy Moyles will act as agent for the firm.

→ John "Rover" wears the jauntiest Scotch cap on the road and it is made of the pure Kentucky tobacco leaf—a new pattern.

Jim Wall looks happy if smiles have anything to say about it. New Signals.

"The work of building the addition to the new breaker at Forest City is being pushed forward rapidly. Mr. Lutz has the contract for building it."

Elder Guinac wears "glasses" when he works at his trade. The fact that "New Signals" has enclosed the word *glasses* in quotation marks suggests that the word might have been a new word in American English at the time.

"Mother Hubbard" **engines:** A camelback locomotive, also known as a *Mother* Hubbard or a centercab locomotive, is a type of steam locomotive with the driving cab placed in the middle, astride the boiler. Camelbacks were fitted with wide fireboxes which would have severely restricted driver visibility from the normal cab location at the rear. These engines used anthracite coal waste, culm, as fuel.

The Night Round House Forman is named Wall.

RAILROAD NOTES.

Items of Interest About the Boys All Along the Line.

Del. and Mike will open the excursion season shortly, between Starrucca and Lanesboro. They have their "coach" neatly trimmed and upholstered.

Engine 70, "mother hubbard" pattern, arrived on Wednesday. Jack Moyles handles the levers on the right side with Bill Sherrer as mate.

A number of engines are "stalled" in the engine house, the result of coming in too violent a contact with each other.

W. B. S. says that matrimonial item referring to him in an Owego paper is fictitions

The mystery supended over engine 61 making such good runs home is cleared up. The explanation is shown in the case in front of Cramer's gallery

Geo. Dubois has returned from a trip to the Hoosier State.

A number of the boys on the upper end are off on an enforced vacation.

"Dad" Blake has the new "mother hubbard" equipped with the American automatic steambrake. Smithy says she's a clipper.

H. E. Johnson has been cultivating at home this week.

Night Round House Foreman Wall was looking over his plantation near Thompson, recently.

E. C. CENTRIC.

"Engine 70, 'mother hubbard' pattern, arrived on Wednesday. Jack Moyles handles the levers on the right side with Bill Sherrer as mate."

"... in the case in front of Cramer's gallery."

" 'Dad' Blake has the new 'mother hubbard' equipped with the American automatic steambrake. Smithy says she's a clipper."

Conductor G. F. Silvernell has resigned from the D&H to accept a position with the Wisconsin Central.

Conductor John Harvey has been promoted to the freight to fill the position formerly held by Silvernell. M. Gorman takes Harvey's place on coal five, Cummings on coal seven, and N. Robbins on coal nine.

"Fireman B. Thomas has stepped to the right side [i.e., become an engineer]. We congratulate you, Ben, on your promotion."

RAILROAD NOTES.

Items of Interest About the Boys All Along the Line.

→ Engineer Bryden says he hopes Silvernell will succeed in his new position. He has secured a position on the Wisconsin Central. We add, that Conductor Silvernell is a first-class railroad man and is qualified to fill any place of importance in the train service of any company.

→ Conductor John Harvey has been promoted to the freight, made vacant by the resignation of Conductor G. F. Silvernell. M. Gorman takes Harvey's place on coal five, Cummings coal seven and N. Robbins coal nine. Numerous other changes will take place during this month.

Owney Rounds will yet succeed as an orator. His address at the F. M. camp meeting at Uniondale was listened to with earnestness, especially by Jake and Levi. His subject was "What I know about Flying Switches and D. & H. couplings.

Will Skeels is a success as a miller.

Fireman B. Thomas has stepped to the right side. We congratulate you, Ben, on your promotion.

Conductor Fowler sprained his ankle severely yesterday morning.

E. C. CENTRIC.

55,000 shares of new D&H stock, at \$75 per share, will be offered to stockholders of record on October 15.

M. W. Maguire has been appointed Superintendent of the Susquehanna Division of the Erie.

Given the erection of the coaling station at Port Jervis, the coal shute apparatus at the Erie passenger station at Lackawaxen is no longer needed and has been removed.

In recent years, the appearance of Erie equipment has been improved.

Passenger conductors on the Pennsylvania Rail Road have been issued strict orders in reference to passing children over five years and under twelve years of age free. SOME RAILROAD NEWS.

D. & H. Bonds Due Jan. 1st—Change on the Eric Road.

The 1891 bonds of the Delaware and Hudson Company, are due on the first of January next. In order to retire the issue of (\$5,500,000 in amount) 55,000 shares of new stock at \$75 per share will be offered to stockholders of record Oct. 15.

Each holder will be entitled to subscribe for one new share for each four shares and a half which he holds. The market price of the stock is a little more than \$136 per share, so that the privilege of subscription is equivalent to a cash divident of about \$18 a share. The difference between the price of the stock and its par value—that is to say, \$25 a share is to be made up out of the surplus held in the treasury of the company.

The Erie management have issued a circular announcing the appointment of M. W. Maguire, former Superintendent of the Western Division, as Superintendent of the Susquehanna Division, vice J. H. Parsons, resigned. C. R. Westcott, former trainmaster of the Western Division, will discharge the duties of the position left vacant by Mr. Maguire's promotion. Mr. Starr, who has been temporarily acting as Superintendent of the Susquehanna Divission, will return to the office of the Superintendent of Transportation.

Quite a notable improvement has been made at the Erie Passenger station at Lackaway and in the removal of the coal shute apparatus that reached over the main tracks at that place. Since the erection of the coaling station at Port Jervis, the shutes at Lackaway en are not necessary. But as an ascessory in case of an emergency there will be a side shute there to be reached by the tracks of the Hawley Branch that intersects the Erie at that point. With all the fixing up completed that is contemplated it will make a great improvement about Lackawaxen depot, which is becoming more and more important every year because of the large number of summer boarders who are being attracted to that beautiful point.

The "bootleg" smokestack seems to be supplanting the former more symmetrical and ornamental kind, such as is in vogue on the Pennsylvania and the Erie locomotives. The equipment of the Erie is constantly improving and their present substantial, neat and business like looking locomotives and cars as compared to those of a few years ago show plainly the progress of this big corporation towards making a favorable comparison with other roads heretofore holding first place in these matters.

The Pennsylvania R. R. Co. is sending strict orders to all the passenger conductors in reference to passing children over five years and under twelve years of age free. They quite recently discharged one of their best conductors for a violation of this rule.

Carbondale Leader, Railroad Notes, August 25, 1892, p. 4

"According to a correspondent, the gravity road may be forced to suspend work partially or altogether if the drouth [drought] continues. . . The Archbald Water company, which supplies the boilers along the gravity railroad failed vesterday, and as a consequence plane G boilers were supplied by the Olyphant Water Company."

"The boys
employed on the
railroads by the
American News
company do a
much more
thriving business
than the casual
traveler would
think..."

RAILROAD NOTES.

The Gravity Road Impeded-Revenue of the Train Boys.

According to a correspondent, the gravity road may be forced to suspend work partially or altogether if the drouth continues. The correspondent says: The drouth of the past few weeks has caused a scarcity of water in the reservoirs near Olyphant, and much apprehension is felt lest the works will have to shut down on that account. The Archbald Water company, which supplies the boilers along the gravity railroad failed yesterday, and as a consequence plane G boilers were supplied by the Olyphant Water company. The connections were made by Excelsior company's hose connecting with the main line and water tank. In this manner the place was supplied during the day. The prospects for coming days are very poor.

All the Erie sidings at Susquehanna are filled with loaded coal cars, awaiting an opportunity to move west.

A Hungarian living at Scranton, while walking along the Delaware & Hudson tracks near Avoca, Tuesday afternoon was struck by a train and so badly injured that he died while being conveyed to the Wilkes-Barre hospital.

The boys employed on the railroads by the American News company do a much more thriving business than the causal travller would think. Some of the youngsters make as high as two dollars a day, though that amount is an exceptional average, being made only by the best workers and those who have the happy faculty of persuading people to buy their wares. With most of them it is "easy come, easy go," for they spend their money as fast as they make it and their only incentive to work is the needed amount to "blow in" with the boys at night. The young salesmen are allowed twenty cents on a dollar. If the news company can pay this commission, besides paying the railroads for the privilege of selling on the cars, and then have a margin left for themselves, what must their gross profits be? Anyone familiar with the prices they charge will readily believe that they can pay several commissions and still have something left.

Thomas Connell has begun suit for \$1,000 damages against the Pennsylvania coal company. The plaintiff claims that the company by reason of its negligence ran a train of cars into a pair of valuable horses at the Old Forge crossing and killed them.

Grand Chief Thurston of the Order of Railway Telegraphers is still in Scranton, having been unable to obtain an interview with General Manager Halstead on account of the absence of Mr. Halstead from the city. Mr. Thurston is satisfied to wait and evidently will not leave until the object of his mission, to have a consultation with General Manager Halstead, is accomplished.

The railroad companies have been very slow about adopting automatic couplings, but it must be becoming evident to the managers that they must have something besides the antiquated link and pins. The old first principle takes off a few hundreds of hands and smashes innumerable fingers during a year, but in other respects it seems to be economical, and is retained right along during the era of improvements when everything else of its date is discarded. But it will not answer during the era of strikes. Trains are uncoupled too easily, and crippled by throwing away the pins. Couplings that will stay will have to be put upon cars.

Pennsylvania Coal Company to be sued for negligence in the death of a valuable team of horses at the Old Forge crossing.

Grand Chief Thurston of the Order of Railway Telegraphers will not leave Scranton until he meets with General Manager Halstead [of the DL&W?].

The antiquated link and pin couplers on rail cars will no longer be acceptable in the era of strikes.

Rumored Offensive Contracts—Employes'
Books Discontinued.

Rumors about contracts to be signed by railroad employees on all the trunk lines. These contracts would be a death blow to all organizations among railroad employees.

Railroad boys have been given something to think of by a rumor which seems to be well founded, and which, if true, is of great importance to them. It is understood that all the trunk lines have decided to draw contracts, which all employees will be required to sign on Jan. 1, and a provision of each contract will be: First that one month's wages shall be reserved at all times during the employee's connection with the company; second, the men shall give the company thirty days notice of intention to leave; that any attempt upon the part of an employee to strike or combine with others to force the company to come to terms will result in the immediate discharge of the employee and the forfeiture of his deposit of one month's pay; fourth that the company reserves the right to discharge an employee at once for cause. Now if this should prove to be the case, the employee would refuse to a man to the charge and apply one of the highest that the contract and the highest that the highest tha sign this contract, and one of the biggest strikes the country has ever seen would be precipitated. This scheme, of course would be a death blow to all organizations among railroad employ-

Collision between a hand car loaded with steel rails on the D&H tracks at Cork Lane and a fast freight. Much damage. Narrow escapes from death.

The other day while a handcar loaded with steel rails was standing on the Delaware & Hudson track at Cork Lane, a fast freight crashed into it, smashing the car in splinters and scattering the rails in all directions. The engine was thrown from the track and rolled over on its side. The engineer and fireman had a narrow escape from being killed. Arthur Brown, a trainman, had his ankle broken by one of the rails striking him. It took several hours to clear the wreck.

On and after September 10, the Erie will discontinue the half-acent-a-mile mileage books to employes, substituting in their place 500-mile books at two cents per mile. This is virtually abolishing the former books, which have undoubtedly been abused. It is generally believed that the Erie in common with other roads, will eventually make a general rate of two cents a mile for everybody. Employes will receive personal passes for long distances. They will pay two cents a mile for short distances—so will their families, their sisters and their cousins and their aunts.

The Ontario & Western railroad has ordered forty new locomotives, of which twelve are out, from the Dickson Manufacturing Co., and 1200 cars from the Peninsular Car company of Detroit, Mich.

The Brotherhood of Locomotive Engineers has had but three strikes in twenty-eight years. It is probably the strongest and is certainly the best labor union in America. That is pretty good evidence of the greater efficiency of non-striking labor organizations. Chief Arthur in explaining its principles, says: "We have always expressed a willingness to submit all grievances, when we could agree with the company, to a board of arbitration, composed of three disinterested persons." That is the only sensible way to arbitrate. It should be the duty of the boards of arbitration to prevent strikes, if possible rather than to hold coroner's inquests on them after they are over.—Buffalo Express.

Forty new locomotives ordered from Dickson Manufacturing Company by the O&W; also 1,200 cars from the Peninsular Car company of Detroit, MI.

"The Brotherhood of Locomotive Engineers has always expressed a willingness to submit all grievances to a board of arbitration. . . . It should be the duty of boards of arbitration to prevent strikes."

On and after September 10, 1892, new mileage charges for Erie employees: 500-mile books at two cents a mile.

1211

In the Caboose

As of October 9, 2015, ten of the 24 volumes in this series were completed and published. In the course of our on-going research on the Delaware and Hudson Canal Company, we sometimes come across data that would have been included in the earlier published volumes in this series had those data been known or available before those volumes were published. But that was not the case. That's fine. It happens all the time in the publishing world. You can't turn back the clock.

In the meantime, however, we would like to share some new finds / some new learning that will be incorporated (turning the clock forward) in the second editions of existing published volumes in this series.

Additions for Volume I:

1. The Honesdale and Clarksville Turnpike Road Company

Thomas Clark was one of the first settlers of Waymart (formerly named Clarksville). Clark had a tavern and store there. His residence, 269 Belmont Street, is now the Jenkins Funereal Home. The turnpike was organized April 2, 1830, and ran from Honesdale to the Milford and Owego Turnpike at Waymart.

On April 18, 1831, George Rix, through this attorney, Thomas Fuller, informed N. B. Eldred Esquire, President of the Honesdale & Clarksville Turnpike Road Company, that no one associated with the construction of that road could enter upon his property until the appraised damages to his property in the construction of that road shall be paid.

Here are four scans of the letter that were sold on E-bay in October 2015:



N. B. Eland By

President of the Hondale of larkswille

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proporty by the land county of the afferies men and payment of the hand a mages as a afordaid - Arthary April 18. 1831 bigned given Right Copy left at Eldner's house the 18. April 1831 by me - Tho. Fuller

Here is a typescript of the text of the letter from Rix's attorney, Fuller, to Eldred:

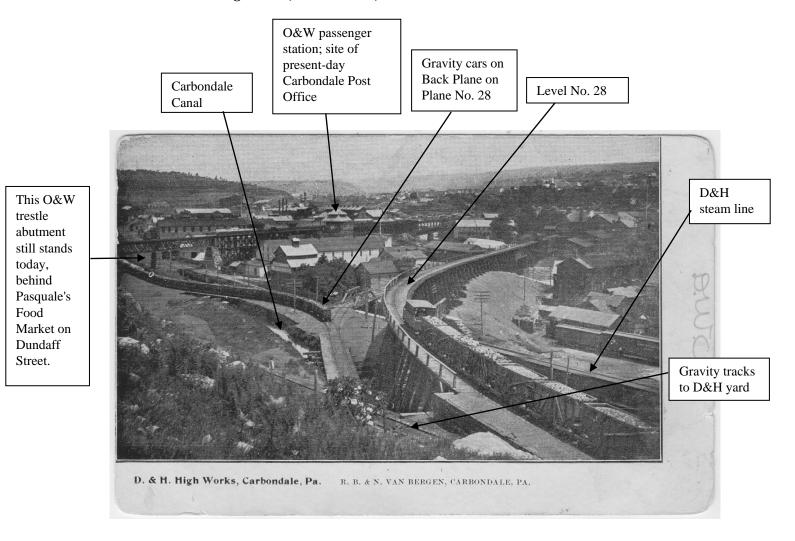
"N. B. Eldred Esq.

President of the Honesdale & Clarksville Turnpike road Company

George Rix takes this method to inform the said Company thru you that he forbids the said Co. their superintendents, engineers artists workmen & labourers to enter upon his lands in or contiguous and near to which the rout and track of the intended rod shall pass until the damages that will be occasioned thereby shall be appraised and paid in the manner prescribed by the act of assembly authorizing the construction of the road & the several acts referred to therein and further that he holds himself at all times in readiness to enter into a reasonable agreement with the said Company in relation to said damages or if they cannot agree than to join the said Company in the appointment of [line omitted in scan] . . . property by the said company . . . previous to the appraisement and payment of the said damages as aforesaid Bethany April 18, 1831 Signed George Rix Copy left at Eldred's house the 18 April 1831 by me Tho. Fuller"

Addition for Volume III:

1. Photo: D. & H. Highworks, Carbondale, Pa.



"D. & H. High Works, Carbondale, Pa." Post card in the collection of the Carbondale Historical Society. Given the fact that the O&W tracks and passenger station are shown in this photograph, we know that this photograph was taken after 1890, and that it is not a Hensel nor a Foster photograph.

2. Photo: Gravity Operations, Carbondale

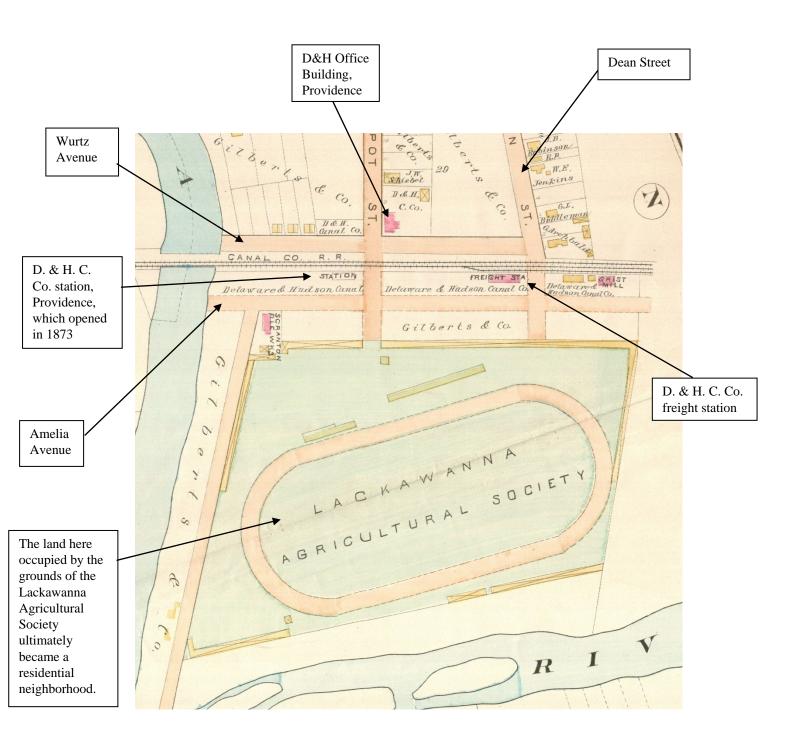


Gravity Operations, Carbondale. Photo in the collection of the Waymart Area Historical Society.

Additions for Volume X:

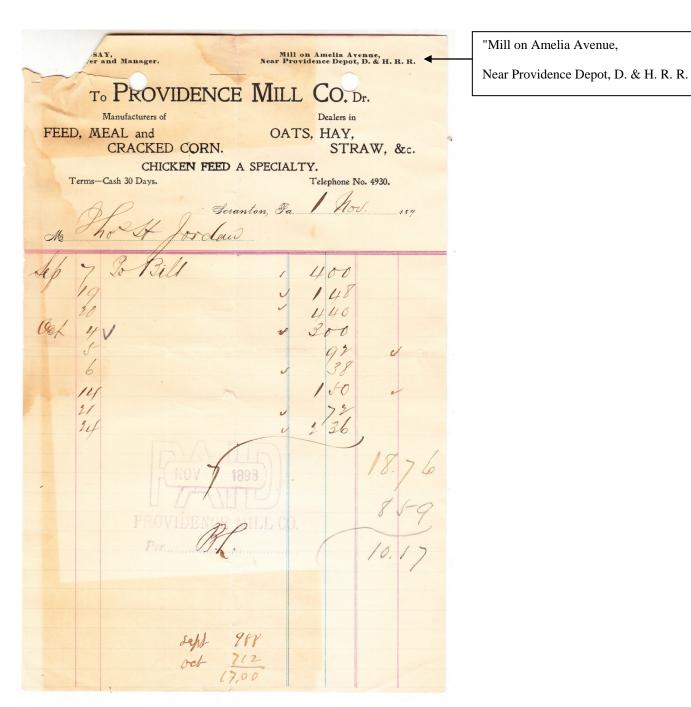
1. D&H Providence Depot

The locations of the D&H Providence Depot is shown on the map on the following page (this map view was published initially in Volume X in this series on p. 97)



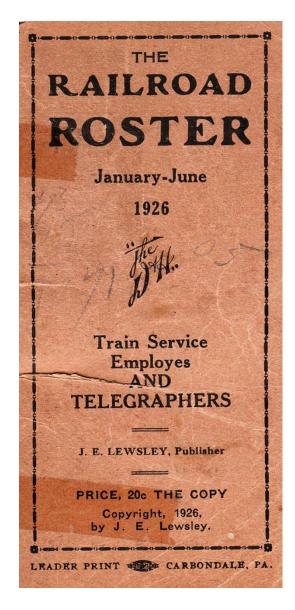
As we can see from this 1888 map (*Richards & Co. Atlas of the City of Scranton and Borough of Dunmore*) the D&H Providence Depot was located on the south side of Depot Street, between Amelia Avenue and the D&H tracks.

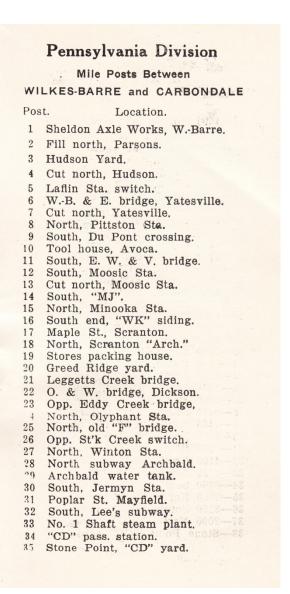
Interestingly, the D&H Depot in Providence is given on the Providence Mill Company's invoice, shown below, dated November 1, 1898, as a location reference for the mill on Amelia Avenue: see the upper right corner of the invoice. This invoice came into the collection of the Carbondale D&H Transportation Museum via John V. Buberniak.



2. Mile Posts on the Pennsylvania Division between Wilkes-Barre and Carbondale

On May 11, 2016, Janet Isger (7900 Quigley Road, Union Dale, PA 18470) donated a copy of "The Railroad Roster /January-June 1926 / The D&H Train Service Employees and TLEGRAPHERS," J. E. Lewsley, Publisher) to the Carbondale D&H Transportation Museum. Her grandfather, Ernest Enslin, and both of her husband Robert's grandfathers (Joseph Isger and Leonard Besecker) were all D&H engineers. She also donated to the museum a D&H VETERAN lapel pin that belonged to Leonard Besecker (lived at 26 Sand Street, Carbondale, phone 1236J). Included in that January-June 1926 Roster is a list of the mile posts on the Pennsylvania Division, between Wilkes-Barre and Carbondale. The title page of that 1926 roster and that list are shown below:





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